



LOWER HAIGHT
PUBLIC REALM PLAN



San Francisco
Planning



SFMTA
Municipal
Transportation
Agency

FINAL REPORT | SUMMER 2017

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Chapter 1

INTRODUCTION

BACKGROUND

This Public Realm Plan lays out a vision for the Lower Haight neighborhood of San Francisco. While numerous projects and plans have been undertaken throughout the neighborhood, none have attempted a comprehensive look for how Lower Haight's most important public spaces can be designed and function to best serve the needs of the people that live, work and visit the area. The purpose of this Public Realm Plan is to provide that vision and framework for the Lower Haight's shared public spaces. It provides a design framework for the neighborhood which it can build on over time.

In early 2015, the Lower Haight Merchants and Neighbors Association (LoHaMNA) approached the office of the District 5 supervisor, London Breed, to explore the possibility of creating a public realm plan for the neighborhood. This community-led request to the City formed the basis for this Public Realm Plan.

With funding from Supervisor Breed's office, the Planning Department took the lead on assembling an inter-agency team to develop the Public Realm Plan, which would include specific improvements to focus areas for the Lower Haight neighborhood. Beginning in 2015, the San Francisco Planning Department began a dialogue with the community about public realm improvements in the neighborhood

The Planning Department was joined by staff from the San Francisco Municipal Transportation Agency (SFMTA) and San Francisco Public Works to form an interagency team to lead the planning effort. A variety of community events, including workshops, tables at community events, and attendance at stakeholder meetings, were held between October 2015 and May 2017 with the goal to develop and refine the specific recommendations in the Plan.

WHY CREATE A PUBLIC REALM PLAN?

The public realm is the space between the buildings. It includes sidewalks, streets, parks— any space the public can access. In addition to design, the success of these spaces relies on programming, management, and activation of the space.

The public realm also creates opportunities for public life. Sidewalks, streets, parks and other public spaces can be designed and programmed to encourage people to pause, gather and play.

PROJECT SCHEDULE

<i>Date</i>	<i>Milestone</i>
FALL 2015	Existing Conditions Analysis
	Open House #1
	Outreach to Community Stakeholders
WINTER 2016	Conceptual Plan Development
	Ongoing Community Outreach
SPRING 2016	Open House #2
FALL 2016	Continue Community Outreach
	Design Refinement
	Interagency Review
SPRING 2017	Final Open House #3
	Finalize Public Realm Plan
TBD 2017	Temporary Public Space Groundplay Installation

PLAN AREA

The study area for the Lower Haight Public Realm Plan, shown below, includes the major commercial area on Haight Street, between Steiner and Webster, as well as the surrounding residential areas. It is bounded by Divisadero Street to the west, Buchanan Street to the east (extending to Gough Street for the Haight and Page

Street corridors), Oak Street to the north and Waller Street to the south. The neighborhood is bordered by strong and well-defined neighborhoods in all directions: Divisadero/NOPA in the west, Hayes Valley in the east, Alamo Square in the north, and Duboce Triangle in the south.

Originally the study area extended further south to Duboce Street, but was revised

northward to Waller Street after further discussions with the community. A plan to consider changes to the Safeway site at Church and Duboce was likewise originally considered but ultimately abandoned as it was addressed in previous public planning processes.



Plan Area

1" = 100'
0 50 100 200 300 400 feet



Legend

--- Study Area

RELEVANT PLANS

BETTER STREETS PLAN

The 2010 Better Streets Plan (BSP) created a unified set of standards, guidelines, and implementation strategies to govern how the City designs, builds, and maintains its pedestrian environment.

The BSP reflects the understanding that the pedestrian environment is about much more than just transportation that streets serve a multitude of social, recreational,

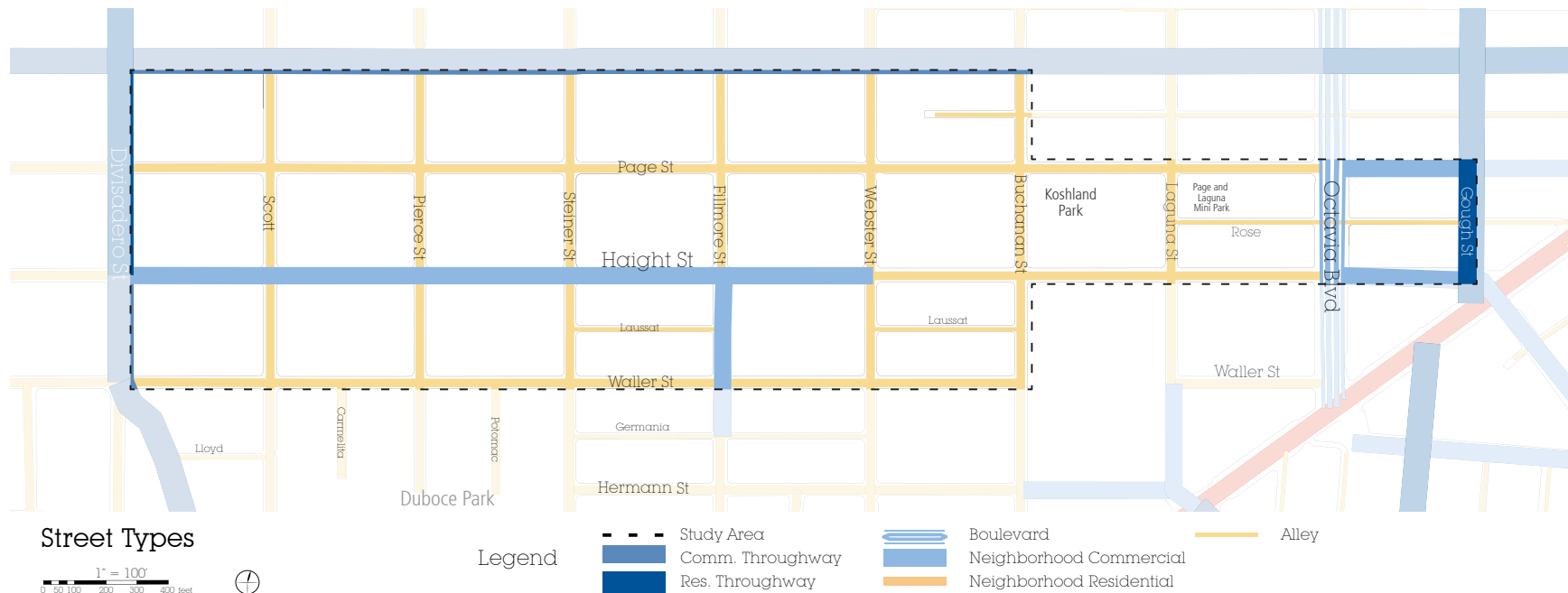
and ecological needs that must be considered when deciding on the most appropriate design. For every street type, the plan identifies standard improvements that should be included.

In the BSP, street types are based on existing land uses, width of the roadway, and other factors. Within the Lower Haight Public Realm Plan area, there are a number of different street type designations, as shown on the map below. Neighborhood Commercial and Neighborhood Residential are the predominant Better Streets Plan types in the Lower Haight.

For every street type, the BSP identifies standard improvements that should be included, such as greening, lighting, pedestrian safety measures, or site furnishings. The designs developed in the Lower Haight Public Realm plan are consistent with the recommendations of the Better Streets Plan.

Status: Adopted, 2010

Website: <http://www.sfbetterstreets.org/>



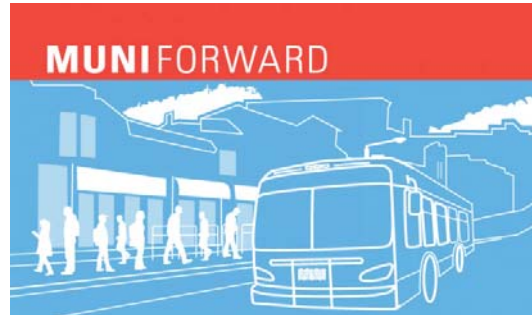
MUNI FORWARD: 7-HAIGHT-NORIEGA RAPID PROJECT + HAIGHT AND HAYES STREET

As part of Muni Forward, a SFMTA project that aims to improve Muni reliability, the agency is implementing pedestrian safety and transit improvements along the 7 Haight/Noriega route along Haight Street between Stanyan and Laguna. These improvements include transit and pedestrian bulbs, traffic signals, right turn pockets and stop consolidation. Utility replacement and repaving are also included, as part of the Public Works department's coordinated Haight and Hayes Street projects.

Utility upgrades began construction in 2016, and Muni Forward improvements begin construction in 2018.

Status (as of May, 2017): Under construction

Websites: <https://www.SFMTA.com/projects-planning/projects/7-haight-noriega-rapid-project>
<http://www.sfpublicworks.org/haight-hayes>



PAGE STREET NEIGHBORWAY

The Page Street Neighborway Project is a multi-phase effort to make the street a safer and more pleasant place to walk and bike to neighborhood destinations and nearby parks, and to provide landscaping opportunities where possible. The "Phase One" segment extends from Webster Street to Market Street, where approximately \$1.5 million of developer impact fees is allocated to plan, design, and implement improvements consistent with the Market-Octavia Area Plan. Concept designs for this segment will be finalized in summer 2017, with start of construction anticipated in early 2019. Near-term improvements and pilot treatments may be ongoing through 2017 and 2018.

The Lower Haight Public Realm Plan and Page Street Neighborway project have been collaborating and coordinating outreach efforts.

Status (as of May, 2017): Design Development

Website: <https://www.SFMTA.com/projects-planning/projects/page-street-neighborway>

WIGGLE NEIGHBORHOOD GREEN CORRIDOR

The Wiggle is an important bicycle route which winds its way along the flattest streets from Market to the Panhandle, bringing residents to local parks and linking bicyclists between downtown and the City's western neighborhoods. The Wiggle's relatively flat grade and lower area elevation creates a collection point for water during seasonal rains. It is an important contributor to the public realm of the Lower Haight neighborhood. The proposed treatments include traffic calming treatments, green infrastructure and bicycle infrastructure treatments. The Wiggle Neighborhood Green Corridor Project is a joint project between the San Francisco Public Utilities Commission (SFPUC), Public Works and the SFMTA.

Status: Construction has begun and will conclude Winter 2017-2018.

Website: <https://www.SFMTA.com/projects-planning/projects/wiggle-neighborhood-green-corridor>



Source: SFMTA Wiggle Green Corridor Project - Pierce and Haight Rendering

WALKSTOPS – COMMUNITY CHALLENGE GRANT

A Walkstop is an installation of wayfinding placards and maps that show local landmarks and points of interest with walking distance and times. The project was funded through the Community Challenge Grant Beautification Fund of the City and County of San Francisco. The program provides funding for community-led transformations efforts that enhance beauty, facilitate community participation and stewardship, and strengthen neighborhoods. A walkstop will be installed at the corner of Haight and Fillmore and will be directed by the walkstop team and Lower Haight Merchant and Neighborhood Association members working with local historians, artists and community volunteers.

Status: Design Development

Website: <http://www.sfccg.org/>



Source: SF Walkstop - Prototype of Walkstop



Source: Bay Area Bike Share - Example of Bike Share Fleet

BAY AREA BIKE SHARE EXPANSION (AKA FORD GO BIKE)

Bay Area Bike Share is the region's bike sharing system with 700 bikes and 70 stations across the region, including locations in San Francisco and San Jose. Intended to provide Bay Area residents and visitors with an additional transportation option for getting around the region.

Bay Area Bike Share is expanding tenfold from 700 to 7,000 bikes starting in Spring 2017. This expansion will roll out in phases and is expected to be complete by 2018. After being vetted through a community planning process in 2016, Bay Area Bike Share proposed placing bike share station at Page Street at Buchanan Street, Page Street at Scott Street, and Pierce Street at Haight Street.

Status: In Progress

Website:

<http://www.bayareabikeshare.com/>

<https://www.fordgobike.com/>



Chapter 2

EXISTING CONDITIONS

The Lower Haight neighborhood has a unique and eclectic artistic identity. It is a neighborhood of murals and funky shops, and is home to one of the City's regular ArtWalks. Its shops and restaurants are small and unique, and so are its buildings, which are small in scale and reflect decades of historical building tradition in San Francisco.

The streets and public spaces of the Lower Haight are as varied as the neighborhood itself. Small narrow alleys are tucked between more major streets. Page and Waller Streets are calm and green, with generous sidewalks, as are most north-south streets like Steiner, Pierce, Scott, and Webster. Fillmore Street is wider, with narrower sidewalks, to accommodate the 22-Fillmore bus, and is a mix of residential and small shops. Haight Street, of course, the neighborhood's commercial core, is a busy neighborhood commercial street pulsing with life and activity.

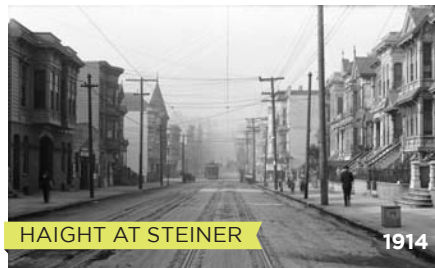
This chapter lays out the existing conditions in the Lower Haight neighborhood that guided development of this Plan.

THE LOWER HAIGHT THEN AND NOW

The Lower Haight neighborhood has seen constant and steady change over the years, as the photo comparisons from the early 20th Century on this page, and contrasting aerials on the facing page, show. The basic layout of the Lower Haight's streets and public realm has changed little over the

past century. What perhaps most stands out as having changed are the trolley tracks of yore, which were on Haight, Page, and Fillmore Streets, but were removed in the 1950s. Also noticeable is the lack of street trees in the early photographs, showing that the neighborhood is greener

than it once was, with most trees (Ficus) planted in the 1970-80s. Remarkably, most buildings have not changed - reflecting the special and often historic building stock the neighborhood is fortunate to boast.



CREDIT: SFMTA



CREDIT: HOODLINE



CREDIT: SFMTA



CREDIT: HOODLINE



CREDIT: SFMTA



CREDIT: HOODLINE



CREDIT: SFMTA



CREDIT: HOODLINE



CREDIT: UCB HISTORICAL SOCIETY



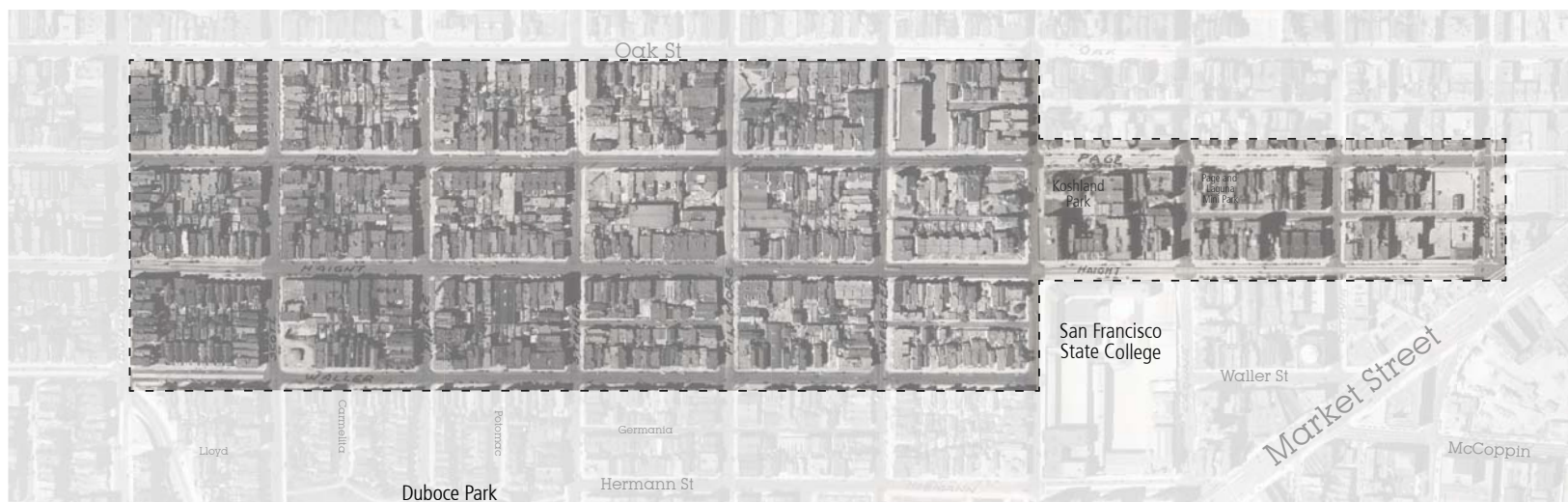
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CREDIT: SFMTA



CREDIT: HAIGHTERATION



NATURAL ENVIRONMENT

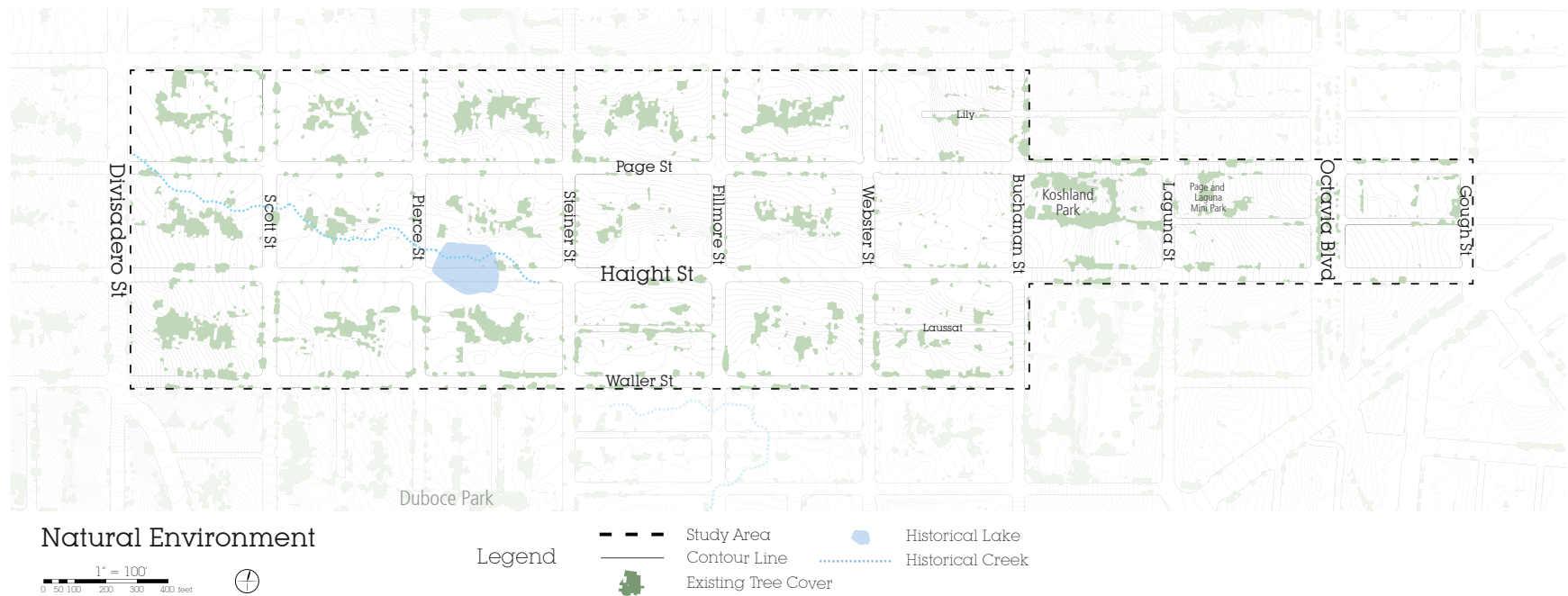
The neighborhood's natural environment has been radically changed over time. While the contours of the area's sloping hills have little changed, long gone are the historical creek and lake that once existed in the heart of the neighborhood, near Haight and Pierce Streets. However, vestiges of the old creek can be found; today's Wiggle green corridor follows the

route of the old creek, coursing through streets following the old drainage to as it provides the flattest and best source for one of the City's premier bike routes.

Tree cover is regular throughout the neighborhood, but denser in the private internal blocks than along the public rights-of-way. There remains room for improvement within the public realm to make the streets even greener.

LAND USE AND BUILT FORM

Existing land use, as shown on the facing page, is primarily residential and mixed-use residential. The main commercial areas include Haight Street, Divisadero Street, and Fillmore Street. Most retail on Haight Street is contained on the ground floor of residential buildings - a testament to how "mixed-use" land use has been the rule for decades in San Francisco.

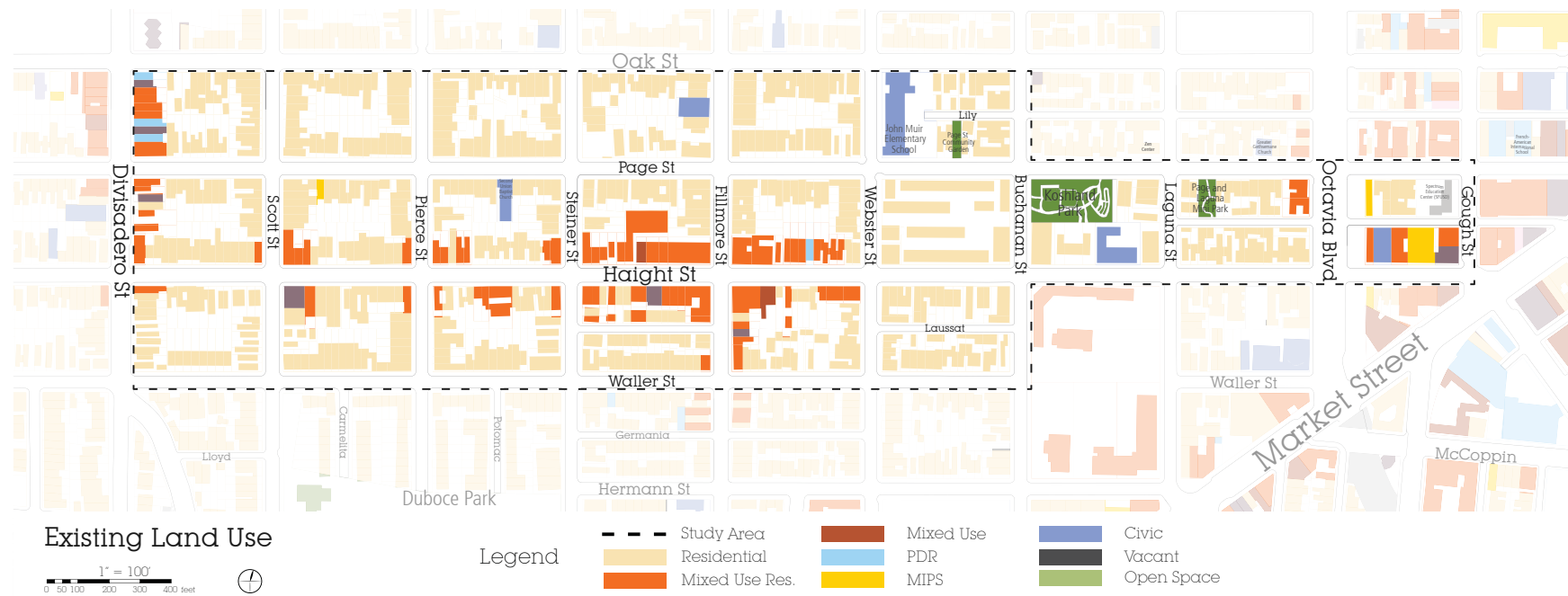
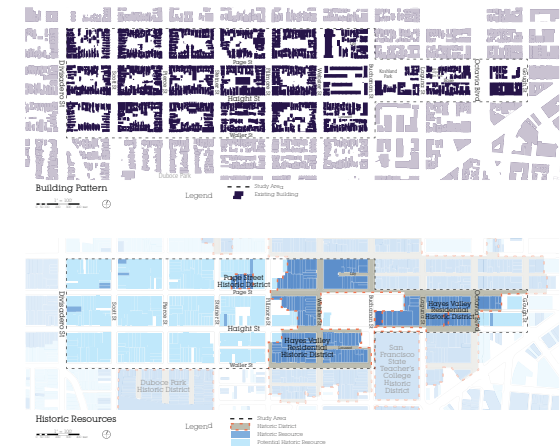


Several civic institutions are located in the neighborhood including John Muir Elementary School on Webster between Oak and Page Streets, a major community anchor. Koshland Park, along Page Street nearby, is a major neighborhood open space, augmented by the Page and Laguna Mini Park and the Page Street Community Garden.

The existing built form, at right, shows a regular and familiar pattern for San

Francisco, with buildings forming strong street walls along the public realm, giving way to internal private open space

There are several historic districts covering extensive areas of the neighborhood, as shown at right. Much of the remaining building stock is either historic or potentially historic by virtue of age. Hundred-year-old Victorians are typical fare in the Lower Haight.



DEMOGRAPHICS & SOCIAL CONDITIONS

The Lower Haight neighborhood, the focus area of this Plan, is home to more than 44,060 residents, roughly 5% of San Francisco's total population. There are nearly 22,000 households, of which 77% are single-person households, significantly higher than the City average of 55% single-person households, reflecting the neighborhood's youth. Just 23% of households are families, far lower than the already low City average of 45%. There are significantly more renters (69%) than homeowners (21%) in the Lower Haight.

The Lower Haight's median income tracks closely with San Francisco's median income of \$75,770, and median rent likewise tracks closely to the City average. The unemployment rate also matches the City rate, at 8%. Lower Haight residents tend to be more educated than the City average, with 40 % of residents having a college degree and 24% an advanced graduate degree, above the City averages of 32% and 21%, respectively.

Car ownership in the neighborhood is lower than ownership patterns citywide. 15% of homeowners get by without a

car., while nearly half of all renters go car-free. Given the higher share of renters compared to owners, this means nearly 40% of all households in the Lower Haight are car-free.

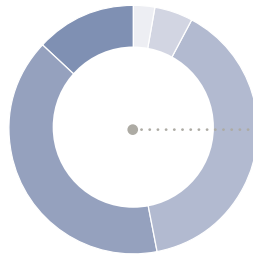
A full 60% of all residents travel to work by either transit (42%), walking (10%), and bicycling (8%), while just 29% of residents drive to work. These numbers highlight the central location of the neighborhood and its convenience to many other areas of the City, leading the way to a sustainable transportation future for San Francisco. They also underscore the importance and availability of transit in the neighborhood, as well as the strong and high-quality bicycling and walking environments. Few San Francisco neighborhoods are as transit-, bike-, and walk-friendly, and there are few better neighborhoods to live without a car.

Please note, that this data was pulled from original study area boundary that was later defined in later phase.



POPULATION**44,060**

vs. 817,501 Citywide

**AGE**

	CITYWIDE	LOWER HAIGHT
Under 5	4%	3%
5 to 17	9%	5%
18 to 34	30%	39%
35 to 59	36%	40%
60 and over	20%	15%

NO. OF HOUSEHOLDS**21,945** 

vs. 345,344 Citywide

MEDIAN HOUSEHOLD INCOME**\$76,510**

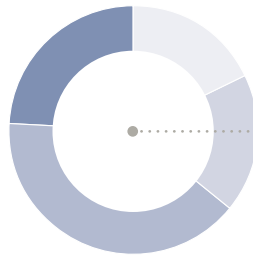
vs. \$75,770 Citywide

HOUSEHOLDS

	CITYWIDE	LOWER HAIGHT
Family Households	45%	23%
Single-Person Households	55%	77%
Non-Family Households	39%	47%
Average Household Size	2.3	2.0
Average Family Household Size	3.3	3.0

% OF HOUSEHOLDS WITHOUT A CAR (HOMEOWNERS)**15%** 

vs. 10% Citywide

**EDUCATION**

	CITYWIDE	LOWER HAIGHT
High School or Less	27%	18%
Some College / AA Degree	21%	18%
College Degree	32%	40%
Post Graduate Degree	21%	24%

% OF HOUSEHOLDS WITHOUT A CAR (RENTERS)**49%** 

vs. 42% Citywide

UNEMPLOYMENT**8%** 

vs. 8% Citywide

HOUSING

	CITYWIDE	LOWER HAIGHT
Total Number of Units	378,186	24,459
Owner Occupied Units	33%	21%
Renter Occupied Units	58%	69%
Median Rent	\$1,428	\$1,432

TRAVEL TO WORK

	CITYWIDE	LOWER HAIGHT
Car	45%	29%
Transit	33%	42%
Bike	3%	8%
Walk	10%	10%

TRANSIT

The Lower Haight is served by a number of major transit lines. Two Muni bus lines travel along Haight Street, while three more bus lines course through the neighborhood, including the 22-Fillmore. In addition, the main Muni Metro subway and F-Market street car travel along Market Street just south of the neighborhood. The Muni Forward 7-Haight project is upgrading stops and infrastructure along much of Haight Street.

MUNI ROUTES

7 + 7R – HAIGHT-NORIEGA

- Connects downtown and the Sunset
- Travels the full length of Haight between Stanyan Avenue and Market Street
- Stops at Divisadero, Pierce, and Fillmore

6 – PARNASSUS

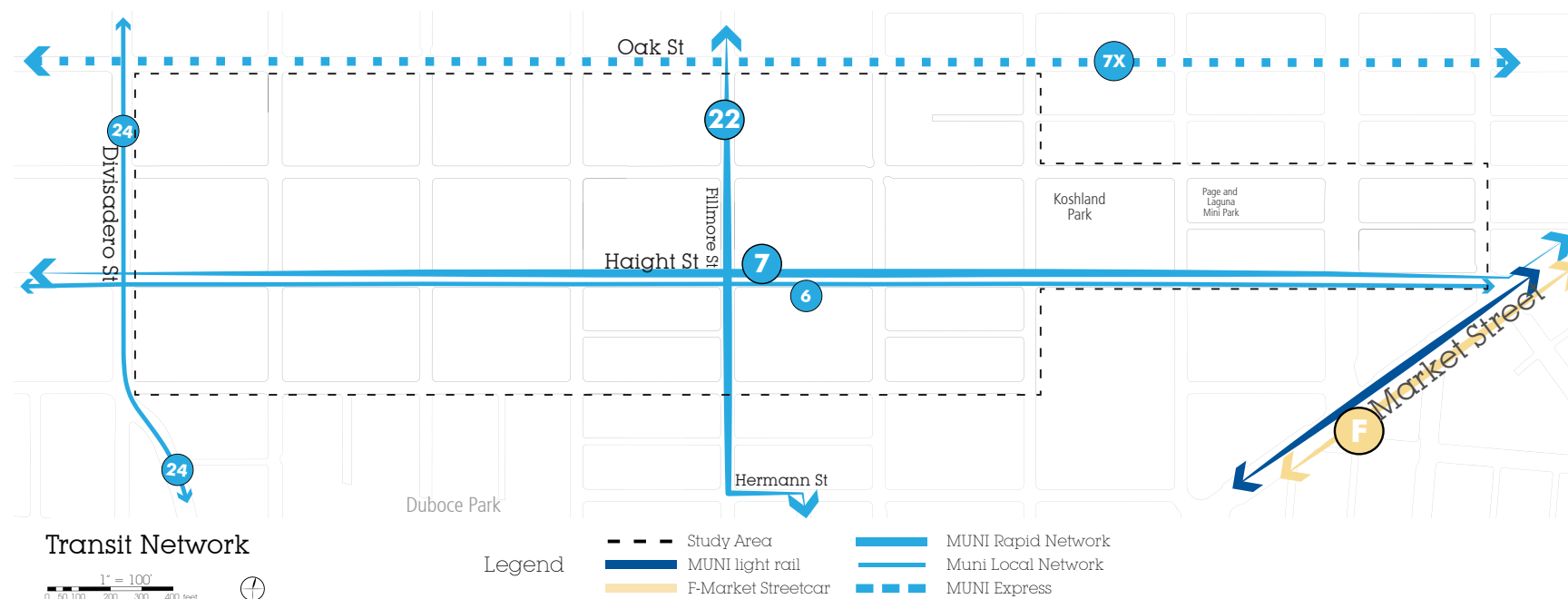
- Connects downtown and the Sunset
- Travels between Masonic and Market
- Stops at Divisadero, Pierce, Fillmore, and Buchanan

22 – FILLMORE

- Connects Mission Bay and the Marina
- Travels between Marina Bd and Hermann
- Stops at Oak and Haight Streets

24 – DIVISADERO

- Connects Pac Heights and the Bayview
- Travels between Jackson and Waller
- Stops at Oak and Haight Streets



BICYCLING AND WALKING

In general, the Lower Haight is blessed with high-quality streets for walking and biking. Most sidewalks are 15' wide and streets are typically calm and tree shaded. However, there are opportunities for improvement in both areas that could be explored.

The Lower Haight does have a strong bicycling infrastructure which includes the Wiggle, the Page Street bike route and a signed bike route of Webster, but additional bicycle infrastructure could be explored in areas such as Page Street.

In addition, Haight Street, the street that should be best for people walking deserves additional amenities. Currently the street has narrower sidewalks, unhealthy trees, and little available space for businesses to activate with cafe seating or other amenities to enliven the street

BIKE ROUTES

THE WIGGLE

- Connects the West side of the City to

Market Street and downtown

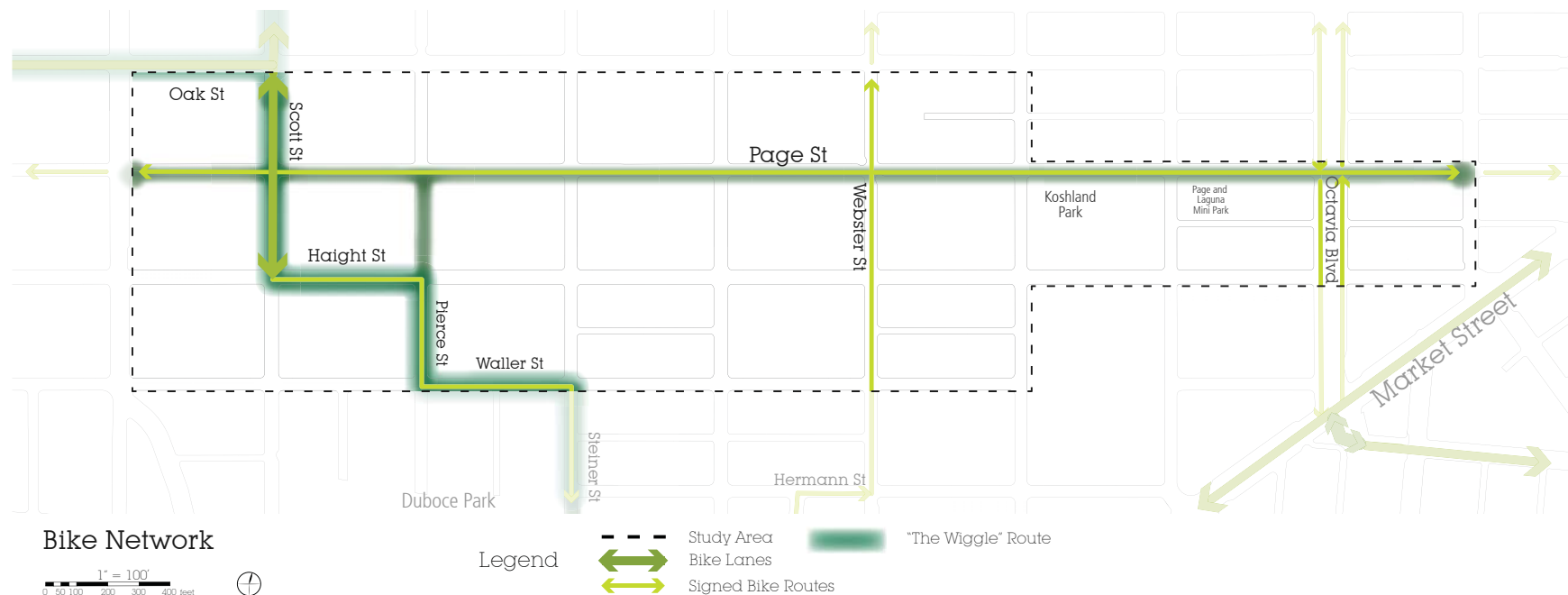
- Travels on Scott, Haight, Pierce, Waller, and Steiner Streets
- Green upgrades planned

PAGE ST

- Connects Golden Gate Park to Market
- Travels between Stanyan and Market

WEBSTER

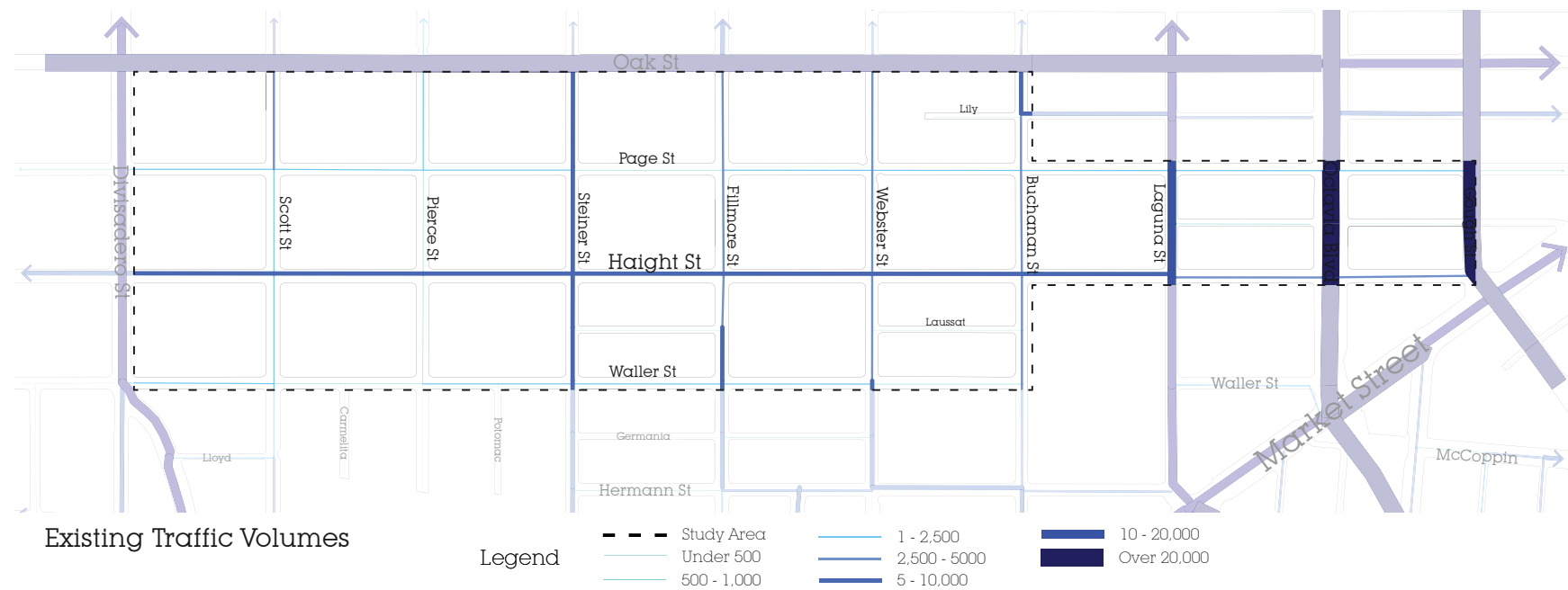
- Connects Church Street to the Western Addition



VEHICLE TRAFFIC

While the Lower Haight is surrounded by major roads with heavy traffic, including Divisadero, Oak, and Gough Streets, there are few major streets running through the neighborhood, and as a result most streets have relatively low vehicle traffic volumes.

Haight Street carries the most vehicle traffic but it has relatively light traffic compared to nearby neighborhood commercial corridors like Divisadero. Of the six north-south streets in the Plan Area, Steiner Street carries the heaviest vehicle traffic, although it too is considerably less than other similar streets nearby, such as Laguna.



DATA SOURCE: SFCTA GIS TRAFFIC MODEL DATA, 2011

PARKING, LOADING, + DRIVEWAYS

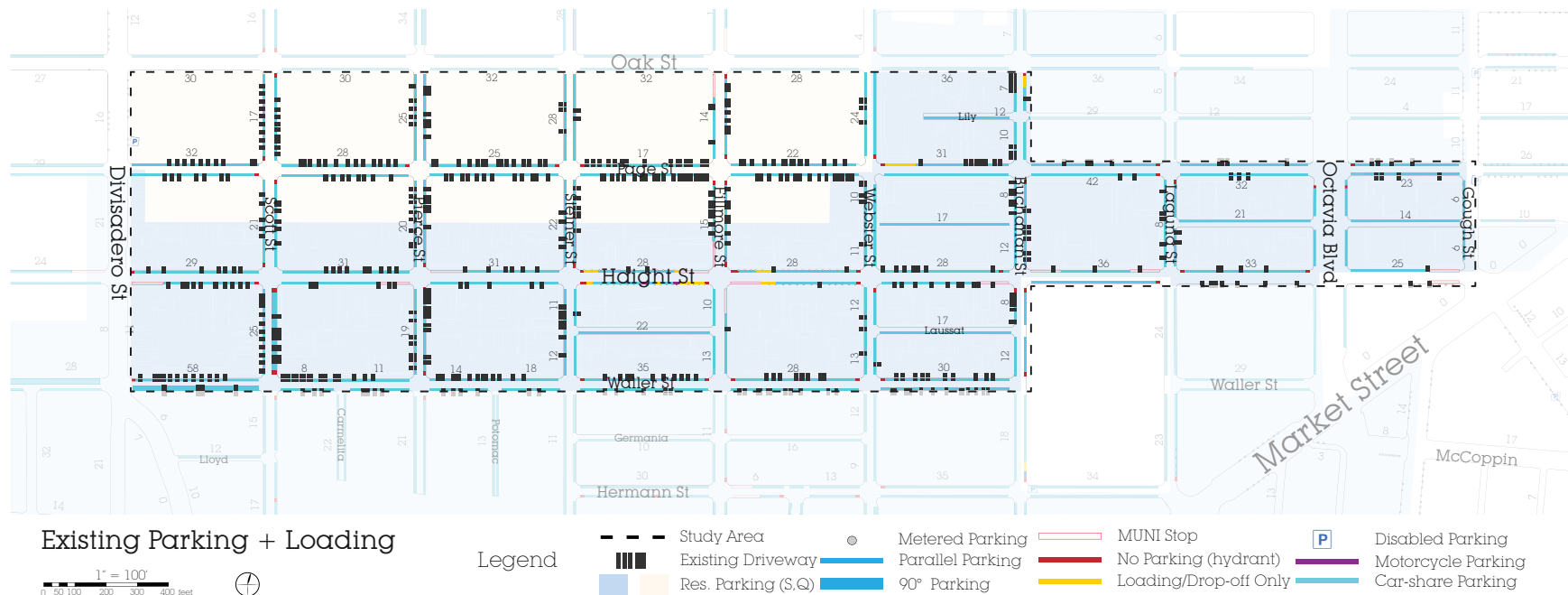
There are roughly 1,082 on-street parking spaces in the Plan Area. (This does not include any spaces on Oak or Divisadero, or on the blocks of Laguna, Octavia, or Gough between Page and Haight). Most are within two residential parking permit areas, Q and S.

Haight Street has around 269 spaces, including 58 metered spaces in the commercial core. In addition to parking

spaces for vehicles, there are parking spaces for motorcycles, car-share vehicles, and disabled users, and white and yellow loading spaces important for commercial businesses.

Driveways have the most significant impact on the availability of on-street parking. For example, the block of Page Street between Buchanan to Laguna has few driveways, and has two and a half times as much on-street parking as the block between Steiner to Webster, which has many driveways.

STREET	# OF RES. PERMIT PARKING	# OF METERED SPACES	# OF LOADING SPACES	# OF OTHER SPACES	TOTAL # OF SPACES
Haight	198	58	11	2	269
Page	246	-	6	-	252
Waller	202				202
Scott	63				63
Pierce	64				64
Steiner	73				73
Fillmore	52				52
Webster	50				50
Buchanan	55		2		57



DATA SOURCE: SF PLANNING DEPT SURVEYS, 2017

LIGHTING

The majority of street lighting today is "roadway"/"cobra"/"utilitarian"-style. There are no pedestrian-scale lights except on Waller Street west of Steiner, and on Octavia Blvd. The predominant type of street lights, as shown in the map below, are simply basic light arms on PG&E power and transmission poles; these are found on Page Street, most north-south streets, and most alleys. The next-most common type is the Cobra-style light, which is found on

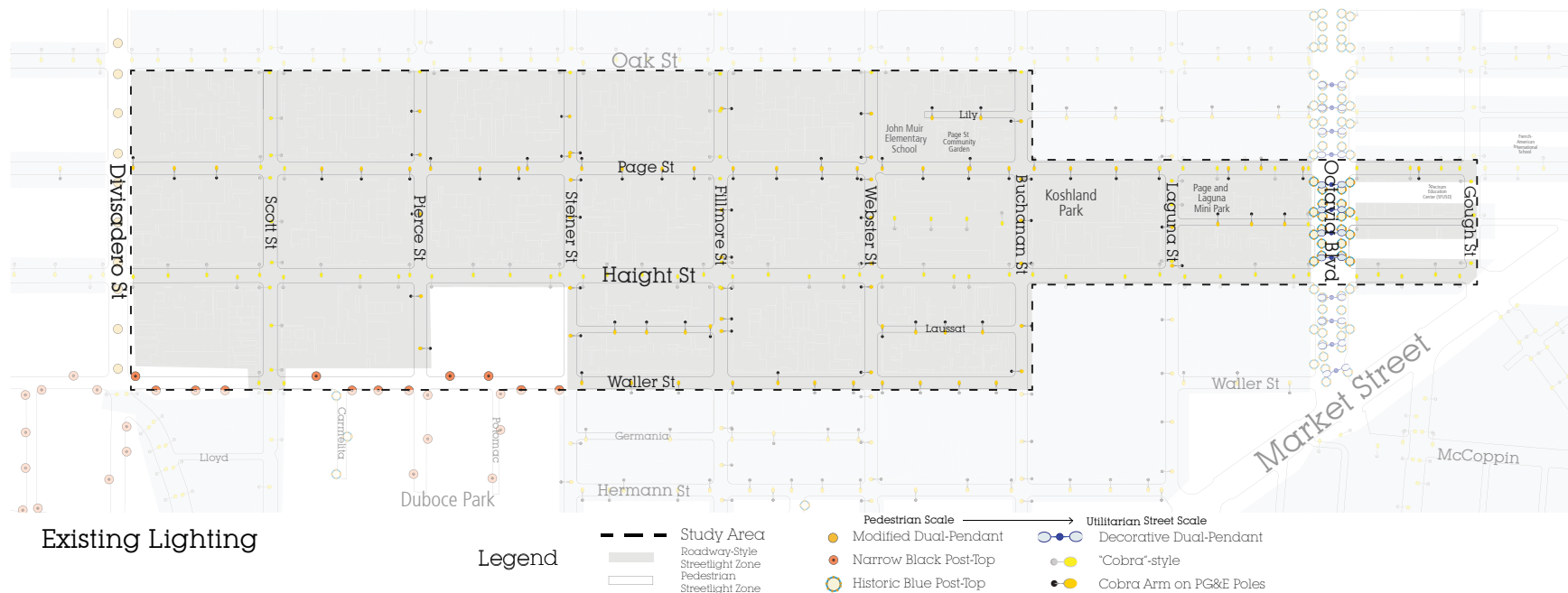
Haight Street, Steiner Street, and at some other intersections.

Extended gaps exist where street lighting is not provided. Certain buildings are dark at night. Areas around Waller, east of Steiner, Church & Duboce, around Duboce Park, at Fillmore & Hermann, and on Page from Pierce to Steiner, were called out by the community as needing better lighting

New pedestrian lighting is expensive. Funding sources are challenging to

identify, so creative citizen-led projects can be explored while funding is found for improved, pedestrian-scale lighting.

Lighting on private buildings can make a significant difference in overall lighting quality on the street.



TREES AND GREENING

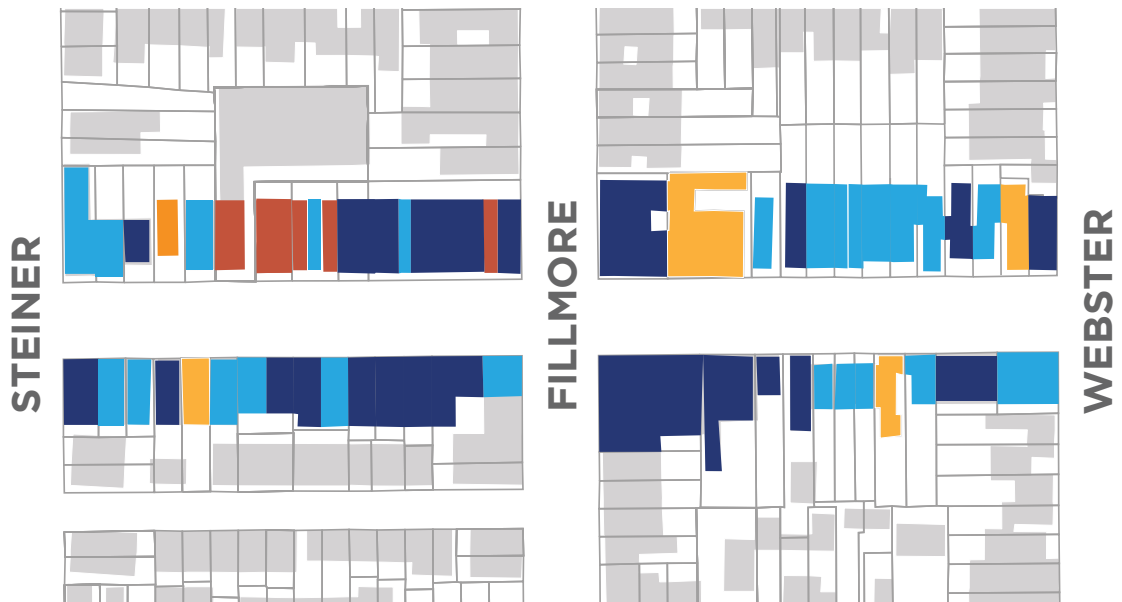
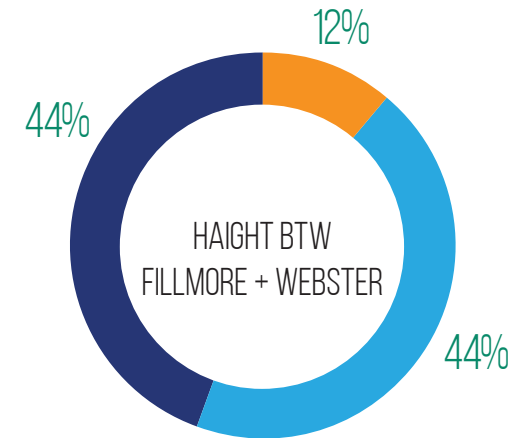
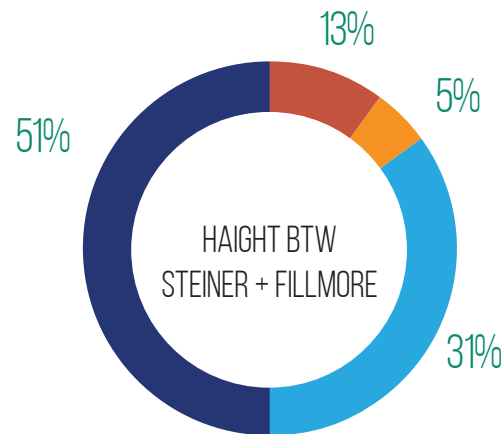
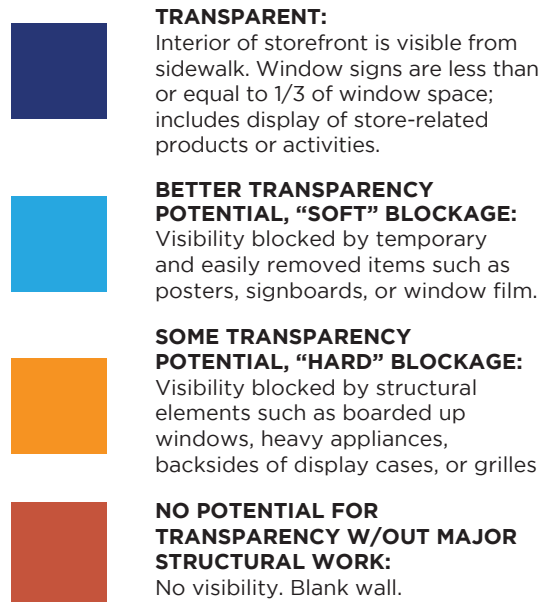
Existing tree cover in the Lower Haight is decent but inconsistent. The presence of street trees is greatly impacted by the presence of driveways, which eliminate the possibility for street trees at driveway locations. But there remains plenty of space for infill of street trees. Haight Street is particularly lacking in street trees, and a majority of its street trees in the commercial core are in poor or failing health.

Sidewalk gardens (shown in red below) are also found sporadically in the Lower Haight. Waller Street has a number of them on multiple blocks, but Page Street only has them on a couple of blocks, inconsistent with its role as a Green Connection. Few north-south streets have sidewalk gardens, and those few blocks that do are a visual reminder of the benefits of sidewalk gardens and the need for a more consistent palette for them throughout the neighborhood.



HAIGHT ST FACADE SURVEY

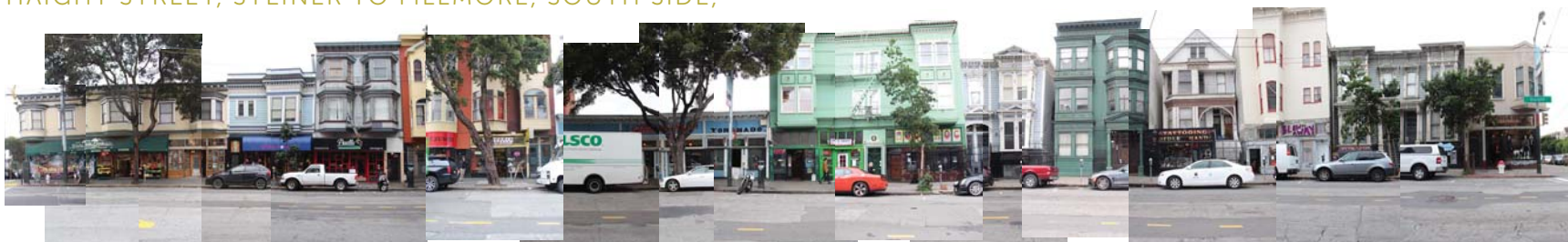
A facade transparency survey for the Haight Street commercial core, from Steiner to Webster, revealed poor storefront transparency. This was observed in some key areas that significantly detract from the commercial and pedestrian experience. Pockets of storefront vacancies also were found, some of which have been consistently vacant or near-vacant for years.



HAIGHT STREET, STEINER TO FILLMORE, NORTH SIDE,



HAIGHT STREET, STEINER TO FILLMORE, SOUTH SIDE,



HAIGHT STREET, FILLMORE TO WEBSTER, NORTH SIDE,



HAIGHT STREET, FILLMORE TO WEBSTER, SOUTH SIDE,





Chapter 3

COMMUNITY ENGAGEMENT

The Lower Haight Public Realm Plan is an interagency effort led by the San Francisco Planning Department in partnership with the San Francisco Municipal Transportation Agency and Public Works. At all phases of the project, from project initiation through visioning and concept design, has been developed in collaboration with local merchants and neighborhood groups through a variety of community engagement efforts.

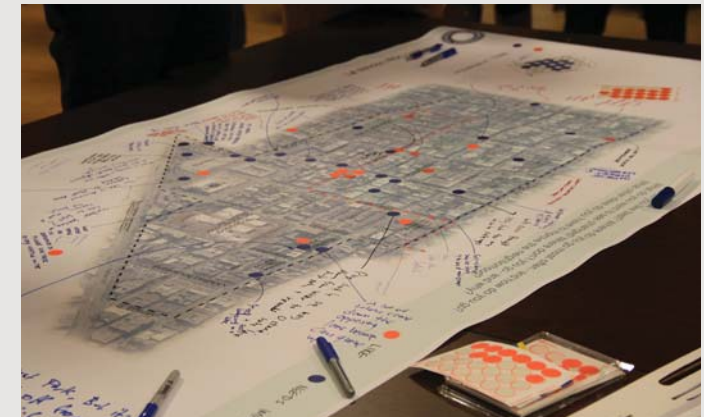
The proposed improvements presented in this document are based on community input gathered between Fall 2015 and Spring 2017. During the planning process, there were three public workshops. The first workshop was focused solely on the Lower Haight neighborhood, and was primarily a visioning and ideas exercise to help narrow down important focus areas, and generate ideas for concept design. At the second workshop, the initial design concepts for the Lower Haight Public Realm Plan, based on input from the first workshop, were presented to the community alongside plans from two related, nearby projects: SFMTA's Page Street Neighborway

project, and the Western Addition Community Transportation Plan. The third workshop, presented revised and refined concept designs and was also done in partnership with SFMTA's Page Street Neighborway project. Notification for all workshops was publicized to the community.

Additional outreach included engagement at a District 5 Town Hall, and stakeholder meetings with a number of different neighborhood groups. Engagement with local merchants and neighborhood groups continued through out the duration of the project.

The project collected written comments from workshop participants, and also utilized "Neighborland", an online citizen engagement tool, to solicit input and ideas. A summary of the materials presented at each workshop and the feedback received is provided in this chapter.

COMMUNITY ENGAGEMENT TIMELINE



Fall 2015
EXISTING
CONDITIONS
ANALYSIS

WINTER-SPRING 2016
CONCEPTUAL DESIGN
DEVELOPMENT

SPRING-SUMMER 2016
INTER-AGENCY
DESIGN REVIEW

2015

May 2015
PROJECT INITIATION

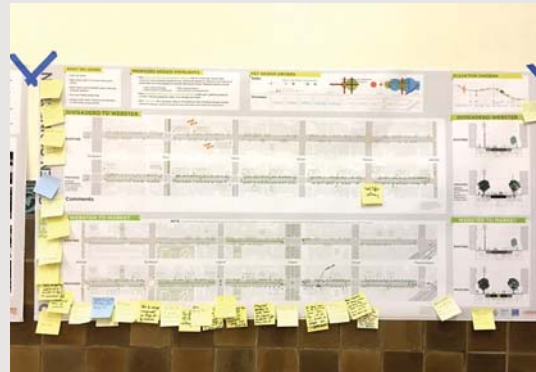
Summer-Fall 2015
COMMUNITY
STAKEHOLDER
OUTREACH

October 2015
DISTRICT 5
TOWN HALL

November 2015
WORKSHOP 1

2016

Spring-Summer 2016
ONGOING COMMUNITY
STAKEHOLDER
OUTREACH



WINTER-SPRING 2017
CONCEPTUAL DESIGN
REFINEMENT

SPRING 2017
DRAFT PLAN
DEVELOPMENT

2017

September 2016
WORKSHOP 2 /
DISTRICT 5 OPEN HOUSE

March 2017
WORKSHOP 3

August 2017
PLAN RELEASED



WORKSHOP 1

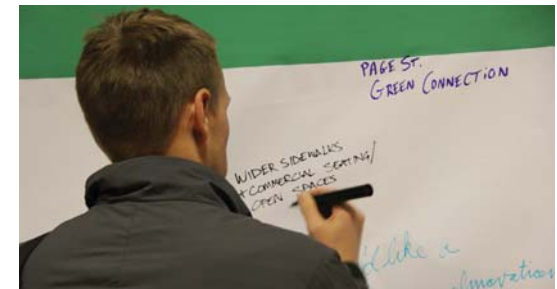
NOVEMBER 18, 2015

The first workshop was attended by over 30 people and took place at the IBEW Local 6 union offices on Fillmore and Hermann, on November 18, 2015. The workshop began with a presentation introducing the Public Realm Plan process, followed by an overview of project goals and an explanation of the workshop's goals and future engagement efforts to get involved.

Following the presentation, attendees were attended to engage with city staff with design boards and encouraged them to discuss and record what they love about the neighborhood, what concerns them, and ideas for improvements.

The neighborhood's funkiness, artistic character, parks, and sidewalk gardens on neighborhood streets were among the things most mentioned that people like about the neighborhood.

Safer streets for walking and biking, street lighting, maintenance/cleanliness, and signage were consistently mentioned as areas in need of improvement.



IDEAS

WORKSHOP 1









The primary goal of workshop 1 was to generate ideas from the community about what improvements they wanted to see more and what negative things they wanted less of in their neighborhood. An open “I want...in the Lower Haight” board (previous page) encouraged the community to share whatever ideas were on their mind, while also introducing them to Neighborland, the online engagement tool.

Other design boards (see example on right) prompted the community with images of potential improvements, which were voted on with the use of yellow (like) and black (dislike) dots.

Several consistent themes emerged from the public’s input on all these boards. The most popular design ideas and solutions were those related to:

- **Walking and biking**, including pedestrian and bicycle safety and amenities
- **Lighting, art, and identity** elements
- **Greening**, including street trees, Green Connections, and sidewalk landscaping; and
- **Active commercial storefronts** and outdoor cafe seating

EXAMPLE OF DESIGN BOARD

<p>STREET TREES</p> <ul style="list-style-type: none"> PROMOTE GREEN AND APPEALING PUBLIC REALM ENVIRONMENT ADD VISUAL APPEAL AND ENHANCE PEDESTRIAN EXPERIENCE CALM VEHICLE TRAFFIC IMPROVE AIR QUALITY AND HEALTH  <p>• REDUCE YEAR-TO-DATE TRAFFIC</p>	<p>SIDEWALK LANDSCAPING</p> <ul style="list-style-type: none"> CONVERTS PORTIONS OF SIDEWALKS INTO ATTRACTIVE LANDSCAPED AREAS ADD VISUAL APPEAL AND ENHANCE PEDESTRIAN EXPERIENCE PROVIDES WILDLIFE HABITAT IMPROVES STORMWATER MANAGEMENT 	<p>DISTINCTIVE BULB OUT PAVING/LANDSCAPING</p> <ul style="list-style-type: none"> ADD VISUAL APPEAL HIGHLIGHTS EXPANDED PUBLIC REALM INCREASES INFILTRATION POTENTIAL (PERMEABLE PAVING) 	<p>RAISED PLANTERS</p> <ul style="list-style-type: none"> PROVIDE GREENING OPPORTUNITIES INCREASE AESTHETICS OF THE STREET CAN BE MOVABLE 
<p>LED globes on trees (safety + solar)</p> <p>Too many trees already have fallen and continue to fall creating a safety hazard. City seems unable to maintain trees already in neighborhood</p> <p>Decorative tree guards and more durable tree species</p> <p>- Plant the right type of trees so they don't uproot/crack the sidewalk w/in years!</p>	<p>Even downtown BE RESPONSIBLE FOR CLEANING AND MAINTAINING THESE AREAS?</p> <p>The new rain gardens on Oak/Fell are great examples!</p> <p>Concrete removal!</p>		<p>WHO WILL BE RESPONSIBLE FOR THEIR MAINTENANCE? HOW WILL BE AVOID THIS NICE LANDSCAPING IDEAS FROM BECOMING JUNKSTERS?</p>
<p>PEDESTRIAN SAFETY FEATURES</p> <ul style="list-style-type: none"> PROMOTE A SAFE PEDESTRIAN ENVIRONMENT FOR ALL SUPPORT SAN FRANCISCO'S CITYWIDE VISION ZERO GOAL OF NO TRAFFIC DEATHS BY 2024 ENHANCE VISIBILITY AT MAJOR INTERSECTIONS 	<p>BICYCLE SAFETY FEATURES</p> <ul style="list-style-type: none"> PROMOTE BICYCLE USE BY RESIDENTS OF ALL AGES ENHANCE CROSSING VISIBILITY, IMPROVE ON-ROAD SAFETY, AND CLARIFY SPACE DELINEATION FOR ALL ROAD USERS 	<p>PEDESTRIAN AMENITIES</p> <ul style="list-style-type: none"> CREATE AN INVITING ENVIRONMENT FOR PEDESTRIANS AND NEIGHBORHOOD SHOPPERS PROVIDE RESTING AND REFRESHMENT OPPORTUNITIES 	<p>BICYCLE AMENITIES</p> <ul style="list-style-type: none"> ENHANCE THE RIDING EXPERIENCE FOR BICYCLISTS INCREASE SAFETY AND COMFORT ALONG THE WIGGLE POTENTIAL BIKE SHARE STATIONS 
	<p>better protect both cyclists + pedestrians along the wiggle w/ how to deal with stop signs?</p>	<p>Love the idea of ped. waystations through out I. Maigh. Solidifies neighborhood as commercial corridor + pedestrian 'play space'.</p>	<p>- Bike lane on page from Webster East to Octavia</p> <p>- Lots of bike share pools</p>

FOCUS AREAS

WORKSHOP 1

Two tables had base maps of the neighborhood and study area, with staff on hand to help residents specify particular areas they liked (orange dots) or areas of concern or opportunity (blue dots). A glance at the boards at right shows a wealth of detailed comments at the block and street corner level. These comments helped the design team hone in on focus areas for further design later in the planning process.

“More pedestrian-scale LED fun lighting”

“Bad intersection for bikes + peds”

“More healthy large trees”

*“Wider sidewalks + commercial seating/
open spaces”*

The feedback received at the first workshop and at other stakeholder outreach events were important in helping guide development of next steps and decide on the key focus areas, which were then fleshed out into initial concept designs and presented at the second workshop.



WORKSHOP 2

SEPTEMBER 5, 2016

The second workshop was attended by over 55 people and took place at John Muir Elementary School. The workshop included two related, nearby projects: SFMTA's Page Street Neighborway project, and the Western Addition Community Transportation Plan.

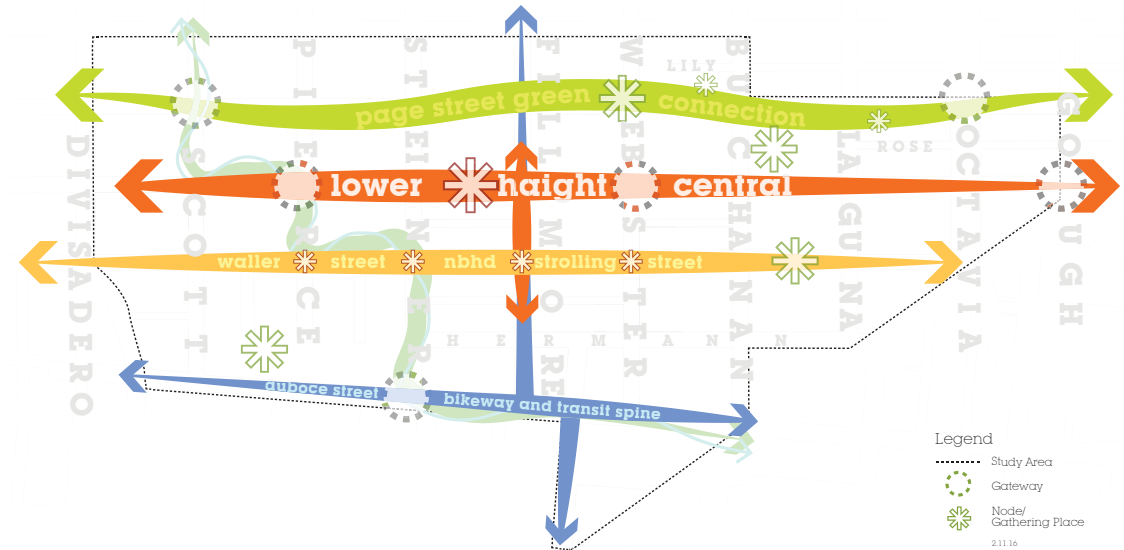
Two key focus areas were identified for further concept design from the first workshop and were presented at this second workshop:

- Page Street, to help bring out its key role as a Green Connection, and
- Haight Street, specifically the commercial core from Steiner to Webster

Several design options for both areas were presented to the community for feedback.

Also presented for feedback was a neighborhood-wide design framework (above right). This was later refined, based on community feedback, to the final framework seen on p. 54-55.

LOWER HAIGHT PUBLIC REALM DESIGN FRAMEWORK



COMMUNITY DESIGN CORE PRINCIPLES

The hundreds of ideas generated at the first workshop were the catalyst in jump-starting the concept design process. From these hundreds of ideas emerged certain themes that came up again and again. These themes informed, shaped, and were eventually formulated into five core community design principles, detailed below. These principles helped guide the creation of the neighborhood-wide design

framework and all concept designs that were presented to the community at the second workshop.

These five principles were well-received and supported by the community, and guided design development in the months to come.



VIBRANT PUBLIC REALM

Active + clean sidewalks, active street facades, public gathering spaces, pedestrian lighting



MORE MOBILITY

Increase safety and comfort for pedestrians and bicyclists



STREETS

Explore special streets/ green connections



GREEN SPACES

More street trees + sidewalk landscaping everywhere



CELEBRATE IDENTITY

Bring out Lower Haight's character and its key role as the neighborhood center

HAIGHT STREET

Haight Street clearly rose to the top as a primary focus area of this Plan. One short-term design for Haight Street was presented showcasing upcoming improvements that would address transit reliability and an improved public realm.

Two long-term design options were presented for the main commercial core:

- Widened Sidewalks: This option would widen sidewalks in the long-term around key nodes, such as Haight and Fillmore, to create focused public space improvements at these areas.

- Special Single-Material Paving Design: This innovative design would use special and consistent paving, as well as flex parking/public space zones, special intersection treatments, and gateway intersections, to create a special commercial core area to maximize public gathering, and cafe space while retaining use flexibility at the curb edge.



Participants generally liked the short-term option, but were not supportive of adding an eastbound bike lane to the street, as it seemed unnecessary. Participants were supportive of both long-term options. In general, there was a lot of interest in and excitement about the possibilities for Haight Street. Based on this feedback, the City decided to move forward with and refine the public realm proposals.

A number of different design treatments were also shown to inspire the community (shown at right), many reflecting the artistic character of the neighborhood, to stimulate further input on design details that would be most appropriate for the neighborhood.



PAGE STREET GREEN CONNECTION CONCEPT DESIGNS

Several design options for the Page Street Green Connection were presented for community input and feedback, as shown on the facing page. Options were divided into segments of the street, based on significant variations in character as well as the proposed level of intervention. All concepts assumed significant upgrades in greening, including street trees and sidewalk gardens.

Segment One, from Divisadero to Webster, proposed two concept design options, both with permeable (material that allows rainwater to pass through) curb lanes:

- Option 1: Green Sharrows, and
- Option 2: Advisory green lanes.

Option 2, the Advisory Bike Lanes, was preferred by a majority of participants.

Segment Two, from Webster to Octavia, proposed two concept design options:

- Option 1: Center Bike Lane, with Laguna Street diverter, and
- Option 2: Downhill greenway, with one-way vehicle circulation.

Option 2, the Downhill Greenway, was preferred by a majority of participants in the workshop.

Segment Three, from Webster to Market, proposed two concept design options, both with permeable curb lanes:

- Option 1: Green Sharrows, and
- Option 2: Two-way greenway.

There was a small preference for Option 2, the two-way greenway, but there was less preference clarity in this segment.

Possible traffic diversion measures proposed as part of some of the concept designs, such as diverters and circulation changes, were generally well-received, with some concern about their impact.





WORKSHOP 3

MARCH 15, 2017

74 people came to the third and final public meeting for the Lower Haight Public Realm Plan. Roughly 54 of attendees were neighborhood residents. The open house was a joint public workshop with the SFMTA's Page Street Neighborway project. The meeting was an open house format in which attendees shared their thoughts and ideas about the design concepts proposed for Haight and Page Streets. Participants also completed a survey on the streetscape elements and priority locations for improvements.



Join us to learn more about two community-based proposals to improve the streets, sidewalks, and public places in your neighborhood.

- The Lower Haight Public Realm Plan, initiated by the Lower Haight community, is working to develop a community-based vision that will complement and enhance the neighborhood's public spaces. Two earlier workshops provided valuable feedback on the community's priorities for public realm improvements on Page Street and the Haight Street commercial core. This workshop will introduce focused concepts for these two main corridors.

- Coordinated with the Lower Haight Public Realm Plan, the Page Street Neighborway is a capital project to make Page Street (from Webster to Market) a more walkable, bikeable, and sustainable corridor in Hayes Valley. The SFMTA is seeking feedback on a draft proposal for sidewalk widening, new bike facilities, traffic calming, and circulation changes before scheduling a public hearing, presenting to the SFMTA Board of Directors for approval, and starting the project's detailed design phase.

This is a drop-in open house format designed so you can learn more about these projects and ask questions of City staff in a face-to-face, friendly environment.

MARCH 15, 2017
WEDNESDAY 6:00 - 7:30 PM

JOHN MUIR ELEMENTARY SCHOOL
380 Webster Street ADA ACCESSIBLE
(between Oak St. + Page St.)

FOR MORE INFO, CONTACT:
Lower Haight Public Realm Plan
Jessica Look, San Francisco Planning
jessica.look@sfgov.org
<http://sf-planning.org/lowerhaight>

Page Street (Webster to Market) Neighborway
casey.hildreth@sfmta.com
SFMta.com/Octavia

Registration is not mandatory, but helpful to ensure we have enough materials and refreshments. Please register at <https://DSOpenHouse2.eventbrite.com>. Light refreshments, spanish interpretation and child care will be provided at event.

For other disability accommodations or language assistance, please contact Candace Soohoo at candace.soofoo@sfgov.org or 415-575-9157 at least 72 hours in advance of the event.



中文詢問電話: (415) 575-9010
Para información en Español llamar al: (415) 575-9010
Para sa impormasyon sa Tagalog tumawag sa: (415) 575-9121

Public Notice for Workshop



Ven a aprender más acerca de dos propuestas basadas en la comunidad para mejorar las calles, banquetas y espacios públicos en tu vecindario.

- **El Plan del Ámbito Público de Lower Haight**, iniciado por la comunidad de Lower Haight, está trabajando para desarrollar una visión basada en la comunidad que complementará y mejorará los espacios públicos del vecindario. Dos talleres previos proporcionaron importante retroalimentación sobre las prioridades de la comunidad para mejoras del ámbito público en Page Street y el corazón del corredor comercial de Haight Street. Este taller introducirá conceptos enfocados para estos dos corredores principales.

- Coordinando con el Plan del Ámbito Público de Lower Haight, la **Vía Vecinal de Page Street** es un proyecto capital para hacer de Page Street (de Webster a Market) una calle más caminable, mejor para las bicicletas y un corredor sustentable en Hayes Valley. SFMTA busca retroalimentación en un borrador de una propuesta para ampliar las banquetas, nuevas instalaciones para las bicicletas, métodos para calmar el tráfico y cambios en la circulación antes de programar una audiencia pública (hearing), presentando a la Junta Directiva para aprobación, e iniciando la etapa detallada del proyecto.

Éste es un taller abierto, puede llegar a la hora que prefiera, diseñado para que pueda aprender más sobre estos proyectos y hacer preguntas al personal de la Ciudad en persona, en un ambiente amigable.

MARZO 15, 2017
MIÉRCOLES 6:00 - 7:30 PM

JOHN MUIR ELEMENTARY SCHOOL
380 Webster Street ADA ACCESSIBLE
(between Oak St. + Page St.)

Para Más Información, Favor de Contactar:
Plan del Ámbito Público de Lower Haight
Jessica Look, San Francisco Planning
jessica.look@sfgov.org
<http://sf-planning.org/lowerhaight>

Vía Vecinal de Page Street (Webster to Market)
casey.hildreth@sfmta.com
SFMta.com/Octavia

No es necesario registrarse, pero nos ayuda a asegurarnos de tener suficientes materiales y bocadillos. Favor de registrarse en <https://DSOpenHouse2.eventbrite.com>. Se proveerán refrigerios, intérpretes en español, y cuidado de niños en el evento.

Accesibilidad: John Muir Elementary es ADA Accesible. Para otro tipo de alojamiento o asistencia de lenguaje, favor de contactar a Candace Soohoo en candace.soofoo@sfgov.org o 415-575-9157 al menos 72 horas antes del evento.



中文詢問電話: (415) 575-9010
Para información en Español llamar al: (415) 575-9010
Para sa impormasyon sa Tagalog tumawag sa: (415) 575-9121



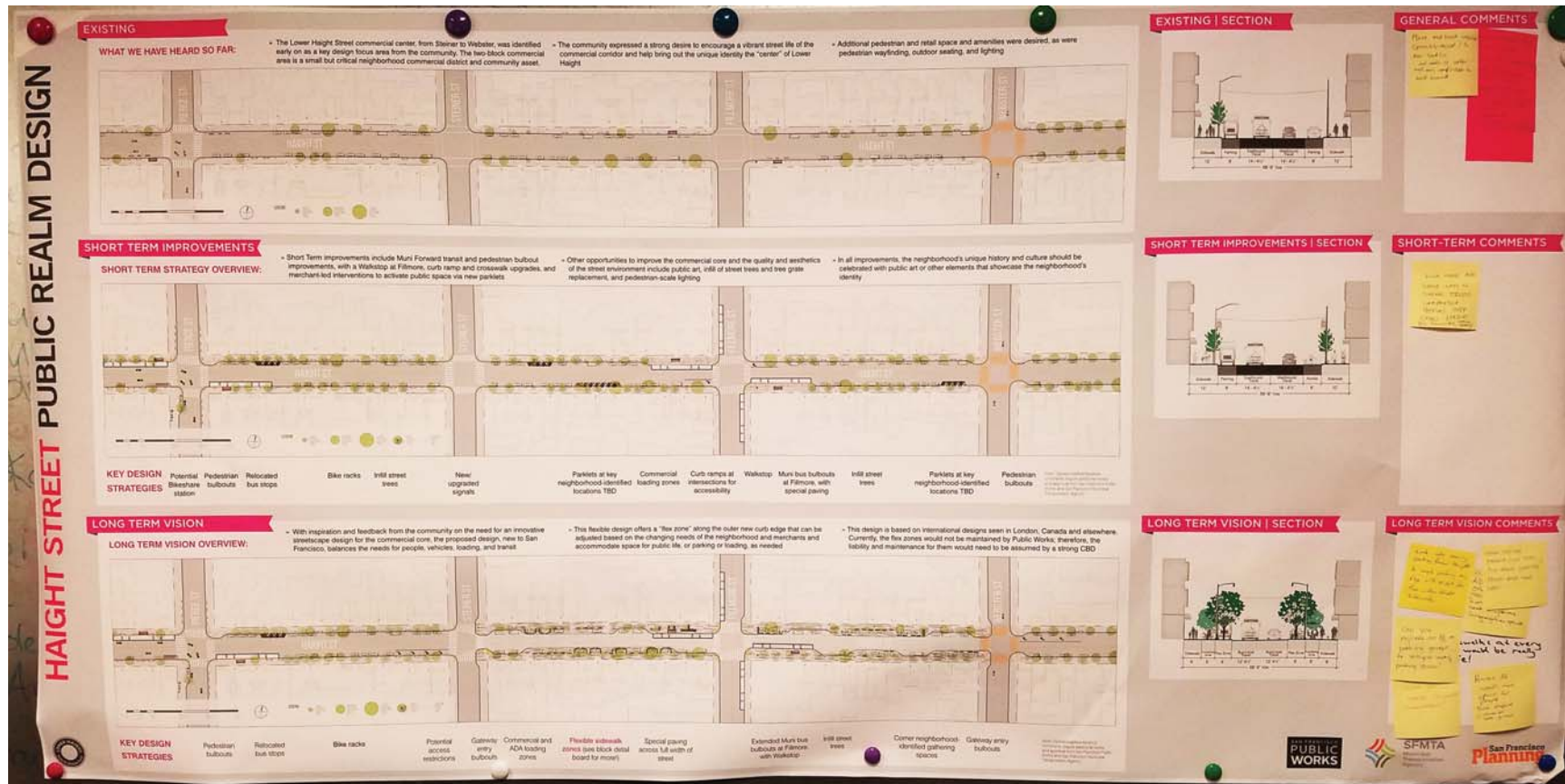
HAIGHT ST DESIGN

Short and long-term concept designs were presented for Haight Street, informed by input from the second workshop. Proposed short-term improvements included Muni Forward transit and pedestrian bulbout improvements, with a Walkstop at Fillmore, curb ramp and crosswalk upgrades, and merchant-led interventions to activate public space via new parklets.

The proposed long-term design concept drew inspiration from the community on the need for an innovative streetscape design for the commercial core. Refined from Workshop Two and new to San Francisco, it balances the needs for people, vehicles, and transit, through a “flex zone” along the outer new curb edge. This zone can be adjusted based on the changing needs of the neighborhood and merchants and can accommodate space for public

life, or parking/loading, as desired.

Other opportunities to improve the commercial core and the quality and aesthetics of the street environment include public art, infill of street trees and tree grate replacement, and pedestrian-scale lighting. In all improvements, the neighborhood’s unique history and culture would be celebrated with public art or other elements that showcase the neighborhood’s identity.



HAIGHT ST FLEX ZONE SPACES

Detailed renderings of how the flex zone spaces would work were also provided for community input and feedback. More detail on this design is provided in the following chapter; basic elements include:

- Swinging benches and planters to enable flexible and easy transitioning of curbside space use
- A beveled or mountable curb; and

- Gates or benches customized with creative artistic elements to highlight the Lower Haight's unique creative identity

The community was generally excited about this design concept for Haight Street. Representative quotes from attendees included:

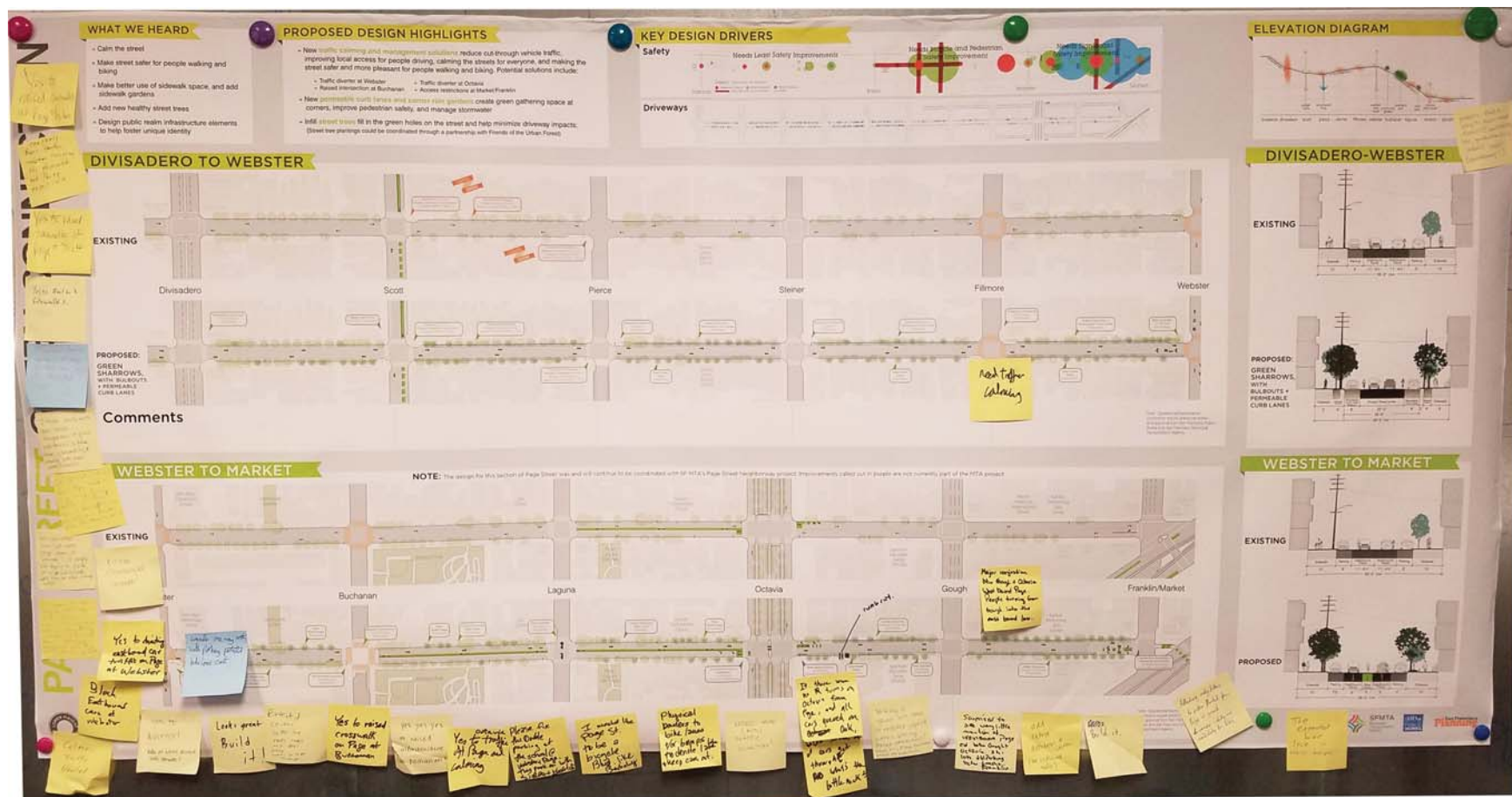
- "Love the place-making, gathering spots. This turns a street into a neighborhood."
- "Great idea for the flex gates! This

reflects the creativity + social vibe of the neighborhood"

- "This is fun, and provides needed sidewalk space + seating"
- "Amazing idea! I would love a little table here to eat outside on a nice day."

There were some concerns about the noise and impact of adding "more bar space" to the neighborhood.





ADDITIONAL OUTREACH

Throughout the plan development process, information and working group sessions were held with a variety of stakeholders and neighborhood organizations, to give individual groups of stakeholders the chance to provide specific feedback. Outreach efforts included:

August, 2015 - LoHaMNA Board Meeting

October, 2015 - District 5 Town Hall

December, 2015 - John Muir Elem. School

January, 2016 - Office of Economic Workforce Development (IIN)

January, 2016 - Market-Octavia Citizens Advisory Committee

February, 2016 - Hayes Valley Neighborhood Association

March, 2016 - San Francisco Bicycle Coalition

April, 2016 - Duboce Triangle Neighborhood Association - Land Use Committee

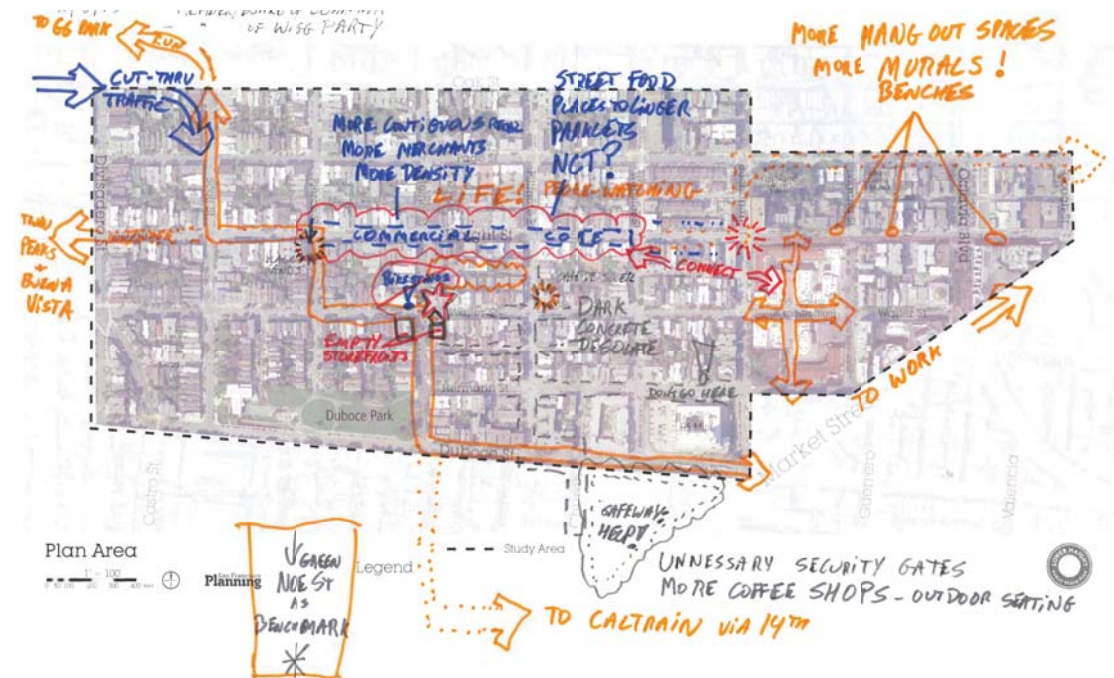
May, 2016 - John Muir Elementary School Parent Club

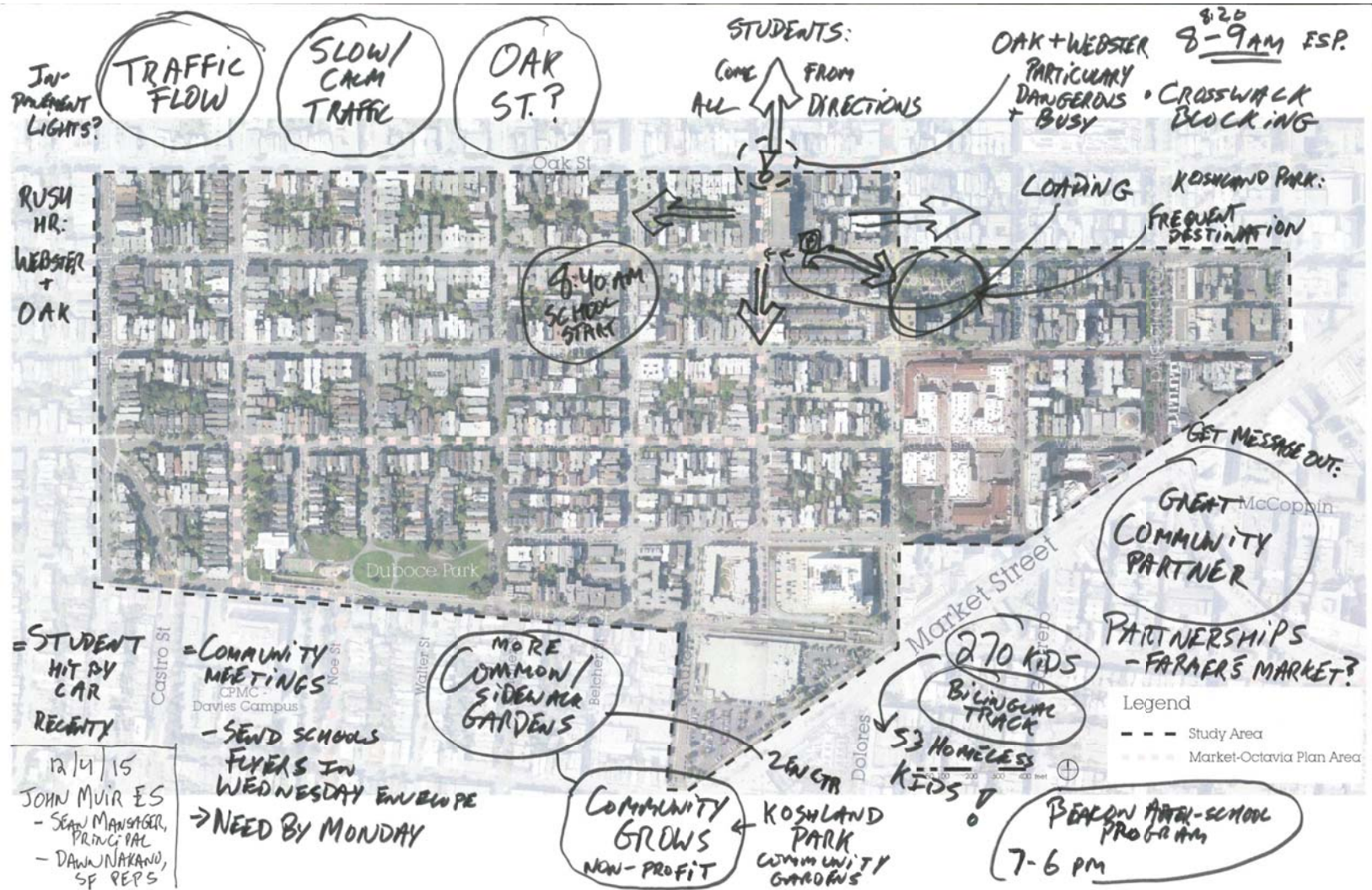
August, 2016 - Zen Center

November, 2016 - LoHaMNA Board

STAKEHOLDER MAPS

Numerous stakeholder interviews were conducted with members of the Lower Haight community. Detailed input and suggestions for improvement were recorded on maps of the plan area, to help guide future concept designs. A sample of some of the map are shown.





Sample Stakeholder Map



Chapter 4

DESIGN CONCEPTS

While much of the public realm in Lower Haight is already of a high quality, relative to other neighborhoods, there is room for improvement. During the community planning process, the focus of the plan was guided to the neighborhood's most fundamental shared public spaces, its streets. The Plan includes a specific community- and agency-vetted long-term design for the Haight Street commercial core, and a concept design plan for Page Street, a Green Connection in the neighborhood. Design concepts for these key areas are detailed in this chapter. Further refinement of these designs will be needed as these projects advance to the design development phase, including maintenance, liability, and funding.

VISION

The vision for the primary elements of this plan emerged directly from the community in workshops, surveys, stakeholder meetings, and other community engagement. This engagement work helped participants define what they most love about the neighborhood, what concerns them, and helped them generate ideas for neighborhood improvements.

Participants expressed a desire to enhance neighborhood character and identity, to improve the overall pedestrian experience and public transportation, to continue the greening of the neighborhood, and to address safety and social issues. The five concepts serve as a guiding framework for the Lower Haight Public Realm Plan as developed by residents, business owners, and other stakeholders in the Lower Haight.



Photo credit: SF Planning Department

VIBRANT PUBLIC REALM

Active + clean sidewalks, active street facades, public gathering spaces, pedestrian lighting



Photo credit: SF Planning Dept

MORE MOBILITY

Increase safety and comfort for pedestrians and bicyclists



Photo credit: SF Planning Dept

INNOVATIVE STREETS

Explore special streets/green connections



Photo credit: SF Planning Dept

GREEN SPACES

More street trees + sidewalk landscaping



Photo credit: SF Planning Dept

CELEBRATE IDENTITY

Bring out Lower Haight's character and its key role as the neighborhood center

DESIGN FRAMEWORK

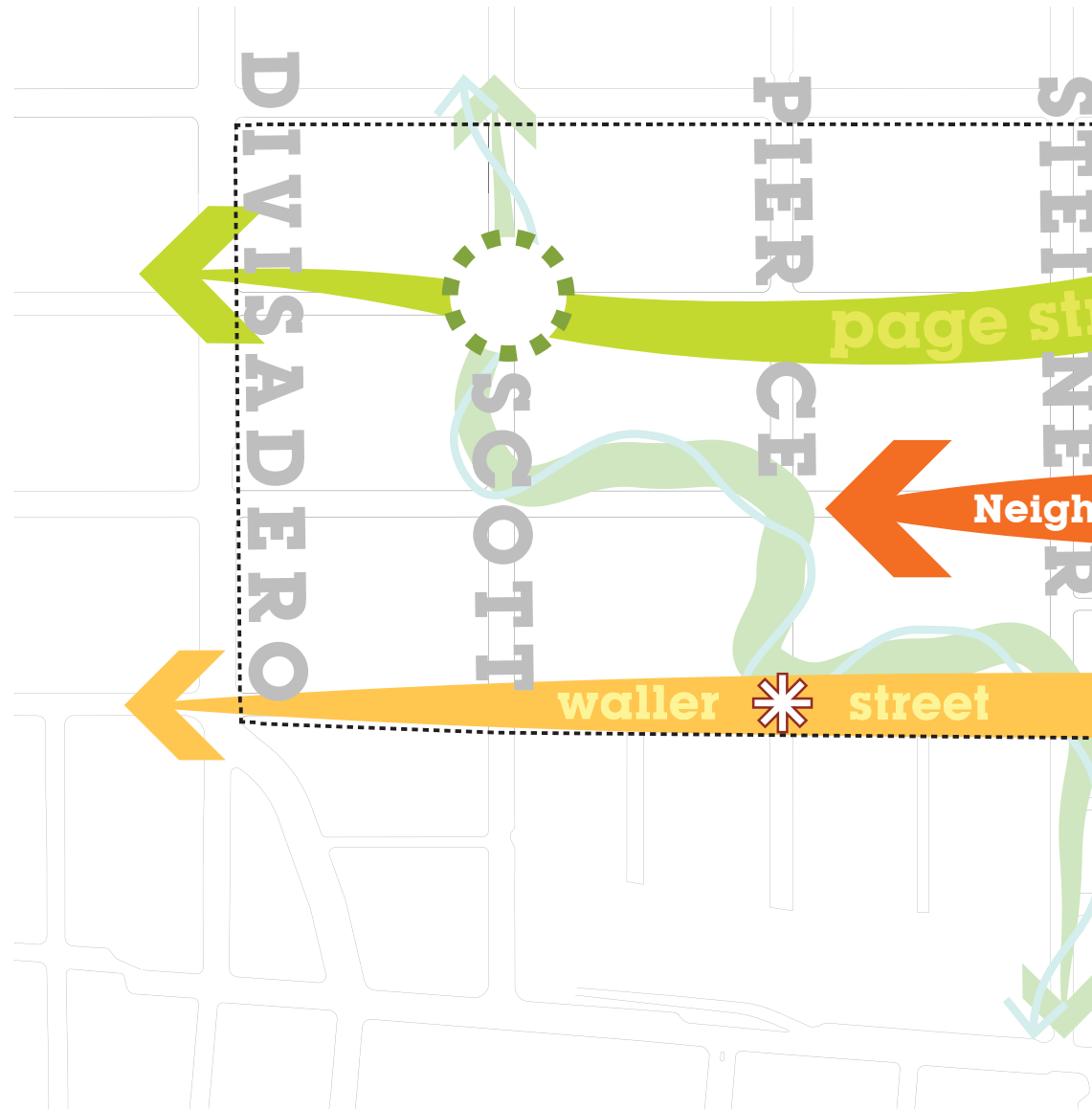
Developed and refined via community meetings and other engagement, the Lower Haight Public Realm Plan design framework highlights the major areas for short- and long-term improvements in the neighborhood.

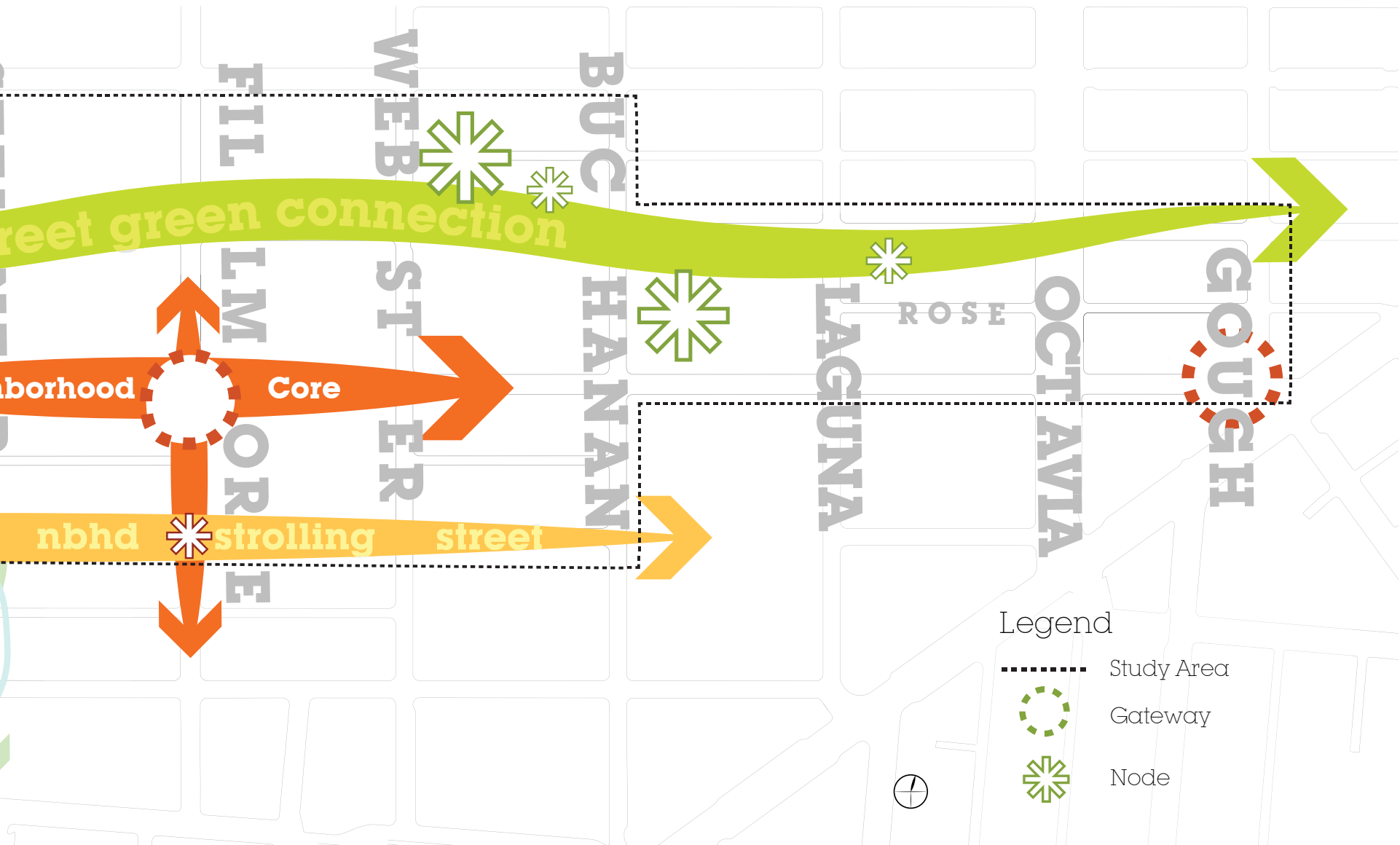
A special design for the two blocks of Haight Street in the commercial core will remake these key blocks and unlock the potential for this important neighborhood commercial area to meet its potential to become the special gathering and meeting place for neighbors.

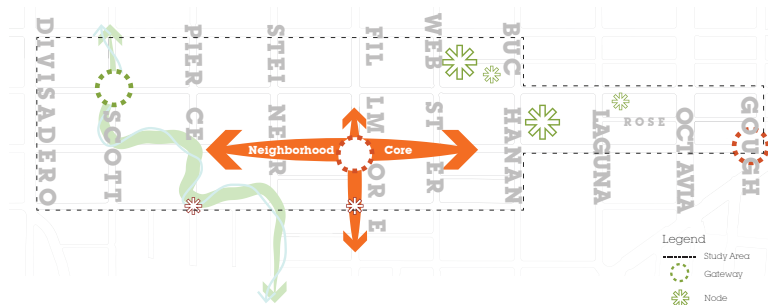
Page Street will be developed as Green Connection, with new trees and greening, safer bicycle and pedestrian facilities, and, where feasible, stormwater management features.

Waller Street, already a wonderful street for walking, can be improved over the years to make it an even more special street for strolling. Finally, other important gateway areas and community nodes can be improved to enhance and celebrate these key points, including but not limited to Haight and Fillmore, and Page and Scott.

Specific improvements for Waller and gateway and node areas will need additional community vetting, but still have been identified as priority areas through this planning process.





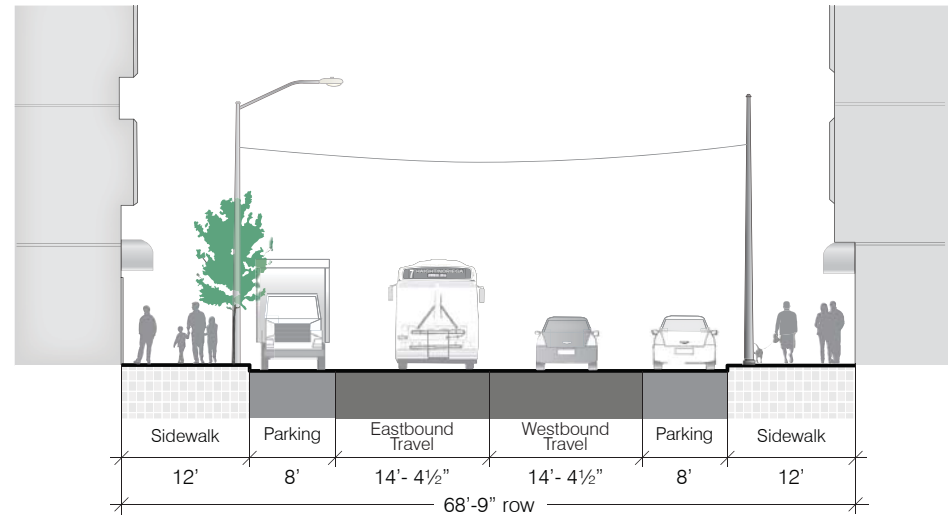


HAIGHT STREET: TODAY

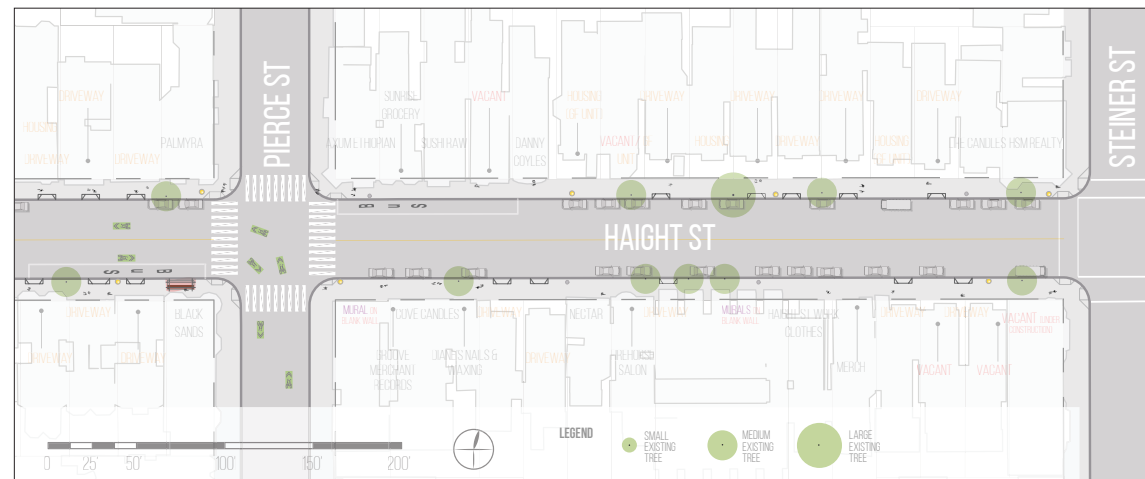
The Lower Haight Street commercial core from Steiner to Webster, was identified early by the community as a key design focus area. The two-block area is a small but critical neighborhood commercial district and community asset.

Today the street has wide travel lanes and relatively narrow sidewalks for a commercial street, offering little extra space for gathering or activation of the sidewalk. Lighting is utilitarian, and sidewalk street paving is standard concrete, with nothing special to highlight the neighborhood's unique and artistic identity.

HAIGHT STREET, EXISTING SECTION, PIERCE TO WEBSTER



HAIGHT STREET, EXISTING, PIERCE TO STEINER



The community expressed a strong desire for a special design for the commercial core of the Lower Haight neighborhood that would enable more vibrant street life and help celebrate this central area's unique identity.

Additional pedestrian and retail space and amenities were desired, as were pedestrian wayfinding, outdoor seating, lighting, and special elements that would provide a strong sense of the Lower Haight's identity, such as local art, music and independent spirit.

HAIGHT STREET COMMERCIAL CORE TODAY

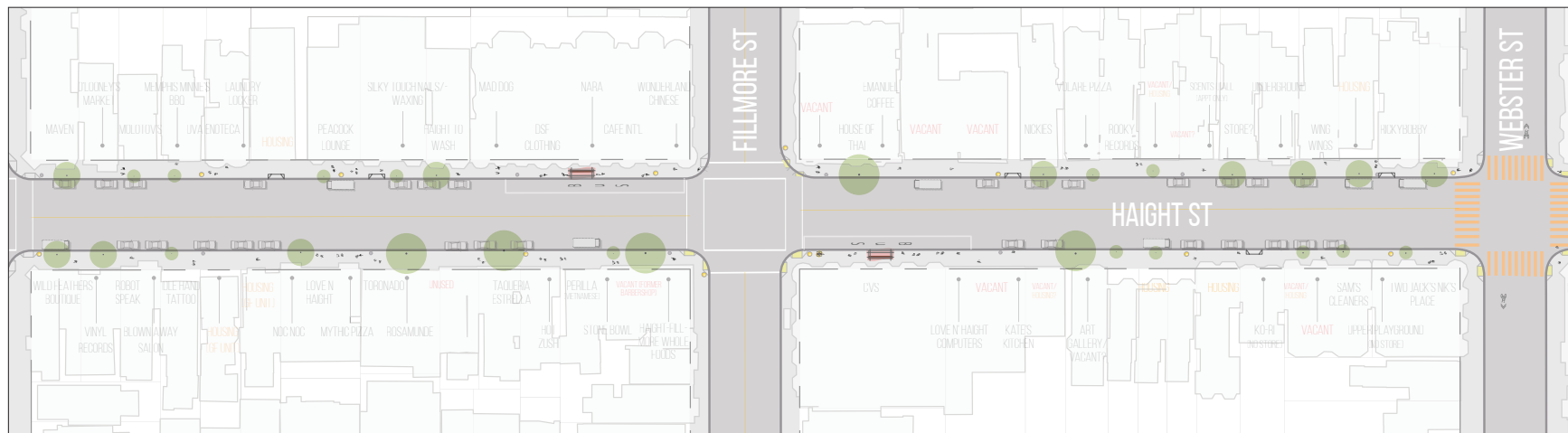


Photo credit: SF Planning Dept.



Photo credit: SF Planning Dept.

HAIGHT STREET, EXISTING, STEINER TO WEBSTER

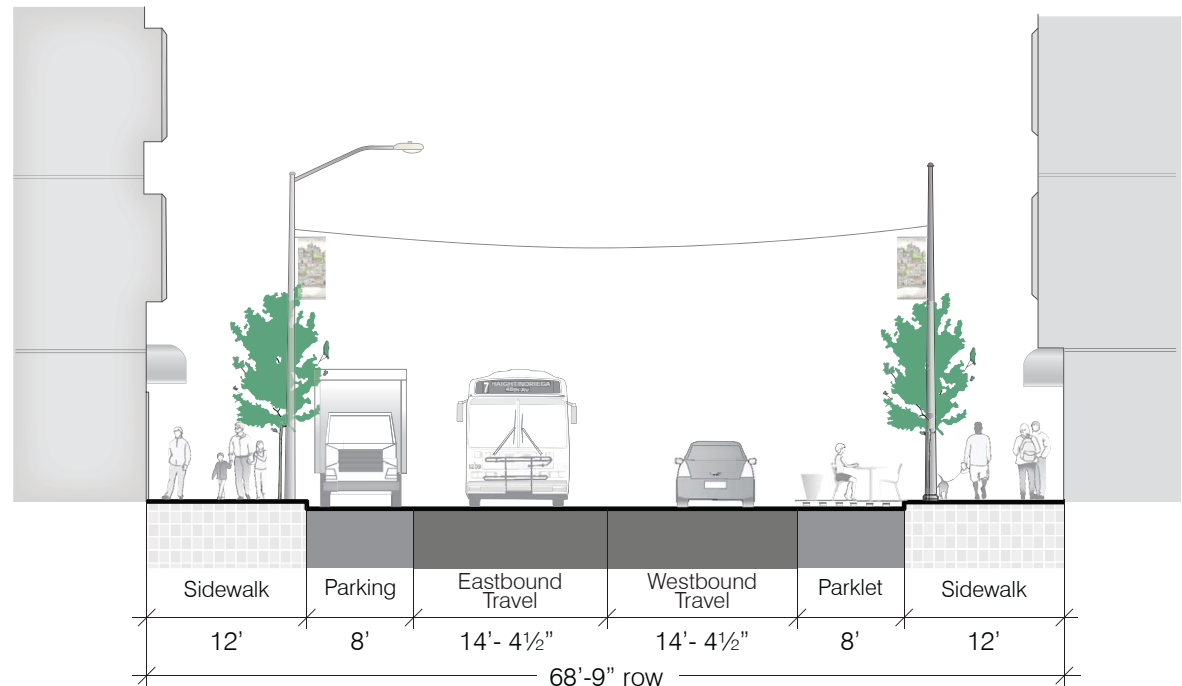


HAIGHT STREET: SHORT-TERM

Short-term improvements for Haight Street are under construction as of the writing of this plan document. Short term improvements include Muni Forward transit and pedestrian bulbout improvements with a Walkstop at Fillmore Street as well as curb ramp and crosswalk upgrades. Additional improvements to the Haight and Pierce intersection are coming via SFMTA and SFPUC's Wiggle Neighborhood Green Corridor project.

Other opportunities to improve the commercial core and the quality and aesthetics of the street environment include public art, infill of street trees, tree grate replacement, and pedestrian-scale lighting. After construction of the streetscape is complete, interested merchants can apply with the City to activate public space via new parklets.

HAIGHT STREET, SHORT-TERM SECTION, PIERCE TO WEBSTER



HAIGHT STREET SHORT-TERM IMPROVEMENTS



Image credit: SFMTA

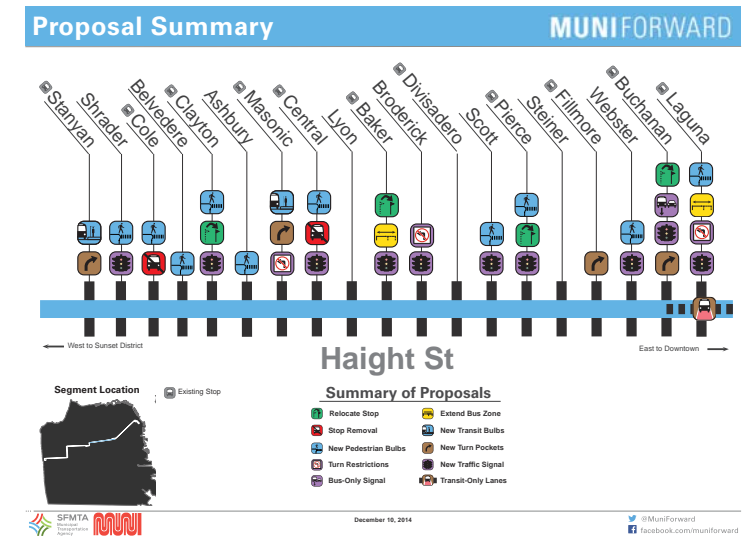
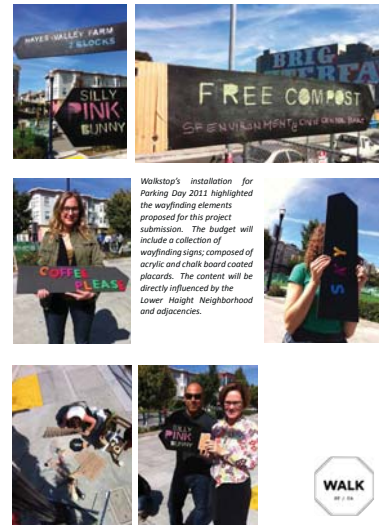


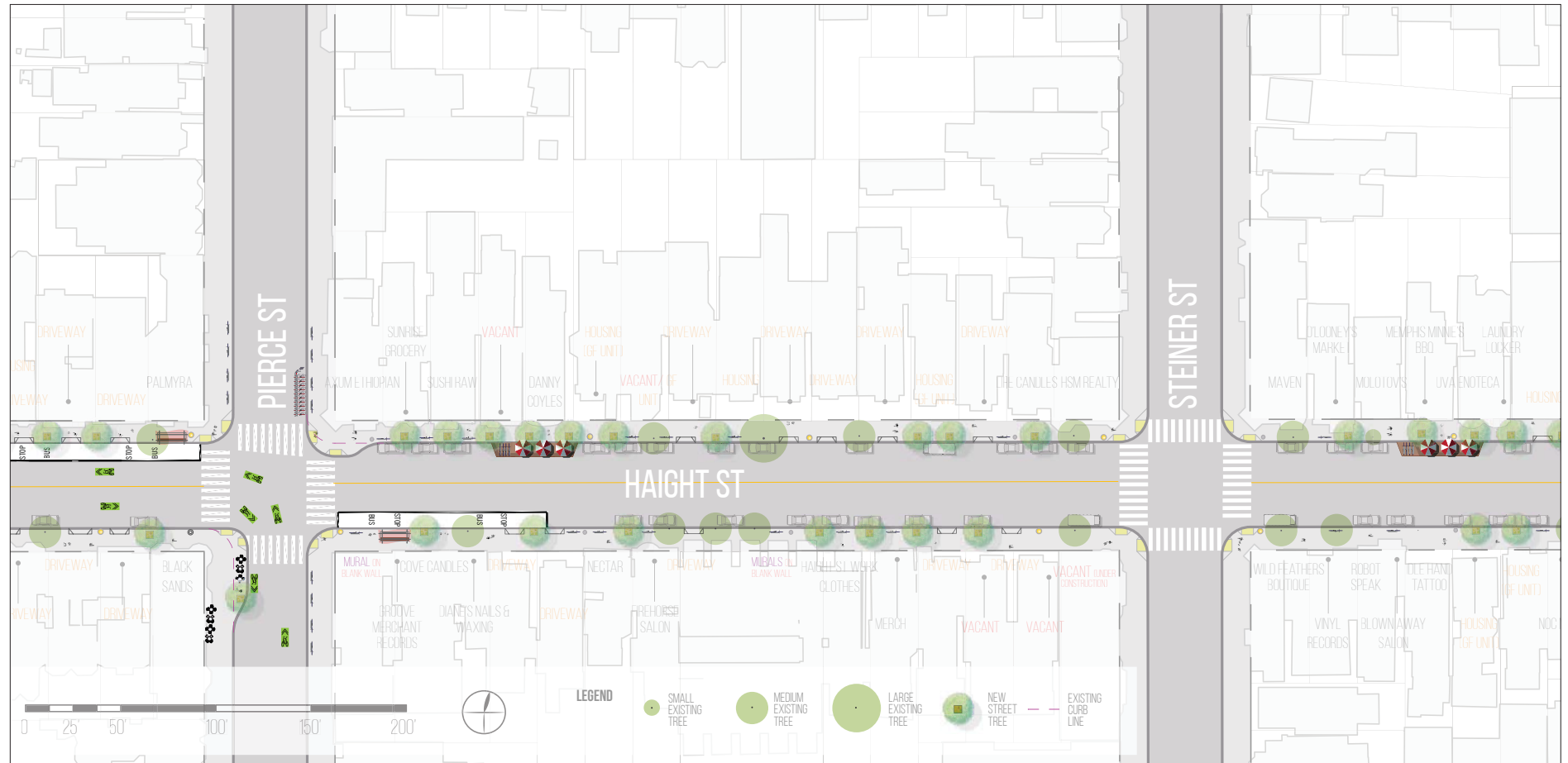
Image credit: SFMTA



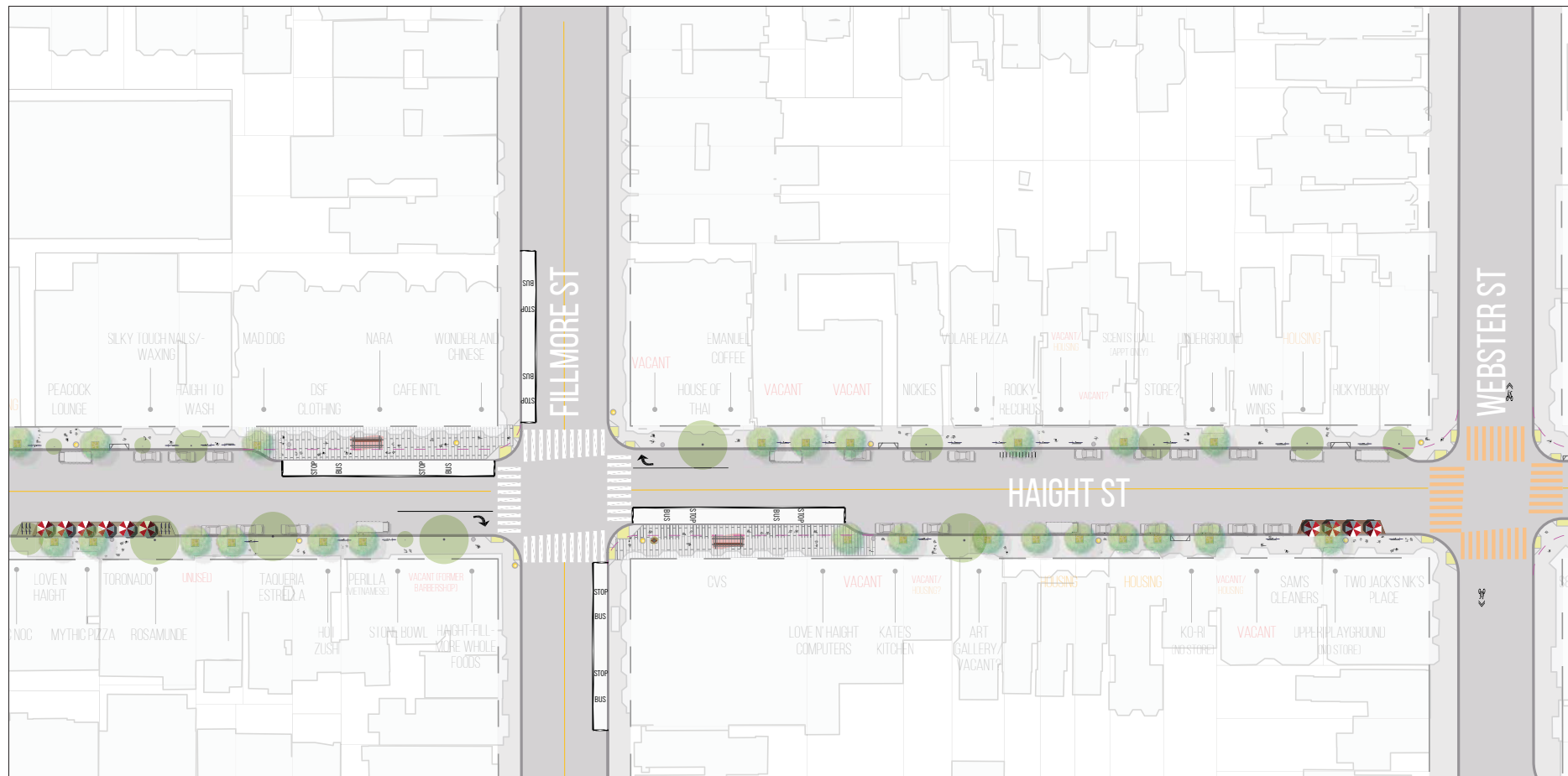
Image credit: SF Walkstop Project



HAIGHT STREET, SHORT-TERM, PIERCE TO STEINER



HAIGHT STREET, SHORT-TERM, STEINER TO WEBSTER



HAIGHT STREET: LONG-TERM

A bold long-term design is proposed for the Haight Street commercial core that would help enliven the heart of the neighborhood. Taking inspiration and feedback from the community on the need for an innovative streetscape design for the commercial core, the proposed design, new to San Francisco, balances the needs for people, vehicles, loading, and transit. The design is focused on two key blocks of Haight Street, between Steiner and Webster Streets, with a central focus on the heart of the Lower Haight at Haight and Fillmore Streets.

The key to the design is the flexibility it allows along the outer curb edge. A special 8' "flex zone" can accommodate space for public life such as cafe tables, public art, or seating; or parking or loading, as needed, and depending on the context and desire of the neighborhood. The benefit of this flex zone space is how easily it can be adjusted based on the changing needs of the neighborhood and merchants. No platform would need to be built for public space, as with a parklet. Use of the flex space could change often, even several times a day, if desired. Design, Implementation and management of these flex zones would need to be further fleshed out.

The proposed long-term design for Haight Street between Steiner and Webster, shown in the section above, includes:

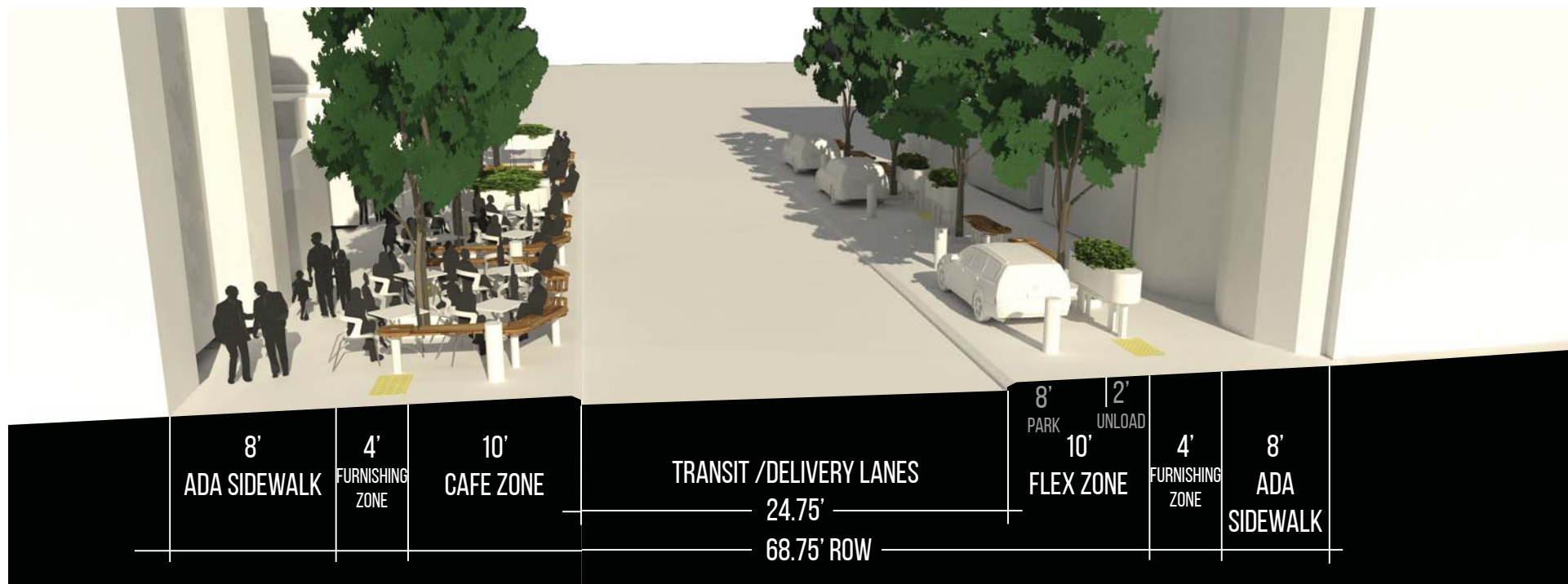
- A 24'9" central travelway for buses and vehicles with special paving to create a visually unifying space for users.
- 10' flex zones to function as public space or parking/loading
- 4' furnishing zones for street trees, seating, bike racks, pedestrian street lights and Muni poles, as well as gates or barriers for the flex zones
- 8' clear sidewalks for pedestrians

The proposed concept plan detailed on the next pages, would also include a number of important design details to unify the design, strengthen the neighborhood's identity, and offer the opportunity for individual artistic enhancement. These would include:

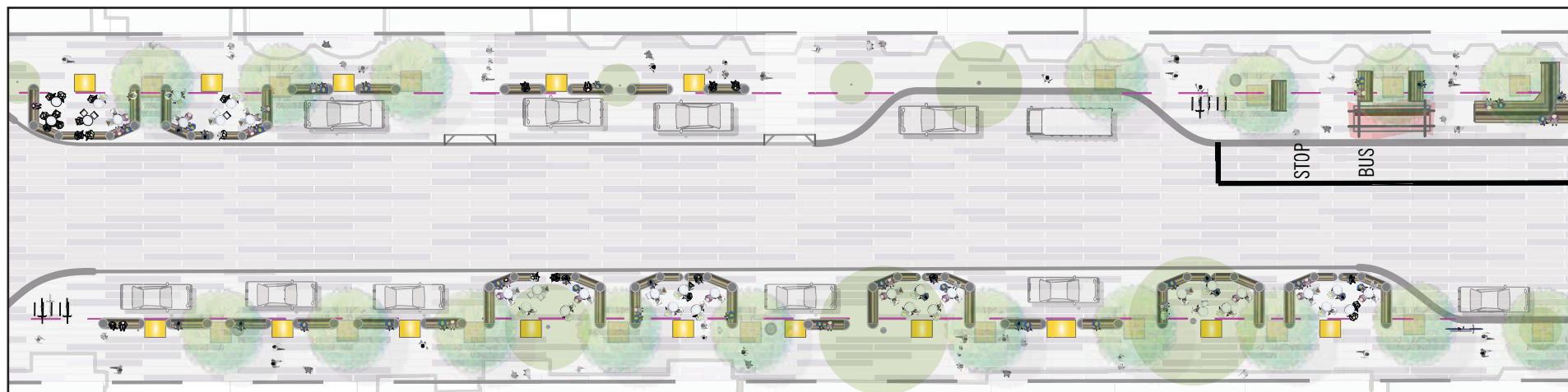
- Special paving from building face to building face to create a pedestrian "single-surface" feel for the entire street
 - New street trees and greening
 - Public art
 - New pedestrian streetlights
- The design is based on international designs seen in London, Vancouver

and elsewhere. Currently, the flex zones would not be maintained by Public Works; therefore, the liability and maintenance for them would need to be assumed by a strong Central Business District (CBD) of Lower Haight Merchants, or by a Green Benefit District (GBD) of merchants and neighbors or other merchant/neighbor association with sustaining funding and strong management.

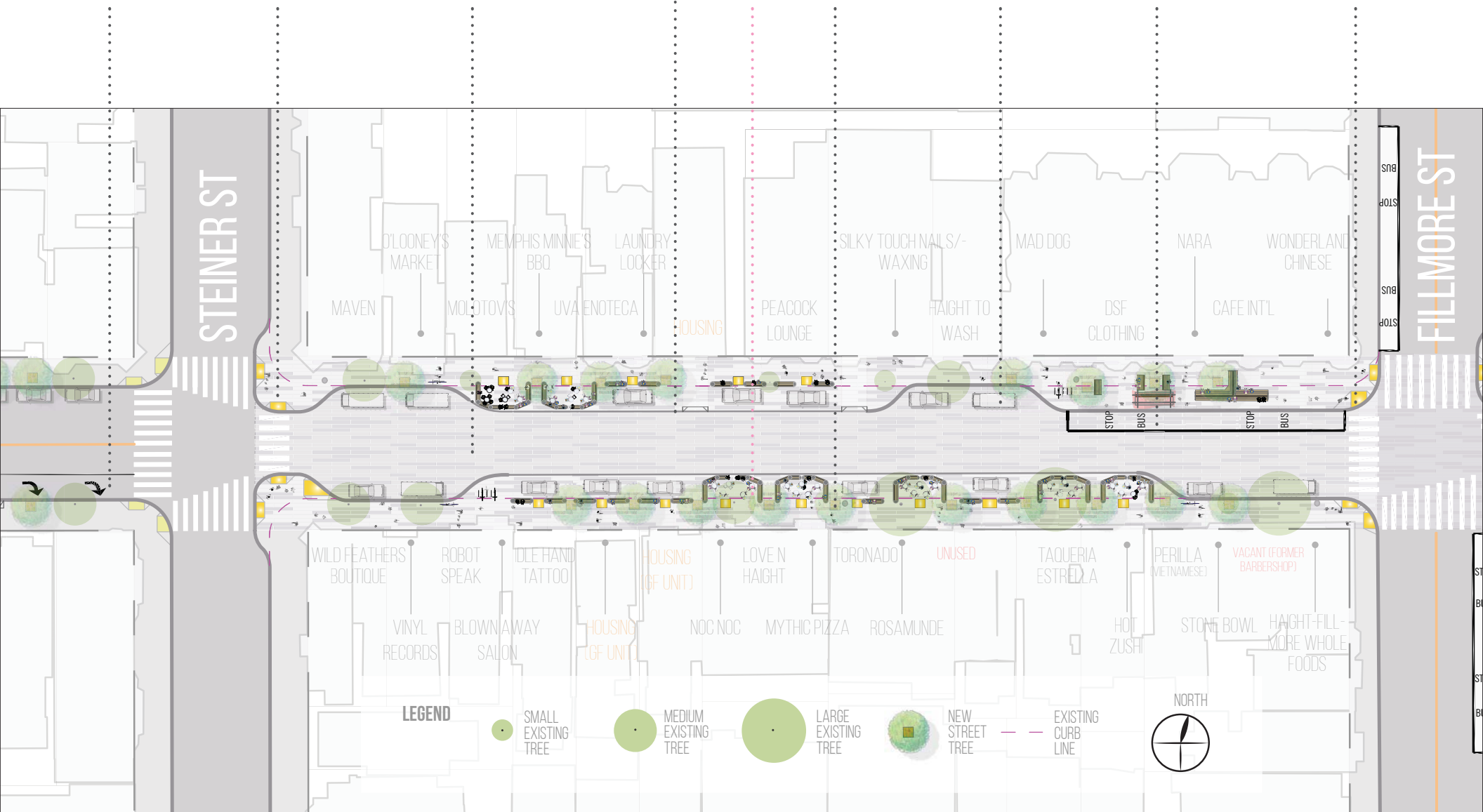
HAIGHT STREET, LONG-TERM DESIGN CONCEPT, SECTION VIEW



HAIGHT STREET, LONG-TERM DESIGN CONCEPT, DETAIL VIEW



HAIGHT STREET, LONG-TERM, STEINER TO FILLMORE

Potential
access
restrictionsGateway
entry
bulboutsSpecial paving
across full
width of streetBevelled
curb**Flexible
sidewalk
zones**Infill
street
treesCommercial
and ADA
loading zonesRelocated &
enhanced bus
stopsPedestrian
corner bulbouts

HAIGHT STREET, LONG-TERM, FILLMORE TO WEBSTER

Special intersection paving and crosswalks

Extended Muni bus bulbouts at Fillmore, with Walkstop

Special paving across full width of street

Flexible sidewalk zones

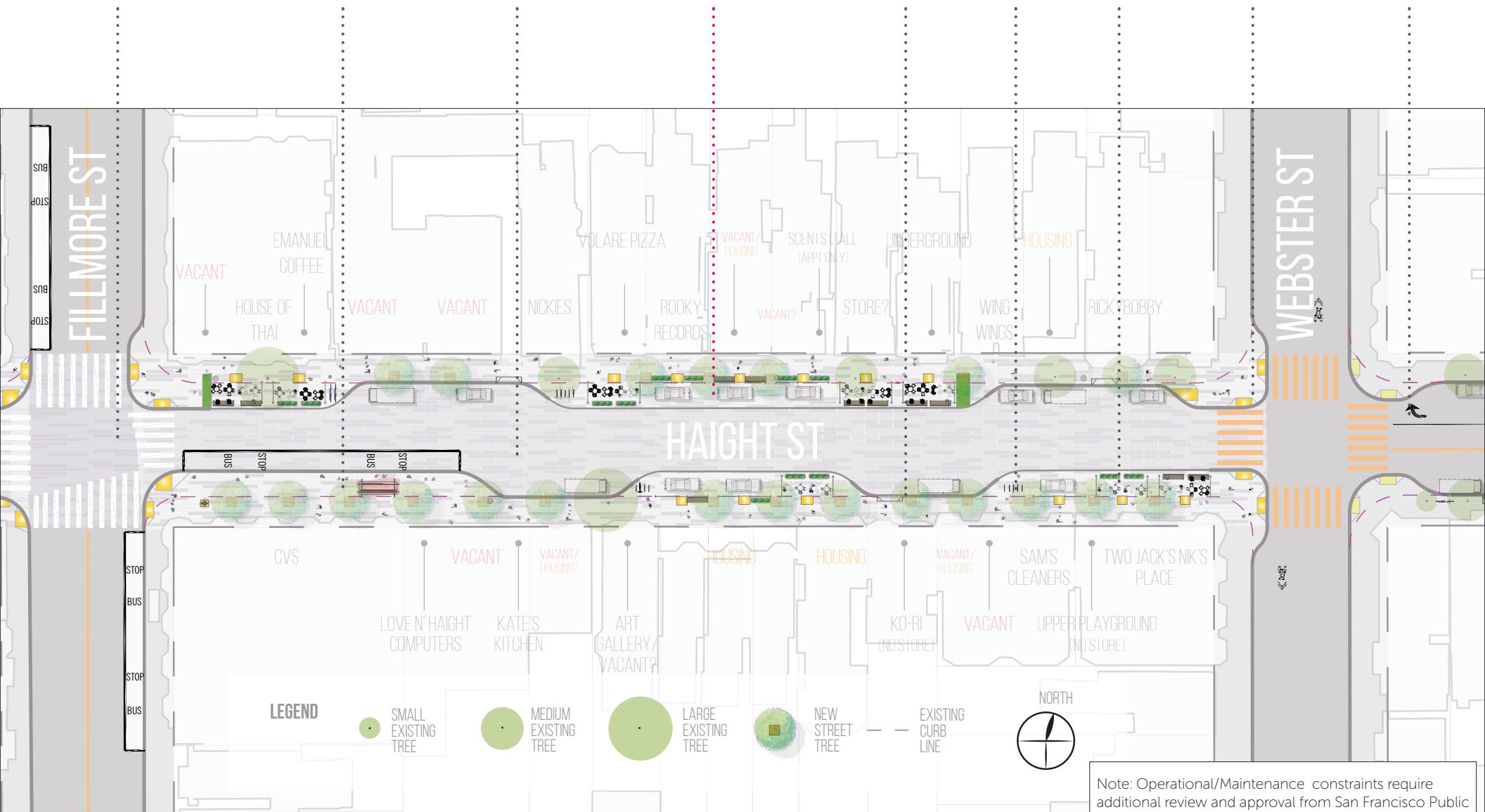
Infill street trees

New bike racks

Corner gathering spaces (identified by neighborhood)

Gateway entry bulbouts

Potential access restrictions



Note: Operational/Maintenance constraints require additional review and approval from San Francisco Public Works, San Francisco Public Utilities Commission and San Francisco Municipal Transportation Agency.

HOW WOULD THESE FLEX ZONES WORK?

1

A **beveled or mountable curb** would allow a vehicle to park in the flex zone when a bench or gate was “closed” The flex zone space would be clearly demarcated and physically separated from the pedestrian-only sidewalk to clearly differentiate these spaces for people with disabilities. Narrow openings with tactile paving or other detectable warning would allow safe passage between these two spaces.



DETAIL VIEW | ALL FLEX GATES/BENCHES “CLOSED”

2

Swinging gates, or benches and planters (as shown here) would enable flexible and easy transitioning of flex zone use, which could accommodate vehicle parking, cafe seating, outdoor retail kiosks, planters, bike parking, or other uses. Gates or benches or other elements could be **customized with creative artistic elements** to highlight the Lower Haight’s unique creative identity



DETAIL VIEW | ALL FLEX GATES/BENCHES “OPEN”

3

The **opening and closing of gates** could be done at certain times of day or days of the week, or customized to the individual fronting use; however it worked, it would need to be coordinated by a BID or merchant/neighbor association, with close review of appropriate City agencies.



DETAIL VIEW | ALL FLEX GATES/BENCHES “OPEN”

SECTION VIEW | ALL FLEX GATES/BENCHES "CLOSED"

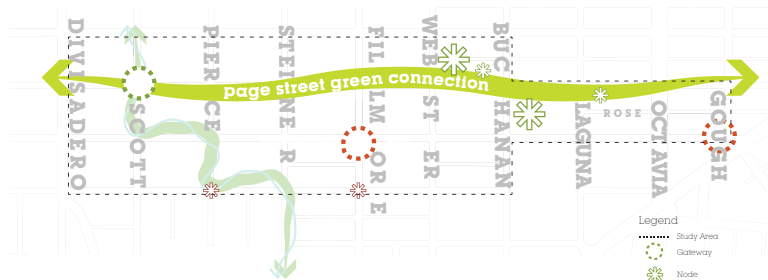


SECTION VIEW | ALL FLEX GATES/BENCHES "OPEN"



PAGE STREET TODAY





PAGE STREET: EXISTING DESIGN

Like Haight Street, Page Street through the Lower Haight was identified early on as a key design focus area from the community. This neighborhood street is an important neighborhood corridor and deserved attention in its role as a Green Connection.

Today Page Street resembles most other neighborhood residential streets in the Lower Haight. It has wide travel lanes for shared vehicle and bicycle travel, with parking lanes on both sides, and wide, 15' sidewalks for pedestrian travel. While there is decent street tree cover and some sidewalk gardens, many blocks suffer from a dearth of trees and greening. The presence of driveways has a significant impact on this; blocks with many driveways have few trees, and vice versa. Overall, considerable room for improvement in greening exists.

PAGE STREET, EXISTING SECTION

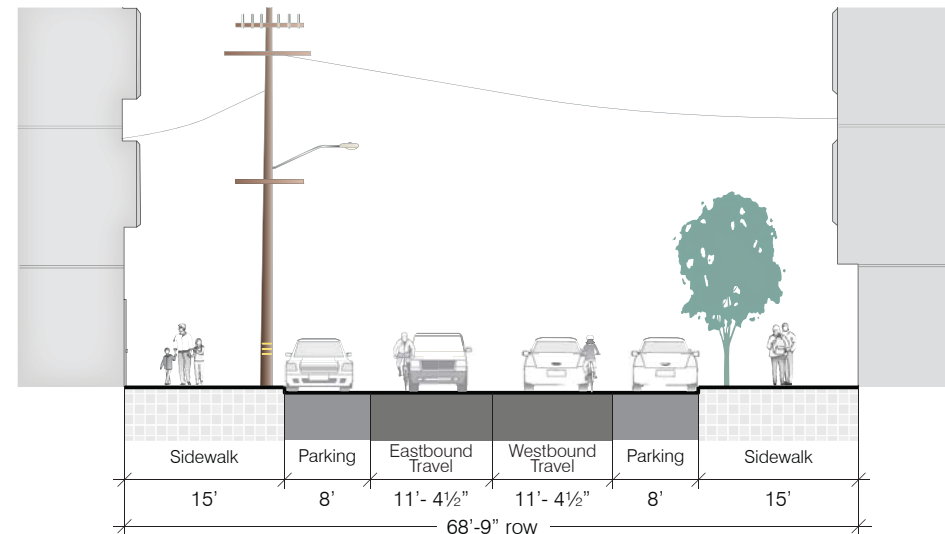


Photo credit: SF Planning. Existing Condition



Photo credit: SF Planning Existing Condition

WHAT IS A GREEN CONNECTION?

Green Connections are special streets and paths that connect people and wildlife to parks and open spaces. These streets provide opportunities for greening and landscaping; enhancing wildlife habitat; managing stormwater; and calming traffic.

In many instances, individual design solutions can address multiple project goals. For example, traffic-calming measures like curb extensions, medians or traffic circles may also provide opportunities for habitat creation, bio-retention planters (rain gardens) and/or programming opportunities.



Example vision of a **Green Connection**

WHAT MAKES THESE STREETS SPECIAL?

Green Connections are envisioned to include a higher level of public realm interventions than standard city traffic calming, pedestrian and bicycle projects.

Green Connections are intended to be routes that people will go out of their way to travel on because they are green, quiet, calm, and safe for walking and biking.

These streets also offer educational opportunities for urban ecology and stormwater management.

Key goals for Green Connections include:

Public Health

Increase active transportation to parks



Prioritize walking and biking



Calm traffic



Focus on conflict points



Celebrate park edges

Sustainability

Enhance urban ecology



Emphasize greening and street trees



Increase planted areas in the ROW



Include green infrastructure to manage stormwater



Design for site hydrology



Avoid habitat sinks



Consider habitat patch scale and density



Create habitat structure



Cultivate eco-literacy



Incorporate target species and habitat



Understand target species' life-cycle

Livability

Support neighborhood stewardship and placemaking



Engage communities in design and stewardship



Design for community gathering spaces



Program spaces to encourage stewardship and placemaking



Create wayfinding and signage

HOW DOES PAGE STREET FIT INTO THE CITYWIDE GREEN CONNECTIONS NETWORK?

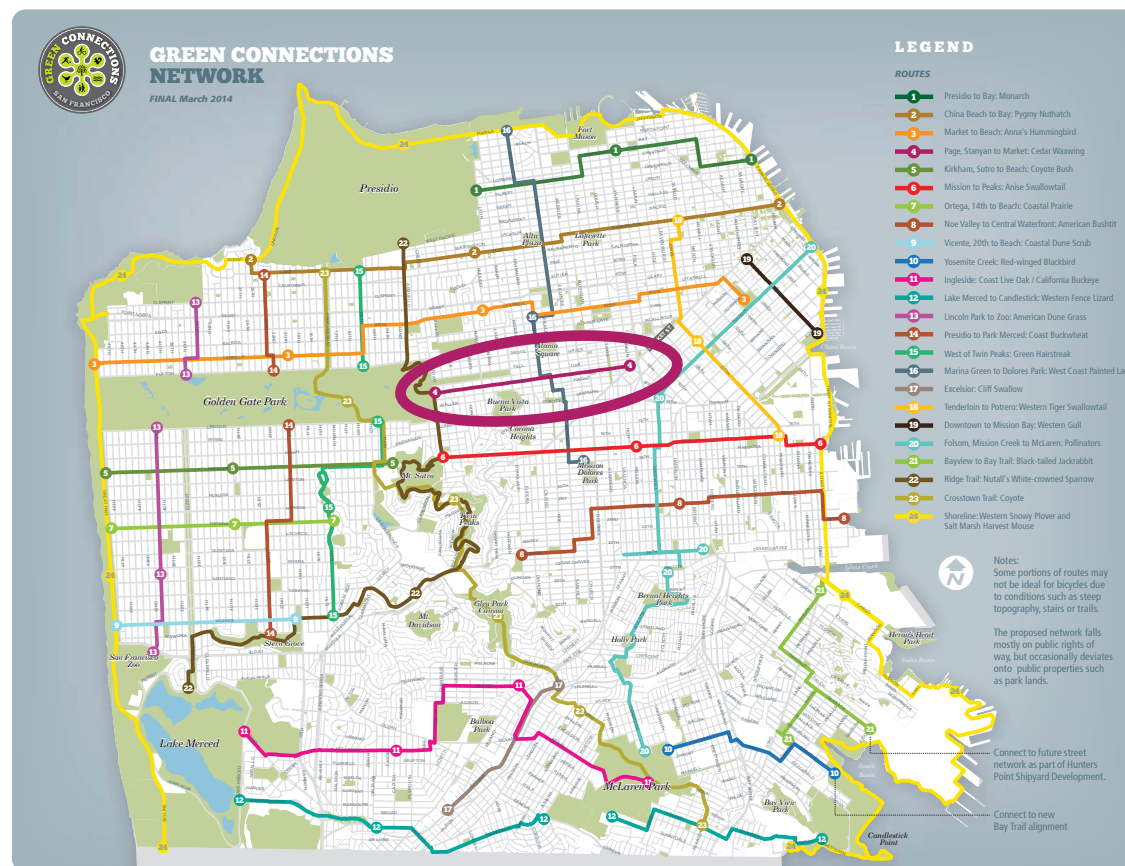
Page Street today is a critical east-west connection through the heart of central San Francisco and the Lower Haight. The entire length of Page Street, from Golden Gate Park at Stanyan, to Market Street, is designated as Green Connections Route #4.

WHAT OTHER ROLES DOES PAGE STREET PLAY?

Page Street is also on San Francisco's bicycle network, and is an important neighborhood residential connector for bicycles through the Lower and Upper Haight neighborhoods. Page Street is particularly important through the Lower Haight as a downhill commuter route for bicyclists from western neighborhoods to access Market Street and from there reach downtown.

Page Street does not carry transit, but is a local vehicular access road for residents.

GREEN CONNECTIONS CITYWIDE NETWORK MAP



DO ALL GREEN CONNECTIONS LOOK AND FEEL THE SAME?

Not at all! Segments along the network will have a different look and feel. The level of intervention will govern what is done to each segment of each route, and will vary based on existing conditions, coordination opportunities, available funding and community interest.

There are 3 specific levels of intervention that could be most appropriate for any segment, including:

- Light Intervention, or “light touch.”
- Moderate Intervention
- Showcase Intervention

These intervention levels are highlighted below, and a map at bottom shows the desired level of intervention for each block of Page Street in the Plan Area. This was determined by and based on numerous design drivers detailed on the next page.



Light Intervention

- » Would include signage and wayfinding, and lower cost improvements like bicycle stencils and street murals.
- » May feature basic greening like sidewalk landscaping gardens and filling in gaps in street trees.
- » May feature basic traffic calming measures.

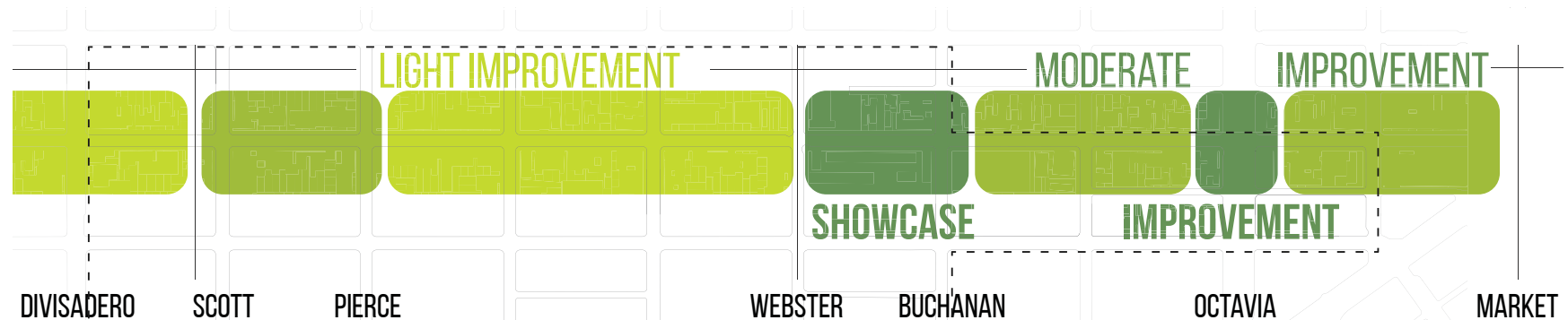
Moderate Intervention

- » Would include the amenities of a light intervention
- » May include minor sidewalk enhancements, curb extensions and stormwater infrastructure, or intersection treatments to calm traffic.
- » More clearly embody the goals of Green Connections, will involve a greater level of community engagement, and may involve a more holistic streetscape design.

Showcase Intervention

- » Most successfully achieves project goals.
- » May feature reductions in traffic volumes and dedication of space for habitat or community gathering.
- » Should be highest-quality active transportation routes safe for all users of all ages, and should serve as demonstration projects to showcase the transformational qualities of Green Connections.

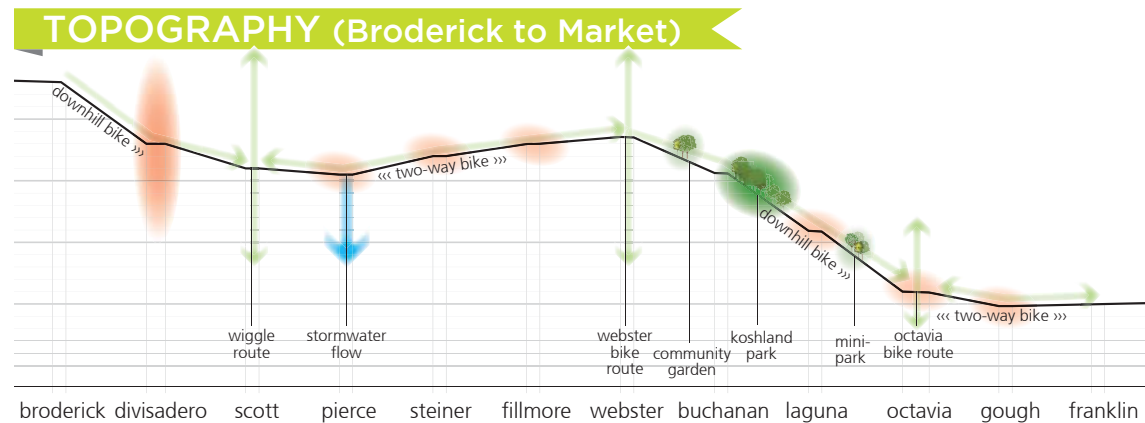
PAGE STREET GREEN CONNECTIONS INTERVENTION LEVEL DIAGRAM



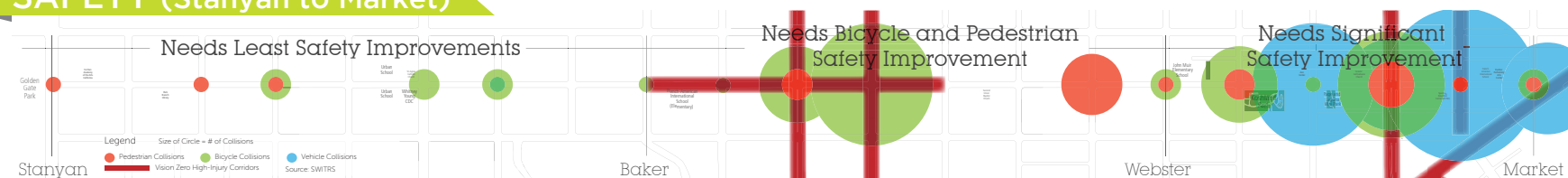
PAGE ST: KEY DESIGN DRIVERS

There were several key design drivers guiding the redesign of Page Street. First and foremost, of course, was its special role as a Green Connection. Other key design drivers included:

- Hills and Topography
- Safety
- Driveways
- Land Use, including Parks and Schools



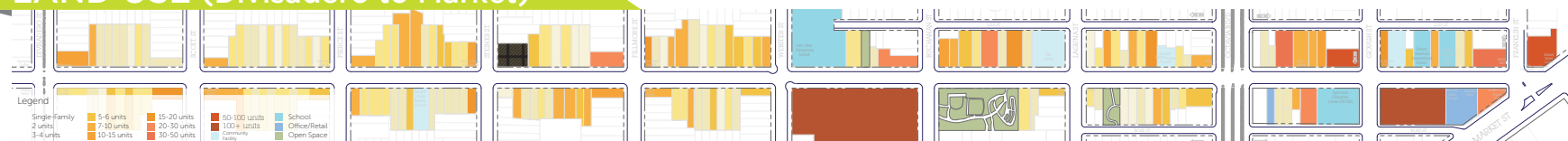
SAFETY (Stanyan to Market)



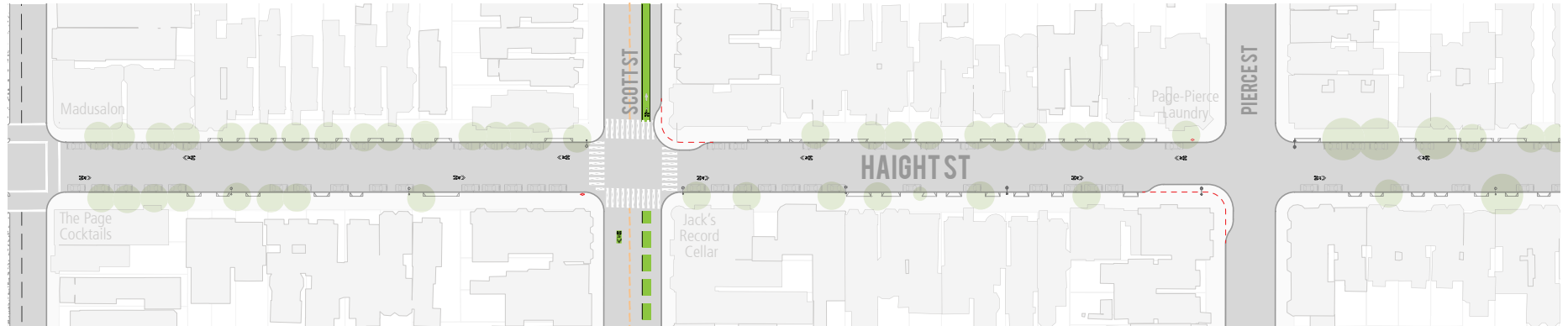
DRIVEWAYS (Divisadero to Market)



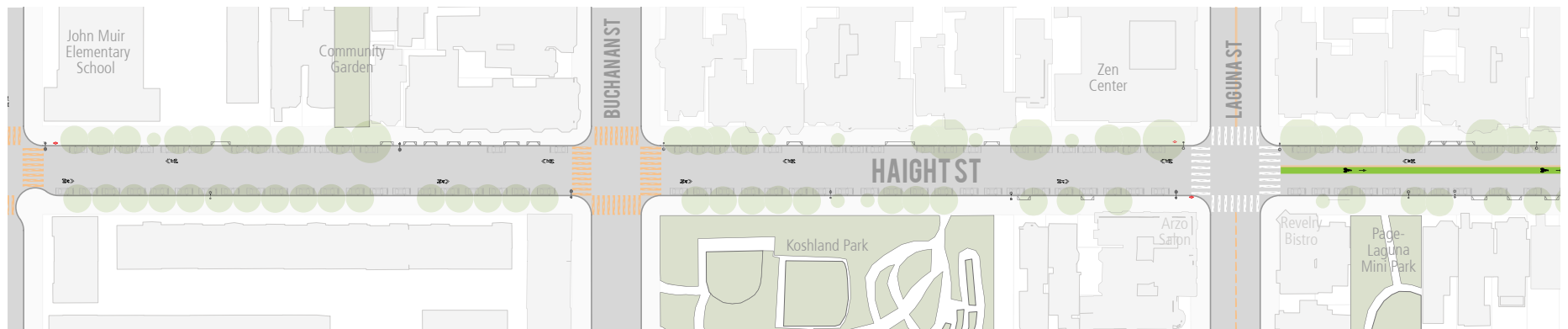
LAND USE (Divisadero to Market)



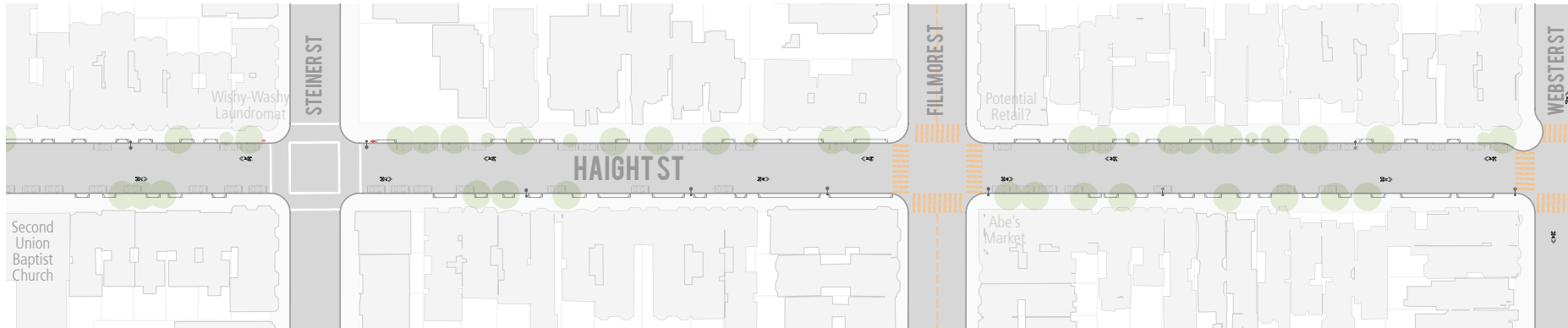
PAGE STREET, EXISTING PLAN VIEW, DIVISADERO TO PIERCE



PAGE STREET, EXISTING PLAN VIEW, WEBSTER TO LAGUNA

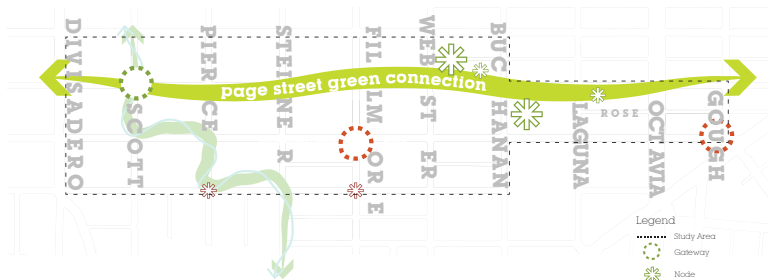


PAGE STREET, EXISTING PLAN VIEW, PIERCE TO WEBSTER



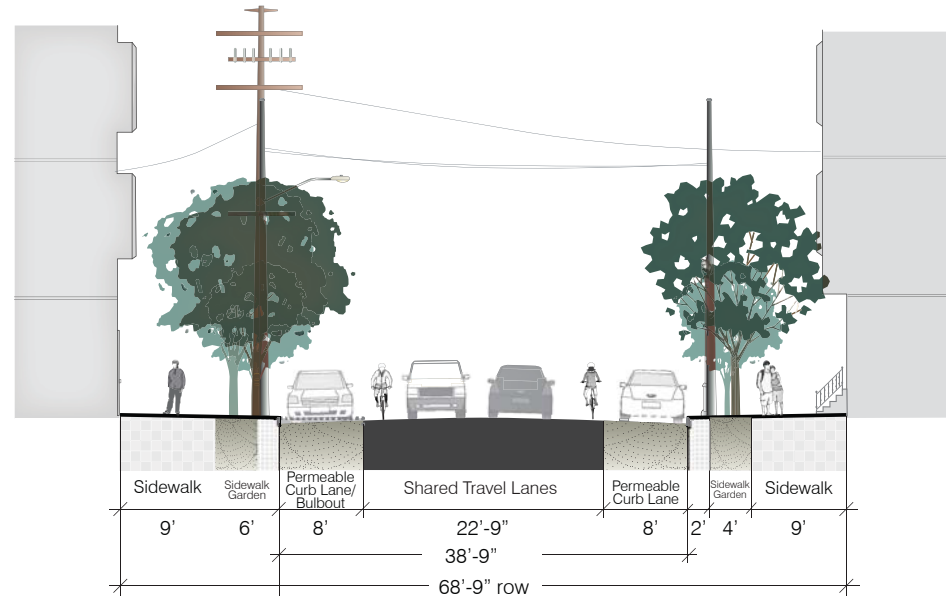
PAGE STREET, EXISTING PLAN VIEW, LAGUNA TO MARKET





The proposed design for Page Street is guided by the community's desire for this street to be the green, calm, and safe neighborhood connector it can be. A significant increase in the amount of greening is priority number one. New sidewalk gardens can be led by residents, while the City and non-profit partners can be responsible for new street trees. Besides greening, a number of different design elements are included in the proposed design. They are shown and detailed on the following page, and on the proposed plans on pages 78-79. Design elements include:

PAGE STREET, PROPOSED SECTION



- **New traffic calming and management solutions**, to reduce cut-through vehicle traffic, improve local access for people driving, calm the streets for everyone, and make the street safer and more pleasant for people walking and biking. Potential solutions include a traffic diverter at Webster, a raised intersection at Buchanan, a traffic diverter at Octavia, and access restrictions at Market/Franklin.
- **New permeable curb lanes and corner rain gardens**, to create green gathering space at corners, improve pedestrian safety, and manage stormwater
- **Infill street trees**, to fill in the green holes on the street and help minimize driveway impacts; (Street tree plantings can be coordinated through a partnership with Friends of the Urban Forest)

PAGE STREET: POTENTIAL DESIGN ELEMENTS

Trees and Gardens



Photo credit: SF Planning Dept.

Extended Center Bike Lane



Photo credit: SFBC

Partial Diverter



Photo credit: NACTO

Raised Intersection



Photo credit: NACTO

Median Divider



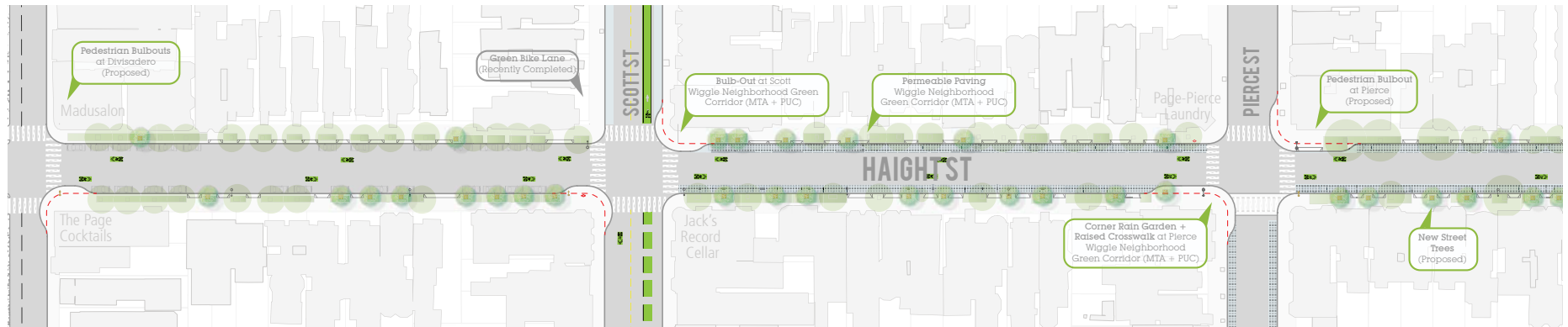
Photo credit: SF Planning Dept.

Rain Gardens

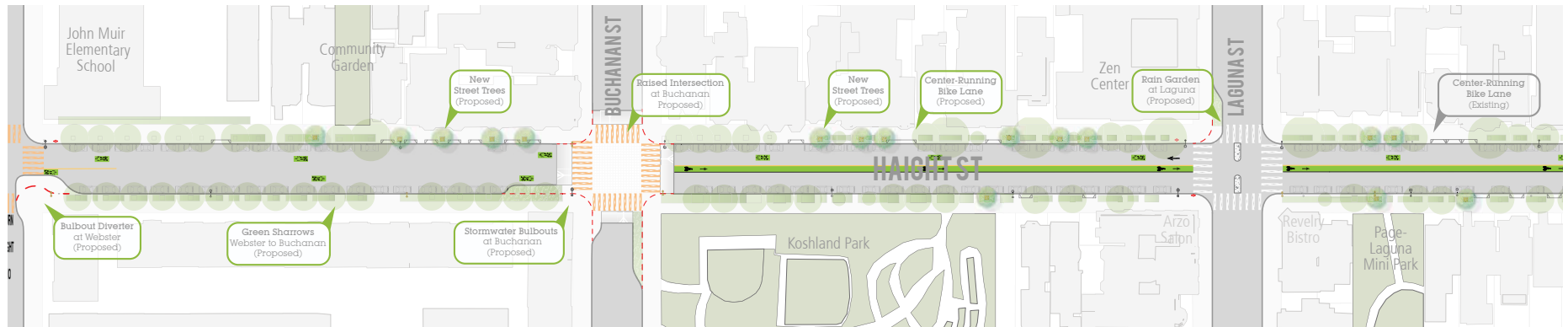


Photo credit: Portland Planning Dept.

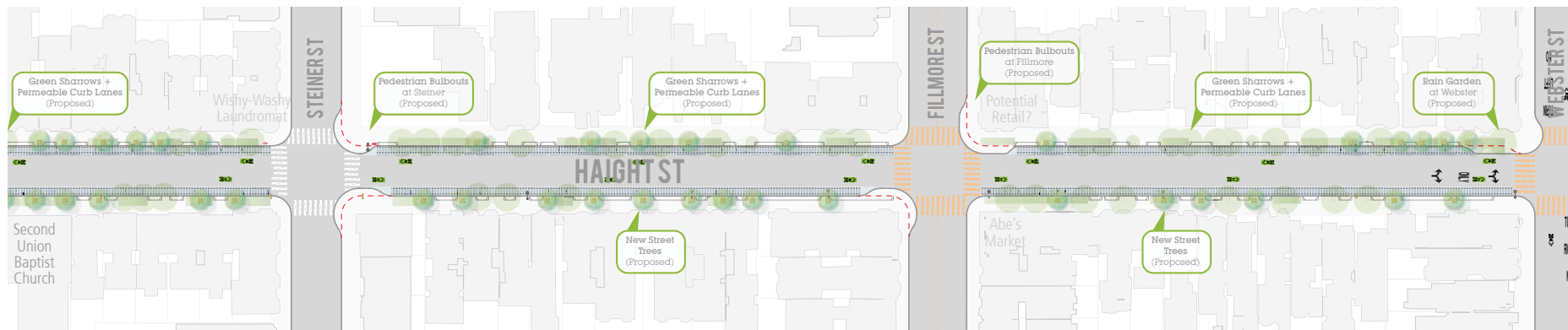
PAGE STREET, PROPOSED PLAN VIEW, DIVISADERO TO PIERCE



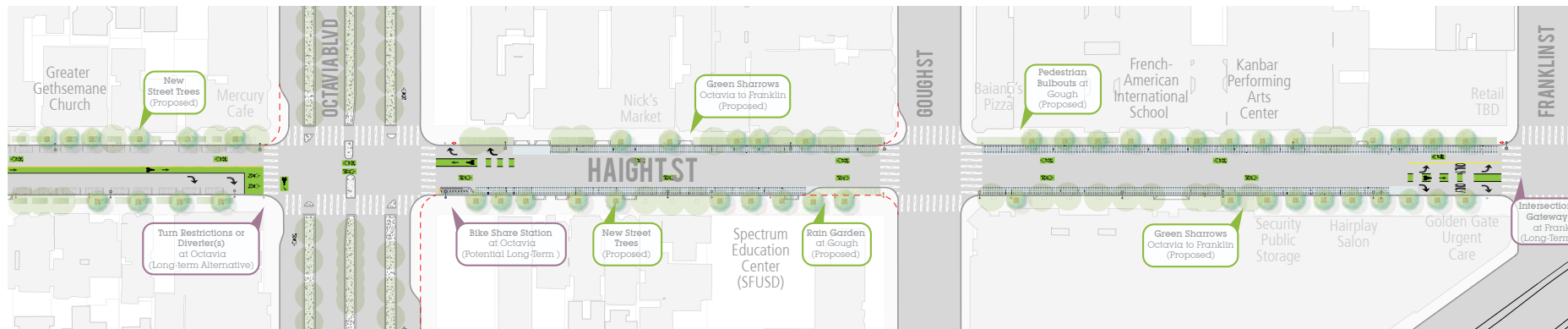
PAGE STREET, PROPOSED PLAN VIEW, WEBSTER TO LAGUNA



PAGE STREET, PROPOSED PLAN VIEW, PIERCE TO WEBSTER



PAGE STREET, PROPOSED PLAN VIEW, LAGUNA TO MARKET





Chapter 5

IMPLEMENTATION

The Lower Haight Public Realm Plan has identified both short and long term projects to pursue, including an ambitious future vision of the Lower Haight Commercial Core. The next steps forward are to identify funding and the appropriate city and civic partners to advance and implement plan concepts. Future discussion for these public realm interventions, will need to focus on maintenance, activation and prioritization of amenities.

At the date of the publication, there has not been a funding source identified for these projects. Therefore, the City and Civic partners are encouraged to explore opportunities as they come along. Public Realm improvements are built and funded mainly by two different mechanisms, public or private sponsorship. A brief outline of these mechanisms is described in this chapter.

CITY SPONSORED PROJECT

Some public realm projects may be fully or partially funded by the City. City-sponsored street improvement projects associated with repaving and utility replacement may also include a streetscape component and are often completed using grant or bond funds. Other City programs, such as the GroundPlay program, allow for private sponsors to partner and implement short-term, temporary installations that can improve the public realm and test new design and programmatic ideas.

PRIVATE SPONSOR

Public realm improvements, including Parklets, public art, sidewalk gardens and other interventions, can be proposed, implemented, maintained, stewarded, and activated with programming by a private sponsor. Sponsors may include benefit and improvement districts (additional information provided in next section), community-based organizations, schools, residents, property owners, business owners, and merchants. Private activation of public spaces can sometimes be the best way to assure consistent activation and programming of public spaces, and the City strongly encourages these public-

private partnerships to benefit the public in the long-term with well-activated and stewarded public spaces. The City offers many programs for private sponsors to design and implement improvements to the public realm.

COMMUNITY BENEFIT DISTRICT

Community Benefit Districts (CBDs) strive to improve the overall quality of life in targeted commercial districts and mixed-use neighborhoods through a partnership between the City and local communities. In California, CBDs are also known as Business Improvement Districts. Once an area has voted to establish a CBD, local property owners are levied a special assessment to fund improvements to their neighborhood. The funds are administered by a non-profit organization established by the neighborhood.

Currently San Francisco has 14 CBDs that provide services ranging from sweeping, graffiti abatements, landscaping and/or power washing the sidewalks. The Office of Economic and Workforce Development (OEWD) has a Community Benefit District (CBD) program that supports the creation

and maintenance of special assessment areas in commercial districts around the City that are dedicated to improving quality of life and economic conditions.

Throughout this planning process, many individuals inquired about a commercial corridor economic assessment, similar to the Castro and Upper Market Retail Strategy. While public funding for this strategy has not been identified, the Lower Haight Merchant and Neighborhood Association, or other similar stakeholder groups could work to privately raise funds for such a project.

CITY APPROVALS

Multiple City agencies have jurisdiction over elements of the streetscape, but for larger projects with non-standard elements such as the Haight Commercial Core, Long Term Strategy, will require close coordination with Public Works. For any non city standard element proposed, a sponsor (not the City) will be responsible for the long term maintenance and liability of these elements. Please see <http://www.sfbetterstreets.org/learn-the-process/permit-process/> for more information.

LOWER HAIGHT PUBLIC REALM PLAN PROJECTS

The table below summarizes the projects that have been identified through the development of this Plan.

PROJECT	ACTION	KEY AGENCY	TIMEFRAME	FUNDING SOURCE	FUNDED
STREETSCAPE AMENITIES	Infill new street trees, tree grate replacement, pedestrian scale lighting and public art. This was identified in the Haight Commercial Core, Short Term Strategy.	MTA, DPW, PUC, Friends of the Urban Forest, Neighbors & merchants,	Near-term (1–5 years)	TBD	No
MAINTENANCE PLAN	Maintain new streetscape amenities, including street trees, tree grates, pedestrian lighting and sidewalk cleanliness	DPW, Neighborhood organizations & merchant groups,	Near-term (1–5 years)	Public Works, Neighborhood organizations	No
PARKLETS	Interested merchants should pursue a parklet applications	Merchants & Property Owners	Near-term (1–5 years)	Private funds	No
BIKE CORRALS	Interested merchants should pursue bike corral applications	Merchants & Property Owners	Near-term (1–5 years)	SFMTA	No
OCCASIONAL PLANNED STREET CLOSURES FOR NEIGHBORHOOD EVENTS	Neighborhood groups should seek street closure permit for neighborhood events.	Neighborhood organizations & merchant groups	Near-term (1–5 years)	Private funds	No
HAIGHT COMMERCIAL CORE, LONG TERM STRATEGY	Plan for long-term improvements to Haight from Steiner to Webster, which includes flex zones. Cost estimates range from \$4 to 4.5 Million with 5% escalation factor for each year between 2017 and the time project might be implemented.	Public Works, MTA,	Long-term (10 - 20 years)	TBD	No
PAGE STREET GREEN CONNECTION	Coordinate short-term improvements to Page Street from Webster to Market Street with SFMTA's Page Street Neighborway project. Initiate long-term planning process for remainder of corridor west of Webster Street.	Public Works, MTA, Planning	Near-term to long-term (1 - 20 years)	TBD	Partially

AGENCY KEY

SFMTA: San Francisco Municipal Transportation Agency
 SFCTA: San Francisco County Transportation Authority
 SFPUC: San Francisco Public Utilities Commission

Public Works: Department of Public Works

