



SAN FRANCISCO PLANNING DEPARTMENT

Permit to Alter Case Report

Hearing Date: June 20, 2012
Filing Date: March 14, 2012
Case No.: 2012.0136H
Project Address: 220 GEARY STREET (aka. 333 POST STREET)
Category: Category I (Significant)
Conservation District: Kearny-Market-Mason-Sutter
Zoning: P (Public)
OS (Open Space) Height and Bulk District
Block/Lot: 0308/001
Applicant: John Funghi
SFMTA
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PROPERTY DESCRIPTION

Historically known as Union Square, the subject property is located at 220 Geary Street (aka. 333 Post Street) and is located on Assessor's Block 0308; Lot 001 on the City block bounded by Geary, Powell, Post and Stockton Streets. It is a Category I (Significant) Property located within the Kearny-Market-Mason-Sutter Conservation District and is within a P (Public) Zoning District with an OS (Open Space) Height and Bulk limit.

220 Geary Street is a 2.6 acre public park in downtown San Francisco, which has historically been the geographic center of the City's retail shopping district. The Square was donated to the City by Colonel John White Gray in 1850, and is thought to have received its name due to the use of the square for public meetings in support of the Union on the eve of the Civil War. The internal organization and landscaping of the square has changed many times, most notably around 1903 when the Dewey Memorial was installed at the center of the square, around 1941 when the underground garage was constructed as part of Timothy Pflueger's redesign of the park, and in 2002 when the park was redesigned by Fotheringham and Phillips.

Although the appearance of Union Square today is most closely associated with its 2002 redesign, the property retains its original boundaries and character as a designed park to convey its significance as an open space within the Kearny-Market-Mason-Sutter Conservation District (KMMS District).

PROJECT DESCRIPTION

The proposed project is to construct a new station entrance at the corner of Stockton and Geary Streets at the southeast corner of Union Square, as part of the Central Subway project. The station entrance will consist of a weather protected room containing a staircase, escalator, two elevators, and space for pedestrian circulation, covered by a roof terrace. The station entrance will be detailed as a landscape element that matches the existing retaining walls, steps, planters and landscaping of Union Square.

PROJECT BACKGROUND

The proposed project was reviewed and approved under the Central Subway Final Supplemental Environmental Impact Statement/Supplemental Environmental Impact Report (SEIS/SEIR) and was certified by the Planning Commission on August 7, 2008, and approved by the SFMTA Board on August 19, 2008. The SEIS/SEIR identified impacts resulting from project construction including noise, dust, vibration historic resource and transit/traffic operational impacts. The SEIS/SEIR identified a potential less-than-significant historic resource impact upon Union Square due to the construction of the station entrance in such a manner that would permanently alter the redesigned plaza and parking garage (completed in 2002). The mitigation measure to minimize this impact involves the use of design and architectural materials that are compatible with the surrounding structure and landscape, and review of the final design for the station by the Recreation and Parks Department.

OTHER ACTIONS REQUIRED

According to Section 4.105 of the San Francisco City Charter and Section 2A.53 of the Administrative Code, a General Plan Referral is required prior to passage by the Board of Supervisors of any proposed action pertaining to acquisition of property by the City or County of San Francisco, vacation of property owned by the City and County of San Francisco, or a change of use or title of such property, the Planning Commission must evaluate in writing its conformity with the General Plan. A General Plan Referral was completed for the proposed Central Subway project on March 6, 2012 and the Department determined that the Project is in conformance with the Objectives and Policies of the General Plan and meets the criteria set forth in Section 101.1 of the Planning Code.

The proposed work will also require Building Permit(s).

COMPLIANCE WITH THE PLANNING CODE PROVISIONS

The proposed project is in compliance with all other provisions of the Planning Code.

APPLICABLE PRESERVATION STANDARDS

ARTICLE 11

City Charter Section 4.135 gives the Historic Preservation Commission (HPC) authority to approve, disapprove, or modify Permit to Alter applications for designated Significant or Contributory properties. In appraising a proposal for a Permit to Alter, the Historic Preservation Commission should consider the factors of architectural style, design, arrangement, texture, materials, color, and other pertinent issues.

SECTION 1111.6 OF THE PLANNING CODE

Section 1111.6 of the Planning Code outlines the specific standards and requirements the Historic Preservation Commission shall use when evaluating Permits to Alter. These standards, in relevant part(s), are listed below:

- (a) The proposed alteration shall be consistent with and appropriate for the effectuation of the purposes of this Article 11.

The proposed project is consistent with Article 11.

- (b) For Significant Buildings/Properties - Categories I and II, and for Contributory Buildings - Categories III and IV, proposed alterations of structural elements and exterior features shall be consistent with the architectural character of the building, and shall comply with the following specific requirements:

- (1) The distinguishing original qualities or character of the building may not be damaged or destroyed. Any distinctive architectural feature which affects the overall appearance of the building shall not be removed or altered unless it is the only feasible means to protect the public safety.

The proposed project involves the insertion of a new transit station at the southeast corner of Union Square. This project is additive in nature and will require minimal disturbance of existing historic fabric, limited to the partial loss of terrace seating at the southeast corner of the square. Overall, the distinguishing qualities of Union Square and the KMMS District will be retained.

- (2) The integrity of distinctive stylistic features or examples of skilled craftsmanship that characterize a building shall be preserved.

As described above, the proposed project is additive in nature and will not result in the loss of distinctive stylistic features or examples of skilled craftsmanship that characterize the property.

- (4) Contemporary design of alterations is permitted, provided that such alterations do not destroy significant exterior architectural material and that such design is compatible with the size, scale, color, material and character of the building and its surroundings.

The proposed station entrance will result in the loss of a portion of the terrace seating at the southeast corner of Union Square. This feature dates to an alteration made in 2002 and the loss of a portion of this feature will not result in the destruction of significant historic architectural material. Overall, the proposed addition is compatible with the size, scale, color, materials and character of the existing retaining walls and structures in Union Square and is compatible with the overall design of the property.

- (6) In the case of Significant Buildings - Category I, any additions to height of the building (including addition of mechanical equipment) shall be limited to one story above the height of the existing roof, shall be compatible with the scale and character of the building, and shall in no event cover more than 75 percent of the roof area.

The proposed project involves the construction of a new one-story building at the southeast corner of Union Square. The proposed design is limited in scale and compatible with the scale and character of the property.

PUBLIC/NEIGHBORHOOD INPUT

The Department has received no public input on the project as of the date of this report.

ISSUES & OTHER CONSIDERATIONS

None.

STAFF ANALYSIS

Staff has determined that the proposed work will be in conformance with the requirements of Article 11 and the *Secretary of Interior's Standards for Rehabilitation*. Proposed work will not damage or destroy distinguishing original qualities or character of the KMMS District. The overall proposal includes the construction of a station concourse that extends south from Union Square below the street to Ellis Street (2 blocks). In order to insert the new station entrance into Union Square, a portion of the existing parking garage will be reconfigured to accommodate the station entrance and a portion of the station's concourse below. The only portion of the new construction that will be visible above grade is the station entrance building located at the southeast corner of Union Square. This analysis is limited to the above ground construction within Union Square.

Overview

The proposal consists of the construction of a new station entrance that draws upon the existing treatment, materials and finishes within Union Square in order to achieve compatibility with the existing character of the subject property and the KMMS District. The building is detailed in a manner intended to blend into Union Square such that the new construction appears as a landscape element similar to the existing retaining walls and planters located within the park. The purpose of this design approach is to reduce the visual impact of the new construction to keep visitors focused on the existing square.

Entrance enclosure

The enclosure for the station entrance is exposed on three sides: to the west, stepped terraces will be integrated into and match the finish and composition of existing terraced seating; to the east, retaining walls will match the finish of the existing walls enclosing the parking garage's ventilation and exit stair; to the south, glass elevators and the station entrance opening will be framed by concrete walls and parapets treated to match the color and finish of the existing retaining walls within the plaza.

The concrete framing along the Geary Street façade will extend as a parapet over the passenger entries. Vines will be planted in the wells at the base of the concrete walls and will eventually cover these walls. Metal cable grids will support the vines and be detailed to appear as architectural elements until the vines grow into place. These elements will be detailed to minimize bulk and massing and to maintain visibility into the square.

The glass facade of the station enclosure (visible along Geary Street) will offer an opportunity for public art.

Except for fire suppression systems and other functional components, the metalwork on the outside of the entry system will have a powder-coated finish to match that of the existing pavilions in Union Square. These elements will be detailed to match the existing fabric of Union Square to ensure compatibility with the property and the KMMS District.

To secure the station entrance during non-operational hours glazed bi-fold overhead doors will be incorporated into the Geary Street façade. During operating hours, these doors will be opened and serve a dual function as weather protection awnings. The doors will be closed at night to secure the station. The materials of the doors will be transparent and less utilitarian than traditional metal roll down gates.

The Department believes that the proposed entrance enclosure is clearly differentiated and articulated as a new design element within Union Square, but compatible with the scale and character of the property and the KMMS District. This compatibility is achieved through the use of non-reflective materials similar to those of the existing pavilions and through the design of the new station as a landscape/retaining wall element within Union Square such that it blends within the surrounding park landscape.

Roof Deck

Vertical clearance requirements for the station entrance will result in the roof elevation being slightly higher than the Square's intermediate plaza level. A translucent glass roof is proposed. The roof will have a thin profile and will provide daylight inside while creating an opportunity to decorate the roof terrace with public art.

A 42-inch-high guardrail will be required along the south and east edges of the roof deck to meet safety requirements. To reduce the bulk and mass of the guardrail, the top 12 inches will be detailed in glass and the guardrail will be stepped along the east wall to allow for maximum visibility into the park from the corner of Geary and Stockton Streets.

The enclosures for the two required elevators will protrude through the roof and above the guardrail. An operational safety clearance above the roof of the elevator cab requires the enclosures to be higher than the guardrail.

The Department believes that the proposed roof deck is clearly differentiated in materials, but compatible with the scale and character of the property and the KMMS District through the use of glazing that will not detract from the surrounding features.

Open Space

The construction of the station entrance will displace 1,200 square feet of existing terraced seating at the southeast corner of the park. The existing terraces will be reconfigured and detailed to match the materials and colors of the surrounding terraced seating. The roof of the station has been designed as a walk-on terrace, providing 950 square feet of open space. Public access to the roof deck will be via steps and a short ramp along the north edge, and via terraced seating to the west. These changes will result in a net loss of approximately 250 square feet of open space and will not affect the overall character of Union Square as an open space within the KMMS District.

Signage

The east wall of the station entrance will include identification signage (individual letters) to clearly yet subtly identify the location of the station entrance. This signage is consistent with the Departments *Draft Design Standards for Signage & Awnings in the KMMS Conservation District*.

Materials

The proposed materials include glass, pre-cast concrete, and powder coated and clear anodized metal. These materials are consistent with the materials of the existing pavilions in Union Square, and are minimally reflective such that they blend within the KMMS District. Cut sheets of the proposed materials are available in the hearing packets.

ENVIRONMENTAL REVIEW STATUS

The Planning Department has determined that the proposed project is exempt/excluded from environmental review; pursuant to CEQA Guideline Section 15301 (Class One-Maintenance and Repair of Existing facility) because the project is an alteration of an existing structure and meets the *Secretary of the Interior's Standards for Rehabilitation*.

PLANNING DEPARTMENT RECOMMENDATION

Planning Department staff recommends APPROVAL of the proposed project as it appears to meet the provisions of Article 11 of the Planning Code regarding Major Alteration to a Category I (Significant) Property and the *Secretary of the Interior Standards for Rehabilitation*.

ATTACHMENTS

- Draft Motion
- Exhibits
- Photographs
- Project renderings
- SFMTA Design Narrative
- Materials cut sheets
- Plans
- Previously Considered SFMTA design schemes

**Permit to Alter
June 20, 2012**

**Case Number 2012.0136H
220 Geary Street (aka. 333 Post Street)**

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SAN FRANCISCO PLANNING DEPARTMENT

Historic Preservation Commission Motion No. XXXX Permit to Alter MAJOR ALTERATION

HEARING DATE: JUNE 20, 2012

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ADOPTING FINDINGS FOR A PERMIT TO ALTER FOR MAJOR ALTERATIONS TO A CATEGORY I (SIGNIFICANT) PROPERTY, INCLUDING CONSTRUCTION OF A NEW TRANSIT STATION ENTRANCE AT THE CORNER OF GEARY AND STOCKTON STREETS, FOR THE PROPERTY LOCATED ON LOT 001 IN ASSESSOR'S BLOCK 0308. THE SUBJECT PROPERTY IS WITHIN THE KEARNY-MARKET-MASON-SUTTER CONSERVATION DISTRICT, A P (PUBLIC) ZONING DISTRICT AND AN OS (OPEN SPACE) HEIGHT AND BULK DISTRICT.

PREAMBLE

WHEREAS, on March 14, 2012, John Funghi ("Applicant") filed an application with the San Francisco Planning Department ("Department") for a Permit to Alter for exterior rehabilitation, including construction of a new transit station entrance at the intersection of Stockton and Geary Streets at the southeast corner of Union Square as part of the Central Subway project. The station entrance will consist of a weather protected room containing a staircase, escalator, two elevators, and space for pedestrian circulation, covered by a roof terrace. The subject property is located on Lot 001 in Assessor's block 0308,

a Category I (Significant) property historically known as Union Square and within the Article 11 Kearny-Market-Mason-Sutter Conservation District (KMMS District).

WHEREAS, on June 20 2012, the Historic Preservation Commission (“Commission”) conducted a duly noticed public hearing and approved the project proposed in Permit to Alter Application No. 2012.0136H through Motion No. XXXX.

WHEREAS, on March 14, 2012, John Funghi (“Applicant”) on behalf of property owner filed an application with the San Francisco Planning Department (“Department”) for a Permit to Alter for construction of a new transit station entrance located on Lot 001 in Assessor’s Block 0308, a Category I (Significant) property, historically known as Union Square.

WHEREAS, the Project was determined by the Department to be categorically exempt from environmental review. The Historic Preservation Commission (“Commission”) has reviewed and concurs with said determination.

WHEREAS, on June 20, 2012, the Commission conducted a duly noticed public hearing on the current project, Case No. 2012.0136H (“Project”), for the Permit to Alter.

WHEREAS, in reviewing the application, the Commission has had available for its review and consideration case reports, plans, and other materials pertaining to the Project contained in the Department’s case files, and has reviewed and heard testimony and received materials from interested parties during the public hearing on the Project.

MOVED, that the Commission hereby grants the Permit to Alter, **APPROVED**, and in conformance with the architectural submittal dated February 15, 2012 and labeled Exhibit A on file in the docket for Case No. 2012.0136H.

FINDINGS

Having reviewed all the materials identified in the recitals above and having heard oral testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and also constitute findings of the Commission.
2. Findings pursuant to Article 11:

The Historical Preservation Commission has determined that the proposed work is compatible with the exterior character-defining features of the subject property and meets the requirements of Article 11 of the Planning Code:

- That the proposal respects the character-defining features of the subject property and the KMMS District;

- That all architectural elements, including reconfiguration of the terrace stairs at the southeast corner of the square, will be done in such a manner to retain as much existing fabric as possible;
- That the new station entrance will be inserted in such a manner that does not impair the essential form and integrity of the subject property and the surrounding KMMS District.
- That the integrity of distinctive stylistic features and examples of skilled craftsmanship that characterize the KMMS District shall be preserved; and,
- That all new materials shall be compatible with the existing materials in composition, design, color, texture and other visual qualities.

For these reasons, the proposal overall, is appropriate for and consistent with the purposes of Article 11, meets the standards of Article 1111.6 of the Planning Code and complies with the *Secretary of the Interior's Standards for Rehabilitation*.

3. **General Plan Compliance.** The proposed Permit to Alter is, on balance, consistent with the following Objectives and Policies of the General Plan:

I. URBAN DESIGN ELEMENT

THE URBAN DESIGN ELEMENT CONCERNS THE PHYSICAL CHARACTER AND ORDER OF THE CITY, AND THE RELATIONSHIP BETWEEN PEOPLE AND THEIR ENVIRONMENT.

GOALS

The Urban Design Element is concerned both with development and with preservation. It is a concerted effort to recognize the positive attributes of the city, to enhance and conserve those attributes, and to improve the living environment where it is less than satisfactory. The Plan is a definition of quality, a definition based upon human needs.

OBJECTIVE 1

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

POLICY 1.3

Recognize that buildings, when seen together, produce a total effect that characterizes the city and its districts.

OBJECTIVE 2

CONSERVATION OF RESOURCES WHICH PROVIDE A SENSE OF NATURE, CONTINUITY WITH THE PAST, AND FREEDOM FROM OVERCROWDING.

POLICY 2.4

Preserve notable landmarks and areas of historic, architectural or aesthetic value, and promote the preservation of other buildings and features that provide continuity with past development.

POLICY 2.5

Use care in remodeling of older buildings, in order to enhance rather than weaken the original character of such buildings.

POLICY 2.7

Recognize and protect outstanding and unique areas that contribute in an extraordinary degree to San Francisco's visual form and character.

The goal of a Permit to Alter is to provide additional oversight for buildings and districts that are architecturally or culturally significant to the City in order to protect the qualities that are associated with that significance.

The proposed project qualifies for a Permit to Alter and therefore furthers these policies and objectives by maintaining and preserving the character-defining features of the subject property for the future enjoyment and education of San Francisco residents and visitors.

4. The proposed project is generally consistent with the eight General Plan priority policies set forth in Section 101.1 in that:

- A) The existing neighborhood-serving retail uses will be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses will be enhanced:

The construction of the Central Subway will have impacts to neighborhood retailers adjacent to and in the vicinity of the station; however, these disturbances will cease once construction is completed. Construction of the Central Subway and the opening of the Union Square station will bring additional visitors and consumers to neighborhood serving retail, with a spin-off effect increasing employment in the area.

- B) The existing housing and neighborhood character will be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods:

Construction of the station would not affect housing.

- C) The City's supply of affordable housing will be preserved and enhanced:

Construction of the station would not affect housing.

- D) The commuter traffic will not impede MUNI transit service or overburden our streets or neighborhood parking:

By providing an exclusive right-of-way that does not have to compete with traffic on congested surface streets, the reliability of transit service would be improved and travel times would be reduced for

transit riders. Temporary disruption to traffic and Muni service is likely to occur during construction activities but will cease once completed.

- E) A diverse economic base will be maintained by protecting our industrial and service sectors from displacement due to commercial office development. And future opportunities for resident employment and ownership in these sectors will be enhanced:

As an improvement in the Union Square plaza, the Central Subway station would not have a direct impact on the displacement of industrial and service sectors.

- F) The City will achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The Union Square station is not located on any active faults and therefore rupture resulting from displacement along a fault is not likely to occur. The subway station would be designed to current seismic standards to withstand a major earthquake (magnitude~7) on the San Andreas Fault.

- G) That landmark and historic buildings will be preserved:

The implementation of the Central Subway project would result in a change to Union Square, however, the design has been adjusted to create a minimum visual and actual impact upon the historic resource. The proposed project is in conformance with Article 11 of the Planning Code and the Secretary of the Interior's Standards for Rehabilitation.

- H) Parks and open space and their access to sunlight and vistas will be protected from development:

The new station would not impact or shadow Union Square.

5. For these reasons, the proposal overall, appears to meet the *Secretary of the Interior's Standards* and the provisions of Article 11 of the Planning Code regarding Major Alterations to Category I (Significant) buildings.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **GRANTS a Permit to Alter** for the property located at Lot 001 in Assessor's Block 0308 for proposed work in conformance with the architectural submittal dated February 15, 2012 and labeled Exhibit A on file in the docket for Case No. 2012.0136H.

APPEAL AND EFFECTIVE DATE OF MOTION: The Commission's decision on a Permit to Alter shall be final unless appealed within thirty (30) days after the date of this Motion No. XXXX. Any appeal shall be made to the Board of Appeals, unless the proposed project requires Board of Supervisors approval or is appealed to the Board of Supervisors as a conditional use, in which case any appeal shall be made to the Board of Supervisors (see Charter Section 4.135). For further information, please contact the Board of Appeals in person at 1650 Mission Street, (Room 304) or call (415) 575-6880.

THIS IS NOT A PERMIT TO COMMENCE ANY WORK OR CHANGE OF OCCUPANCY UNLESS NO BUILDING PERMIT IS REQUIRED. PERMITS FROM THE DEPARTMENT OF BUILDING INSPECTION (and any other appropriate agencies) MUST BE SECURED BEFORE WORK IS STARTED OR OCCUPANCY IS CHANGED.

I hereby certify that the Historical Preservation Commission ADOPTED the foregoing Motion on June 20, 2012.

Linda D. Avery
Commission Secretary

AYES:

NAYS:

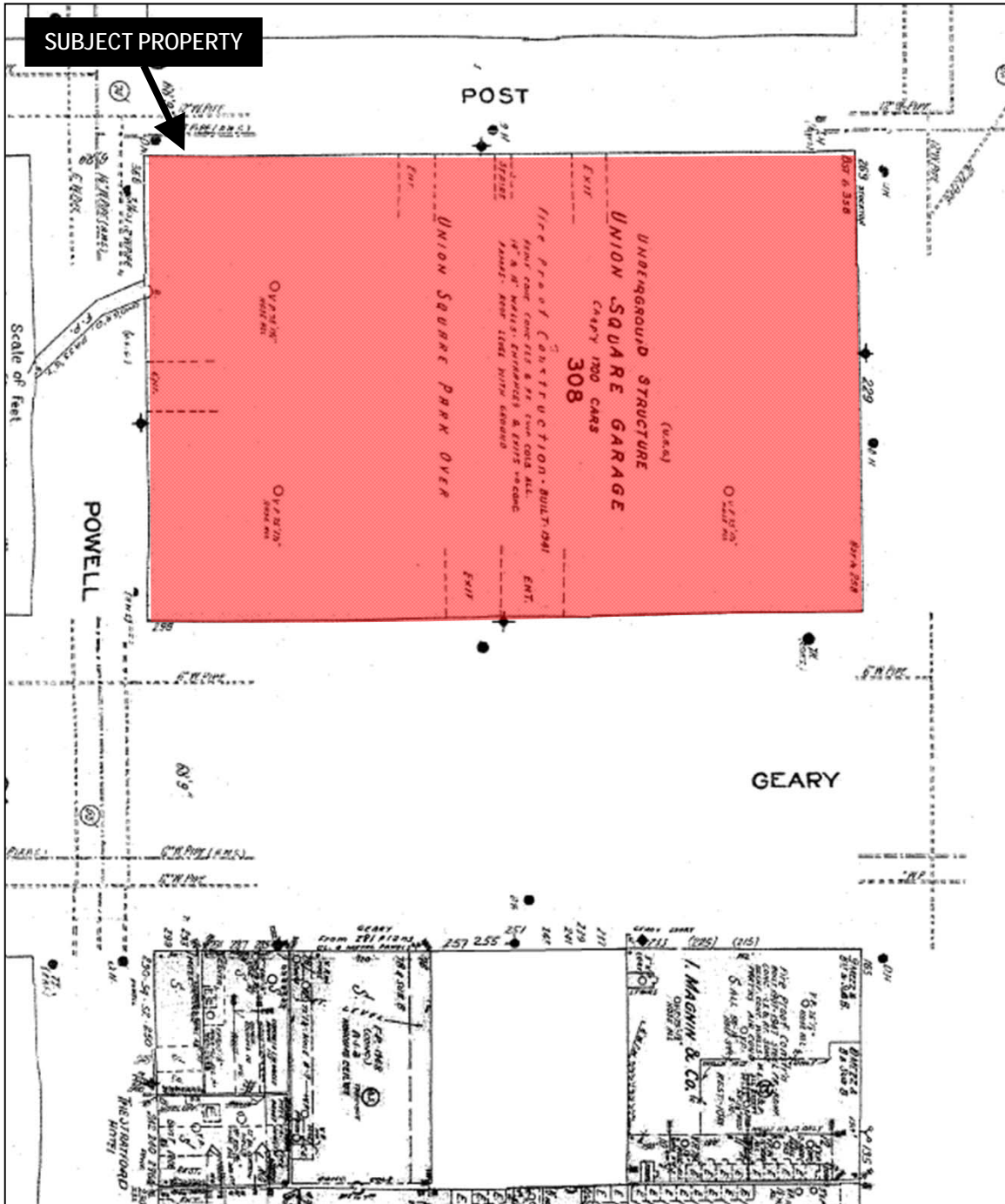
ABSENT:

ADOPTED: June 20, 2012

Parcel Map



Sanborn Map*



*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



Major Permit to Alter Hearing
Case Number 2012.0136H
220 Geary Street/333 Post Street (Union Square)

Zoning Map



Major Permit to Alter Hearing
Case Number 2012.0136H
220 Geary Street/333 Post Street (Union Square)

Aerial Photo

SUBJECT PROPERTY



Birdseye Photo

SUBJECT PROPERTY



Major Permit to Alter Hearing
Case Number 2012.0136H
220 Geary Street/333 Post Street (Union Square)

Site Photo



Union Square, view along Geary Street looking north.

Major Permit to Alter Hearing
Case Number 2012.0136H
220 Geary Street/333 Post Street (Union Square)

Site Photo



Union Square, view along Stockton Street, looking southwest.

Major Permit to Alter Hearing
Case Number 2012.0136H
220 Geary Street/333 Post Street (Union Square)



View of NW Corner of Geary and Stockton



View along Stockton Street
(Location of Proposed Vent Shafts)



Aerial View from Gery Street
(Location of Proposed Station Entrance)



Aerial View toward Stockton and Geary Streets



Existing Planters to Remain at NW Corner of Geary and Stockton (along Geary)



Location of Proposed Union Square Entrance (View from Geary Street)



View of adjacent Parking Garage Entrance along Geary Street



Existing stair at Geary Street (to be reconstructed)



View of intermediate Plaza Level (looking East)



View from Upper Plaza Level (looking toward NW corner of Stockton and Geary)



Existing Stone Pilaster at Intermediate Plaza Level (to remain)



View from Intermediate Plaza Level (looking West)



View from Nieman Marcus



View from Corner of Stockton and Geary



Aerial View of Station Entrance Looking toward Stockton and Geary Streets



View from Intermediate Plaza Level looking East



View from Glass Roof Deck looking South



Side-by-Side Views of Glass Roof Deck



View of Proposed Vent Shafts along Stockton Street

SFMTA CENTRAL SUBWAY

Union Square / Market Street Station

Union Square Entrance

DESIGN NARRATIVE

Union Square is the retail heart of San Francisco and is a legacy of the Gold Rush era. The 2.6 acre site got its name from demonstrations in support of the Union Army during the Civil War. In 1850, the square was dedicated to Mayor John Geary, who was the first administrator following the discontinuation of the Spanish land grant. The square was an assemblage of dirt paths and floral planters. In 1903, the Corinthian column was erected as a monument to Admiral George Dewey's exploits in the Spanish American War. Less well known was another dedication to President McKinley, then recently assassinated. Artist Robert Aitken created the statue of "Victory" atop the tower.

In 1939, Union Square was transformed by construction of the world's first underground parking garage. Its paths and planters were elevated and set in stone and concrete above the garage. The parking structure and park were designed by the acclaimed modernist architect Timothy Pflueger (also known for 450 Sutter nearby). The layout of straight paths and elevated planters provided intimate and quiet spaces that seemed an antidote to the busy retail scene as the city flourished following World War 2. The Bay Area celebrated social freedoms of the 1960s and 70s, but Union Square suffered. A transient population brought crime and rioting that threatened shoppers, workers and tourists.

In 1998, the San Francisco Prize sponsored a competition for conceptual designs to transform Union Square. Although the proposals were not meant to be built and, therefore, most established architects and landscape architects did not submit, Mayor Willie Brown turned the competition into a real commission. The team of Fotheringham and Phillips won the competition, and their design was built in 2002. Their scheme was completely different from the existing Pflueger design. Wherein the existing square possessed the serious ambience of a monument; was made of gray Raymond granite and concrete; planted mainly with pruned shrubs in variations of green; and comprised relatively narrow paths and central terrace for viewing the Dewey column the new square was presented as an open plaza with a carnival of materials: dark to light grays and beige to pink tans along with gray, green and pink stones as well as over two dozen types of flowering shrubs including exotic fragrant trees. On the square a series of metal framed pavilions with bowed roofs house amenities and service cores. The Dewey column was resurrected in the center of the square.

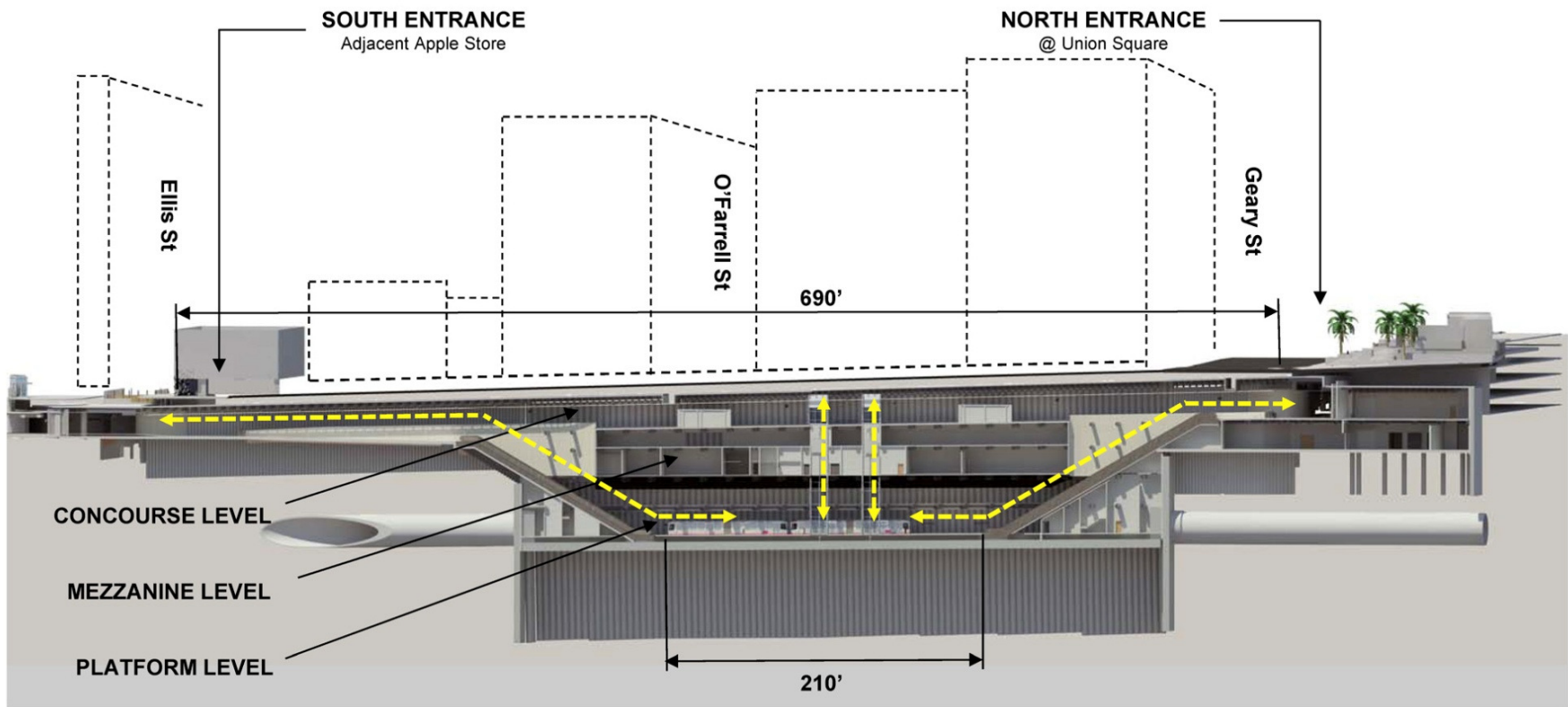
In 2019, SFMTA's Central Subway will open with station access at Union Square on Geary Boulevard. The Union Square entrance is the visible portion of Union Square / Market Street Station, a new underground light rail transit station that is over 700 feet long and nearly 100 feet deep under Stockton Street. At the south end the station connects to the Muni/BART Powell Station under Market Street, and at the north end it emerges at Union Square on Geary Boulevard near Stockton Street. The station entrance is a weather-protected room containing one staircase, one escalator, two elevators and space as needed for circulation to and from each.

Throughout the final design process, a number of city agencies and community stakeholders provided input and/or imposed constraints, often competing, that influenced the design of the Union Square Entrance:

Entity	Primary Interests/Concerns
Union Square Business Improvement District	Minimize impacts to plaza (visual and usage)
San Francisco Arts Commission Civic Design Review Committee	Architectural statement of international stature
San Francisco Recreation and Park Department	Minimize impacts to plaza (visual and usage)
San Francisco Mayor’s Office of Disability	Improve accessibility; elevator redundancy (2 vs. 1)
San Francisco Municipal Transportation Agency (SFMTA)	Minimize impacts to parking garage (parking takes)
San Francisco Municipal Transportation Agency	Safety, security and maintainability of station entrance

Through many workshops and design iterations, SFMTA, the Union Square Business Improvement District and the Arts Commission came to recognize that, while the new station entrance could have set a high architectural standard, the best solution was to make it invisible – to blend in with the surrounding Union Square; to design the entrance to look more like landscape than a building. Although the entrance must enclose the volume required by 2 elevators, an escalator and a staircase and, therefore, must protrude from the ground, it has been designed to mimic retaining walls and planter terraces. The passenger entrance on Geary Boulevard was fashioned more like a void than a gateway, a self-negating aesthetic similar to the vehicle entrance of the garage. The remaining sides of the building have been tucked into the Square’s terraces so as to be indistinguishable, and its roof was designed to serve as a terrace and overlook. Visually, the most prominent elements of the entry will probably be the station identification sign and the public art that will decorate the elevator glass and the roof terrace.

Because the station entrance is a new building in Union Square, its design incorporates existing treatments, materials and finishes except when function or safety have required the introduction of new elements. As a new building the station entrance could have been modeled after one of the Square’s pavilions and adopted their architecture. It could have incorporated a curved roof profile, included an awning comprised of narrow metal tubes, had a structure of cylindrical metal columns and exposed steel beams and been enclosed with banded concrete walls. But as a building located below the plaza level and tucked into its stepped terraces, the station entrance could also have been modeled on the Square’s system of retaining walls. This latter option was selected to reduce the visual impact of the new building and to keep visitors focused on the existing plaza and its pavilion. It was decided to insert a “non building” in the form of retaining walls, steps, planters and landscape.



Union Square / Market Street station (UMS): Union Square entrance and palm trees on the right

The station entrance is tucked into Union Square's parking garage. An existing vehicle ramp must be reconfigured and the garage's existing structure must be altered to accommodate the station entrance and a portion of the station's concourse below. To minimize alteration in the parking garage and to conserve maximum open space amenity at the plaza the station entrance has been designed to be as compact as possible in both plan and elevation. Approximately 80 parking spaces are permanently impacted by the station entrance. At the surface, the station entrance permanently displaces approximately 1,200 square feet of terraced seating but incorporates approximately 950 square feet of accessible glass roof deck, resulting in a net loss of approximately 250 square feet of open space.



Union Square today: the station entry will be by the palm trees on the right



Union Square with UMS north entrance

UMS Transverse Section looking North



Existing Union Square plaza pavilions with metal trellises and striated concrete panels



Existing garage entrance with stepped wall at Union Square



Existing terraced seating at Union Square



Existing terraced wall with vines at Union Square

To integrate functionally with Union Square's surface and to provide uninterrupted open space amenity, the roof of the station entrance has been designed as a walk-on terrace. Public access to the roof deck is via steps and a short ramp along the north edge, and via terraced seating along the west edge.

A 42-inch-high guardrail is required along south and east edges of the roof deck; to reduce its bulk and mass, the top 12 inches of the guardrail are glass. The guardrail atop the parapet over the parking garage entrance was not copied because its horizontal rails are climbable.

Vertical clearance requirements for the station entrance result in the roof elevation being slightly higher than the Square's intermediate plaza level. A grass-covered roof deck was contemplated but was rejected because it would have required the roof to be raised an additional 3 feet. Instead, a translucent glass roof was selected for its thin profile and to provide daylight inside while creating an opportunity for art enrichment.



View looking east towards stepped wall with glass guardrail

The enclosures for the two required elevators protrude through the roof and above the guardrail. An operational safety clearance above the roof of the elevator cab prevented the enclosures from being lower than the guardrail. In this case the tops of the enclosures have been raised several inches as insurance against people climbing onto them. The glass facades of the enclosures are an opportunity for art enrichment.



View looking south towards Geary Street showing raised elevator enclosures above guardrail height

The station entrance must be securely closed during non-operating hours. This is being accomplished with glazed bi-fold overhead doors. During operating hours the doors serve as awnings or rain protection. The support and mechanism for such doors are less bulky than that of rolling grilles to insure maximum heights for the two openings.



View towards UMS station southeast corner

The enclosure for the station entrance is exposed on three sides: to the west, stepped terraces are integrated into and match the finish and composition of existing terraced seating; to the east, retaining walls match the finish of the existing walls enclosing the parking garage's ventilation and exit stair; to the south, concrete walls and parapets that frame the station entrance (opening) and elevators are treated to match the finish of the existing retaining walls within the plaza.



Aerial view from southwest towards UMS station entry

On the west side of the station entrance, the new steps to the roof deck are integrated, but at right angle to the existing seating terraces facing Geary Boulevard. The base of the existing seating terraces is modified with a series of right angle terraces along the front of the station entrance. The new terraces are specified to match the materials and colors of the existing terraced seating.

On the east side of the station entrance, a new enclosure wall rises behind the existing palm trees; it is of the same material, color and finish as other terraced walls within the plaza. With the exception of the portion that includes station identification signage (individual lettering), the new wall is covered by vines.



Rendering of UMS east wall stepped with glass guardrail

On the south face of the station entrance, the concrete framing the entry is identical in finish and color to the retaining walls surrounding the parking garage entrance. The concrete framing extends as a parapet over the passenger entries. Vines planted in wells at the base of the concrete walls will eventually cover them. Metal cable grids to support the vines are detailed to appear architectural until the vines grow into place. Except for fire and other functional components, the metalwork on the outside of the entry is powder-coated to match that of the existing pavilions.



World Leader in Flexible Space Management



The Hydrau-Lift
is the only
completely finished
architectural
Bi-Fold door system
available for
field glazing
on the market today.

Hydrau-Lift™ Bi-Fold Door System



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World Leader in Flexible Space Management

Hydrau-Lift Bi-Fold Door System

The Hydrau-Lift™ Bi-Fold door system uses technology that improves on the limitations of cable or strap-driven Bi-Fold systems. The Hydrau-Lift Bi-Fold utilizes a patented hydraulic operating system (SST) provided exclusively by Hufcor.

The Hydrau-Lift system can be ordered from Hufcor with an optional pre-designed storefront glass system that can be either factory or field installed. As such, the Hydrau-Lift is the only completely finished architectural Bi-Fold door system available for glazing on the market today.

Product Features:

- Welded steel frames with narrower profiles (3" – 4" tubes) than the same-sized aluminum systems on the market.
- Optional stainless steel tube framing for corrosive environments (such as ocean fronts, indoor swimming pools, etc).
- Pre-designed and engineered openings to accept face materials (glass or custom skin finishes) that weigh up to 12 psf.
- Precision welds at the frame joints compared to large "industrial" welds on aluminum framed products currently in the market.
- Hydraulic lift mechanisms are located on the very edge of the door frames, eliminating cables, cable "kick-outs" and lift bars on the doors -- offering the cleanest and least obstructive door faces from a commercial Bi-Fold door.
- The hydraulic operators also eliminate the pinching hazards that could happen with cable and strap systems.

- The SST hydraulic cylinder operating system applies 1000+ lbs of "active force" on the door when closed, establishing a weather tight seal against the door perimeter frame and seals.

- Hydrau-Lift Bi-Folds are equipped with a built-in roller latching system located on the very edges of the vertical door frames. The mechanical roller latch firmly secures the doors in place against the support frame. Hydrau-Lift does not require lock actuators or additional motor systems to run "automatic" locks, nor do they require electric or magnetically operated locks to pull the door frames in tight to the seals.

- The hydraulic operator provides security in off hours. Optional manual master-keyed deadbolt locking systems can be installed on the Hydrau-Lift system.

- Pre-installed, heavy-duty, mechanically fastened weather seals surround the horizontal outer edges and at the joint between the panels. The vertical weather seals are applied on the vertical support frame to ensure a tight fit after installation.

- The hydraulic cylinders provide 1000 lbs of "active force" against the doors. As such, the seals are actually compressed tight and the door held flat in the opening. In gravity based systems (those that utilize overhead cables and ceiling mounted motor units), end users have experienced problems with doors not folding flat as shown in the photo. One root cause is the inability of the door's own weight to compress the seals and pull the door tight. Another is the need for the door weight to pro-



For More Information Visit: www.hufcor.com



World Leader in Flexible Space Management

Hydrau-Lift Bi-Fold Door System

vide enough force against the cable kickouts and lifting cables. The Hydrau-Lift pulls the door flat in the opening, and as such eliminates the need for actuators or magnetic catches.

- Hydraulic release valves allow the doors to be lowered in the event of a power failure or system repair. The release valve is part of the hydraulic pump unit, typically remotely installed in a service or storage closet, at ground level. On cable or strap-driven models, the top mounted motor units must be hand cranked at the motor to allow the panels to be lowered in the event of power failures – in some cases, as much as 30' above the floor!

- Hufcor Hydrau-Lift Bi-Fold door frames are factory finished with red oxide primer prepared for field painting. Optional factory painting is available, but not recommended due to the potential of scratching during shipping and installation. Aluminum framed Bi-Fold doors require field priming and then a field paint coating.



- Hufcor Hydrau-Lift doors are steel and are not supplied with a clear anodized frame. (Current aluminum Bi-Fold manufacturers provide a mill finish product. If clear anodized frames are required, the welding is done after the anodizing on aluminum framed systems, thus eliminating the clean look of the clear anodized frame that an architect or client may be looking for.)

- Horizontal top and vertical support leg framing posts are designed to help plumb and square the rough open-

ing and act as a built-in guide for the guide roller.

- The top horizontal guide rail adds lateral stability to the system

- Hufcor Hydrau-Lift can be ordered with a pre-designed aluminum storefront system that accepts a one-inch 1" insulated glass or ¼" single lite glass or polycarb unit. The system comes standard with clear or bronze anodized finishes or optional standard RAL powder coat. Custom Kynar powder coated colors are available for an upcharge.

- The standard system contains a 1/8" EDPM thermal barrier at the pressure plates and cover. And, Hydrau-lift factory installs and additional thermal membrane between the storefront base plate and door frame.

About Hufcor

Hufcor is a privately held U.S.A. corporation and is the world's leading manufacturer of operable glasswall, and accordion partitions. As the world leader in flexible space management, Hufcor combines innovative engineering and design concepts to offer our customers the broadest range, most technologically advanced, high quality products available.

Hufcor partitions are available through local, factory-trained Distributors, offering design assistance, installation and service. For more information about Hufcor partitions, contact your local distributor. For the name of your nearest representative go to www.hufcor.com.

Hufcor, Inc. P. O. Box 591, 2101 Kennedy Rd.,
Janesville, WI 53547, USA

Phone: 800.542-2371, Ext 214 608.756.1241

FAX: 608.758.8253

Email: info@hufcor.com • www.hufcor.com

For More Information Visit: www.hufcor.com

About Hufcor

PARTITIONS
OPERABLE | GLASSWALL | ACCORDION

Hufcor, the world leader in the manufacture of partitions, has a reputation for products with exceptional quality and durability. Operable, accordion, portable and glass partitions provide the flexibility to quickly create large or small spaces.

Hufcor partitions are available through local, factory-trained Distributors, offering design assistance as well as installation and service.

For more information about Hufcor partitions, contact your local distributor. For the name of your nearest representative, go to www.hufcor.com.

The Hufcor Commitment to Green Manufacturing, Sustainability and LEED™

Hufcor is committed to being an industry leader when it comes to protecting the environment, by reducing pollutants and minimizing our environmental impact on air, water and land. Today, the Hufcor commitment and progress toward Green manufacturing is greater than ever, as we continue to develop standards that support various LEED (Leadership in Energy and Environmental Design) points to assist the achievement of LEED certification goals.



World Leader In Flexible Space Management

Worldwide Headquarters

P. O. Box 591, 2101 Kennedy Rd., Janesville, WI 53547, USA

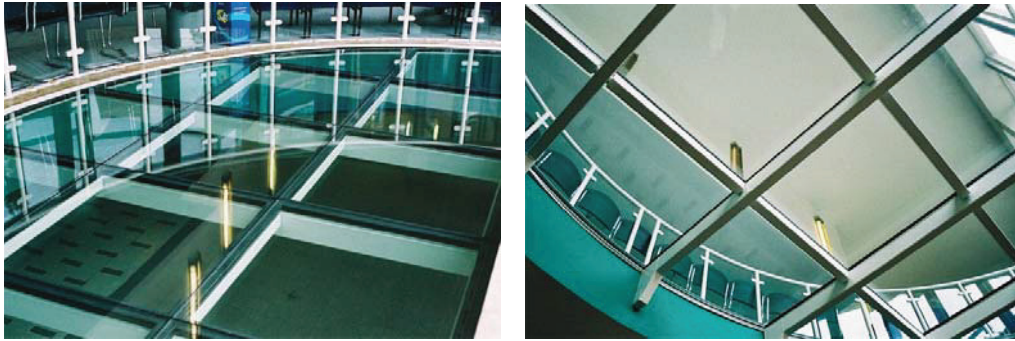
Toll-Free: 800.542-2371, Ext 214 • Telephone:

608.756.1241 FAX: 608.758.8253

Email: info@hufcor.com • Website: www.hufcor.com

F-4326





1. General Principles

a) Integrity vs Insulation

The LITEFLAM™ fire rated glass floor systems all incorporate Integrity and Insulating fire-resistant glass, specifically Contraflam™, manufactured by Vetrotech Saint-Gobain. The insulating element of this glass limits heat transfer through to the structural glass enabling the structural glass to carry applied load for the specified duration.

Some Architects will request integrity only fire rated glass floor. This is generally not possible for the following reason. The structural glass element of a fire rated glass floor normally comprises a multi laminate glass bonded using standard poly-vinyl-butylal (pvb) membrane. This membrane starts to soften above 150 degrees Celsius and melts completely by 250 degrees Celsius. Thus an integrity only fire safety glass deployed under a structural glass will allow heat to transfer through to the structural glass allowing the pvb to soften and the multi laminate structural glass to fail under applied load. This softening/failure process under fire is relatively quick. An unprotected glass floor could fail in as short a period as 3 to 4 minutes under applied load in fire conditions. Whereas the LITEFLAM system will maintain its' integrity with up to 90 minutes of fire exposure.

b). Load capacity and fire-rating ASTM-E119-00

Every LITEFLAM floor system distributed by GreenLite Glass Systems (GGS) has been tested under applied load in one of two categories.

Domestic Loading	:	3.0 kN/m ²	/	60 psf
Commercial Loading	:	4.8 kN/m ²	/	100 psf
Integrity and Rating	:	60min and 90min systems are available		

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c). Support

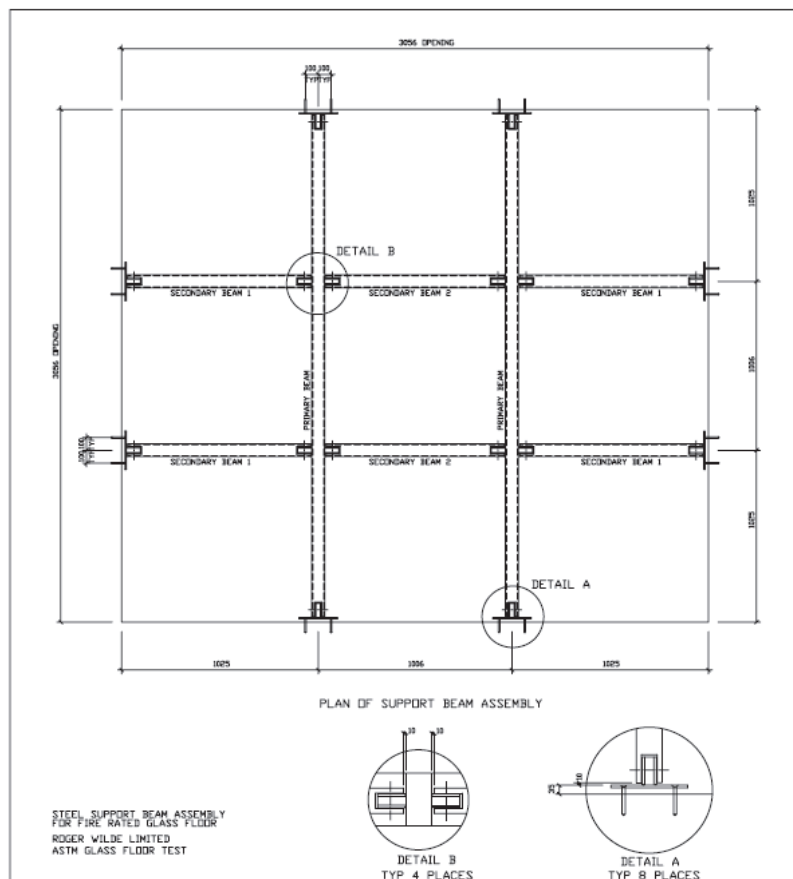
Fire rated glass floors require four sided support. The Contraflam fire rated glass used in the LITEFLAM systems is a gel filled glass (single or multilayer gel depending on fire resistance required) and is not designed to carry applied load. Therefore every LITEFLAM system includes a patented “load bypass” or “load transfer” system to isolate the fire rated glass from the floor loadings.

Additionally the supporting steel structure for the multipanel systems includes a provision for expansion at every jamb to minimise possible buckling due to steel expansion in fire conditions.

d). Construction

All LITEFLAM systems combine the fire rated glass and structural glass into a single mild steel frame protected by intumescent paint. This enables the fire rated and structural glass to be placed in close proximity to each other and offers a much superior aesthetic to previous systems.

All LITEFLAM multi-panel systems have been tested using mild steel rolled hollow sections (HS Tube Steel) protected with intumescent paint as primary and secondary support. It is possible to use other sections such as channels or tees subject to Fire Authority approval. An Architect proposing such variations from that tested should be encouraged to check back to his Regulatory Authority before finalising his specification for a particular project.



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e). Test Criteria

All LITEFLAM systems have been tested to ASTM E119-00 – US and Canada and either BS476 Part 21 1987 or BS EN 1365-2:2000 – the relevant British Standards.

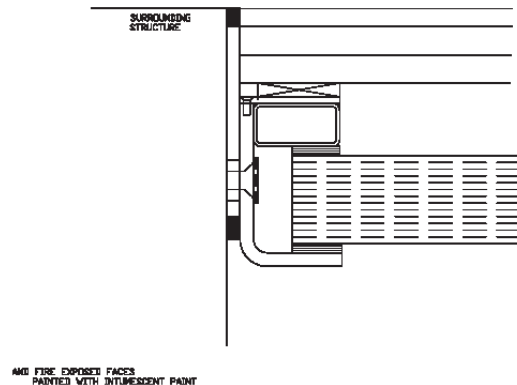
f). Slip Resistance

All LITEFLAM systems (except the Liteflam Domestic product) are supplied as standard with LITEFLOOR top sheet which incorporates a slip resistant carborundum frit in a variety of standard colours and patterns.

g). Separate Sheet/Composite Sheet Systems

LITEFLAM systems are offered in both separate structural glass/fire rated glass systems and double glazed systems. The choice as to which to offer is based upon a number of not necessarily mutually compatible criteria.

Separate Sheet System



TYPICAL EDGE DETAIL

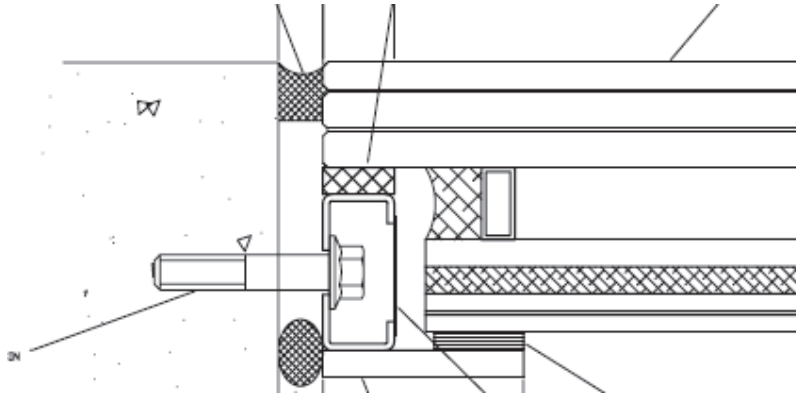
Advantages: Better for larger panel sizes due to ease of installation. Total combined weight of structural and fire rated glass can be 160Kg/M² typically. So a 2.0m by 1.0m panel would weigh 320Kg! Another good selling point is that only structural glass top sheet needs replacing if broken thus saving the cost of the fire rated sheet.

Disadvantages: Not so suitable for external use. Difficult to obtain clean environment between sheets in site conditions.

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Double Glazed System



Advantages : Ease of installation. Available for both commercial market and domestic market as standard "Liteflam" unit. Factory constructed quality available with low-E hard coating to meet Part L insulation requirements if necessary. More suitable for external use.

Disadvantages : Heavy in larger panel sizes. Whole unit has to be replaced at higher cost if damaged.

h). Panel Sizes Available

60/60 systems

1.0m x 2.0m (39.375" x 78.75")

1.5m x 1.5m (59.0" x 59.0")

90/90 systems

1.0m x 2.0m (39.375" x 78.75")

1.5m x 1.33m (59.0" x 52.375")

Assessment letters are available for increased panel sizes by special request.

i). Specification sheets

Non fire rated specification sheet attached.

60/60 specification sheet attached.

90/90 specification sheet attached.

j). Framing system

The LITEFLAM frame systems have been designed to be the smallest and most aesthetically pleasing frames available in the market place.

See attached schematic detail.

k.) External Use

The systems can be modified for external use. Particular care has to be taken to waterproof the interface between the glass floor and the surround.

LITEFLAM has standard solutions to this problem.

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I). Further information available on our websites www.greenliteglass.com and www.liteflam.com



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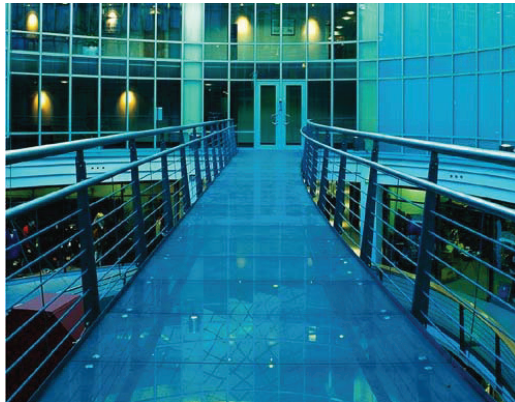
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Distribution and Technical Service for **SAINT-GOBAIN-GLASS**

LITEFLOOR® GLASS FLOORS, STAIRS & WALKWAYS



LITEFLOOR R 46/3 with non-slip



LITEFLOOR CL 28/3 with blue opaque interlayer



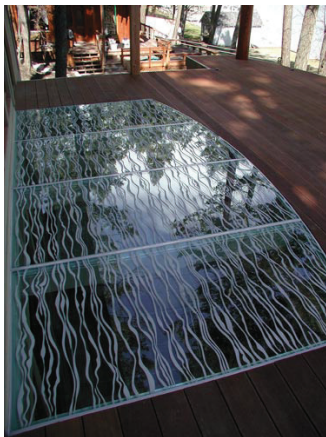
LITEFLOOR CL 24/3 clear

As the leading single source manufacturer of structural glass panel systems, LITEFLOOR® offers unique and innovative product lines that can be designed into virtually any type of application to meet almost all structural, slip resistant and aesthetic design criteria.

LITEFLOOR is a walkable laminated safety glass with optional slip-resistant properties and/or structural integrity. Depending on the respective particular requirements of the application there are two types (LITEFLOOR R and CL) and various compositions available:

LITEFLOOR R A laminated glass of usually two sheets of structural layers of glass with a third slip-resistance partially transparent / translucent top layer. Coloured, transparent or opaque interlayers are also optional.

LITEFLOOR CL A laminate of two or more sheets of annealed, heat-strengthened or tempered glass without slip-resistance surface. Vision-protection can be achieved using opaque interlayer options.



Structural Criteria:

Structural Concept:

LITEFLOOR laminated safety glass typically comprises three single sheets. The (two) lower sheets withstand the normal service loads and are of Float glass or heat-strengthened glass (PLANIDUR). The upper sheet provides slip-resistance through its patented coating and is heat-strengthened.

Structural stability when overloaded:

The single sheets are bonded together, even in breakage, by a highly tear-resistant interlayer. The bond remains intact. A reduction in structural stability remains, depending on the type and amount of damage.

Examination of such structural stability (following intentional damage) has been carried out on LITEFLOOR R. The results of these investigative tests are to be considered when applying for construction approval from the regulating authority

Evaluation Diagram 4-side line supported

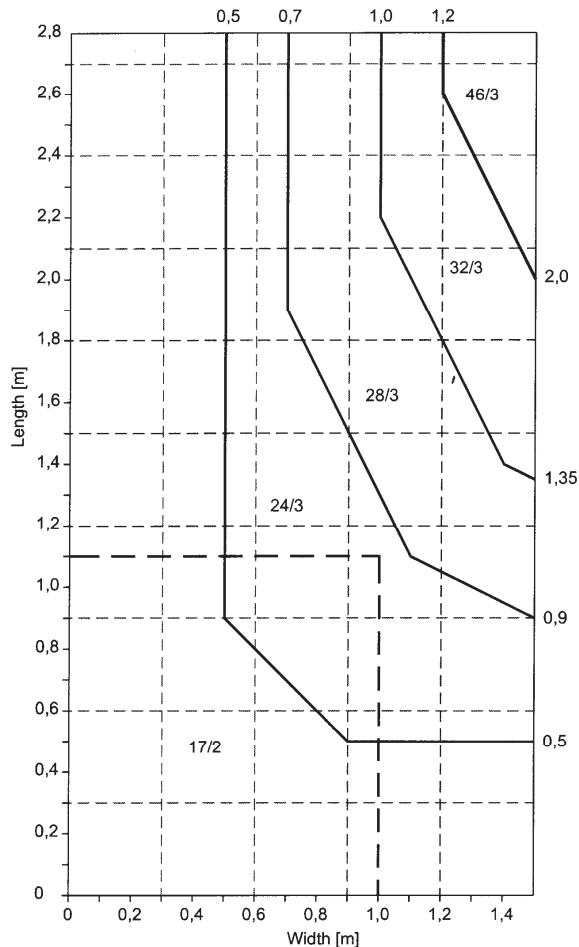
Self-weight + Service load $p = 4 \text{ kN/m}^2$

- all σ TVG = 29 N/mm^2
- all σ Float = 12 N/mm^2
- all $f = L/200$
- without lamination action
- Top sheet not structural (except 17/2)

SGG LITE-FLOOR R
to ÖNORM B4012

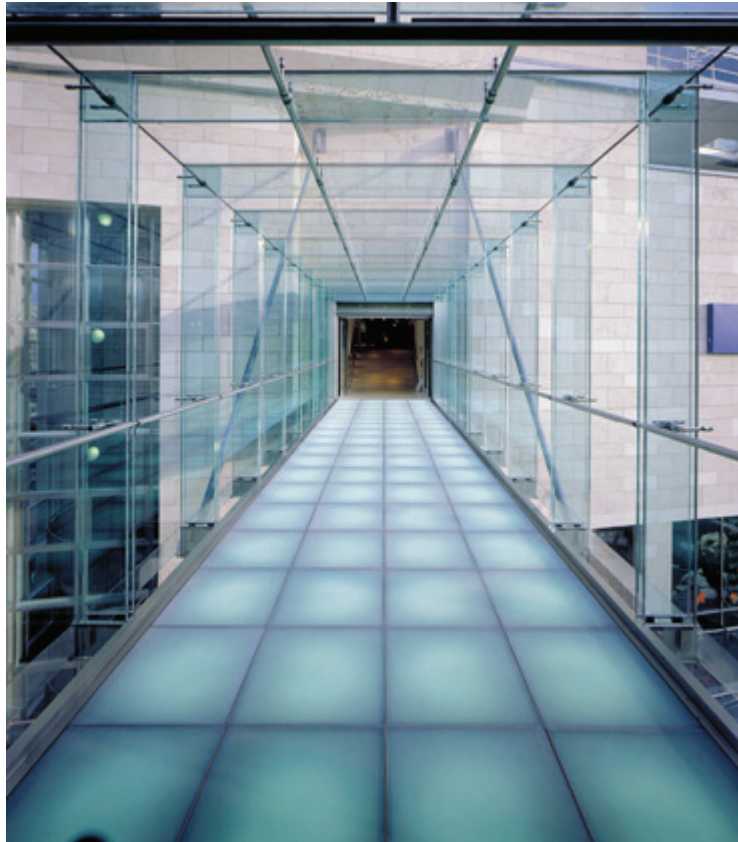
minimum size: 20 x 50 cm
maximum size: 150 x 220 cm

Evaluation example: - - -
Sheet 1,0 x 1,1 m
Application Type 24/3

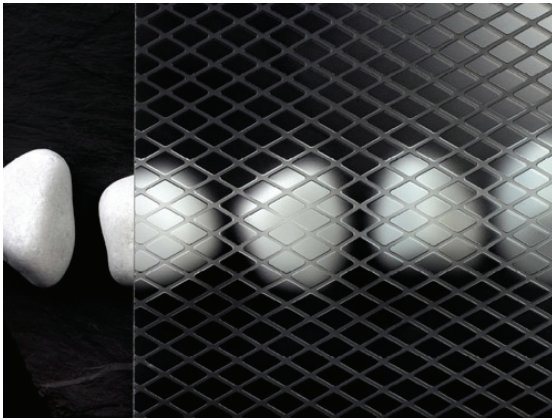


Criteria for the structural requirements are derived from the spans between the supporting members. Depending on the supporting design LITEFLOOR laminated glass can be supported on two or more points, two-sides, or all four sides of the glass. See support [design detail](#). The dimension between the supporting edges or points is used to determine glass type and thickness.

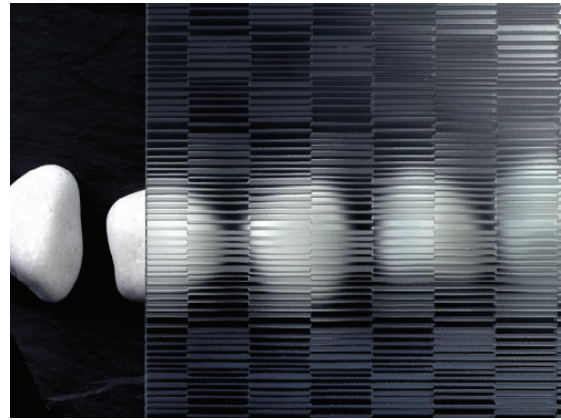
Uniform Building Codes for floor loading are also used to determine the structural glass composition. Live loads, dead loads and point loads requirements are project specific.



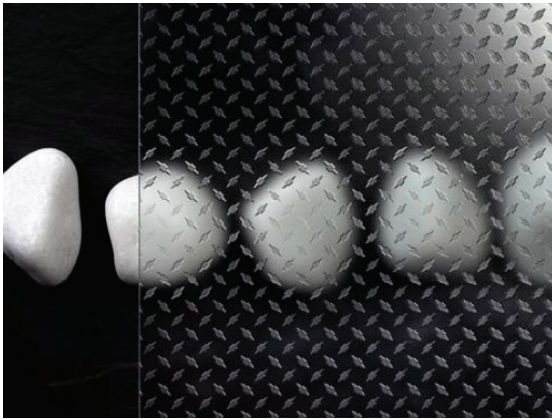
LITEFLOOR EXT - Raised Textured Surface *NEW****



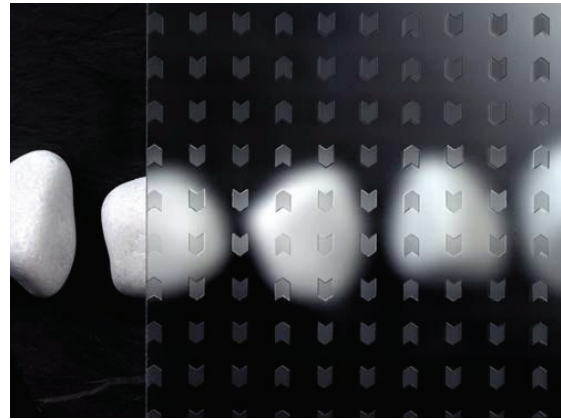
Nexus



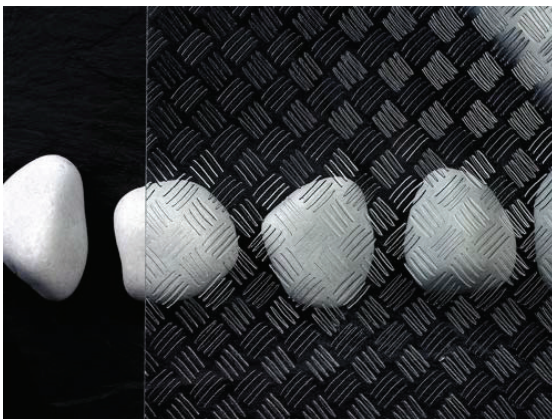
Codex



Incus



Versus



Aluminum

LITEFLOOR R Slip-Resistant coatings

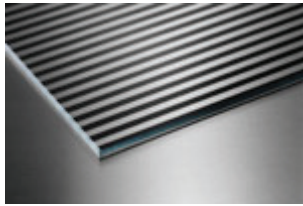
Special hardwearing slip-resistant screen print (specialty frit - enamel ceramic paint with sand particles), which is fused to the top layer of the glass, is available in various standard colours and screen-print patterns. This screen print and orientation can also be customized.

Screen-Print Patterns

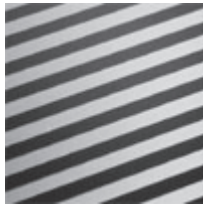
LITEX is a partially enameled tempered safety glass in accordance with EN 12150 that is laminated to the structural element of LITEFLOOR. Litex is non-fading, light- and weather resistant, and resistant to temperature fluctuations.

Partial transparency is achieved by using the Litex Standard designs, whereby the decorative effect can be enhanced depending on the design and colour. Additional Designs can be found in our brochure "Emalit / Seralit". You can request this brochure at: info@greenliteglass.com

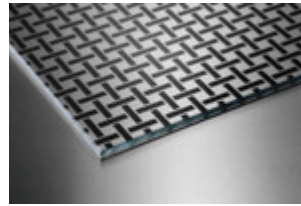
It is also possible to produce custom screen-prints. Custom screen-prints must be submitted via CAD design format.



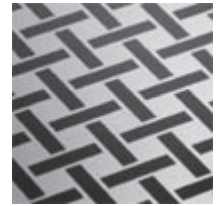
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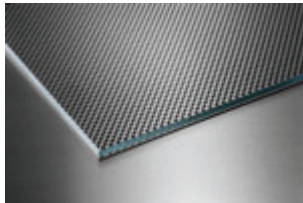
Detail



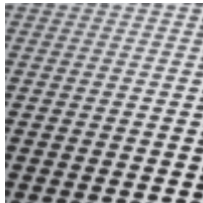
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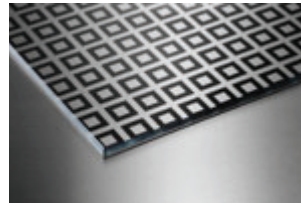
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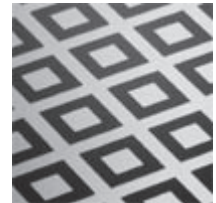
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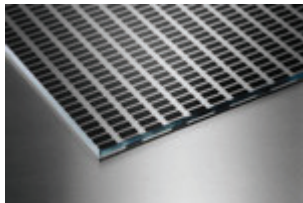
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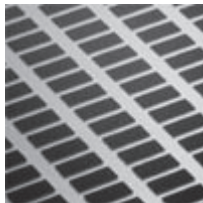
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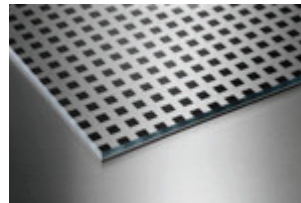
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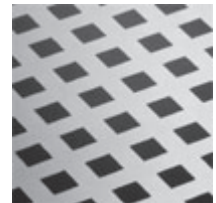
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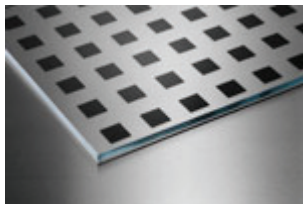
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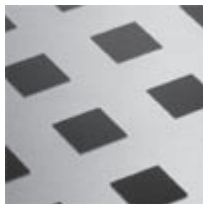
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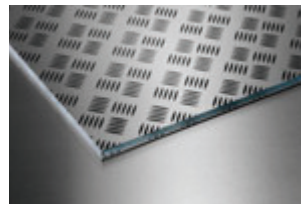
Detail



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Detail



Design-Code: 560

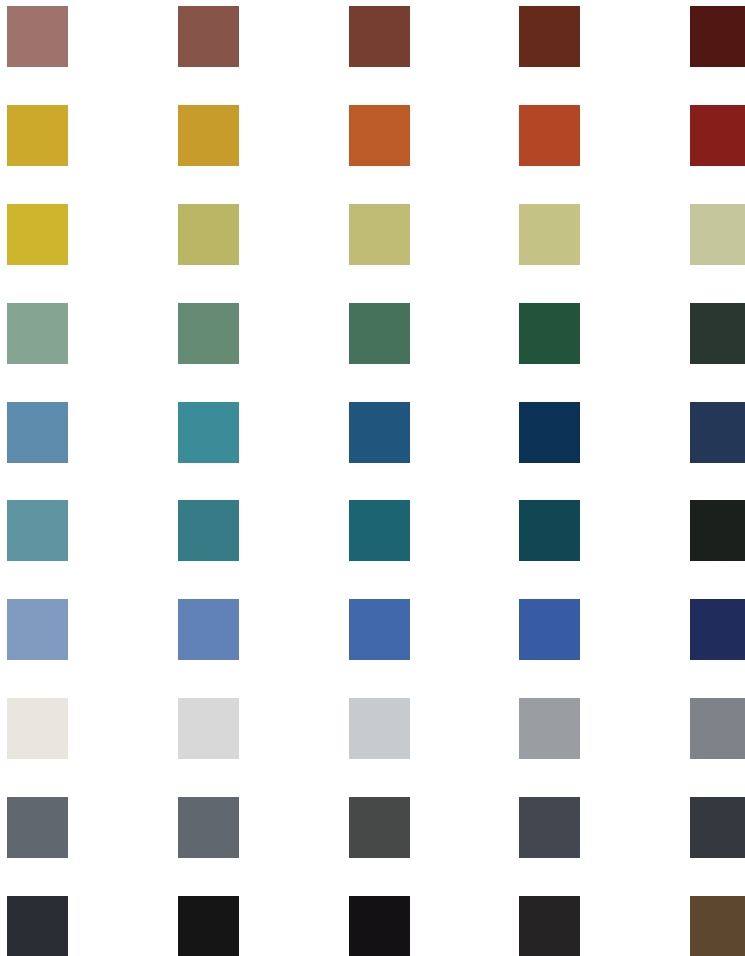


Detail

Slip Resistant Screen-Print Colour Options

Standard Design: Standard colour 7022-5WS (brown)

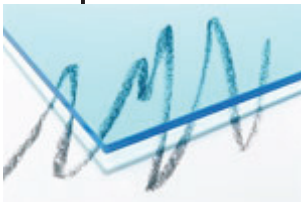
Emalit Custom colours possible:



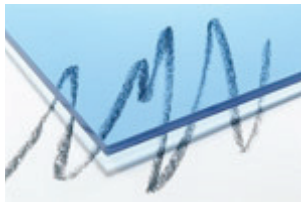
Transparent or Opaque Coloured Interlayer:

Aesthetics can be further customized by the introduction of a transparent coloured interlayer.

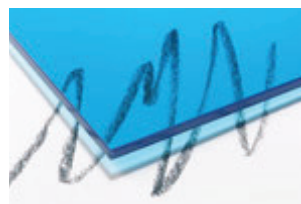
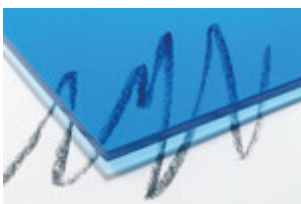
Transparent



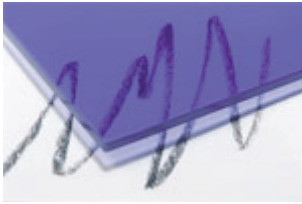
Colour: Blue-D



Colour: Blue-DBD



Colour: Blue-KBWB



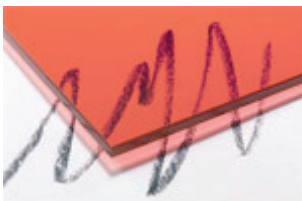
Colour: Red-Blue-KWU



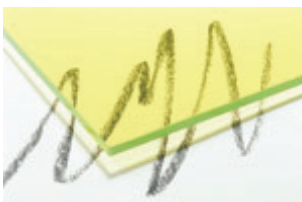
Colour: Violet-KUWU



Colour: Rose-U



Colour: Red-UFUX



Colour: Yellow-X



Colour: Green-FW XF

Colour: Bue-KBWF



Colour: Grey-FF



Colour: Rose-UU



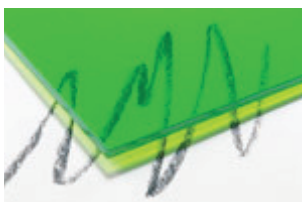
Colour: Red-UFU



Colour: Orange-UX



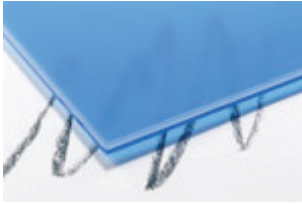
Colour: Yellow-KH



Colour: Green-KHWH

For sensitive applications, obscurity can be achieved using a translucent PVB interlayer.

Translucent



Colour: Blue-MBWB matt



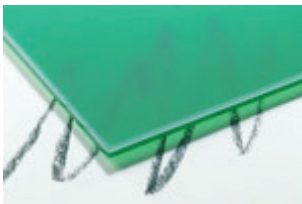
Colour: Rose-MUU matt



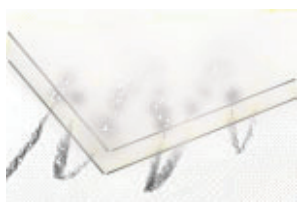
Colour: Orange-MUX matt



Colour: Yellow-MH matt



Colour: Green-MFWXT m



Colour: Diffused White

Curtain Wall & Overhead Glazing



Curtain Wall

Kawneer offers a comprehensive line of architectural aluminum building products and systems for commercial construction – entrances, framing systems, windows and curtain wall systems. For over a century, Kawneer has been recognized as an innovator. Since that time, we've balanced experience with change and ongoing improvement. We strive every day to develop solutions that promote integrated, sustainable building practices, and we are dedicated to providing the tools our customers need to succeed. Thermal breaks, condensation management and building integrated photovoltaic (BIPV) and unitized curtain walls are just a few examples of how we have responded to customer needs.

Hurricane Resistant and Blast Mitigation Products

Kawneer offers single-source responsibility with a comprehensive group of independently tested hurricane resistant and blast mitigation products. Many hurricane resistant products have received Notices of Acceptance (NOAs) issued by Miami-Dade County BCCO and Florida State Approvals. Blast mitigation products meet requirements of the General Services Administration (GSA), Interagency Security Committee (ISC) and Department of Defense (DoD). For more information, visit Kawneer.com.

Custom Curtain Wall

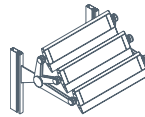
Selected for the world's most recognized buildings

Engineering and design expertise | Established leadership in major applications around the world

1600 PowerShade®

Provides a single-source solution and optimal shade while generating solar power and reducing energy use

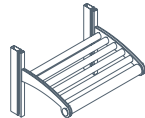
Pre-engineered sunshade system effectively reduces solar heat gain | Dual-position pivot system provides optimal angle and extension for shading any location | Can be directly connected to 1600 Wall System®1



Versoleil™ SunShade: Outrigger System

An economical, pre-engineered and integrated sunshade with mix-and-match design choices

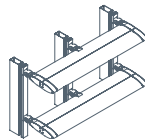
Standardized design compatible with 1600 Wall System®1, 1600UT System™1, 1600 Wall System®5 and 1600 SS™ | Shades interiors and conserves energy with 30" and 36" projection options | Provides a single-source solution | Outriggers, fascia caps and louvers are available in several shapes



Versoleil™ SunShade: Single Blade System

Shades of versatility in form and function

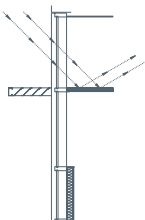
Standardized solution for seamless integration with 1600 Wall System®1, 1600 Wall System®2, 1600 Wall System®5, 1600 SS™ and 1600 SS™ SSG curtain wall systems | Highly versatile design allows for blades to be mounted in horizontal or vertical configurations | Innovative mounting design allows for multiple angles in both horizontal and vertical configurations | Multiple blade sizes ranging from 6" to 14" deep



InLighten® Light Shelf

Reduces the need for artificial lighting by passively channeling natural daylight into an occupied space

Light shelf features aluminum chassis and painted Aluminum Composite Material (ACM) panel surfaces | Attaches directly to 1600 Wall System®1 and System®2 | Two designs include fascia cap or continuous panel "rout and return" | Four standard and custom fascia profiles available



Custom 2500 PG Unitwall™ (4-side SSG), HPS® (High Performance Sliding) Doors, 2000T Terrace Doors, Alcoa – Reynobond® ACM 4mm PE and 4mm FR Panels
W Hotel, Austin, Texas
LEED® Registered project
Architects: Andersson-Wise Architects, Design Architect, Austin, Texas, and BOKA Powell, Architect of Record, Dallas, Texas
Glazing Contractor: Win-Con Enterprises, New Braunfels, Texas



1600 Wall System®2 with 1600 SunShade®, Alcoa – Reynobond® and Reynolux® Panels
Arkansas Department of Environmental Quality, Little Rock, Arkansas
LEED® Gold certified
Architects: Taggart Foster Currence Gray Architects, Inc., North Little Rock, Arkansas, and Williams & Dean Associated Architects, Little Rock, Arkansas
Glazing Contractor: ACE Glass Co., Inc., Little Rock, Arkansas



1600 Wall System®1, Trifab® VG (VersaGlaze®) 451T Framing, 1600 SunShade®, 500 Heavy Wall™ Entrances
Consol Energy Center, Pittsburgh, Pennsylvania
LEED® Gold certified
Architects: Populous, Kansas City, Missouri, and Astorino, Pittsburgh, Pennsylvania
Glazing Contractor: D-M Products, Inc., Bethel Park, Pennsylvania, and Universal Glass, Detroit, Michigan



1600 Wall System®3, 1600 Wall System®4, with AA®3900 Thermal Sliding Doors, 2000T Terrace Doors and 350 Medium Stile Entrances
Glass House, Denver, Colorado
Architect: The Preston Partnership, LLC, Atlanta, Georgia
Glazing Contractor: El Paso Glass Company, Denver, Inc., Aurora, Colorado



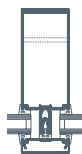
1600 Curtain Wall, 2250 IG (Inside Glazed) Curtain Wall, 2000 Skylight, Trifab® VG (VersaGlaze®) 450 Framing, 350 Standard Entrances
Art Gallery of Alberta, Edmonton, Alberta, Canada
Architects: Randall Stout Architects, Los Angeles, California, USAHIP Architects, Edmonton, Alberta, Canada
Glazing Contractor: Flynn Canada, Acheson, Alberta, Canada



1600 SS™ Curtain Wall, 7500 Wall®, InLighten® Light Shelf, 350 Medium Stile Entrances, Alcoa – Reynobond® ACM Panels
Advocate Lutheran General Hospital and Children's Hospital Patient Care Tower, Park Ridge, Illinois
LEED® Gold certified
Architect: OWP/P | Cannon Design, Chicago, Illinois
Glazing Contractor: Arcadia Products, Inc., Northbrook, Illinois

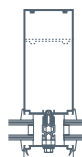
1600 PowerWall® and 1600 PowerSlope®

Combines 1600 Wall System®1 and 1600 Sloped Glazing and photovoltaic (PV) panels that convert light energy from the sun directly into electricity without using fossil fuels
Fully compatible with 1600 Wall System®1 and 1600 Sloped Glazing for vertical and sloped glazed applications



1600 Wall System®1 and 1600 Wall System®2

Reliable curtain wall systems with versatile features
Pressure equalized systems for low- to mid-rise applications
System1 – outside glazed, captured system
System2 – outside glazed, structural silicone glazed system
Concealed fastener joinery creates a smooth, monolithic appearance | Continuous thermal separator reduces heat gain and loss | Large- and small-hurricane missile impact tested | Blast tested to ASTM F 1642 standards



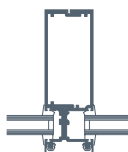
1600UT (Ultra Thermal) System™1 and 1600UT System™2

Ultra-innovative solution raises standards for thermal performance
System1 – outside glazed, captured system
System2 – outside glazed, structural silicone glazed system
Built on the success of the flagship 1600 curtain wall platform, the 1600UT System™ delivers high thermal performance, versatility, reliability and value | Ideal for low- to mid-rise commercial applications where high thermal performing façades are needed | Accepts double or triple glazing



1600 Wall System®3 and 1600 Wall System®4

Inside/outside glazed curtain wall system
System3 has a 2-1/2" profile
System4 has a 2-1/4" profile
Incorporates IsoStrut® Thermal Break for superior structural and thermal performance | Allows for a structural silicone glazing option | Integral exterior cover and thermal isolator reduce installed cost | Inside glazing reduces installation costs | Outside glazing option for spandrel re-glazing



1600 Wall System®5

An inside glazed curtain wall/ribbon window system
2-1/2" profile | Offered in 7-1/2" system depth and optional 6" system depth | Shear block construction for fabrication versatility | Structural silicone glazed option | Optional split mullion design with screw spline construction for ease of installation and fabrication | Allows two-color design options



1600 SS™ (Screw Spline) / 1600 SS Unitwall™ (Pre-glazed option)

Shop assembled for faster field installation
Outside glazed captured or SSG curtain wall | Concealed screw spline joinery allows for shop assembly | Unique interlocking mullion design eliminates the need for anti-buckling clips | System depths and sightline match 1600 Wall System®1 and System®2 | 1600 SS Unitwall™ allows units to be pre-glazed in the shop | 1600 SS™ blast tested per ASTM F 1642 and GSA -TS01-03 standards



Clearwall™ Curtain Wall

Achieves an all-glass monolithic aesthetic in a field glazed, screw spline application
Innovative toggle assembly captures glass, eliminating field application of structural silicone | 6 glazing options use the toggle-based glass retention system | No structural silicone required for Clearwall™ SS/SB glazing options, which use the toggle to directly capture recessed spacer glass | Other glazing options use a metal interface attached to standard 1" insulating glass | Screw spline joinery method allows shop assembly of ladder sections, reducing field labor | Deeper mullions with shear block joinery allow for higher free span applications

7500 Wall®

Exceptional thermal performance and resistance to condensation
Incorporates high-performance IsoWeb® thermal break |
Accommodates 1" double-glazed or 2" triple-glazed insulating glass units | Exterior pressure glazed system

2500 PG Unitwall™

A high-performance unitized curtain wall system
Available in stock length, fabricated or pre-assembled and glazed units | Unitized construction accelerates installation | Available systems include 4-sided captured, vertical and horizontal SSG (Structural Silicone Glazed) or 4-sided SSG | 2-1/2" x 7-1/2" profile | Suitable for new construction or remodel | Dual finish capabilities

1600 L-R (Low-Rise) Wall®

Economical stock length system for low-rise applications
Outside glazed pressure plate system available in captured or two-sided vertical SSG option | Standard 5-3/4" or 7-1/4" depth systems | Shear block construction utilizing concealed fasteners

2250 L-R (Low-Rise) Wall

The economical system designed for low-rise applications
Economical stick system with no-compromise performance | Utilizes a polymer thermal clip, allowing for easier installation and labor savings | 2-1/4" sightline | Design flexibility allows a structural silicone glazed option | Mullion drained design

2250 IG (Inside Glazed)

An economical, inside glazed ribbon window/curtain wall system
Narrow 2-1/4" profile | Offered in 4-1/2", 6" and 7-1/2" system depths | Shear block and screw spline construction for easy fabrication | Structural silicone glazed option | Allows two-color design options | Thermally improved



Finishes

Anodized Finishes

Long-lasting, protective coatings resist abrasion, corrosion and UV rays, and meet or exceed AAMA 611

Architectural Metals

Light satin is a Class I finish with a chrome-like brightness and light brushed texture | Available on a variety of products

Clear Finishes

Available in Anodized Class I (#14) or Class II (#17)

Permanodic® Finishes

Available in Color Anodized Class I (#40 Dark Bronze, #29 Black, #28 Medium Bronze, #26 Light Bronze and #18 Champagne)

Painted Finishes

Fluoropolymer Coatings – Enduring color with high performance and durability
Available in many standard choices and unlimited custom colors | These include Permafluor™ and Permadize® finishes, which meet or exceed AAMA 2604 or AAMA 2605

Powder Coatings – Create a "green" element with solvent-free high performance, durability and scratch resistance

Permacoat™ finishes available in 24 standard colors | Powder Coatings meet or exceed AAMA 2604

Notice

Laws and building and safety codes governing the design and use of windows, glazed entrance, framing, curtain wall and overhead glazing products vary widely. Kawneer does not control the selection of product configuration, operating hardware or glazing materials, and assumes no responsibility thereof.

Information contained in this catalog is subject to change without notice.

Other Products from Kawneer

Entrances & Framing 08 41 00; Windows 08 51 00

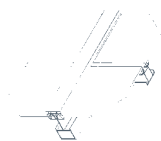
For More Information

Visit Kawneer.com for more detailed information, including the most current Guide Specifications, CAD libraries and BIM product models.

Overhead Glazing

1600 Sloped Glazing and 2000 Skylight

Curtain wall performance in overhead glazing systems
Stick system – fully factory fabricated | Silicone glazed or captured purlin options | Flush grid exterior has a 2-1/2" sightline | Various mullion depths to meet wind and snow loads | Handles hip and valley rafter applications



Cover photo:

1600 Wall System®1, 1600 Wall System®2, Trifab® VG (VersaGlaze®) 451T Framing, Trifab® VG (VersaGlaze®) 450 Framing, 350 Medium Stile Doors, Custom 1600 SunShade®, Alcoa – Reynobond® ACM (Aluminum Composite Material) Panels
Georgia Gwinnett College Library and Learning Center, Lawrenceville, Georgia
LEED® Gold certified
Architect: LEO A DALY, Atlanta, Georgia
Glazing Contractor: Glass Systems, Inc., Lithonia, Georgia

Kawneer Company, Inc.
Technology Park / Atlanta
555 Guthridge Court
Norcross, GA 30092

kawneer.com
kawneergreen.com
770 . 449 . 5555





Figure 1 - Malus Floribunda (Crab Apple)



Figure 2 - Prunus Campanulata (Flowering Cherry)



Figure 3 - Tibouchina Urvilleana (Princess Flower)



Figure 4 - Phormium Tenax (Maori Sunrise)



Figure 5 - Pieris 'Forest Flame'



Figure 6 - Rhododendron 'Creamy Chiffon'



Figure 7 - Rhododendron 'Elizabeth'



Figure 8 - Rhododendron 'Halfdan Lem'



Figure 9 - Rhododendron 'Paprika Spice'



Figure 10 - Rhododendron 'Patty Bee'



Figure 11 - Rhododendron 'Purple Splendour'



Figure 12 - Distictis Buccinatoria (Red Trumpet Vine)



Figure 13 - *Hardenbergia Violacea* 'Happy Wanderer' (Lilac Vine)



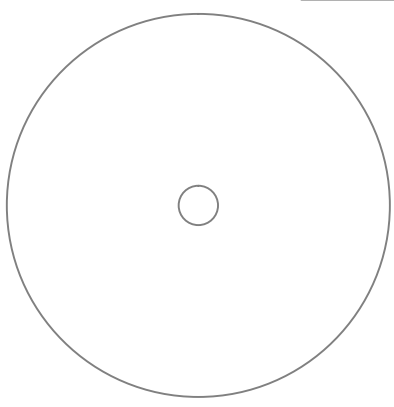
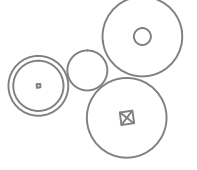
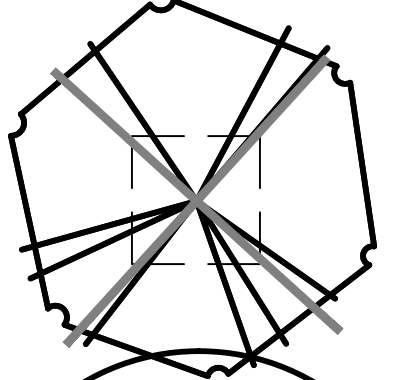
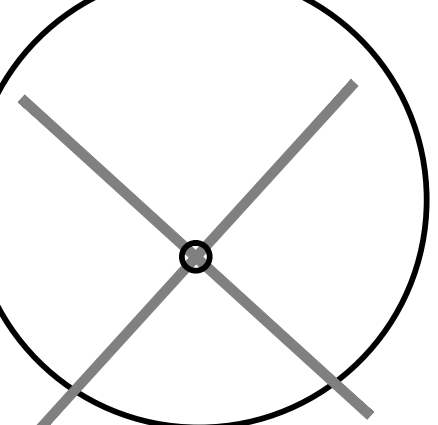
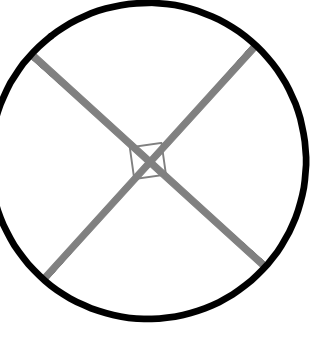
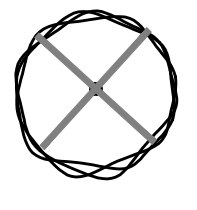
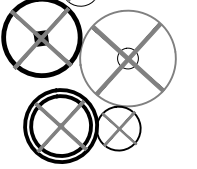
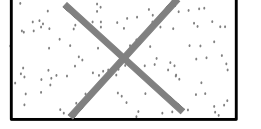
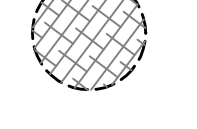
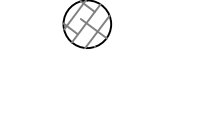

Figure 14 - *Pandorea Pandorana* 'Golden Shower' (Wonga Wonga Vine)



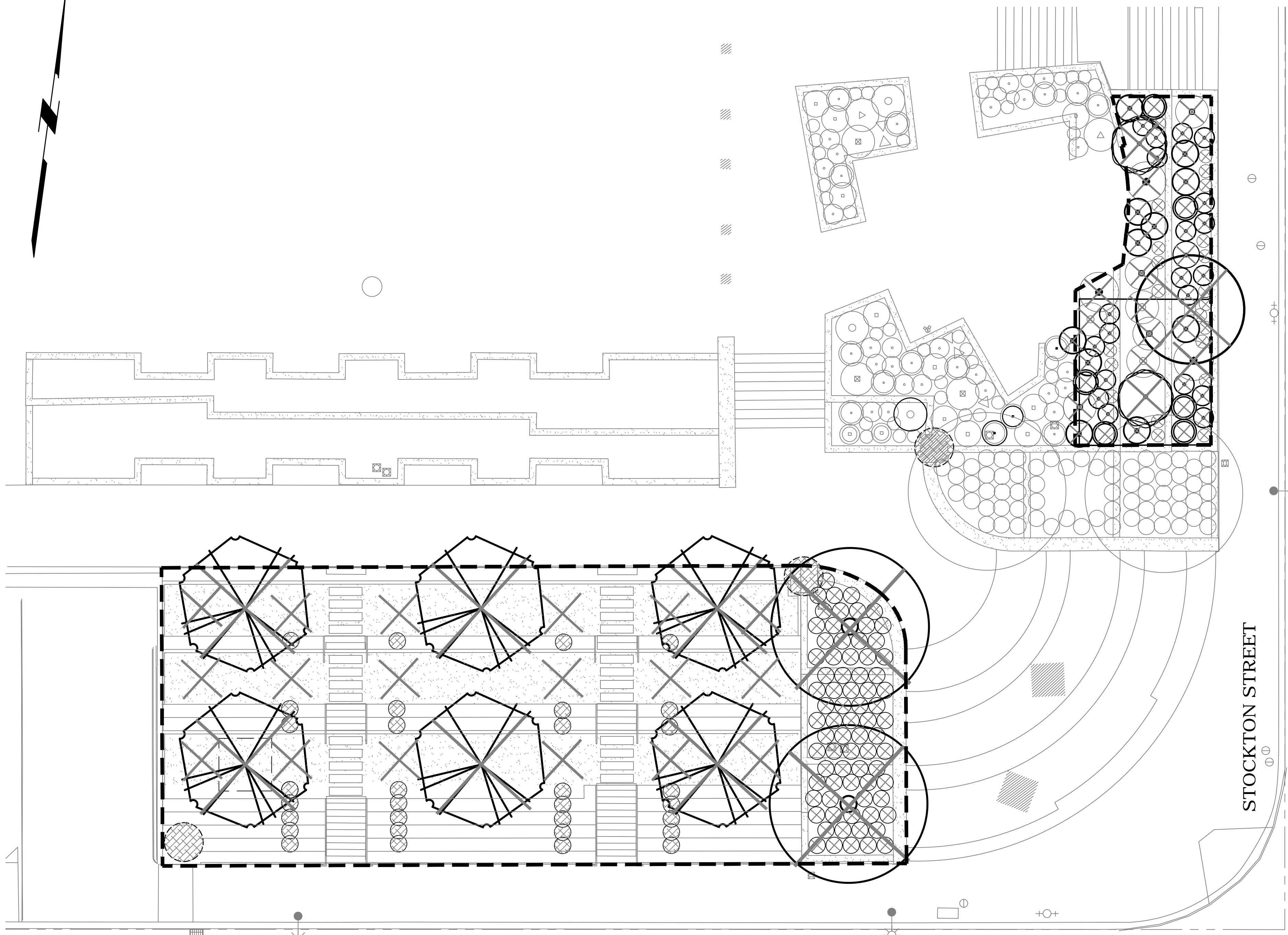
Figure 15 - *Parthenocissus Tricuspidata* (Boston Ivy)

- ◇ NOTES:
- PROTECT (E) PALM TREES DURING REMOVAL OF NORTH AND EAST PLANTER WALLS. SEE TREE PROTECTION SPECIFICATIONS FOR SPECIFIC REQUIREMENTS.

LEGEND

-  (E) TREES TO BE REMAIN / PROTECT IN PLACE.
-  (E) SHRUBS TO BE REMAIN.
-  (E) TREES AND TREE WELL TO BE REMOVED, SEE DEMOLITION SPECIFICATIONS.
-  (E) TREES TRANSPLANTED BY THE CONTRACTOR.
-  (E) TREES TO BE REMOVED, SEE DEMOLITION SPECIFICATIONS.
-  (E) TREES TO BE REMOVED, SEE DEMOLITION SPECIFICATIONS.
-  (E) SHRUBS TO BE REMOVED.
-  (E) LAWN TO BE REMOVED.
-  (E) DIA. CONCRETE PLANTER POTS WITH SEASONAL COLOR FLOWERS TO BE RELOCATED TO DPW STORAGE.
-  (E) DIA. CONCRETE PLANTER POTS WITH SEASONAL COLOR FLOWERS TO BE RELOCATED TO DPW STORAGE.
-  LIMIT OF WORK

10' 0 10' 20'
SCALE = 1"=10'



GEARY STREET

STOCKTON STREET

PLAN

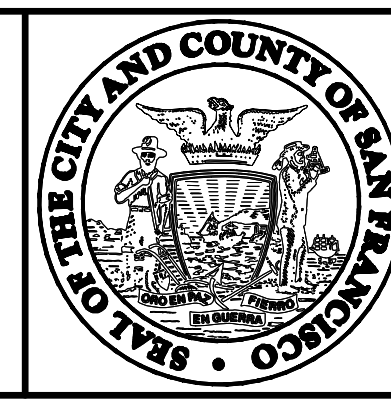
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DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID	0			

central subway design group

STEVENS & ASSOCIATES

DESIGNED: C. SHARMA
 DRAWN: C. SHARMA
 CHECKED: M. STEVENS
 REVIEWED: D. YAVORSKY
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 – CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

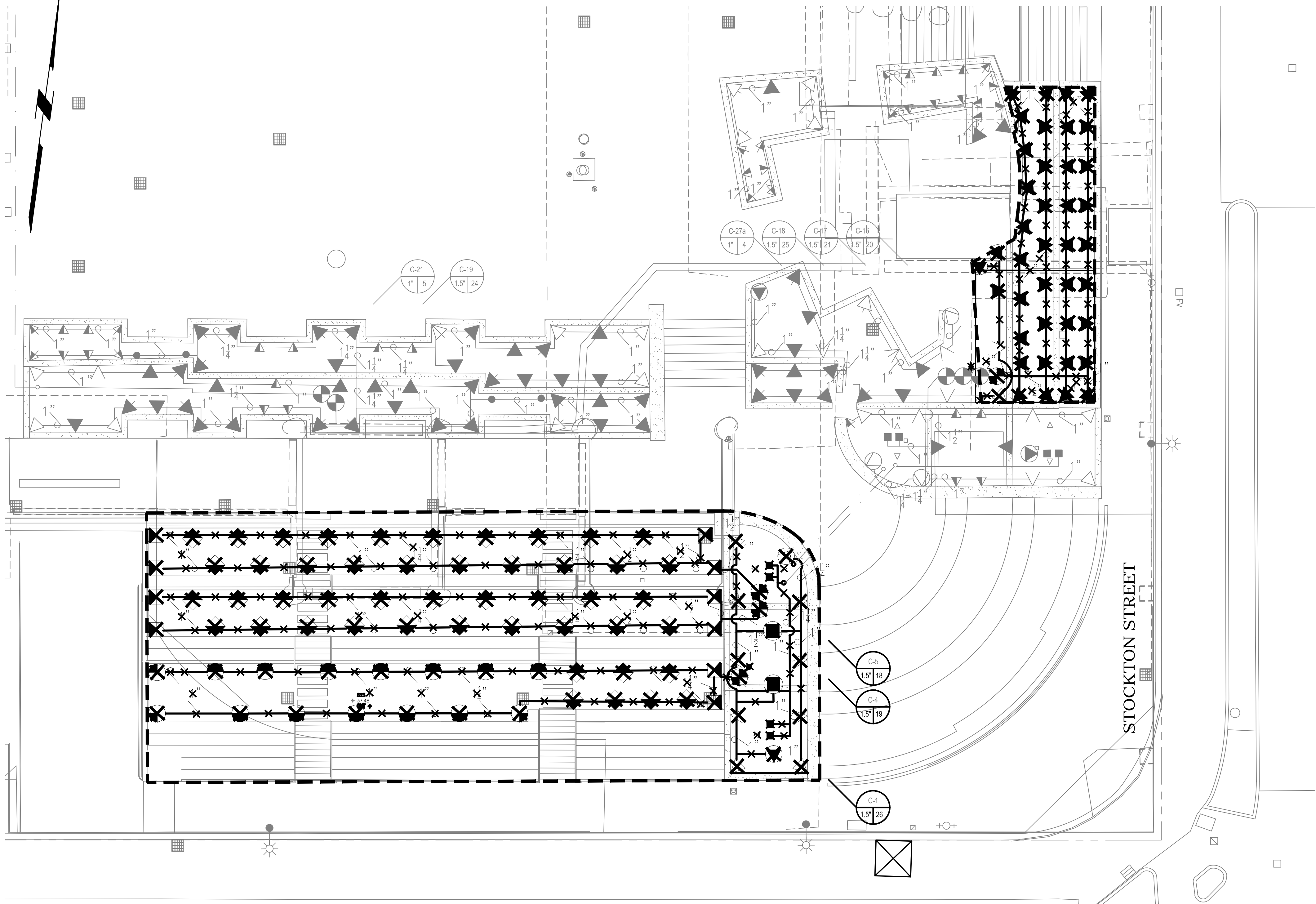
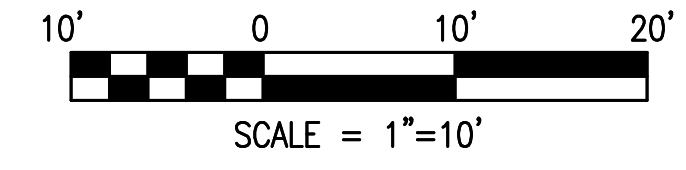
LANDSCAPING
 PLANTING DEMOLITION PLAN
 UNION SQUARE PLAZA

CONTRACT NO.	1253
SFMTA CONTROL NO.	CL-18379
DRAWING NO.	LA-301
SHEET NO.	0

- ◇ NOTES:
- PROTECT (E) PALM TREES DURING REMOVAL OF NORTH AND EAST PLANTER WALLS.

LEGEND

- ▽ (E) POP-UP SPRAY TO REMAIN.
- ▽ (E) POP-UP SPRAY TO REMAIN.
- (E) POP-UP SPRAY TO REMAIN.
- ▽ (E) POP-UP SPRAY TO REMAIN.
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- ▽ (E) POP-UP SPRAY TO REMAIN.
- (E) POP-UP SPRAY TO REMAIN.
- (E) BUBBLER TO REMAIN.
- ⊕ (E) REMOTE CONTROL VALVE TO REMAIN.
- ◇ (E) QUICK COUPLING VALVE TO REMAIN.
- (E) MAINLINE TO REMAIN.
- - - (E) LATERAL LINE TO REMAIN.
- (E) CONTROLLER AND STATION NUMBER TO REMAIN.
- (E) FLOW (GPM) TO REMAIN.
- (E) REMOTE CONTROL VALVE SIZE TO REMAIN.
- ✕ (E) POP-UP SPRAY TO BE REMOVED.
- ✕ (E) POP-UP SPRAY TO BE REMOVED.
- ✕ (E) POP-UP SPRAY TO BE REMOVED.
- ✕ (E) POP-UP SPRAY TO BE REMOVED.
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- ✕ (E) POP-UP SPRAY TO BE REMOVED.
- ✕ (E) REMOTE CONTROL VALVE TO BE REMOVED.
- ✕ (E) QUICK COUPLING VALVE TO BE REMOVED.
- ✕ (E) LATERAL LINE TO BE REMOVED.
- (E) CONTROLLER AND STATION NUMBER TO REMAIN.
- (E) FLOW (GPM) TO BE REVISED.
- (E) REMOTE CONTROL VALVE SIZE TO BE REVISED.
- - - (E) LIMIT OF WORK.



PLAN

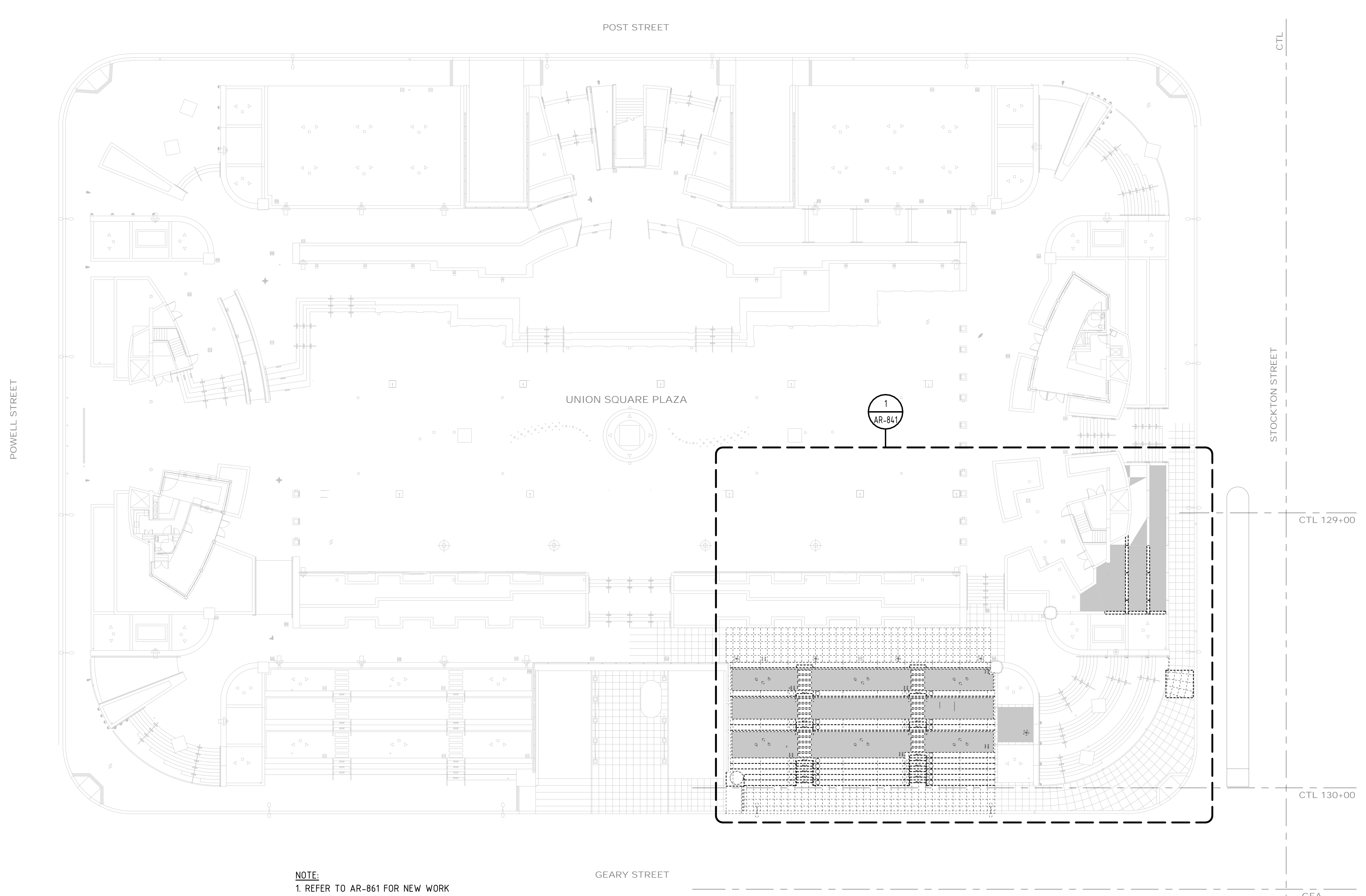
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02/15/2012 ISSUED FOR BID DATE DESCRIPTION REV NO. BY CHECKED APPROVED	 	DESIGNED: C. SHARMA DRAWN: C. SHARMA CHECKED: M. STEVENS REVIEWED: D. YAVORSKY RECOMMENDED: A. READ APPROVED: R. EDWARDS DATE: 02/15/2012		CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED DIRECTOR OF TRANSPORTATION	THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION LANDSCAPING IRRIGATION DEMOLITION PLAN UNION SQUARE PLAZA	CONTRACT NO. 1253 SFMTA CONTROL NO. CL-18380 DRAWING NO. LA-302 SHEET NO. 0
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BORDER REVISED 11/04/2011
 WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 49 CFR PARTS 15 AND 1520. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSON WITHOUT A "NEED TO KNOW", AS DEFINED IN 49 CFR PARTS 15 AND 1520, EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION.

2/23/2012 11:07:15 AM

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NOTE:
1. REFER TO AR-861 FOR NEW WORK

OVERALL PLAN - UNION SQUARE ENTRANCE

1
AR-840
1" = 20'-0"

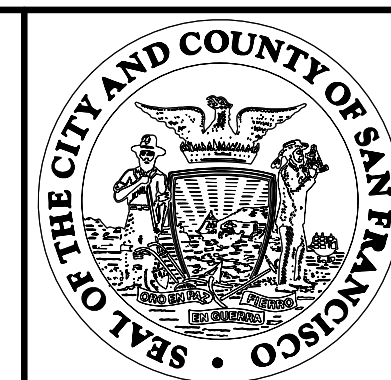


DATE	ISSUED FOR BID	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID		0			

central subway design group

Robin Chiang & Company

DESIGNED
R. CHIANG
DRAWN
J. GAINES
CHECKED
D. FUNG
REVIEWED
R. CHIANG
RECOMMENDED
A. READ
APPROVED
R. EDWARDS
DATE
02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

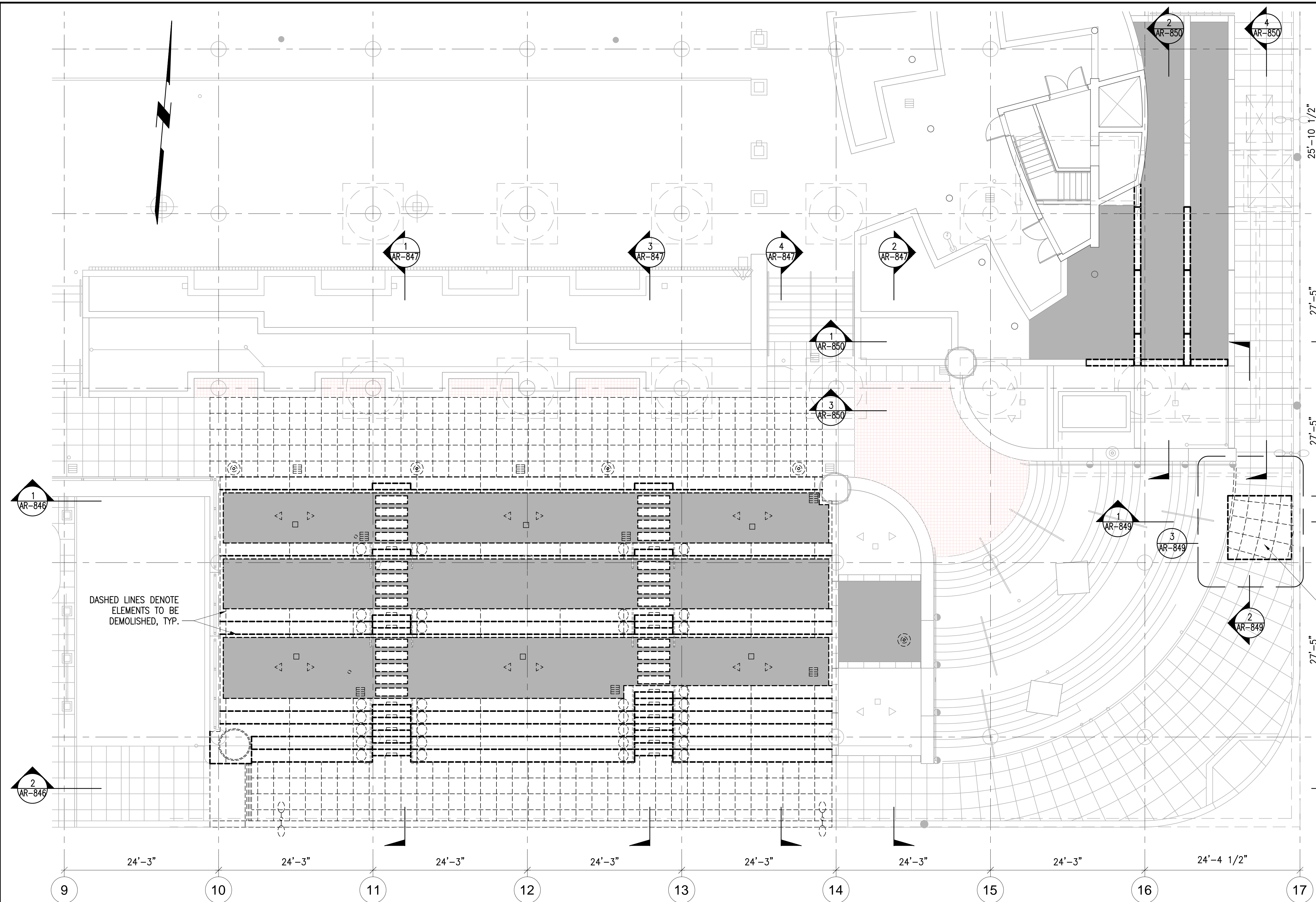
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE / MARKET STREET STATION

ARCHITECTURAL
SITE PLAN - EXISTING
UNION SQUARE GARAGE

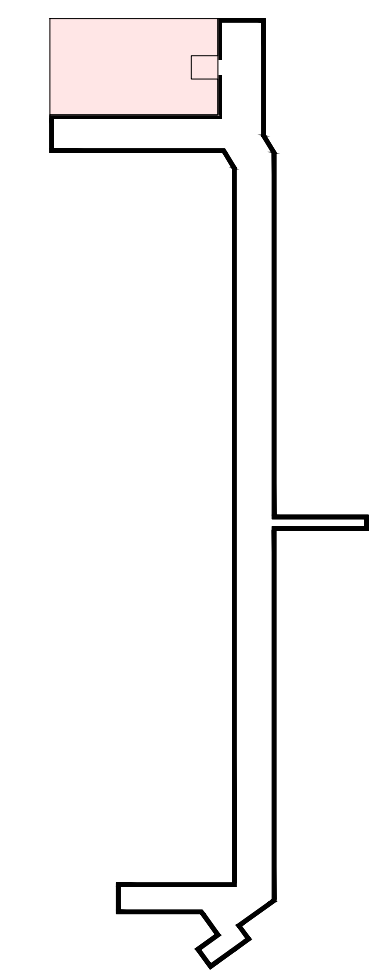
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DRAWING NO.	AR-840
SHEET NO.	0
REVISION	0

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 jgaines Thu Feb 23, 2012 11:03 am AR-841 DEMOLITION PLAN



GENERAL SHEET NOTES:

1. REFER TO STRUCTURAL DEMO DWGS FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS FOR GARAGE CONCRETE FLOOR & ROOF SLABS, COLUMNS & FOOTINGS, AND WALLS, TYP
2. REFER TO ELECTRICAL DEMO DWGS FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS FOR ALL ELECTRICAL LIGHTING, POWER, AND FIRE ALARM DEVICES, TYP
3. REFER TO MECHANICAL, HVAC, AND PLUMBING DEMO DRAWINGS FOR ADDITIONAL HVAC AIR AND PLUMBING REMOVAL REQUIREMENTS, TYP
4. CROSS REFERENCE CIVIL DWGS CV-101 THROUGH CV-103 & CV-120 FOR DESCRIPTIONS OF ITEMS TO BE DEMOLISHED.
5. SHADED AREAS DENOTE LANDSCAPING AREAS TO BE REMOVED. SEE 'LA' DWGS, TYP, FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS.
6. ANY DISCREPANCIES BETWEEN THE VARIOUS DISCIPLINE DEMO DWGS SHALL BE RECONCILED BEFORE PROCEEDING, TYP.



KEY PLAN



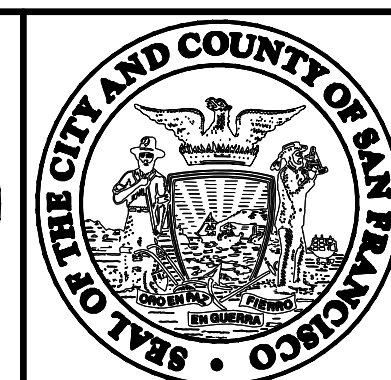
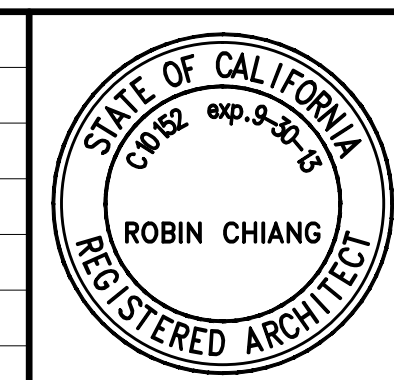
SURFACE LEVEL

1 DEMOLITION PLAN
 AR-841 SCALE: 1/8"=1'-0"

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID	0			

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

Robin Chiang & Company



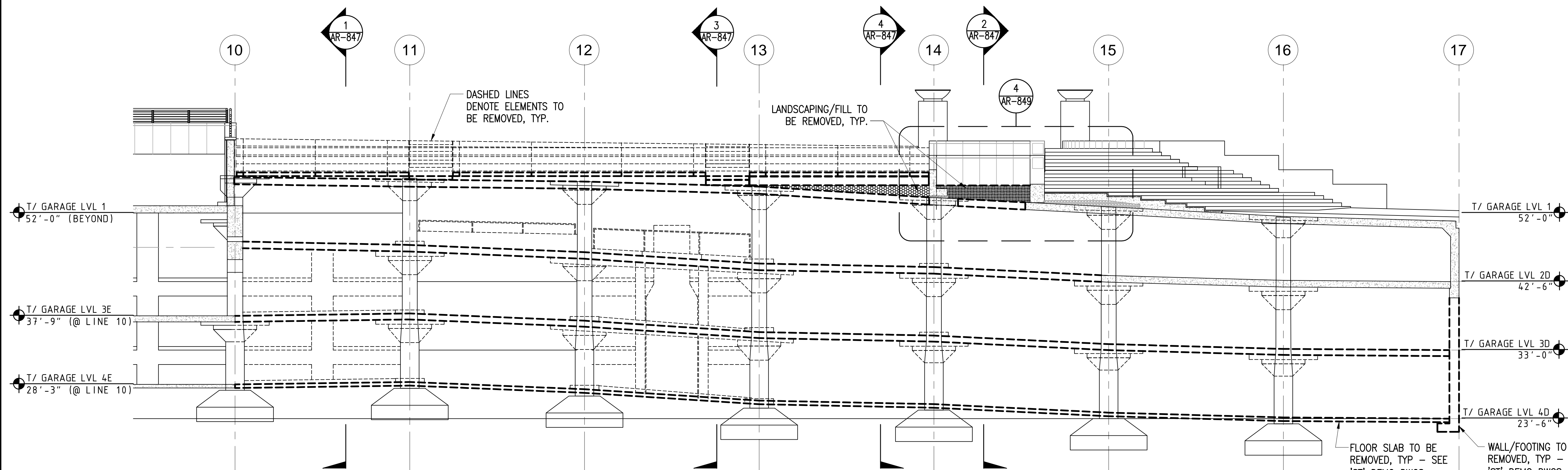
CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION
ARCHITECTURAL DEMOLITION PLAN
 UNION SQUARE GARAGE - SURFACE LEVEL

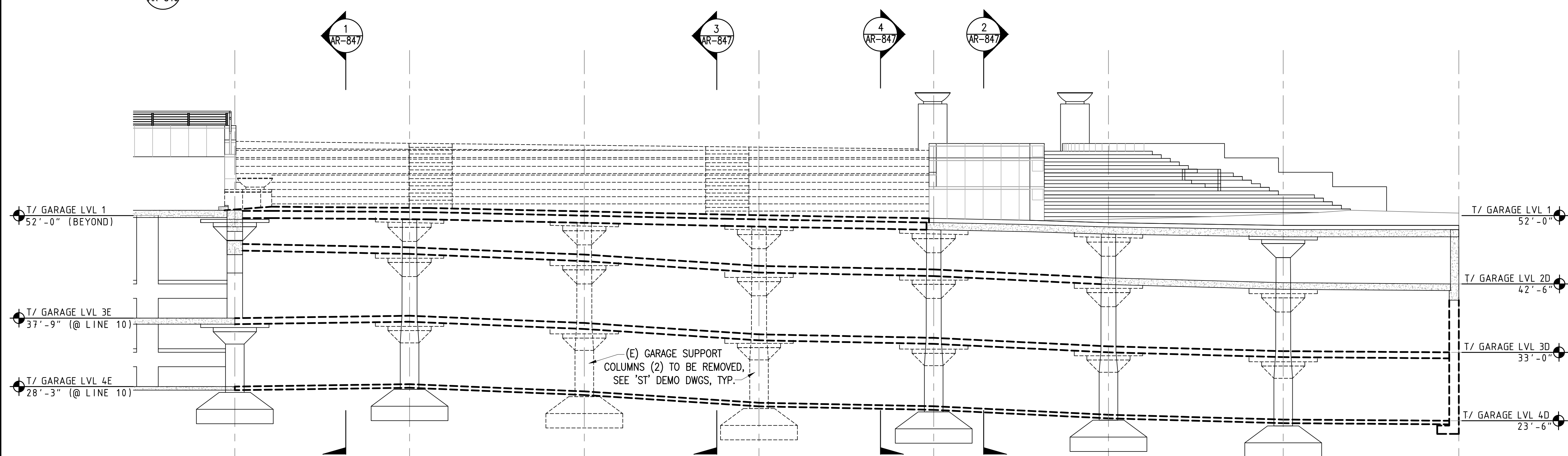
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SFMTA CONTROL NO.	CL-18390
DRAWING NO.	AR-841
SHEET NO.	0

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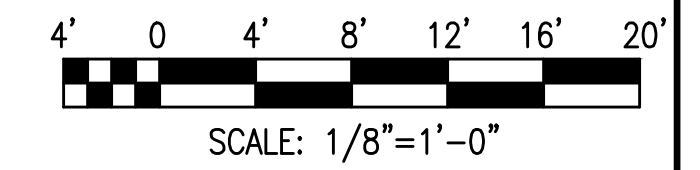
- REFER TO STRUCTURAL DEMO DWGS FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS FOR GARAGE CONCRETE FLOOR & ROOF SLABS, COLUMNS & FOOTINGS, AND WALLS, TYP
- REFER TO ELECTRICAL DEMO DWGS FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS FOR ALL ELECTRICAL LIGHTING, POWER, AND FIRE ALARM DEVICES, TYP
- REFER TO MECHANICAL, HVAC, AND PLUMBING DEMO DRAWINGS FOR ADDITIONAL HVAC AIR AND PLUMBING REMOVAL REQUIREMENTS, TYP
- CROSS REFERENCE CIVIL DWGS CV-101 THROUGH CV-103 & CV-120 FOR DESCRIPTIONS OF ITEMS TO BE DEMOLISHED.
- FOR LANDSCAPING AREAS TO BE REMOVED SEE 'LA' DWGS, TYP, FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS.
- ANY DISCREPANCIES BETWEEN THE VARIOUS DISCIPLINE DEMO DWGS SHALL BE RECONCILED BEFORE PROCEEDING, TYP.



1
SECTION
NORTH ENTRANCE DEMOLITION (BTWN GRIDLINES B&C)



2
SECTION
NORTH ENTRANCE DEMOLITION (@ SIDEWALK BEYOND GRIDLINE A)



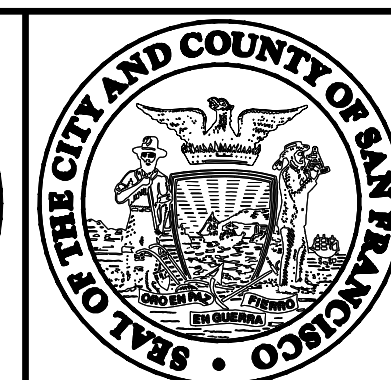
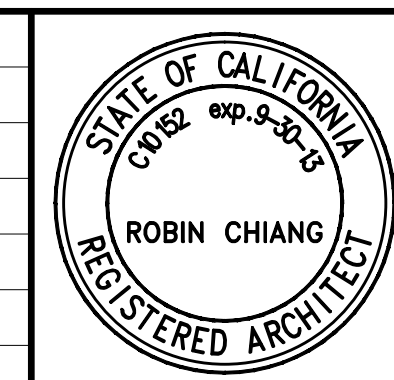
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 jgaines Thu Feb 23, 2012 3:07 pm AR-846 DEMOLITION SECTIONS

02/15/2012	ISSUED FOR BID	0			
DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED

central subway design group

Robinson Chiang & Company

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

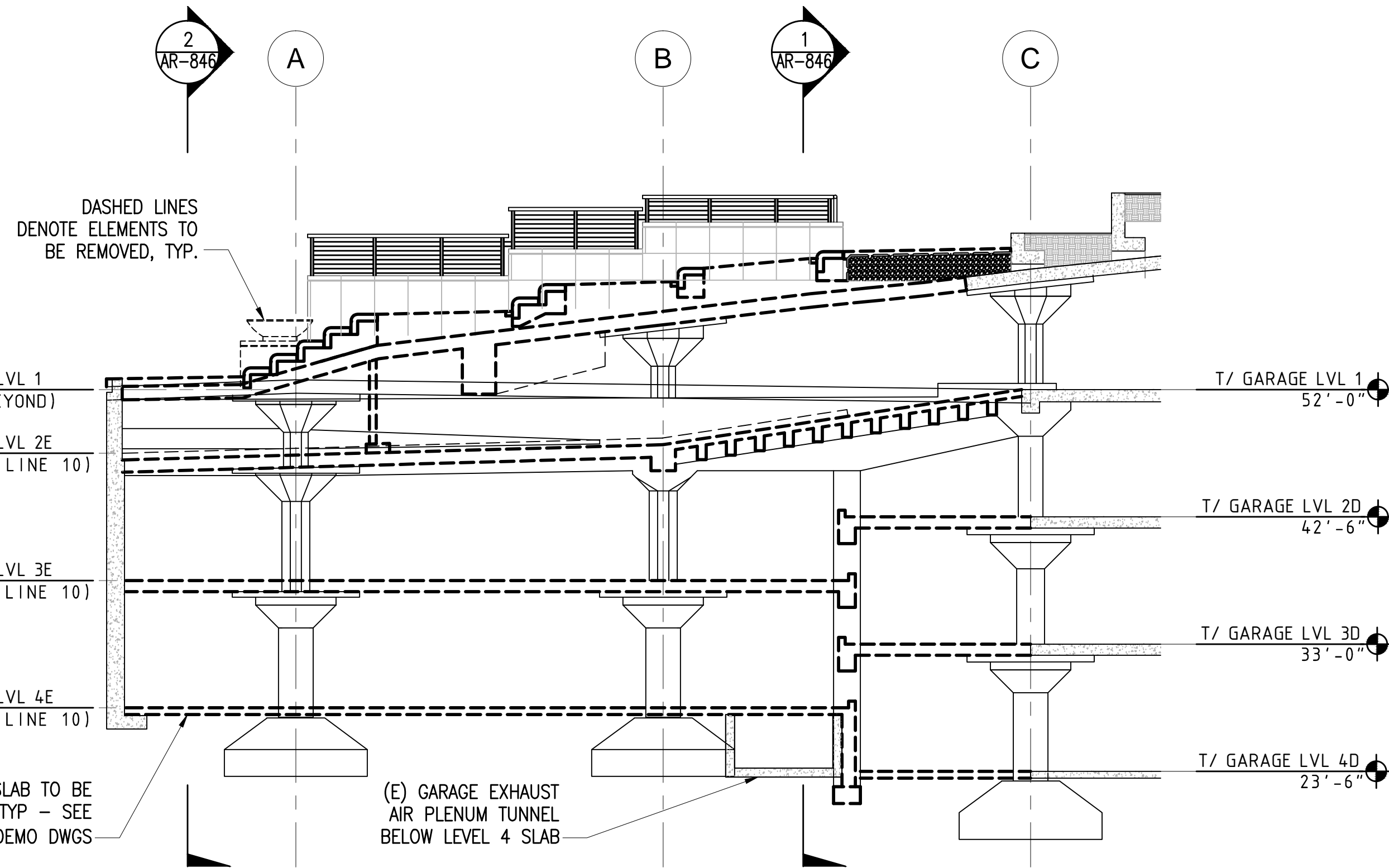
THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 DEMOLITION SECTIONS
 UNION SQUARE GARAGE - SHEET 1 OF 2

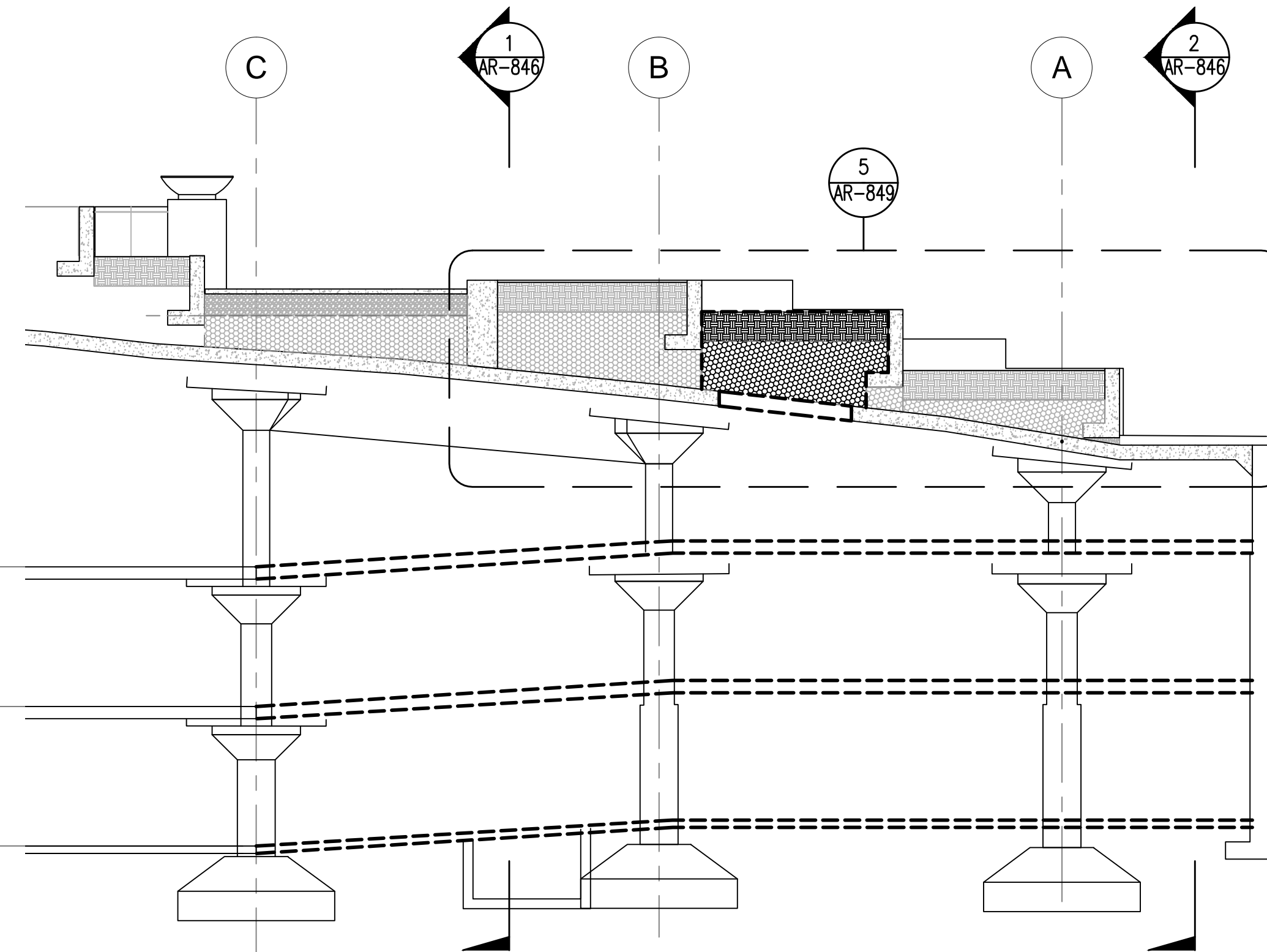
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DRAWING NO.	AR-846
SHEET NO.	0

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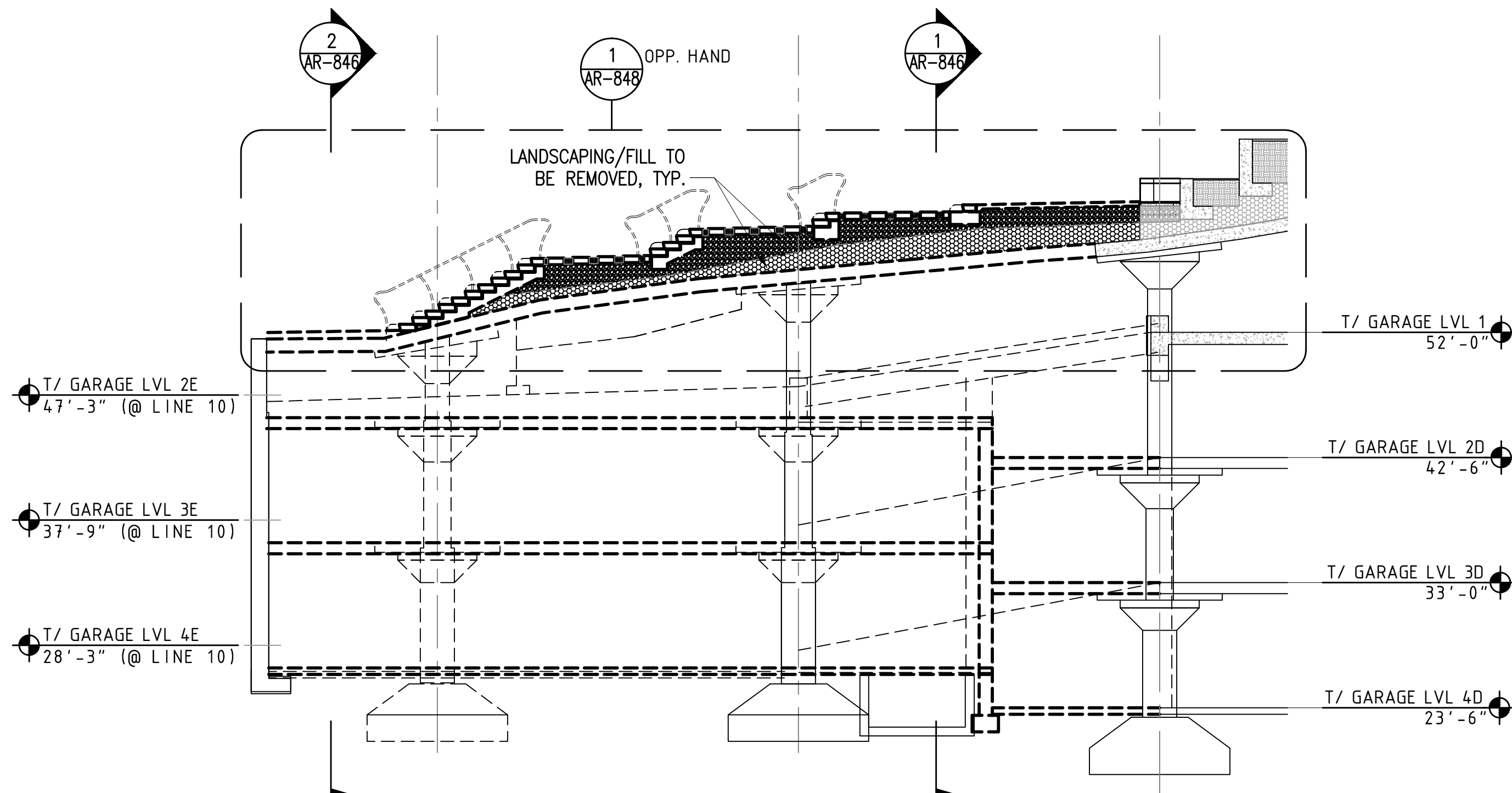
1. REFER TO STRUCTURAL DEMO DWGS FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS FOR GARAGE CONCRETE FLOOR & ROOF SLABS, COLUMNS & FOOTINGS, AND WALLS, TYP
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6. ANY DISCREPANCIES BETWEEN THE VARIOUS DISCIPLINE DEMO DWGS SHALL BE RECONCILED BEFORE PROCEEDING, TYP.



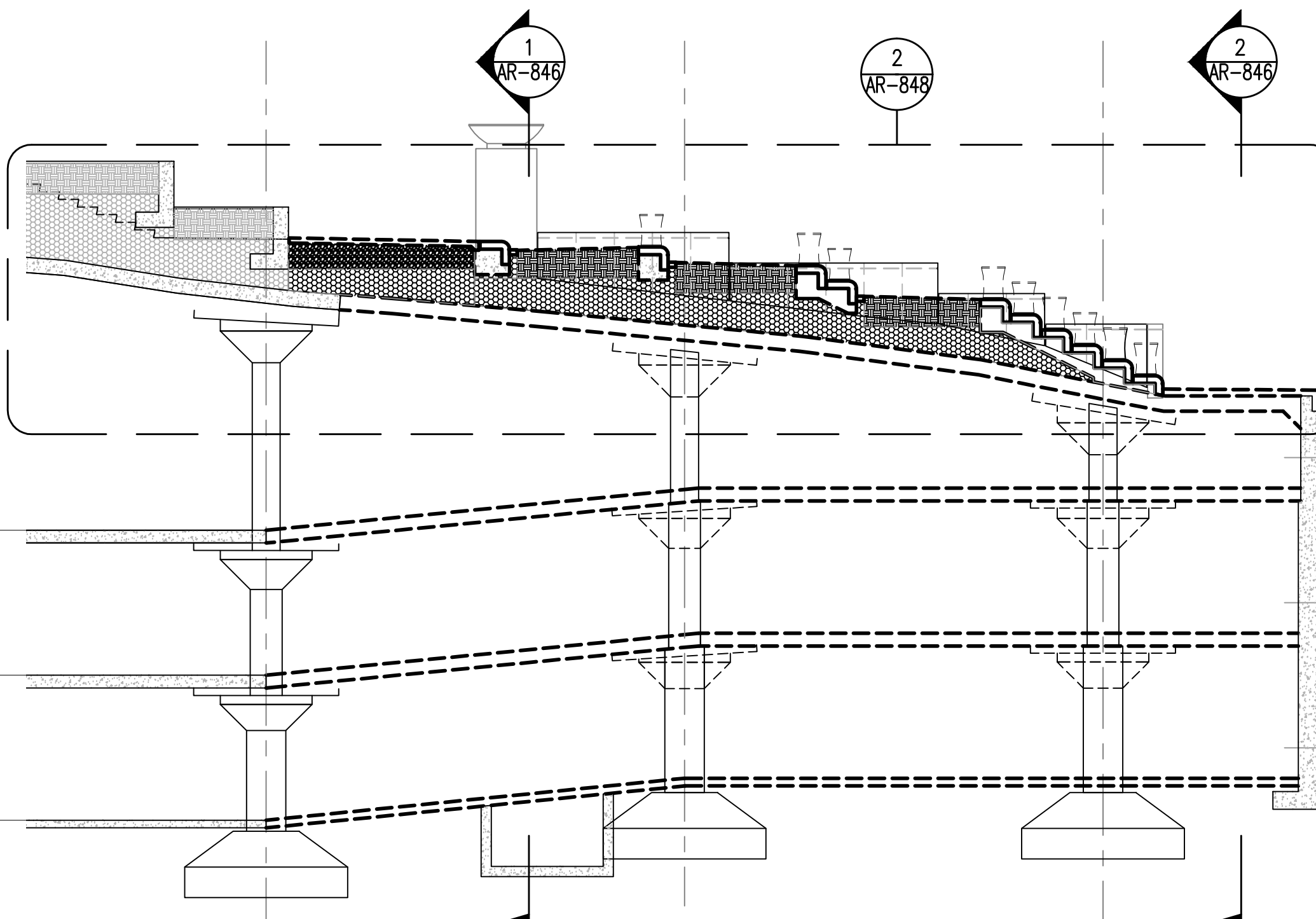
1 SECTION
NORTH ENTRANCE DEMOLITION (WEST @ TERRACES BTWN GRIDLINES 10&11)



2 SECTION
NORTH ENTRANCE DEMOLITION (EAST @ PLANTERS BTWN GRIDLINES 14&15)



3 SECTION
NORTH ENTRANCE DEMOLITION (WEST @ STAIRS BTWN GRIDLINES 12&13)



4 SECTION
NORTH ENTRANCE DEMOLITION (EAST @ SEATING TERRACES BTWN GRIDLINES 13&14)



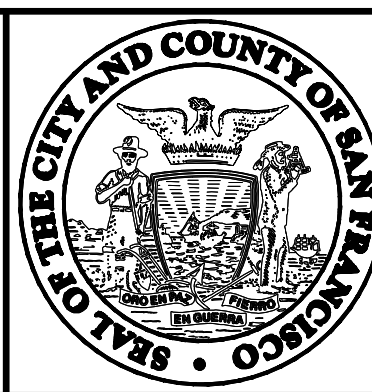
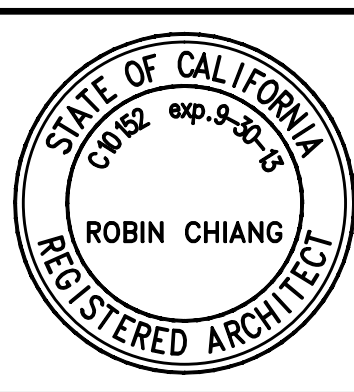
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 jgaines Thu Feb 23, 2012 3:07 pm AR-847 DEMOLITION SECTIONS

DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID	0			

central subway design group
 Robin Chiang & Company

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

DIRECTOR OF TRANSPORTATION

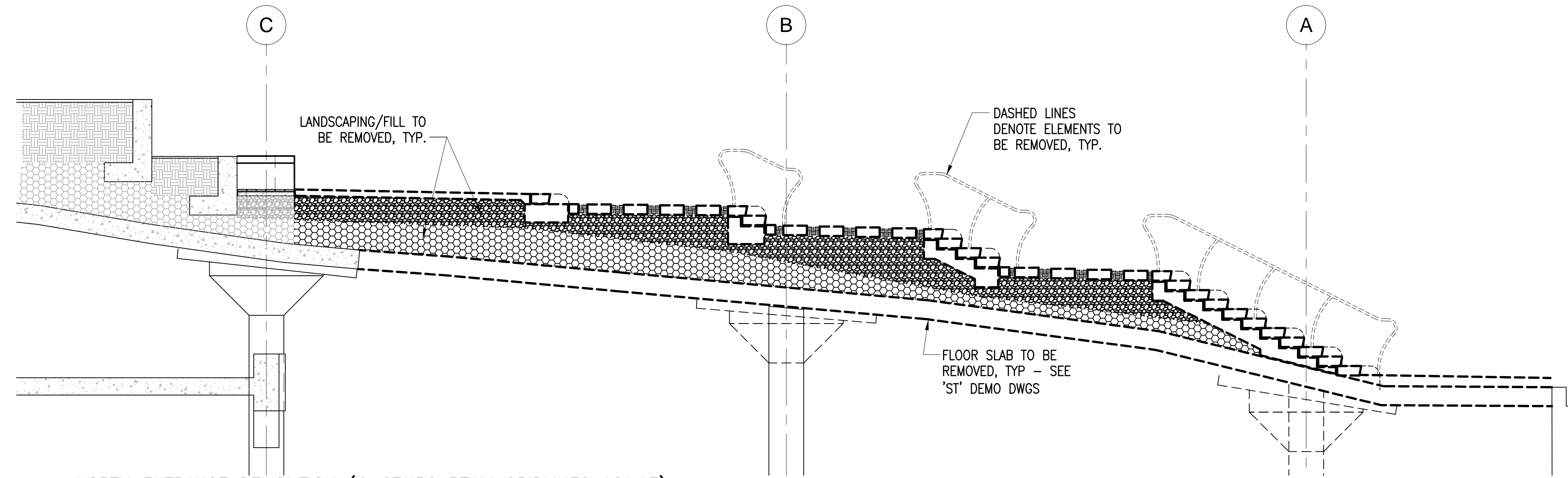
THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 DEMOLITION SECTIONS
 UNION SQUARE GARAGE - SHEET 2 OF 2

CONTRACT NO.	1253
SFMTA CONTROL NO.	CL-18396
DRAWING NO.	AR-847
SHEET NO.	0

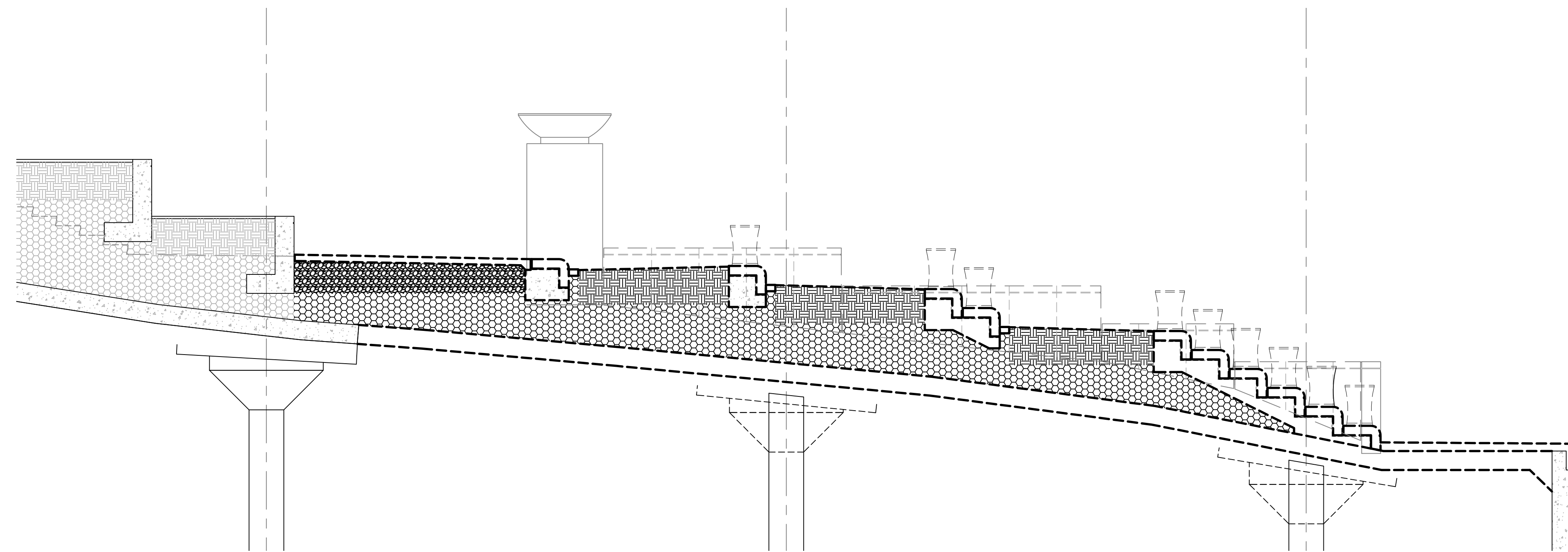
GENERAL NOTES:

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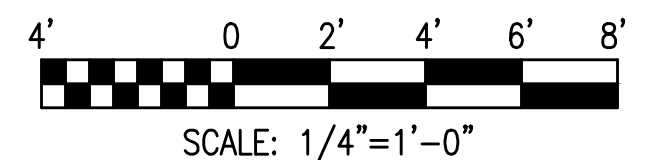
NORTH ENTRANCE DEMOLITION (@ STAIRS BTWN GRIDLINES 12&13)

1 SECTION
AR-848



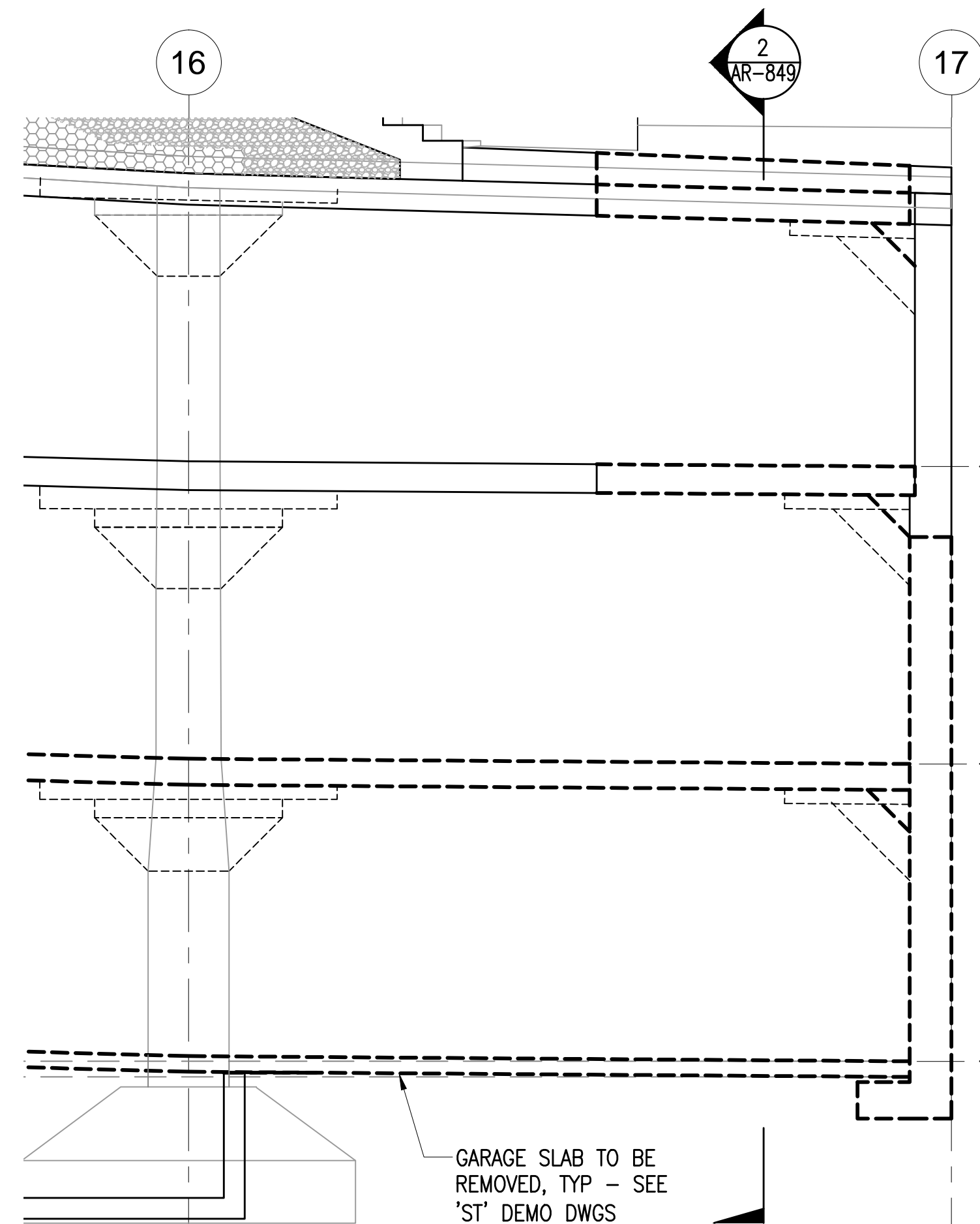
NORTH ENTRANCE DEMOLITION (@ SEATING TERRACES BTWN GRIDLINES 13&14)

2 SECTION
AR-848

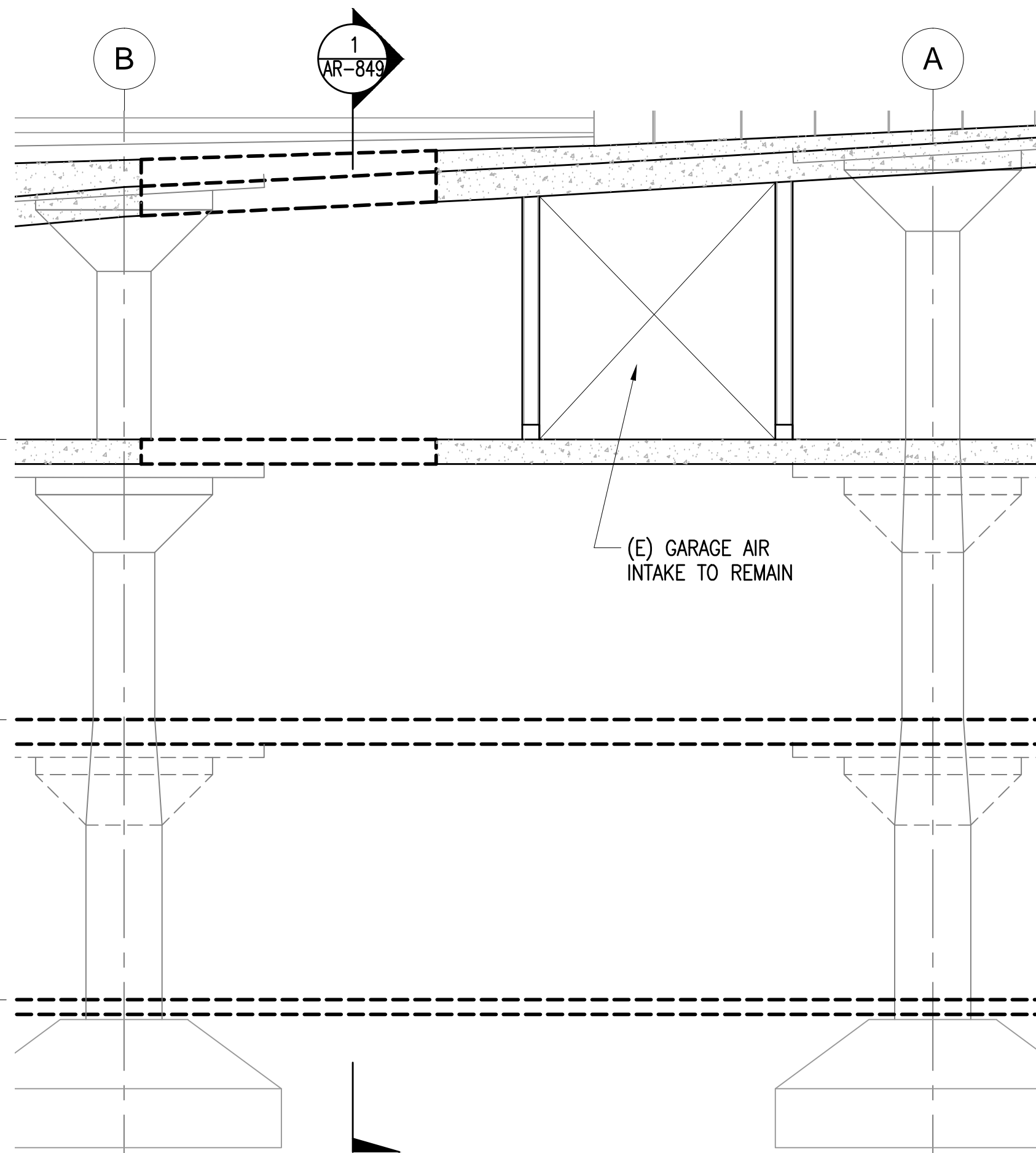


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 jgaines Thu Feb 23, 2012 3:07 pm AR-848 ENLARGED DEMOLITION SECTIONS

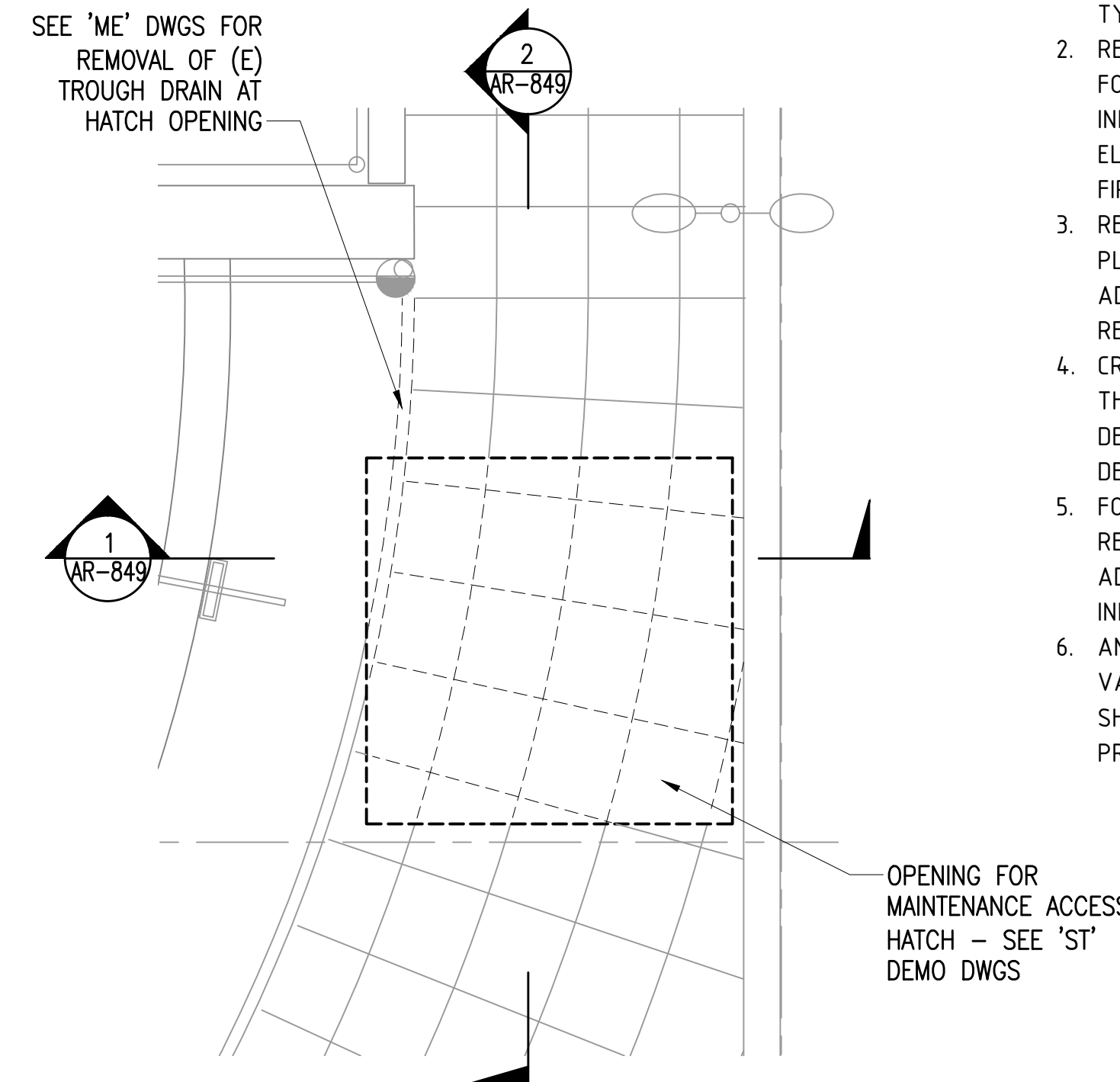
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DATE DESCRIPTION		REV NO. BY CHECKED APPROVED		REVISION		SHEET NO.		SFMTA CONTROL NO. CL-19428		
central subway design group				Robin Chiang & Company				DRAWING NO. AR-848		REVISION 0
DESIGNED: R. CHIANG DRAWN: J. GAINES CHECKED: D. FUNG REVIEWED: R. CHIANG RECOMMENDED: A. READ APPROVED: R. EDWARDS DATE: 02/15/2012								CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED DIRECTOR OF TRANSPORTATION		THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION ARCHITECTURAL ENLARGED DEMOLITION SECTIONS UNION SQUARE GARAGE - PLAZA LEVEL



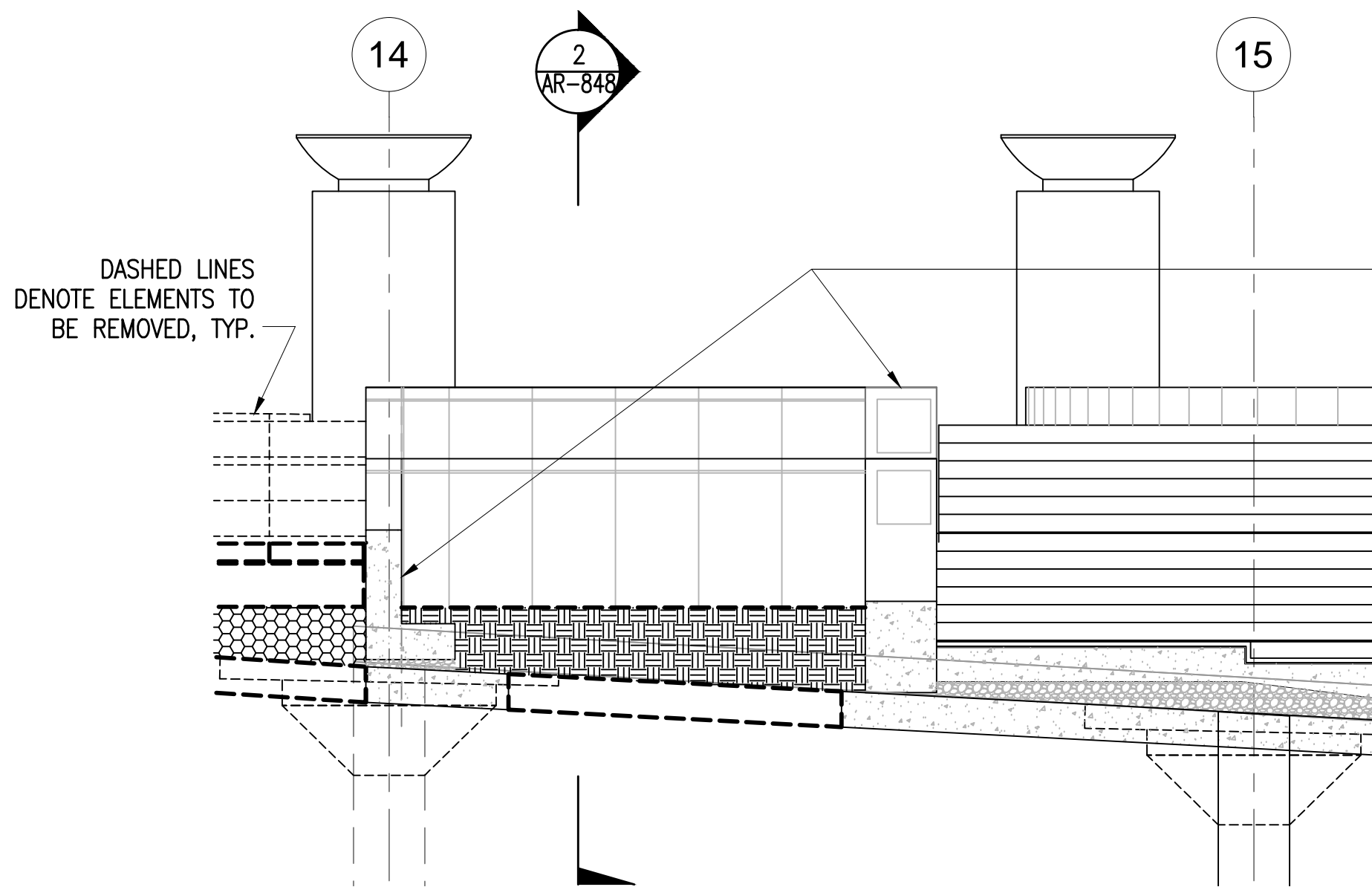
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AR-849
MAINTENANCE HATCH DEMOLITION (BTWN GRIDLINES B&C) SECTION



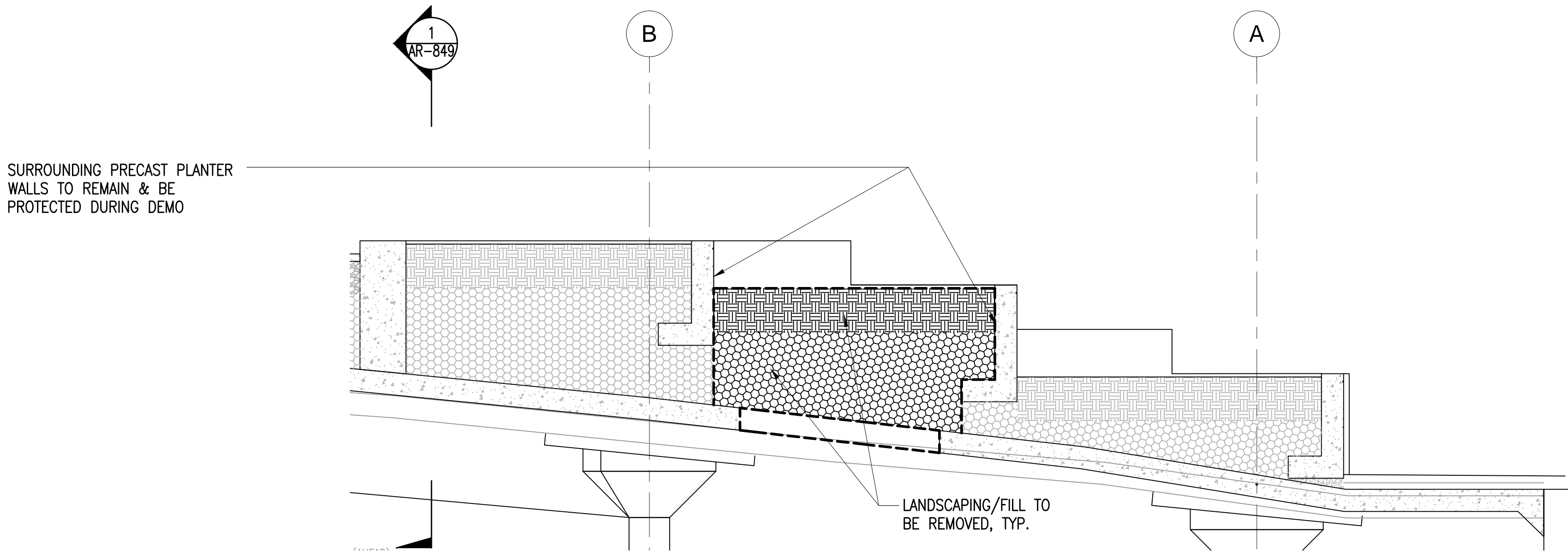
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AR-849
MAINTENANCE HATCH DEMOLITION (BTWN GRIDLINES 16&17) SECTION



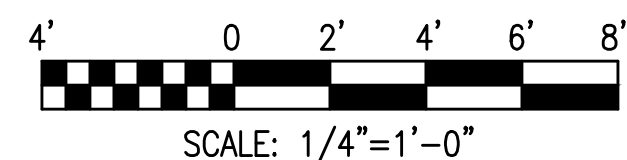
3
AR-849
MAINTENANCE HATCH DEMOLITION AT SURFACE LEVEL PLAN



4
AR-849
NORTH ENTRANCE DEMOLITION (BTWN GRIDLINES B&C) SECTION



5
AR-849
NORTH ENTRANCE DEMOLITION (@ PLANTERS BTWN GRIDLINES 14&15) SECTION


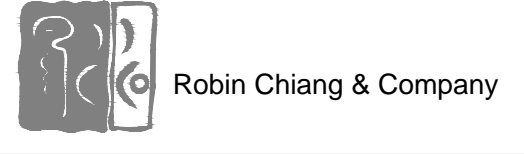


GENERAL NOTES:

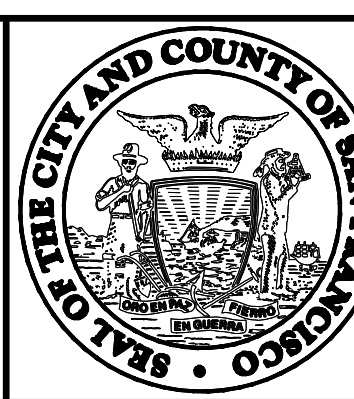
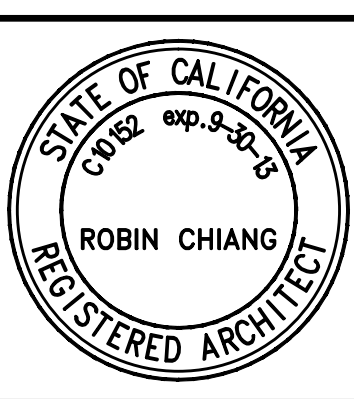
1. REFER TO STRUCTURAL DEMO DWGS FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS FOR GARAGE CONCRETE FLOOR & ROOF SLABS, COLUMNS & FOOTINGS, AND WALLS, TYP
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\\sv6as003\m544.1\FinalDesign\DRAWINGS\CS155-2\1253\SHEET FILES\04_ARCHITECTURAL\01_ARCHITECTURAL\12530401AR849.dwg
 jgaines Thu Feb 23, 2012 3:07 pm AR-849 ENLARGED DEMOLITION PLANS & SECTIONS

DATE	ISSUED FOR	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID	0			


 central subway design group

 Robin Chiang & Company

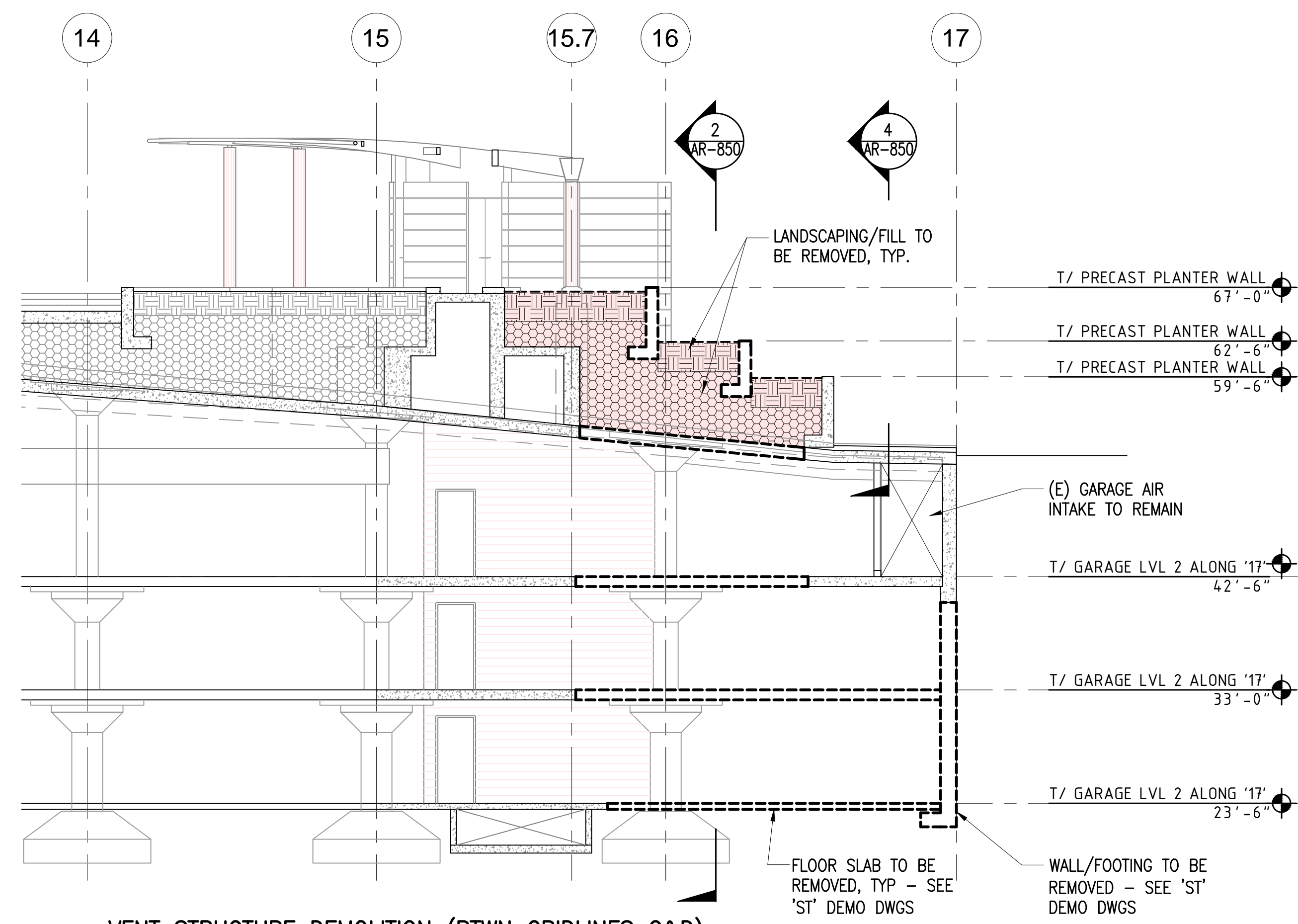
DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



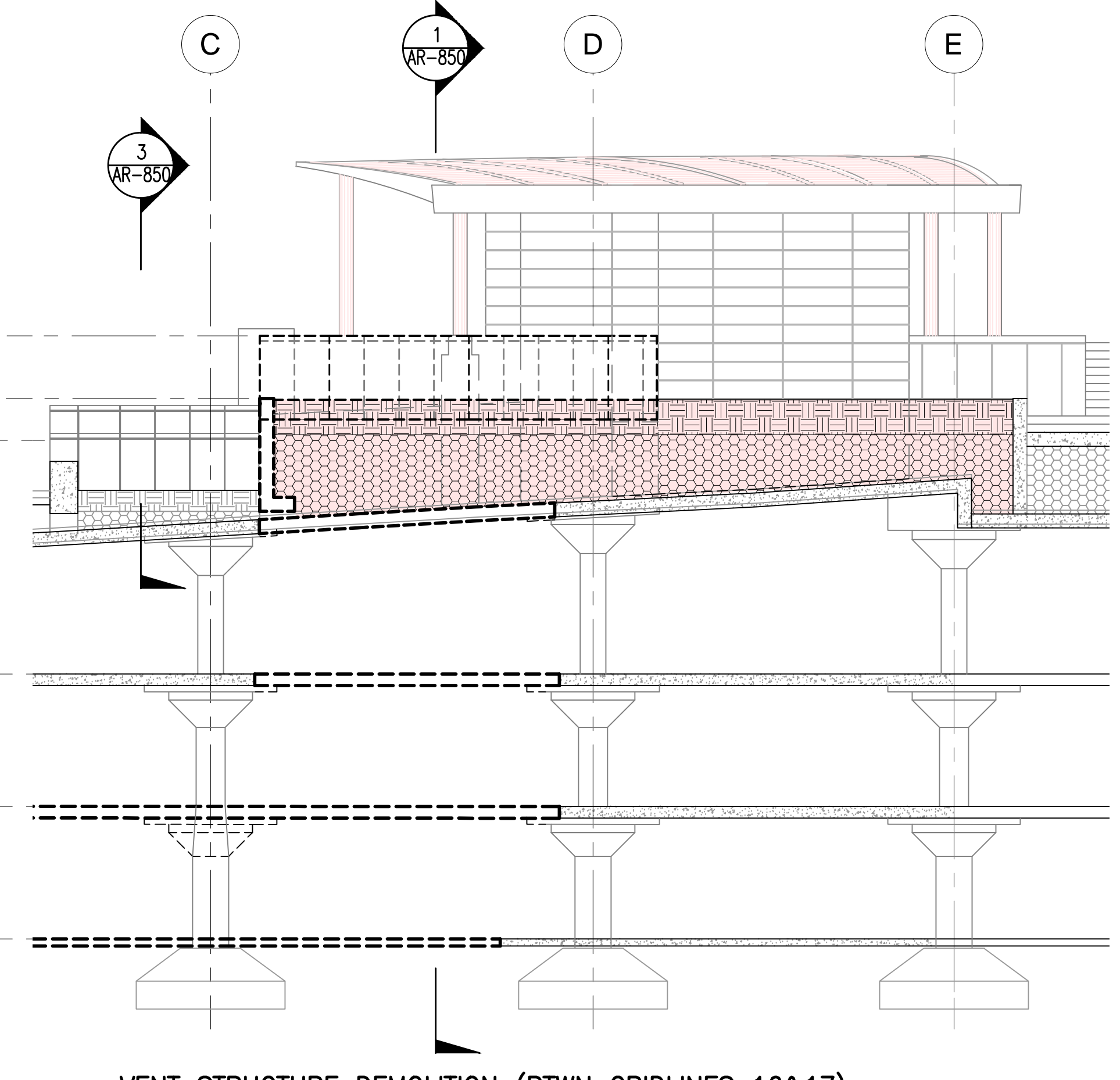
CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION
 ARCHITECTURAL
 ENLARGED DEMOLITION PLANS & SECTIONS
 MAINTENANCE ACCESS SHAFT & PLAZA LEVEL

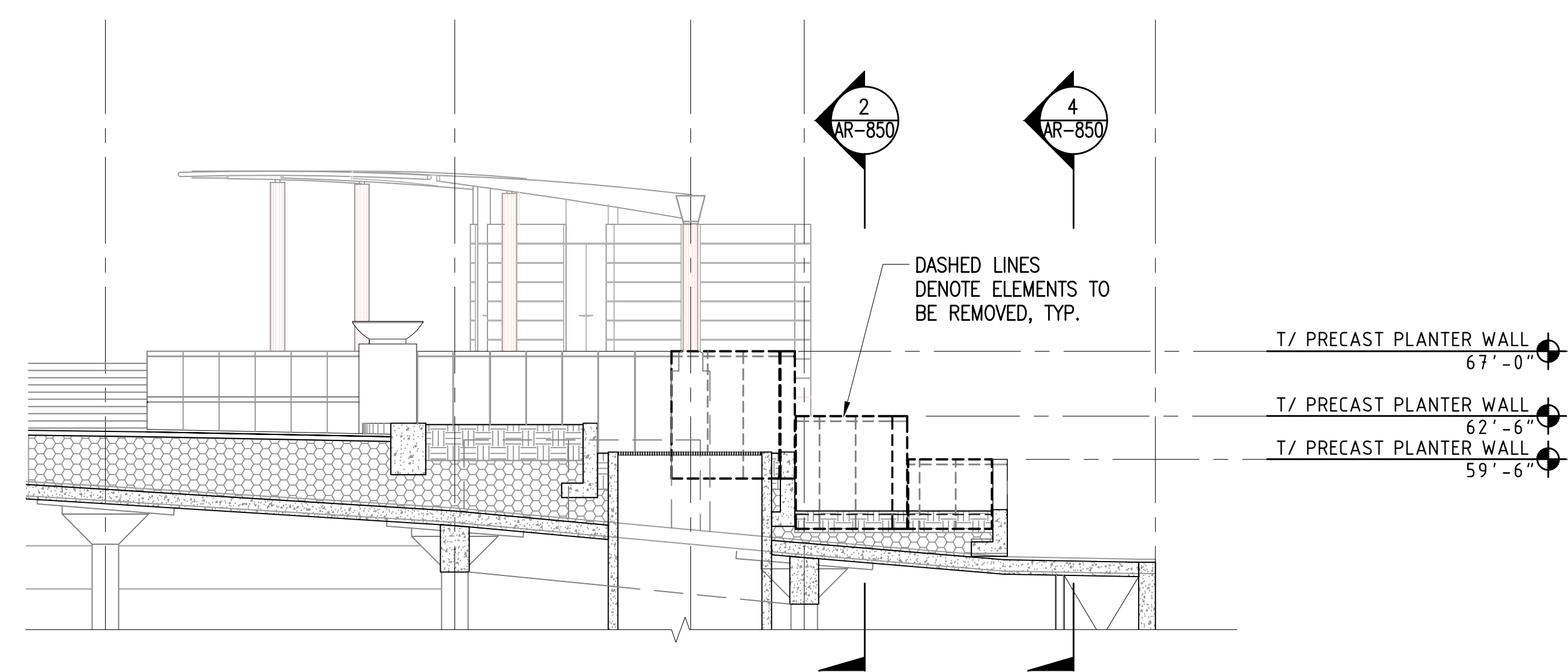
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SFMTA CONTROL NO.	CL-19429
DRAWING NO.	AR-849
SHEET NO.	0



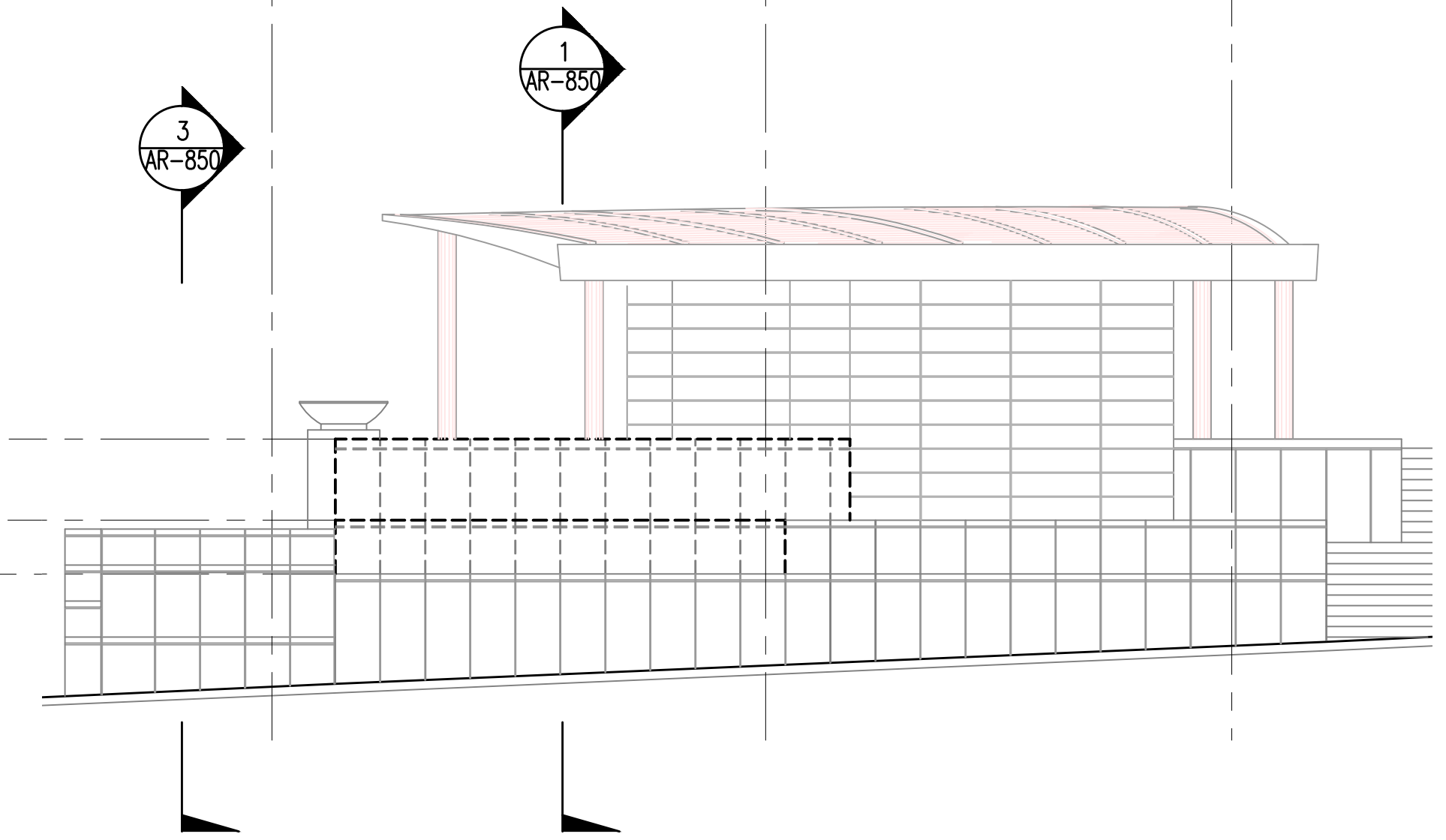
1
AR-850
VENT STRUCTURE DEMOLITION (BTWN GRIDLINES C&D) SECTION



2
AR-850
VENT STRUCTURE DEMOLITION (BTWN GRIDLINES 16&17) SECTION



3
AR-850
VENT STRUCTURE DEMOLITION (BTWN GRIDLINES B&C) SECTION



4
AR-850
VENT STRUCTURE DEMOLITION (THROUGH SIDEWALK) SECTION

GENERAL SHEET NOTES:

1. REFER TO STRUCTURAL DEMO DWGS FOR ADDITIONAL DEMOLITION INFO/REQUIREMENTS FOR GARAGE CONCRETE FLOOR & ROOF SLABS, COLUMNS & FOOTINGS, AND WALLS, TYP
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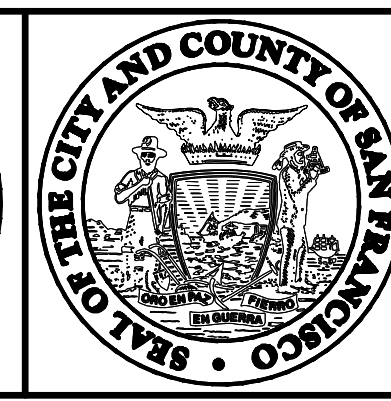
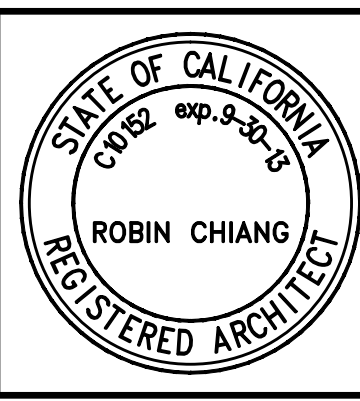


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 jgaines Thu Feb 23, 2012 3:08 pm AR-850 DEMOLITION SECTIONS

02/15/2012	ISSUED FOR BID	0			
DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED

central subway design group
 Robin Chiang & Company

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
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 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

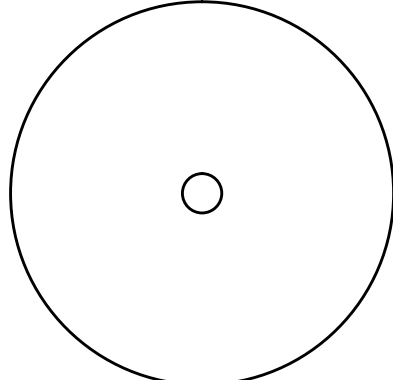
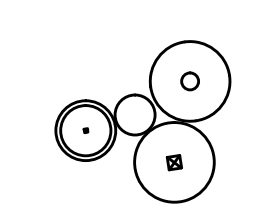
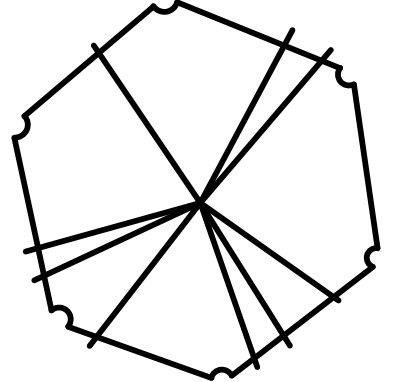
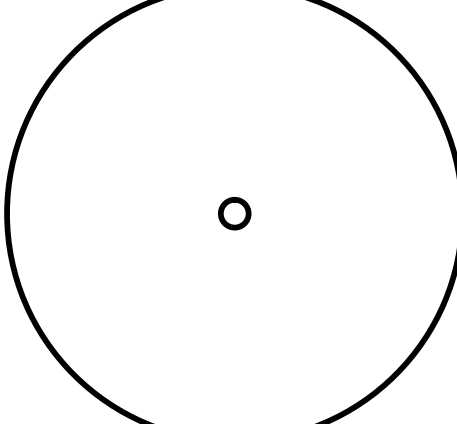
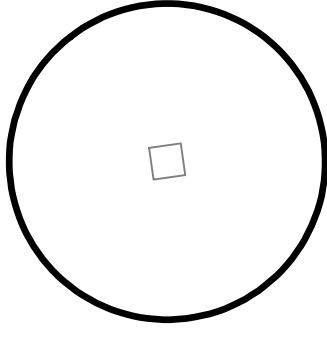
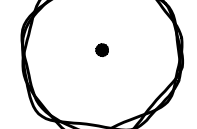
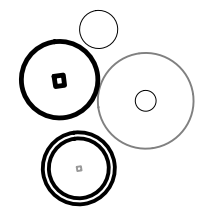

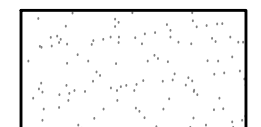
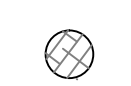



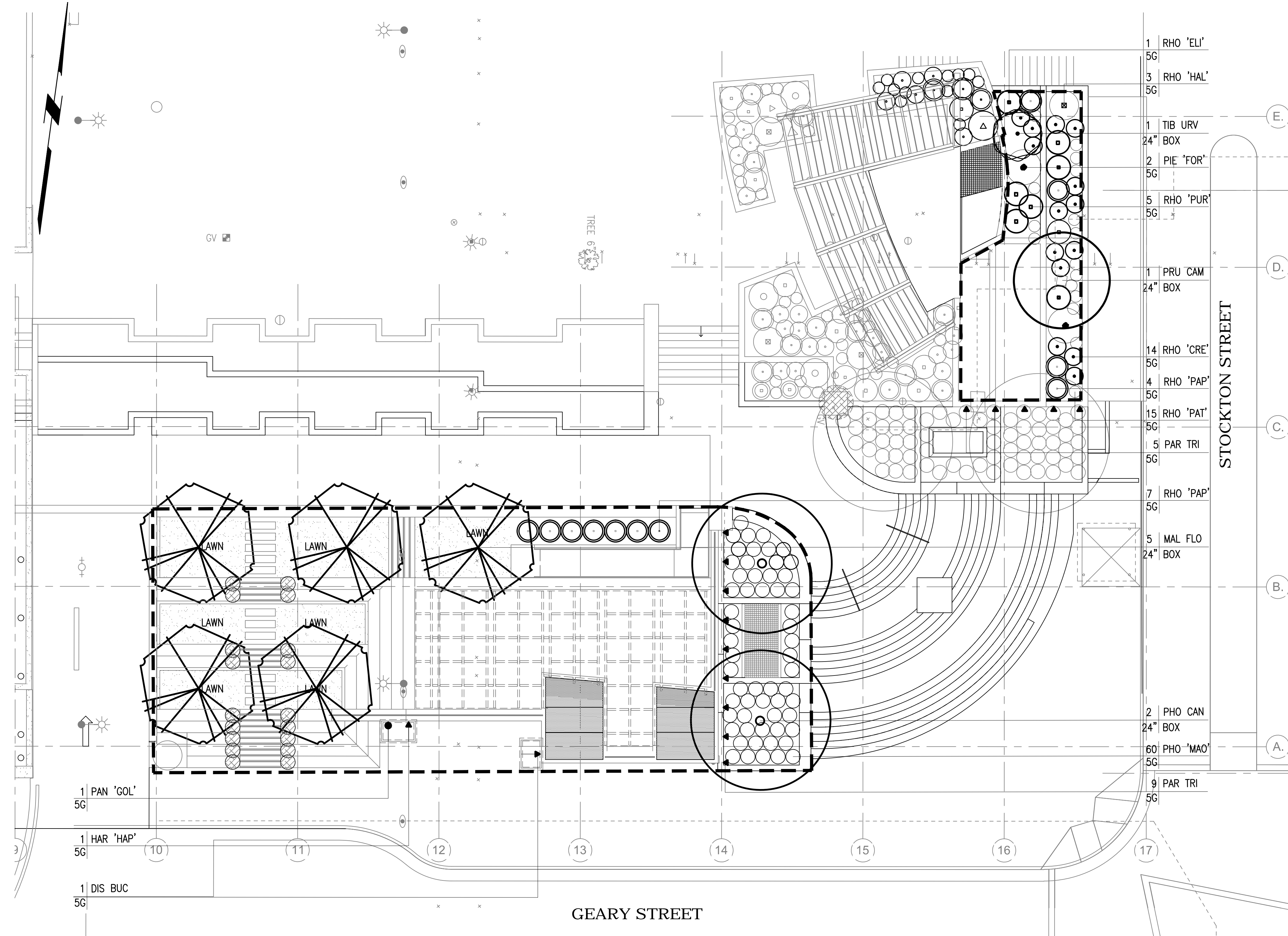
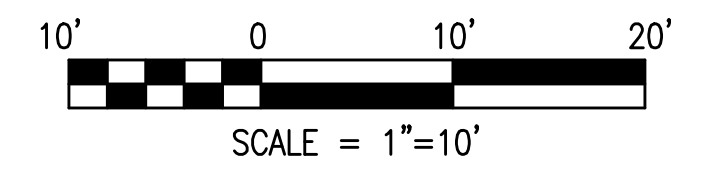
CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION
ARCHITECTURAL DEMOLITION SECTIONS
EMERGENCY VENTILATION SHAFT

CONTRACT NO.	1253
SFMTA CONTROL NO.	CL-19430
DRAWING NO.	AR-850
SHEET NO.	0

LEGEND

-  (E) TREES TO BE REMAIN.
-  (E) SHRUBS TO BE REMAIN.
-  (N) TREES TO BE PLANTED, SEE DETAIL 1, PLANT LEGEND, SHEET LA-502, AND SPECIFICATIONS.
-  (E) TREES REPLANTED BY THE CONTRACTOR AND REINSTALLED BY THE CONTRACTOR.
-  (N) TREES TO BE PLANTED, SEE DETAIL 1, PLANT LEGEND, SHEET LA-502, AND SPECIFICATIONS.
-  (N) TREES TO BE PLANTED, SEE DETAIL 1, PLANT LEGEND, SHEET LA-502, AND SPECIFICATIONS.
-  (N) SHRUBS TO BE PLANTED, SEE DETAIL 2, PLANT LEGEND, SHEET LA-502 AND SPECIFICATIONS.
-  (N) VINES TO BE PLANTED, SEE SHEET LA-502, AND SPECIFICATIONS.
-  (N) LAWN, SEE PLANT LEGEND, SHEET LA-502, AND SPECIFICATIONS.
-  (E) DIA. CONCRETE PLANTER POTS WITH SEASONAL COLOR FLOWERS, TO BE BROUGHT BACK FROM DPW STORAGE AND RELOCATED AS SHOWN.
-  LIMIT OF WORK



PLAN

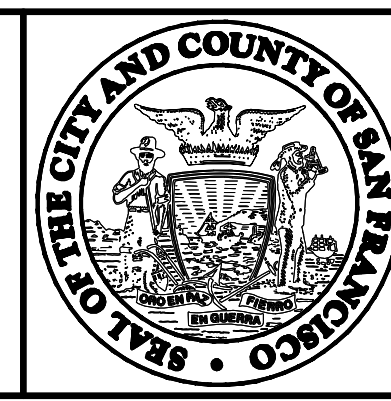
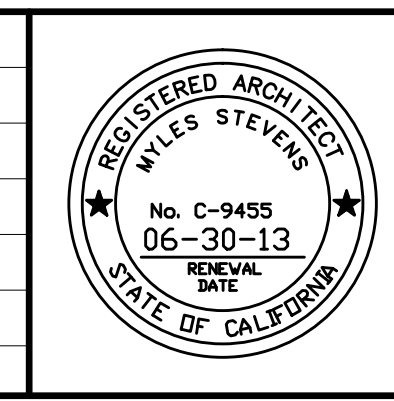
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02/15/2012	ISSUED FOR BID		0		
DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED

DESIGNED: C. SHARMA
 DRAWN: C. SHARMA
 CHECKED: M. STEVENS
 REVIEWED: M. HUDSON
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

central subway design group

STEVENS & ASSOCIATES



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

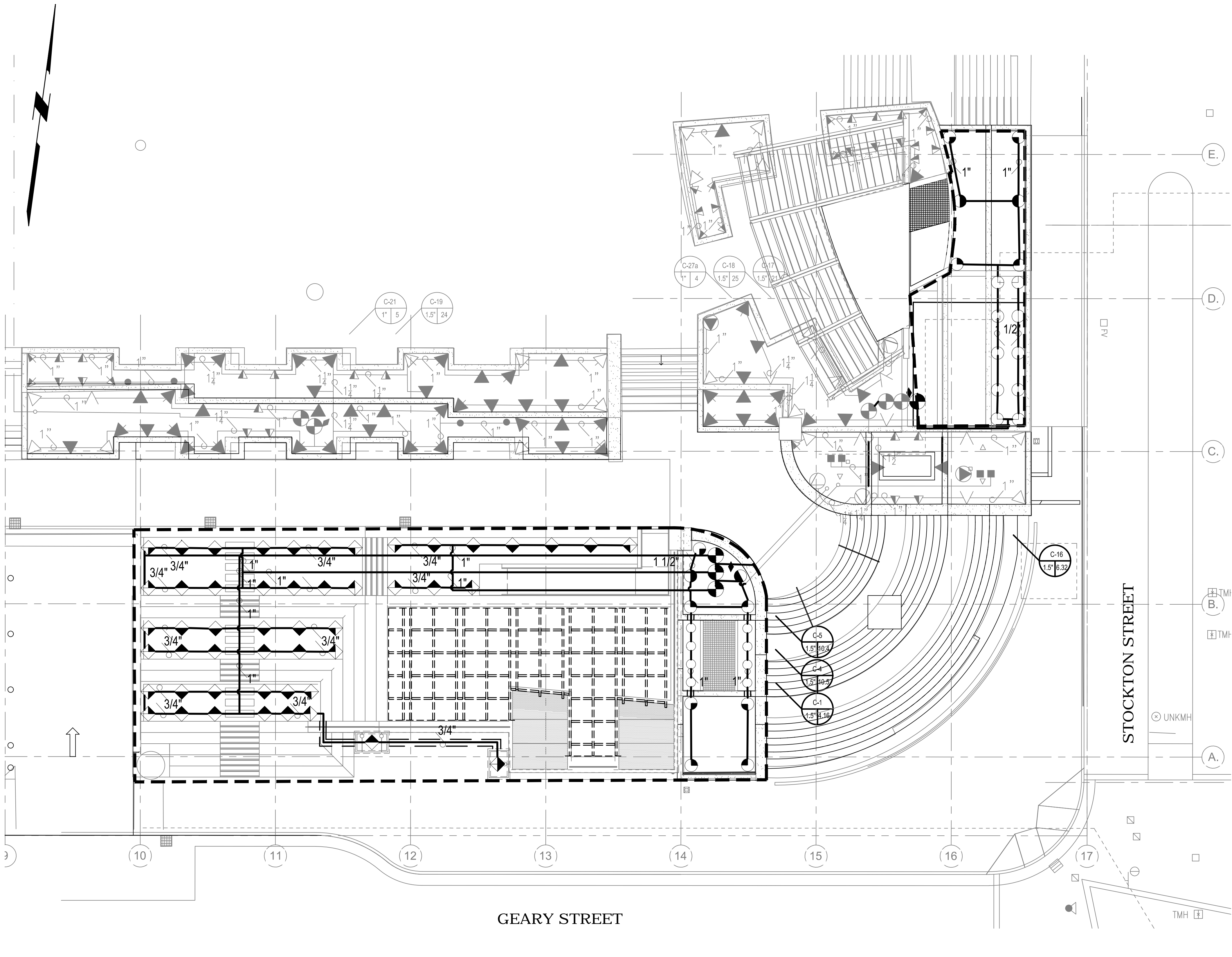
**LANDSCAPING
 PLANTING PLAN
 UNION SQUARE PLAZA**

CONTRACT NO. 1253	
SFMTA CONTROL NO. CL-18532	
DRAWING NO. LA-401	REVISION
SHEET NO.	0

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 JAnglin Mon Apr 09, 2012 - 2:32 pm LA-402

- NOTES:**
- FOR IRRIGATION DETAILS AND LEGEND, SEE SHEET LA-504.
 - MAINTAIN CONTINUOUS IRRIGATION DURING CONSTRUCTION.

- LEGEND**
- ∇ (E) POP-UP SPRAY TO REMAIN.
 - ▽ (E) POP-UP SPRAY TO REMAIN.
 - ◊ (E) POP-UP SPRAY TO REMAIN.
 - ▽ (E) POP-UP SPRAY TO REMAIN.
 - ▽ (E) POP-UP SPRAY TO REMAIN.
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 - ▼ (E) POP-UP SPRAY TO REMAIN.
 - ▼ (E) POP-UP SPRAY TO REMAIN.
 - ▼ (E) POP-UP SPRAY TO REMAIN.
 - (E) BUBBLER TO REMAIN.
 - ⊕ (E) REMOTE CONTROL VALVE TO REMAIN.
 - ◆ (E) QUICK COUPLING VALVE TO REMAIN.
 - (E) MAINLINE TO REMAIN.
 - - - (E) LATERAL LINE TO REMAIN.
 - (E) CONTROLLER AND STATION NUMBER TO REMAIN.
 - (E) FLOW (GPM) TO REMAIN.
 - (E) REMOTE CONTROL VALVE SIZE TO REMAIN.
 - (N) POP-UP SPRAY TO BE INSTALLED.
 - ◊ (N) POP-UP SPRAY TO BE INSTALLED.
 - ◆ (N) POP-UP SPRAY TO BE INSTALLED.
 - ▽ (N) POP-UP SPRAY TO BE INSTALLED.
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 - ▼ (N) POP-UP SPRAY TO BE INSTALLED.
 - ▼ (N) POP-UP SPRAY TO BE INSTALLED.
 - ▼ (N) POP-UP SPRAY TO BE INSTALLED.
 - ⊕ (N) REMOTE CONTROL VALVE TO BE INSTALLED.
 - ◆ (N) QUICK COUPLING VALVE TO BE INSTALLED.
 - - - (N) LATERAL LINE TO BE INSTALLED UNDER SLAB. COORDINATE ROUTING AND SLAB PENETRATIONS WITH OTHER TRADES.
 - - - (N) SLEEVE TO BE INSTALLED.
 - (N) CONTROLLER AND STATION NUMBER.
 - (N) FLOW (GPM).
 - (N) REMOTE CONTROL VALVE SIZE.
 - (LIMIT OF WORK.



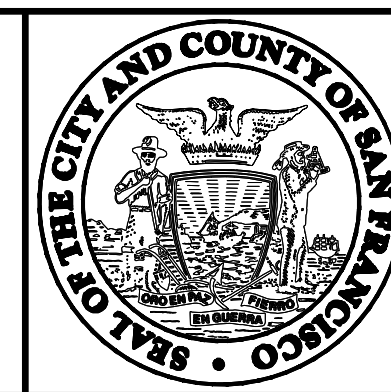
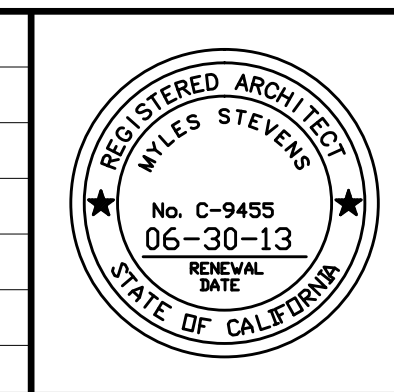
PLAN

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID	0			

DESIGNED BY: C. SHARMA
 DRAWN BY: C. SHARMA
 CHECKED BY: M. STEVENS
 REVIEWED BY: M. HUDSON
 RECOMMENDED BY: A. READ
 APPROVED BY: R. EDWARDS
 DATE: 02/15/2012

central subway design group

STEVENS & ASSOCIATES



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

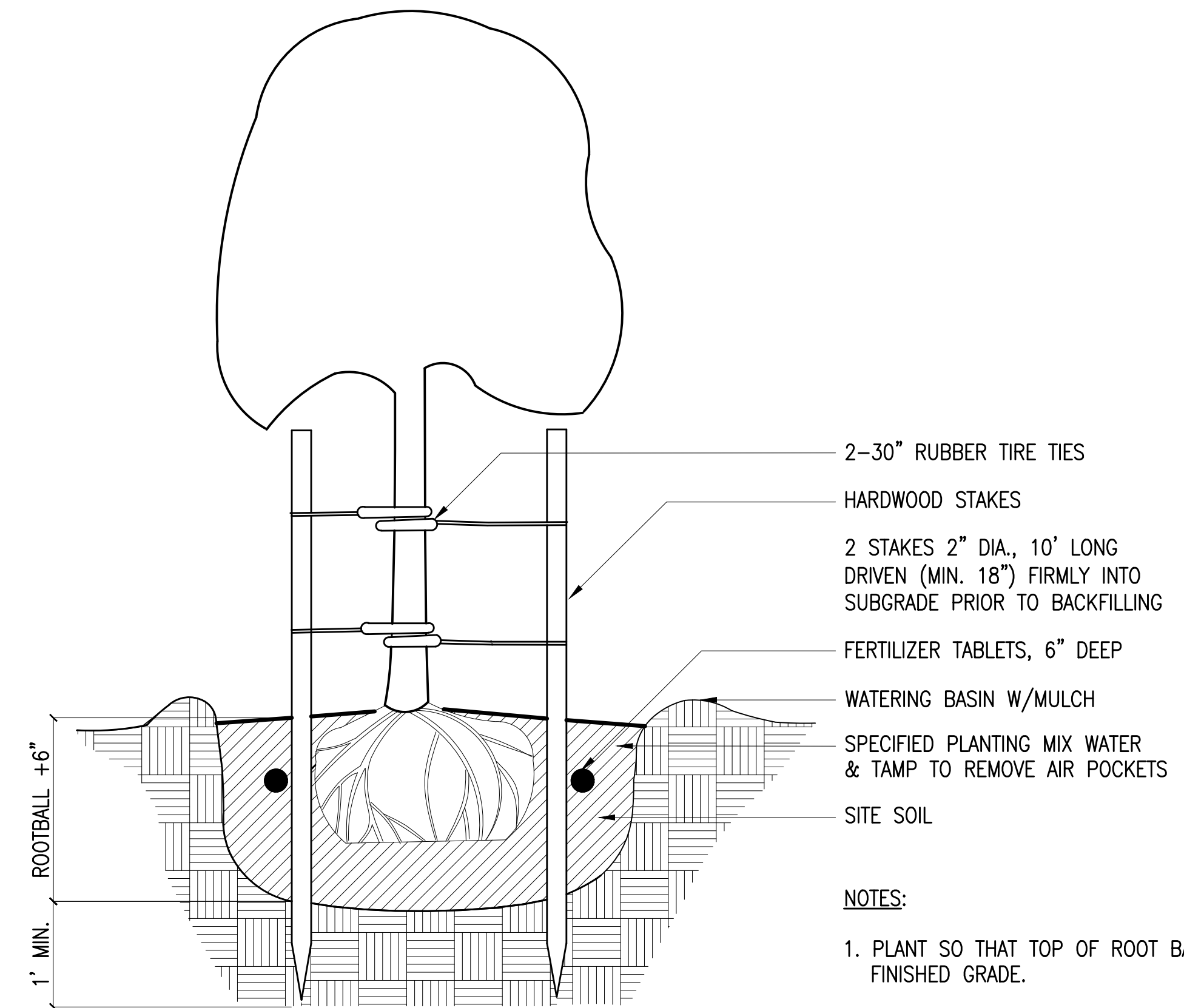
**LANDSCAPING IRRIGATION PLAN
 UNION SQUARE PLAZA**

CONTRACT NO.	1253
SFMTA CONTROL NO.	CL-18533
DRAWING NO.	LA-402
SHEET NO.	0

PLANT LEGEND

SYMBOL	NO.	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS
TREES					
	MAL FLO	5	MALUS FLORIBUNDA	CRAB APPLE	24" BOX SPECIMEN
	PHO CAN	2	PHOENIX CANARIENSIS	CANARY ISLAND DATE PALM	TRANSPLANTED BY THE CONTRACTOR AND REINSTALLED BY THE CONTRACTOR
	PRU CAM	1	PRUNUS CAMPANULATA	FLOWERING CHERRY	24" BOX SPECIMEN
	TIB URV	1	TIBOUCHINA URVILLEANA	PRINCESS FLOWER	24" BOX SPECIMEN
SHRUBS					
	PHO 'MAO'	58	PHORMIUM TENAX 'MAORI SUNRISE'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
	PIE 'FOR'	2	PIERIS 'FOREST FLAME'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
	RHO 'CRE'	14	RHODODENDRON 'CREAMY CHIFFON'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
	RHO 'ELI'	1	RHODODENDRON 'ELIZABETH'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
	RHO 'HAL'	3	RHODENDRON 'HALFDEN LEM'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
	RHO 'PAP'	11	RHODENDRON 'PAPRIKA SPICE'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
	RHO 'PAT'	15	RHODODENDRON 'PATTY BEE'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
	RHO 'PUR'	5	RHODODENDRON 'PURPLE SPLENDOR'	NO COMMON NAME (NCN)	5 GALLON 2' HEIGHT MINIMUM 2' SPREAD MINIMUM
VINES					
	DIS BUC	1	DICTYIS BUCCINATORIA	RED TRUMPET VINE	5 GALLON AS SHOWN ON PLAN
	HAR 'HAP'	1	HARDENBERGIA VIOLACEA 'HAPPY WANDERER'	LILAC VINE	5 GALLON AS SHOWN ON PLAN
	PAN 'GOL'	1	PANDOREA PANDORANA 'GOLDEN SHOWER'	WONGA WONGA VINE	5 GALLON AS SHOWN ON PLAN
	PAR TRI	14	PARTHENOCISSUS TRICUSPIDATA	BOSTON IVY	5 GALLON 5' O.C.
LAWN					
	LAWN	X	LAWN	TALL/CREeping RED FESCUE	SOD X

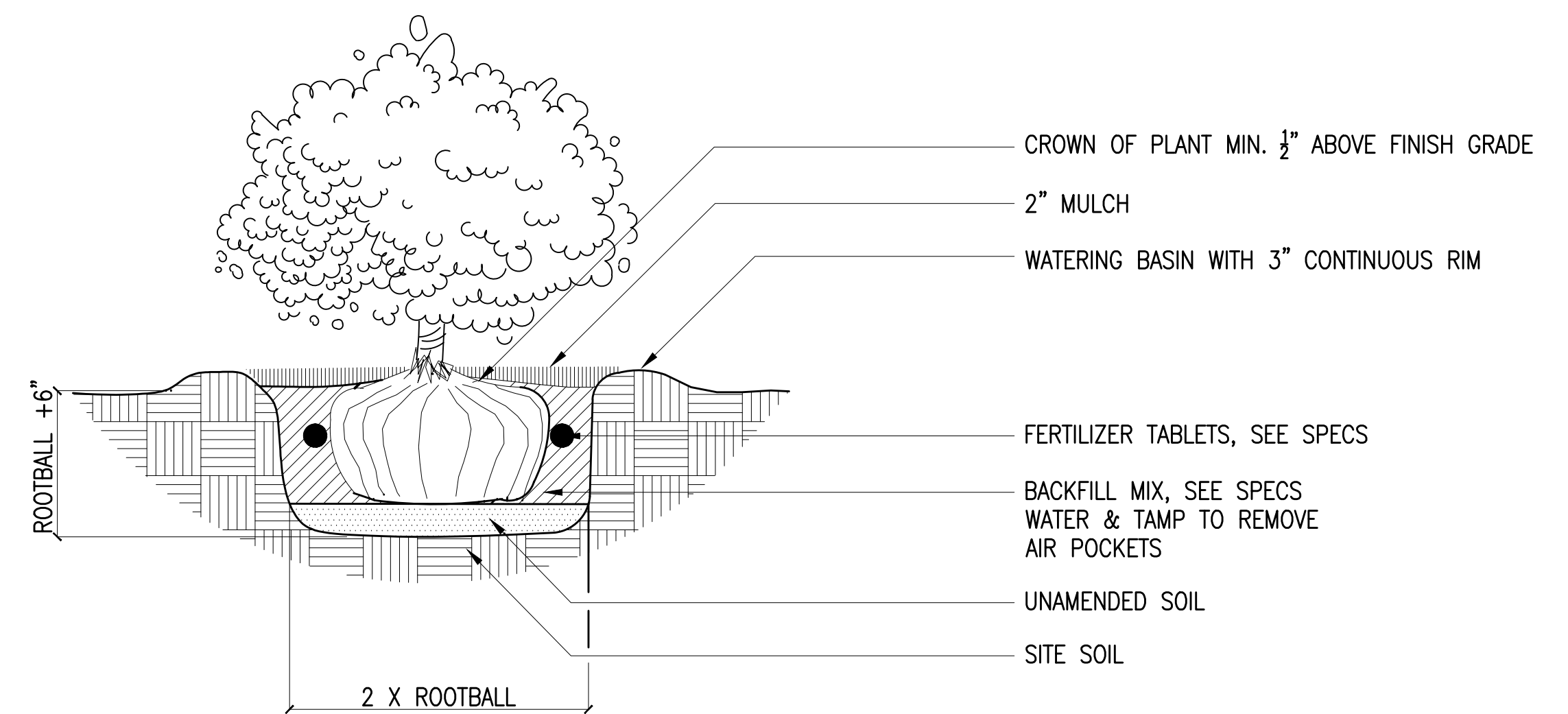
ALL SPECIES MUST BE APPROVED BY/COMFORMED WITH DPW BEFORE PLANTING



- NOTES:**
1. PLANT SO THAT TOP OF ROOT BALL IS EVEN WITH THE FINISHED GRADE.
 2. STAKE ABOVE FIRST BRANCHES OR AS NECESSARY FOR FIRM SUPPORT.

TREE PLANTING AND STAKING

1
-
DETAIL
SCALE: NTS

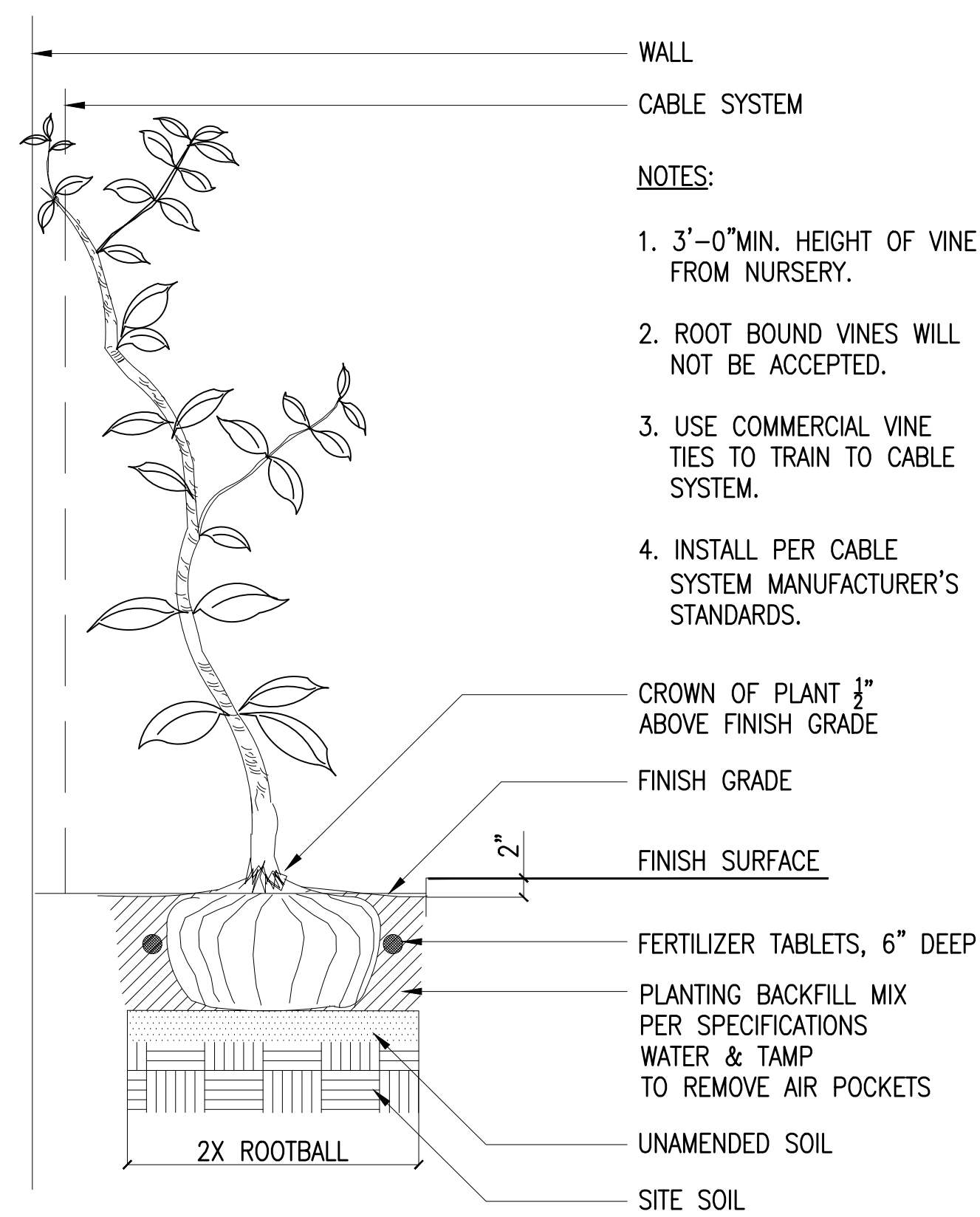


SHRUB PLANTING

2
-
DETAIL
SCALE: NTS

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 janglim Fri Apr 06, 2012 - 2:48 pm LA-502

02/15/2012 ISSUED FOR BID		0										CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED DIRECTOR OF TRANSPORTATION		THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION		CONTRACT NO. 1253 SFMTA CONTROL NO. CL-18535	
DATE DESCRIPTION		REV NO. BY CHECKED APPROVED		DESIGNED: C. SHARMA DRAWN: C. SHARMA CHECKED: M. STEVENS REVIEWED: M. HUDSON RECOMMENDED: A. READ APPROVED: R. EDWARDS DATE: 02/15/2012		No. C-9455 06-30-13 RENEWAL DATE		No. C-9455 06-30-13 RENEWAL DATE		APPROVED		LANDSCAPING PLANTING DETAILS - UNION SQUARE PLAZA SHEET 1 OF 2		DRAWING NO. LA-502 SHEET NO. 0		REVISION 0	



- NOTES:**
1. 3'-0" MIN. HEIGHT OF VINE FROM NURSERY.
 2. ROOT BOUND VINES WILL NOT BE ACCEPTED.
 3. USE COMMERCIAL VINE TIES TO TRAIN TO CABLE SYSTEM.
 4. INSTALL PER CABLE SYSTEM MANUFACTURER'S STANDARDS.

VINE SUPPORT SYSTEM - SECTION

3
-
DETAIL
SCALE: NTS

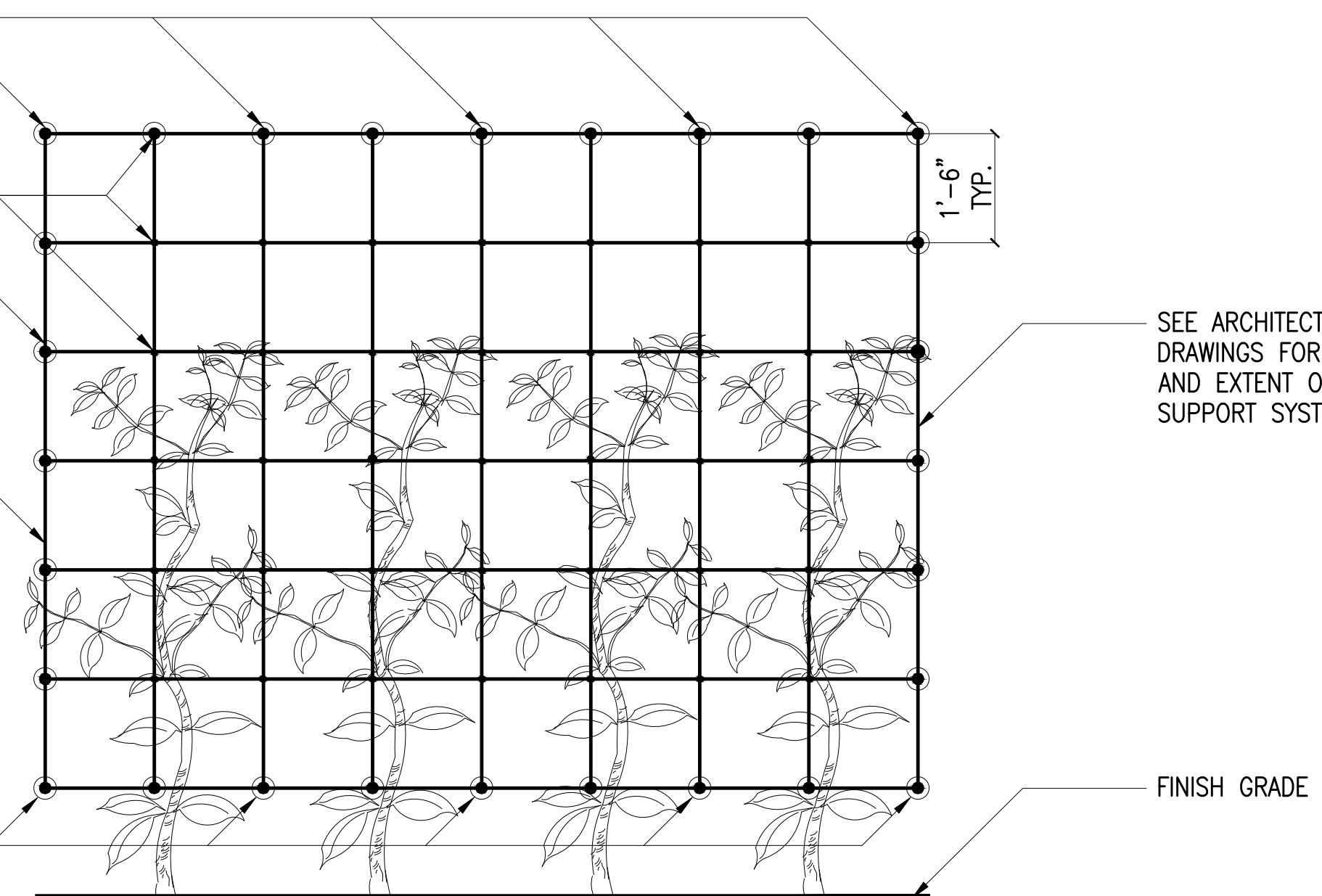
WALL ANCHOR MOD. L1,
CONSISTING OF: 919-061-SPACEBAR,
836-0800-070-GROUND PLATE
W/ANCHOR 803-0600-08.

CROSS CLAMP 90 DEGREE
STAINLESS #858-0400, (TYP.)

CABLE # 1K120-0400 (TYP.)
W/ANCHOR 874-0400-01.
LIGHT CLAMP RING W/THIMBLE.

TIE VINES TO VINE SUPPORT
CABLE SYSTEM (TYP.)

WALL ANCHOR MOD.L1



- NOTES:**
1. MATERIALS AND ATTACHMENTS FOR THE VINE SUPPORT SYSTEM ARE BASED ON 'FACADESCAPE' PLANT SUPPORT SYSTEM, MANUFACTURED BY 'CARL STAHL DECORCABLE', (800) 444-6271, WWW.DECORCABLE.COM. PROVIDE SAME OR APPROVED EQUAL.

VINE SUPPORT SYSTEM - ELEVATION

4
-
DETAIL
SCALE: NTS

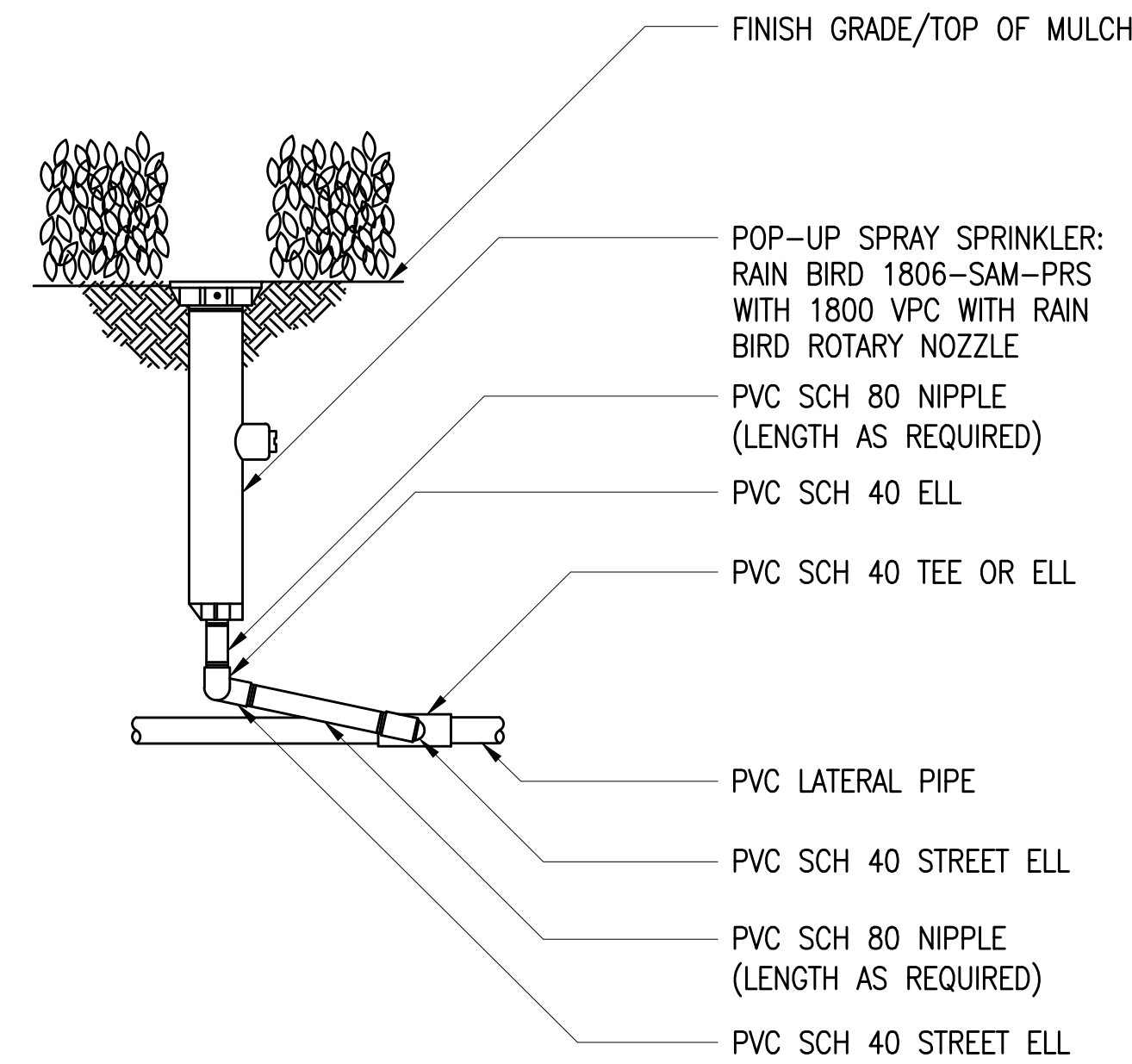
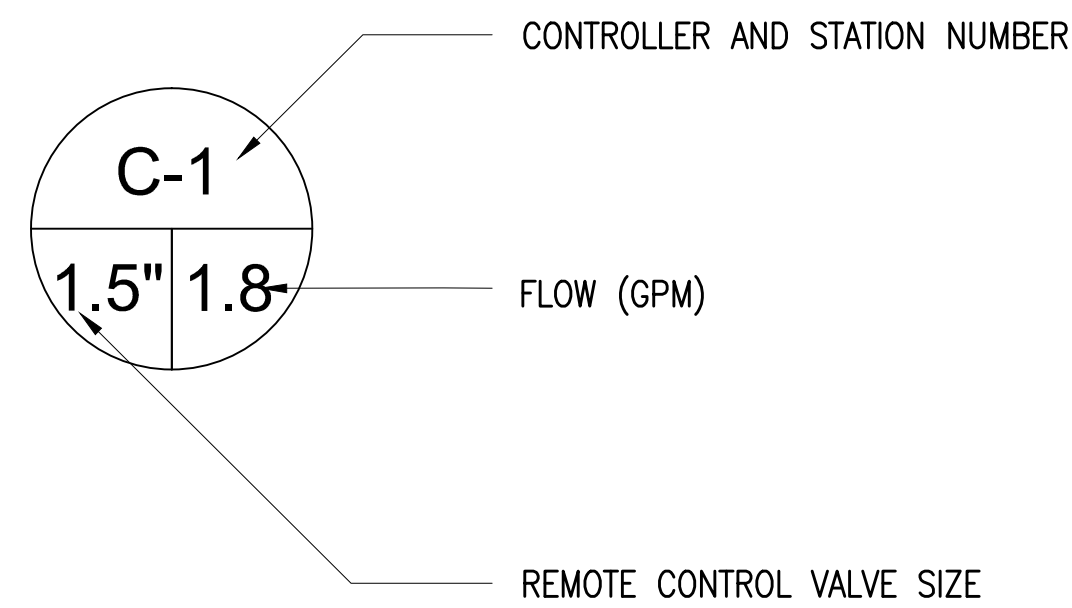
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Janglim Fri Apr 06, 2012 1:00 pm LA-502

								THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION		CONTRACT NO. 1253 SFMTA CONTROL NO. CL-18536	
						APPROVED		LANDSCAPING PLANTING DETAILS - UNION SQUARE PLAZA SHEET 2 OF 2		DRAWING NO. LA-503 SHEET NO. 0	
02/15/2012	ISSUED FOR BID	0									
DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED						

IRRIGATION LEGEND

SYMBOL	MODEL NO.	DESCRIPTION	PSI	GPM	RADIUS MIN. - MAX.
SPRINKLERS					
◆ ◆	1806-SAM-PRS/MPR 8	RAIN BIRD POP-UP-SPRAY	30	0.1-1.16	6-8
▼ ▼	1812-SAM-PRS/U8H,Q	RAIN BIRD POP-UP-SPRAY	30	0.26-0.52	6-8
● ●	1812-SAM PRS/12 HEVAN	RAIN BIRD POP-UP-SPRAY	30	0.59-1.18	9-12
⊕ ⊖	1812-SAM PRS/ 5MPR H,Q	RAIN BIRD POP-UP-SPRAY	30	0.10-0.40	3.75-5
○	1812-SAM PRS/ SQ H,Q	RAIN BIRD POP-UP-SPRAY	30	0.12-0.20	4-5

MANUFACTURER	SYMBOL	MODEL NO.	DESCRIPTION
RAINBIRD	◆	44LRC	QUICK COUPLER IN VALVE BOX.
RAINBIRD	⊕	PEB SERIES	RAINBIRD REMOTE CONTROL VALVE (SIZE AS INDICATED)
	—	CLASS 200	LATERAL LINE: 1120 SCHEDULE 40 PVC PLASTIC PIPE WITH SCHEDULE 40 PVC PLASTIC SOLVENT WELDED FITTINGS. PIPE, ROUTING AND INSTALLATION TO BE PROVIDED UNDER THIS CONTRACT. 12 INCH SOIL COVER. SIZE AS NOTED.
	— —		SLEEVE, SIZE 2X LATERAL/MAINLINE

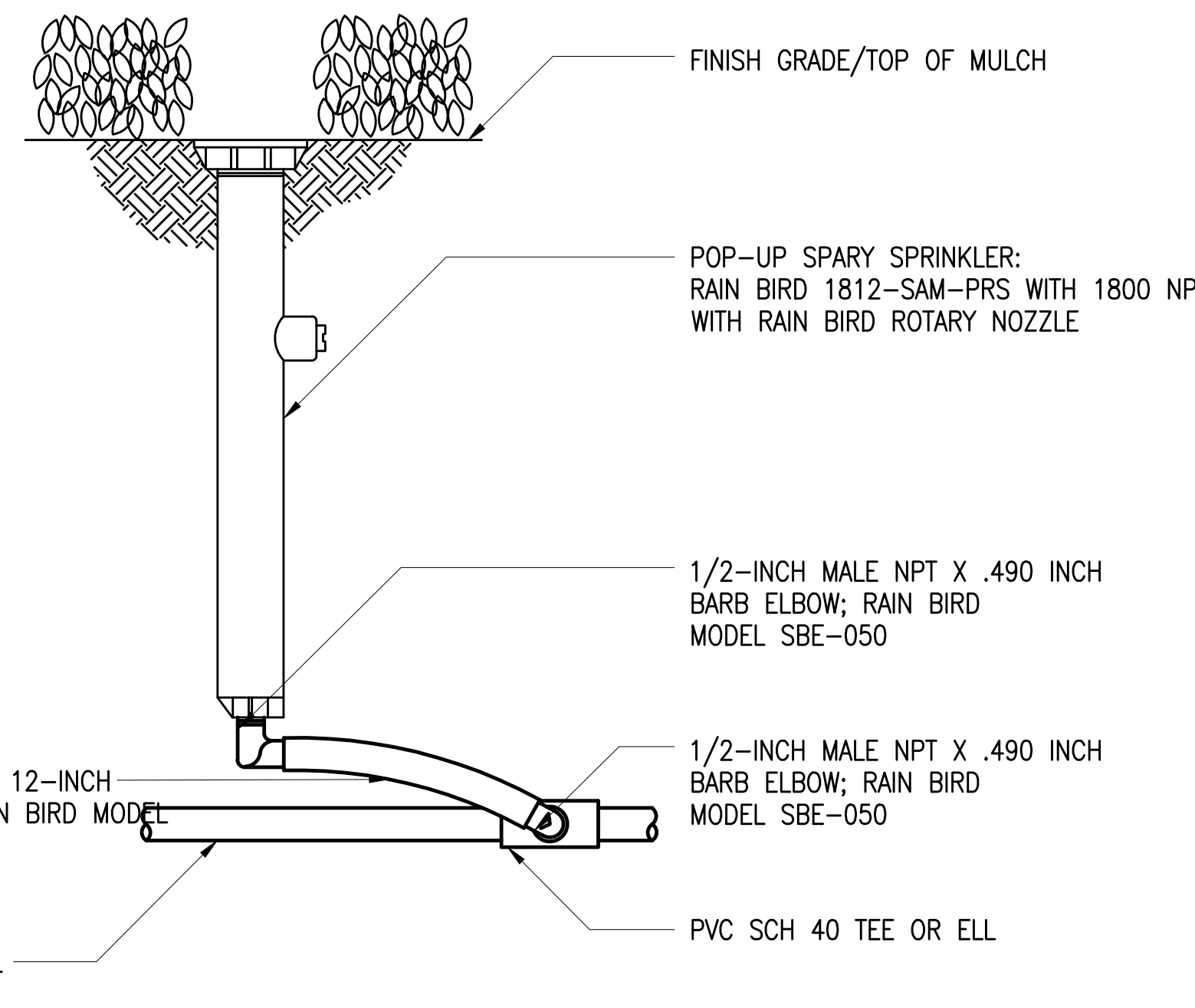


POP-UP SPRAY SPRINKLER WITH 1806 SWING JOINT

1
-
DETAIL
SCALE: NTS

CHOOSING NOZZLES AND OPTIONAL FEATURES

PLASTIC MPR NOZZLE OPTION-NOZ-PLASTIC	U-SERIES NOZZLE OPTION-NOZ-U-SERIES	
VAN NOZZLE OPTION-NOZ-VAN	ROTARY NOZZLE OPTION-NOZ-ROTARY	
SAM SERIES OPTION-SAM	PRS SERIES OPTION-PRS	SAM-PRS SERIES OPTION-SAM-PRS
VANDAL-PROOF CAP OPTION-VPC	NON-POTABLE CAP OPTION-NP	SAM SERIES WITH VANDAL-PROOF CAP OPTION-SAM-VPC
SAM SERIES WITH NON-POTABLE CAP OPTION-SAM-NP	PRS SERIES WITH VANDAL-PROOF CAP OPTION-PRS-VPC	PRS SERIES WITH NON-POTABLE CAP OPTION-PRS-NP
SAM-PRS SERIES WITH VANDAL-PROOF CAP OPTION-SAM-PRS-VPC	SAM-PRS SERIES WITH NON-POTABLE CAP OPTION-SAM-PRS-NP	



POP-UP SPRAY SPRINKLER 1812 WITH SWING PIPE

2
-
DETAIL
SCALE: NTS

CHOOSING NOZZLES AND OPTIONAL FEATURES

PLASTIC MPR NOZZLE OPTION-NOZ-PLASTIC	U-SERIES NOZZLE OPTION-NOZ-U-SERIES	
VAN NOZZLE OPTION-NOZ-VAN	ROTARY NOZZLE OPTION-NOZ-ROTARY	
SAM SERIES OPTION-SAM	PRS SERIES OPTION-PRS	SAM-PRS SERIES OPTION-SAM-PRS
VANDAL-PROOF CAP OPTION-VPC	NON-POTABLE CAP OPTION-NP	SAM SERIES WITH VANDAL-PROOF CAP OPTION-SAM-VPC
SAM SERIES WITH NON-POTABLE CAP OPTION-SAM-NP	PRS SERIES WITH VANDAL-PROOF CAP OPTION-PRS-VPC	PRS SERIES WITH NON-POTABLE CAP OPTION-PRS-NP
SAM-PRS SERIES WITH VANDAL-PROOF CAP OPTION-SAM-PRS-VPC	SAM-PRS SERIES WITH NON-POTABLE CAP OPTION-SAM-PRS-NP	

SELECT THE DESIRED SWING ASSEMBLY BY TURNING ON THE APPROPRIATE LAYER:

CONTRACTOR ASSEMBLED SWING ASSEMBLY
OPTION-SA
AND EITHER TEXT-SA-LEADERS
OR TEXT-SA-BUBBLES

RAIN BIRD SWING ASSEMBLY
OPTION-RB-SA
AND EITHER TEXT-RB-SA-LEADERS
OR TEXT-RB-SA-BUBBLES

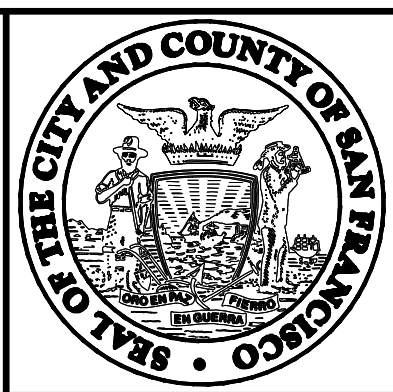
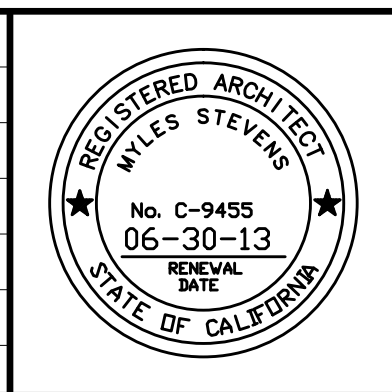
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 janglim Fri Apr 06,2012 1:01 pm LA-504

DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID	0			

central subway design group

STEVENS & ASSOCIATES

DESIGNED: C. SHARMA
 DRAWN: C. SHARMA
 CHECKED: M. STEVENS
 REVIEWED: M. HUDSON
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

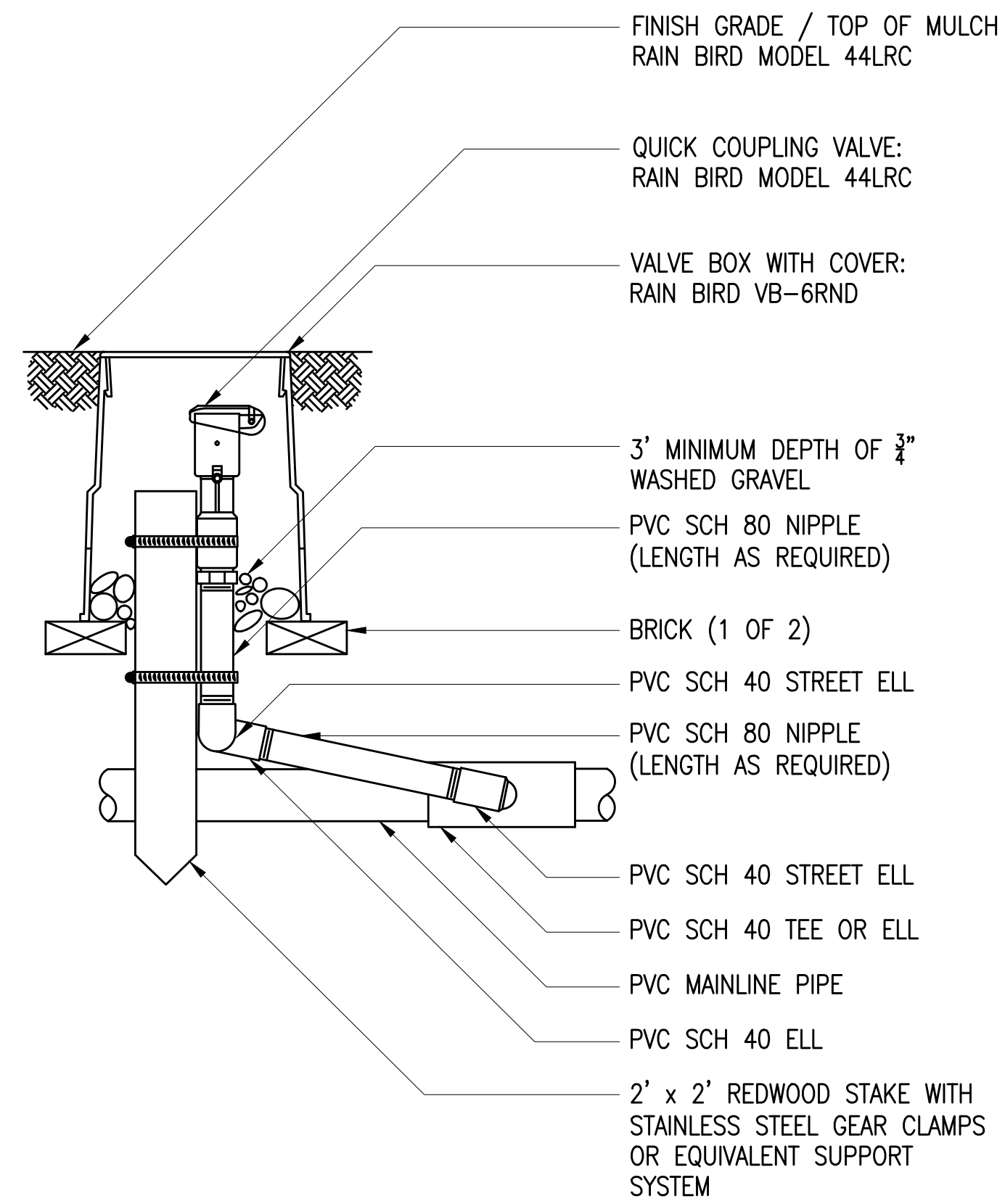
APPROVED

DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

LANDSCAPING
 IRRIGATION DETAILS
 SHEET 1 OF 3

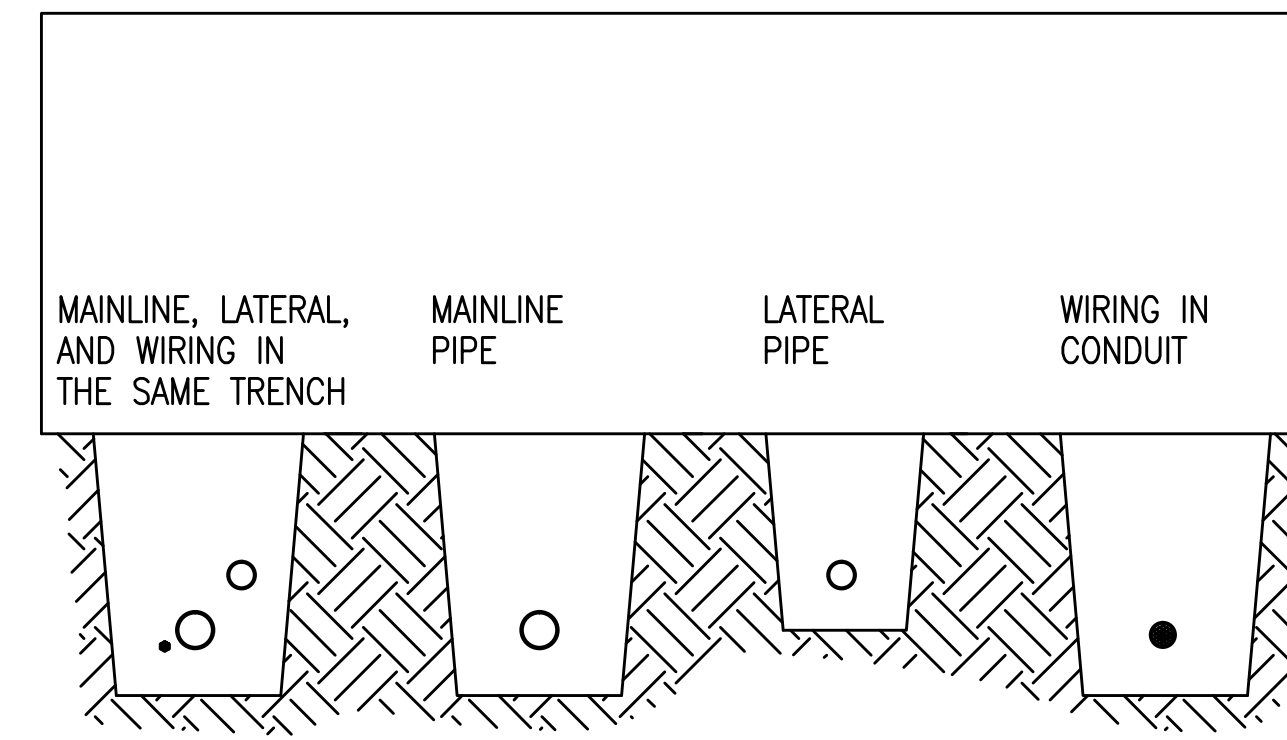
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SFMTA CONTROL NO. CL-18537	
DRAWING NO. LA-504	0
SHEET NO.	



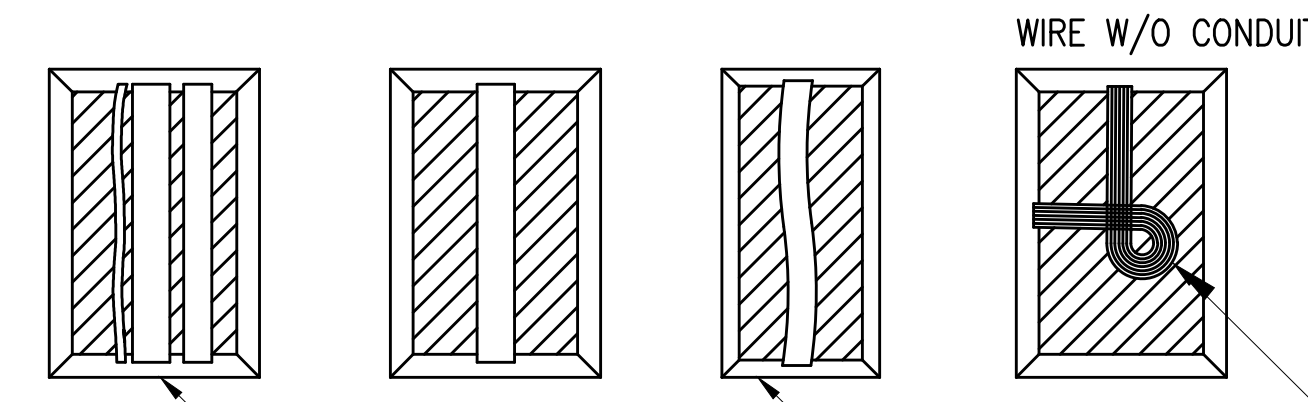
CHOOSING VALVES
VALVES
 OPTION-MODEL-44RC
 OPTION-MODEL-44LRC
 OPTION-MODEL-44NP

QUICK COUPLING VALVE MODEL 44RC SERIES

3
 -
DETAIL
 SCALE: NTS



SECTION

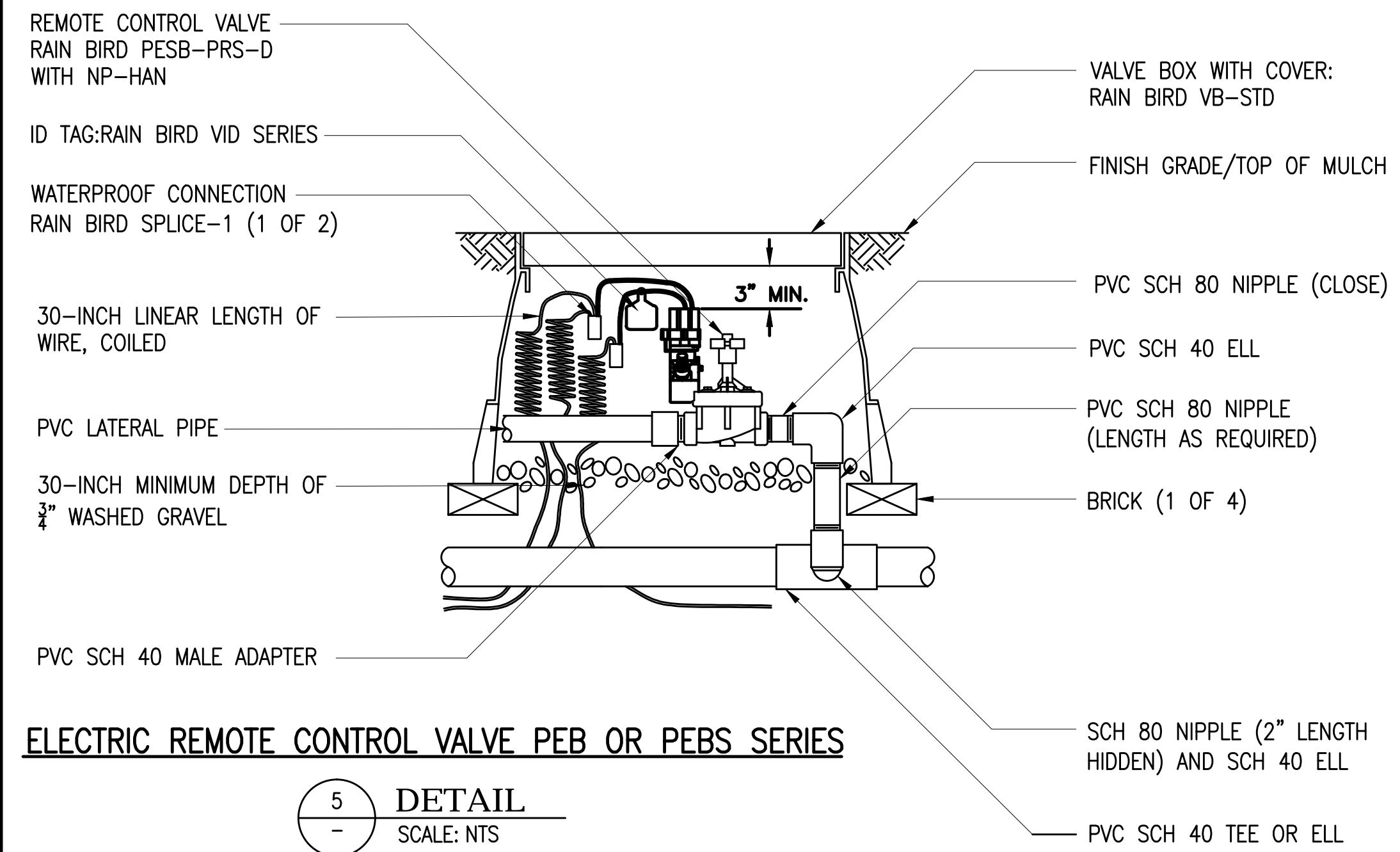


PLAN

- NOTES:**
1. SLEEVE BELOW ALL HARDSCAPE ELEMENTS WITH CLASS 200 PVC TWICE THE DIAMETER OF THE PIPE OR WIRE BUNDLE WITHIN.
 2. UTILITY TRENCHES SHOULD BE EXCAVATED A MINIMUM OF FOUR INCHES BELOW THE BOTTOM OF PIPES OR CONDUITS AND HAVE CLEARANCES OF AT LEAST FOUR INCHES ON BOTH SIDES.
 3. PIPES OR CONDUITS SHALL BE BEDDED ON A MINIMUM OF FOUR INCHES OF SAND OR FINE GRAVEL.
 4. AFTER PIPES AND CONDUITS HAVE BEEN TESTED AND APPROVED, THEY SHOULD BE COVERED TO A DEPTH OF 6 INCHES WITH SAND OR FINE GRAVEL, AND THEN SHALL BE MECHANICALLY TAMPED.

TRENCHING DETAIL

4
 -
DETAIL
 SCALE: NTS



CHOOSING VALVES AND OPTIONS

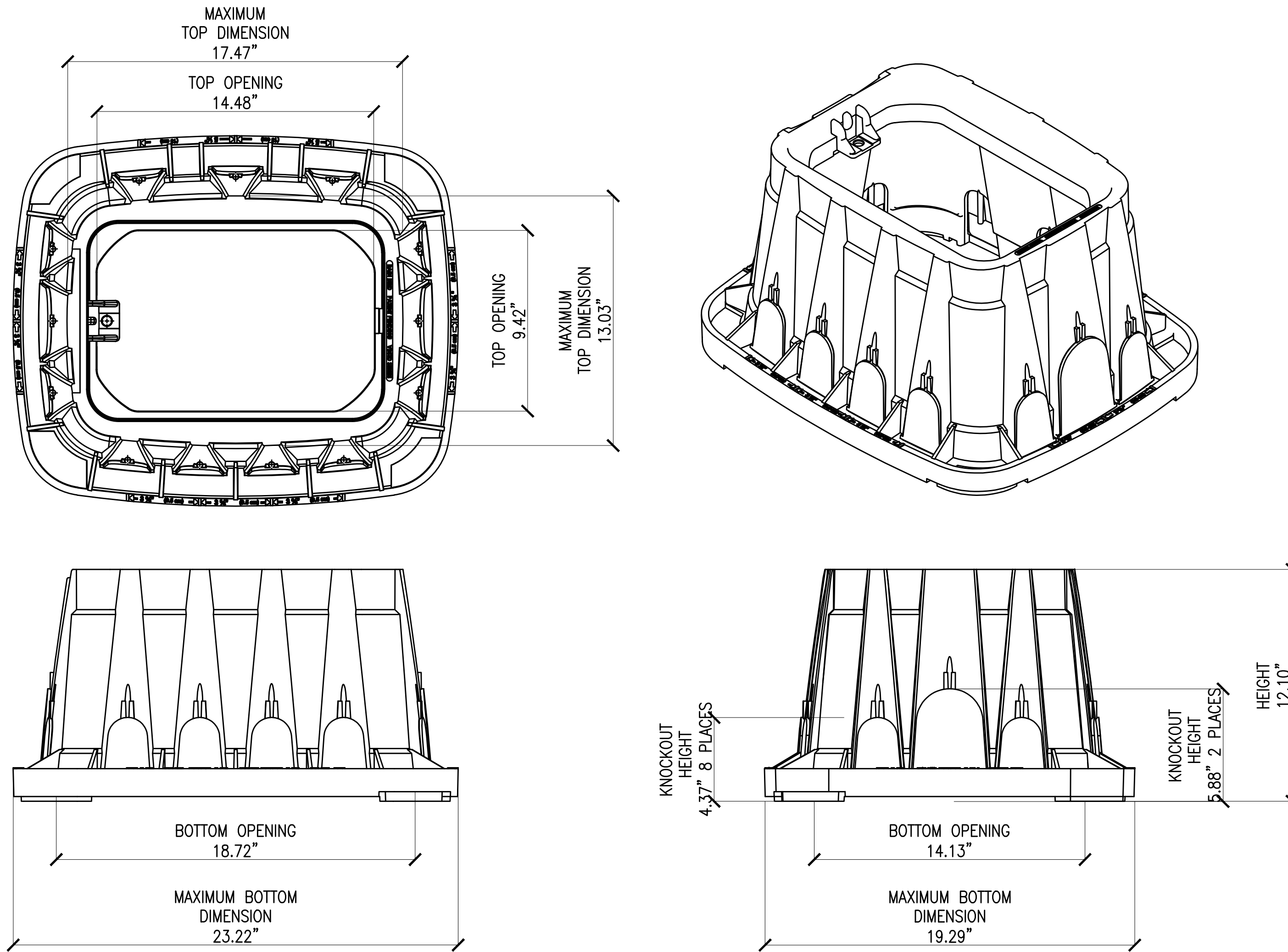
REMOTE CONTROL VALVE OPTION-SOLENOID AND EITHER: TEXT-PEB-LEADERS OR TEXT-PEB-BUBBLES	PRESSURE REGULATING REMOTE CONTROL VALVE OPTION-PRS-D AND EITHER: TEXT-PRS-D-LEADERS OR TEXT-PRS-D-BUBBLES
SCRUBBER VALVE OPTION-SOLENOID AND EITHER: TEXT-PESB-LEADERS OR TEXT-PESB-BUBBLES	PRESSURE REGULATING SCRUBBER VALVE OPTION-PRS-D AND EITHER: TEXT-PESB-PRS-D-LEADERS OR TEXT-PESB-PRS-D-BUBBLES
BSP THREADS EITHER: TEXT-BSP-LEADERS OR TEXT-BSP-BUBBLES	NP HANDLE EITHER: TEXT-NPHAN-LEADERS OR TEXT-NPHAN-BUBBLES

ELECTRIC REMOTE CONTROL VALVE PEB OR PEBS SERIES

5
 -
DETAIL
 SCALE: NTS

\\sv6nas003\M544.1\FinalDesign\DRAWINGS\CST155-2\1253\SHEET FILES\02_CIVIL\15_LANDSCAPING\12530215LA505.dwg
 janglim Fri Apr 06,2012 1:02 pm LA-505

								CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED DIRECTOR OF TRANSPORTATION		THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION		CONTRACT NO. 1253 SFMTA CONTROL NO. CL-18538	
DATE: 02/15/2012 DESCRIPTION: ISSUED FOR BID		DESIGNED: C. SHARMA DRAWN: C. SHARMA CHECKED: M. STEVENS REVIEWED: M. HUDSON RECOMMENDED: A. READ APPROVED: R. EDWARDS DATE: 02/15/2012		02/15/2012 ISSUED FOR BID		0 BY: _____ CHECKED: _____ APPROVED: _____		LANDSCAPING IRRIGATION DETAILS SHEET 2 OF 3		DRAWING NO. LA-505 SHEET NO. _____		REVISION 0	



STANDARD VALVE BOX DIMENSIONS

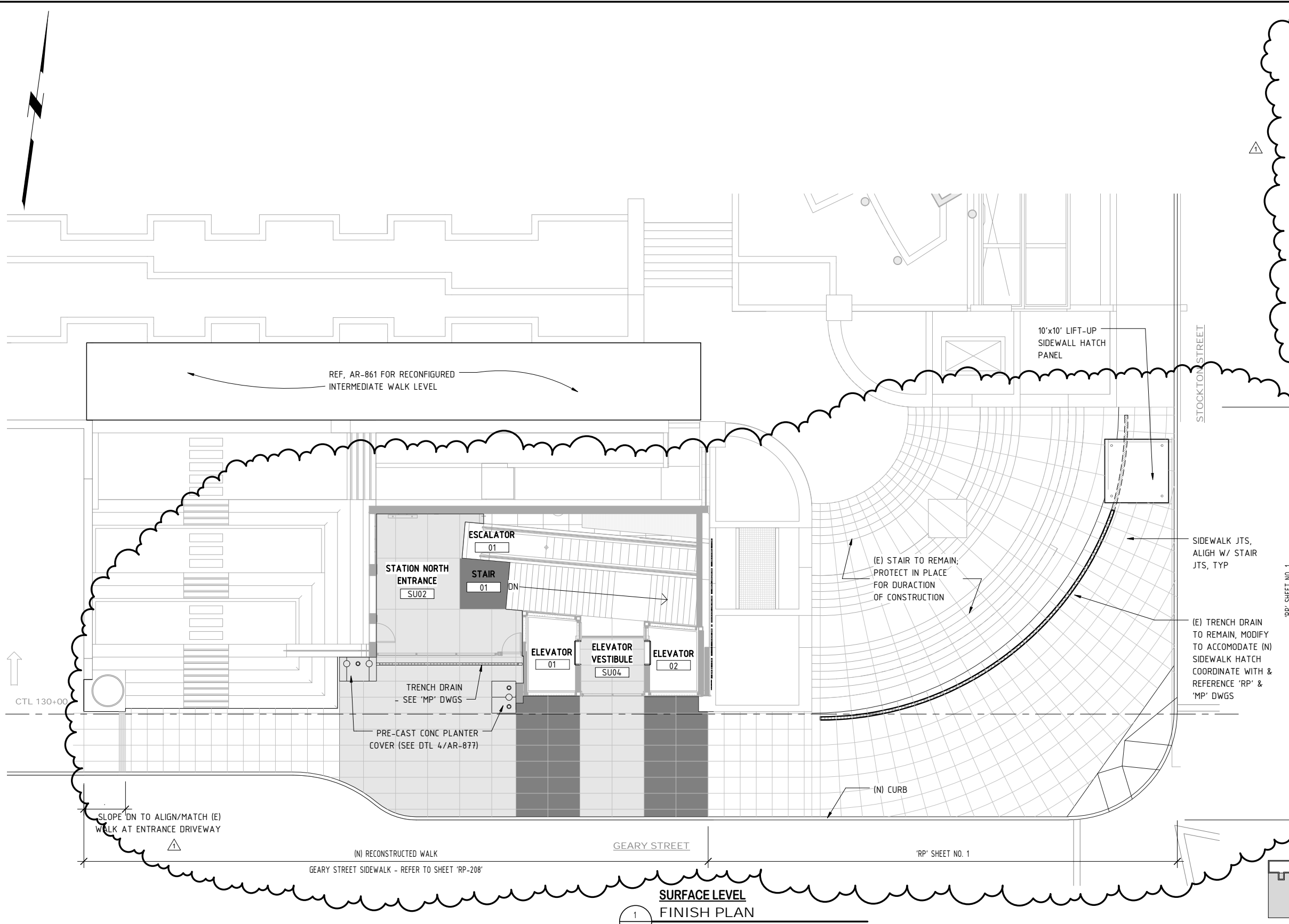
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DETAIL
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02/15/2012	ISSUED FOR BID	0									
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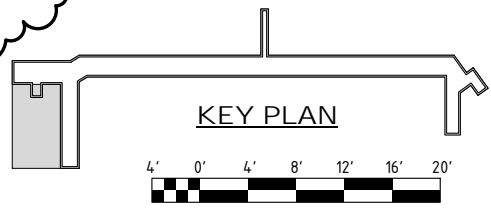
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- GENERAL SHEET NOTES**
1. REFER TO 'RP' (RENOVATION PAVEMENT) DWGS FOR NEW SIDEWALK BULB-OUT AND REPAVEMENT OF CORNER TO COMPLY W/ REQUIRED GRADES TO MEET ADA PATH OF TRAVEL REQUIREMENTS.
 2. ALL AREAS ARE EXPOSED CONCRETE, TYP, UON
 3. REFER TO FINISH SCHEDULE AR-801 AND AR-802 FOR ADDITIONAL INFORMATION
 4. COORDINATE AND REFERENCE 'RP' DWGS FOR FINAL SIDEWALK REPAVEMENT GRADES, TYP

- PAVING SCHEDULE**
- COLORED CONCRETE PAVING: 'SOMBRERO BUFF' W/ LIGHT SAND BLAST FINISH
 - COLORED CONCRETE PAVING: MATCH TO COLOR OF (E) UPPER PLAZA. LABELED AS 'SHADOW SLATE' W/ LIGHT SAND BLAST FINISH PER PLAZA AS-BUILT DRAWINGS - VERIFY IN FIELD
 - COLORED CONCRETE PAVING: 'SHADOW SLATE' ALTERNATE LIGHT & MEDIUM SAND BLAST FINISH (MATCH TYP CONC REPAVEMENT PER 'RP' DWGS)



SURFACE LEVEL FINISH PLAN
1 AR-201 1/8" = 1'-0"

FOR ORIGINAL SIGNATURES, SEE CL- 18747, REV. 0.

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

central subway design group

Robyn Chiang & Company

DESIGNED: R. CHIANG
DRAWN: K. BIGOSINSKI
CHECKED: D. FUNG
REVIEWED: R. CHIANG
RECOMMENDED: A. READ
APPROVED: R. EDWARDS
DATE: 02/15/2012

REV. 0 SEALED BY R. CHIANG

CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE / MARKET STREET STATION

ARCHITECTURAL
FINISH PLAN - SURFACE LEVEL
NORTH ENTRANCE

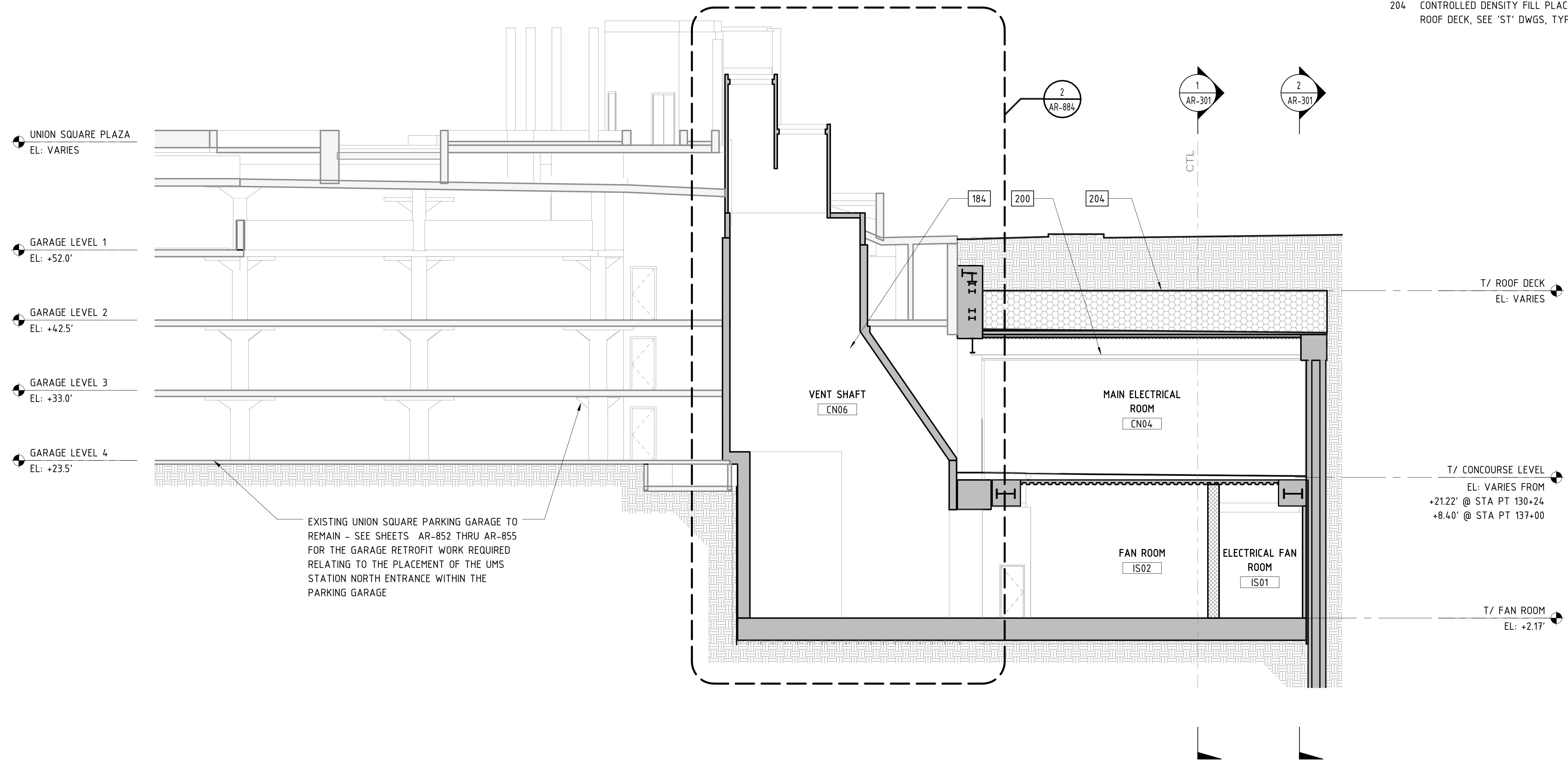
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DRAWING NO. AR-201	
SHEET NO.	

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KEYNOTES

- 106 INCLINED TANGENT PILES ALONG STATION PERIMETER, SEE 'ST' DWGS, TYP
- 184 EMERGENCY FAN VENT SHAFT (PASS THROUGH EXISTING UNION SQUARE GARAGE), TYP
- 200 STEEL ROOF SUPPORT BEAMS W/ 1HR FIREPROOFING PROTECTION, SEE 'ST' DWGS AND SEE AR-011 FOR FIREPROOFING REQUIRMENTS AT STRUCTURAL FRAME AND METAL DECK, TYP
- 204 CONTROLLED DENSITY FILL PLACED O/STRUCT ROOF DECK, SEE 'ST' DWGS, TYP



TRANSVERSE (NORTH) @ UMS-S 129+26

1 SECTION
AR-331 1/8" = 1'-0"



DATE	ISSUED FOR BID	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID		0			

central subway design group

Robin Chiang & Company

DESIGNED
R. CHIANG

DRAWN
K. BIGOSINSKI

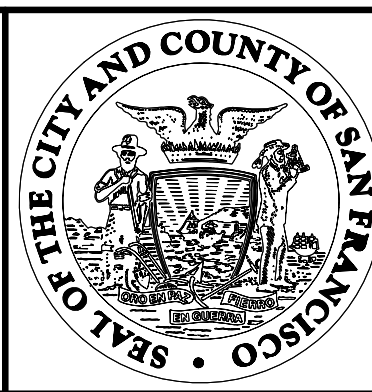
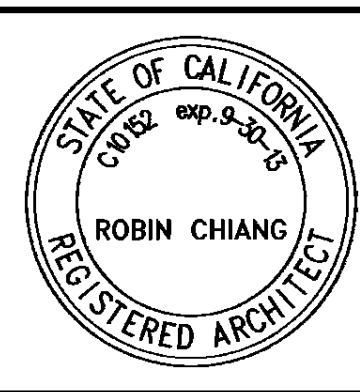
CHECKED
D. FUNG

REVIEWED
R. CHIANG

RECOMMENDED
A. READ

APPROVED
R. EDWARDS

DATE
02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE / MARKET STREET STATION

ARCHITECTURAL
SECTION - TRANSVERSE (NORTH)
UNION SQUARE - SHEET 1 OF 3

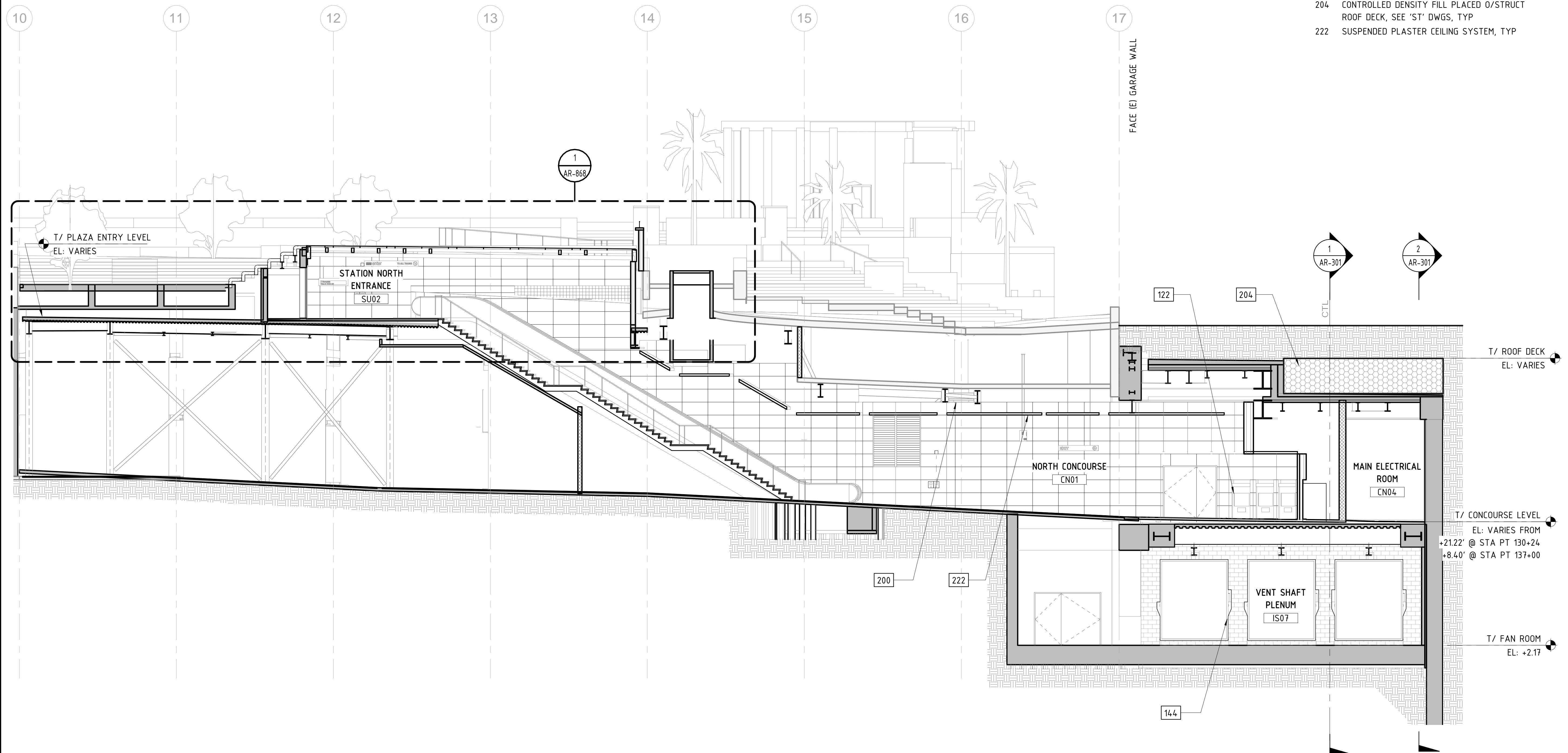
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KEYNOTES

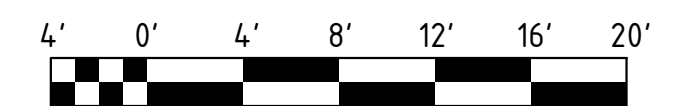
- 122 TICKET VENDING MACHINES (TVM) AND FUTURE ATM LOCATIONS, EXACT NUMBER TBD
- 144 EMERGENCY FAN UNITS (3), SEE 'MV' DRAWINGS, TYP
- 200 STEEL ROOF SUPPORT BEAMS W/ 1HR FIREPROOFING PROTECTION, SEE 'ST' DWGS AND SEE AR-011 FOR FIREPROOFING REQUIREMENTS AT STRUCTURAL FRAME AND METAL DECK, TYP
- 204 CONTROLLED DENSITY FILL PLACED O/STRUCT ROOF DECK, SEE 'ST' DWGS, TYP
- 222 SUSPENDED PLASTER CEILING SYSTEM, TYP



TRANSVERSE (NORTH) @ UMS-S 129+81

SECTION

1
AR-332 1/8" = 1'-0"

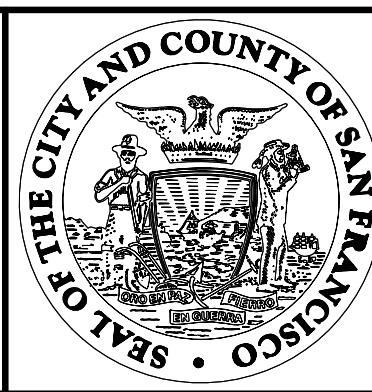
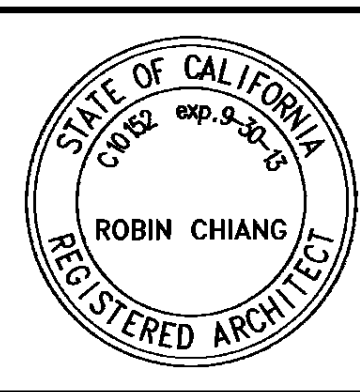


DATE	ISSUED FOR BID	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID		0			

central subway design group

Robin Chiang & Company

DESIGNED
R. CHIANG
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K. BIGOSINSKI
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D. FUNG
REVIEWED
R. CHIANG
RECOMMENDED
A. READ
APPROVED
R. EDWARDS
DATE
02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE / MARKET STREET STATION

ARCHITECTURAL
SECTION - TRANSVERSE (NORTH)
UNION SQUARE - SHEET 2 OF 3

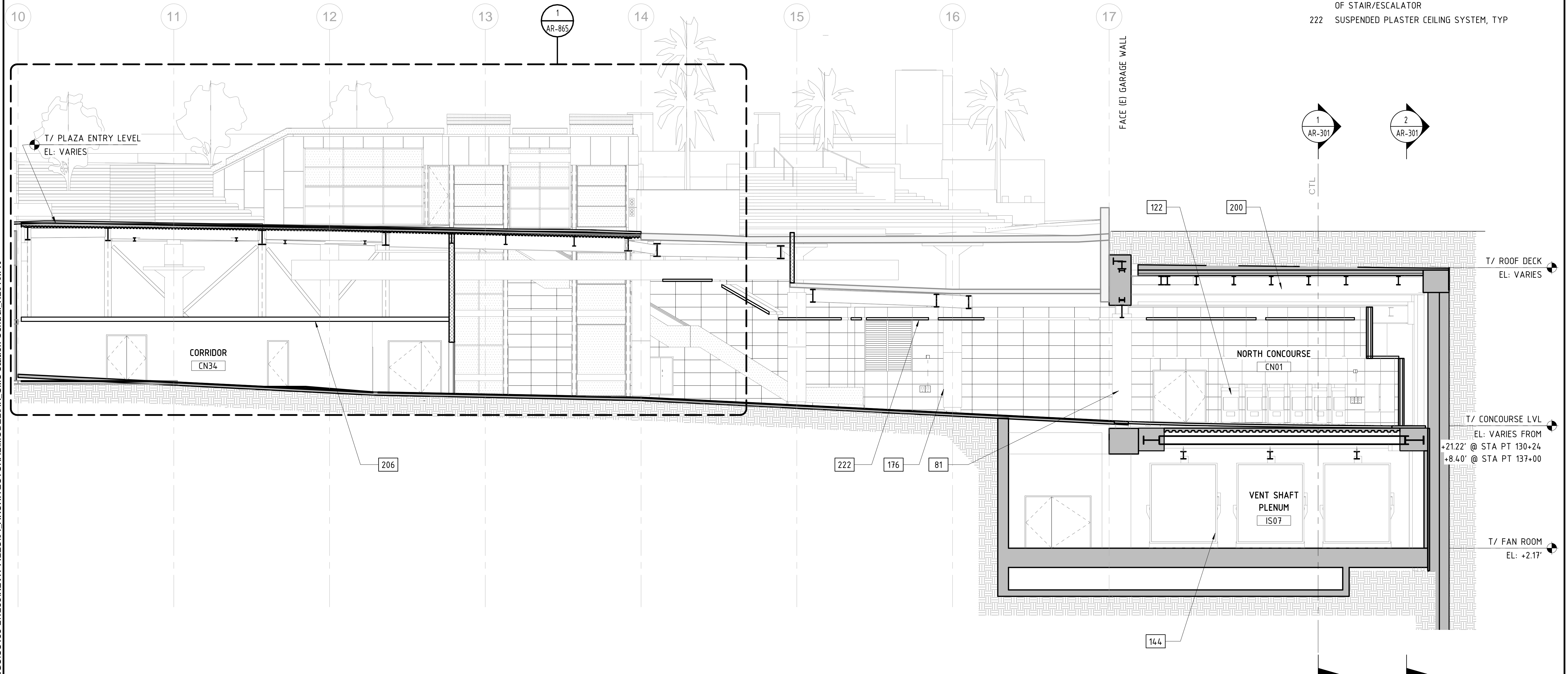
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KEYNOTES

- 81
- 122 TICKET VENDING MACHINES (TVM) AND FUTURE ATM LOCATIONS, EXACT NUMBER TBD
- 144 EMERGENCY FAN UNITS (3), SEE 'MV' DRAWINGS, TYP
- 176 EXISTING UNION SQUARE GARAGE SUPPORT COLUMNS TO RECEIVE STEEL JACKET, SEE 'ST' DWGS, TYP
- 200 STEEL ROOF SUPPORT BEAMS W/ 1HR FIREPROOFING PROTECTION, SEE 'ST' DWGS AND SEE AR-011 FOR FIREPROOFING REQUIREMENTS AT STRUCTURAL FRAME AND METAL DECK, TYP
- 206 2-HOUR FIRE RATED FRAMED CLG AT UNDERSIDE OF STAIR/ESCALATOR
- 222 SUSPENDED PLASTER CEILING SYSTEM, TYP



TRANSVERSE (NORTH) @ UMS-S 130+02.5

1 SECTION
AR-333 1/8" = 1'-0"



DATE	ISSUED FOR BID	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID		0			

central subway design group

Robin Chiang & Company

DESIGNED
R. CHIANG

DRAWN
K. BIGOSINSKI

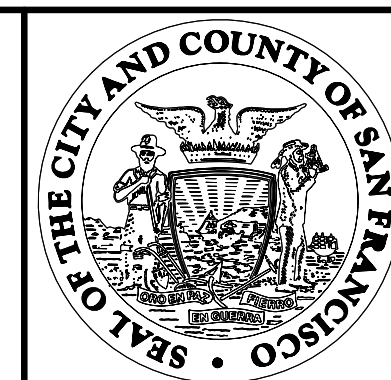
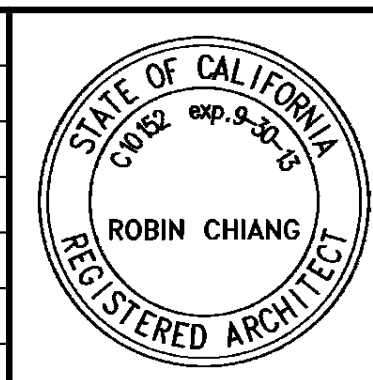
CHECKED
D. FUNG

REVIEWED
R. CHIANG

RECOMMENDED
A. READ

APPROVED
R. EDWARDS

DATE
02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE / MARKET STREET STATION

ARCHITECTURAL
SECTION - TRANSVERSE (NORTH)
UNION SQUARE - SHEET 3 OF 3

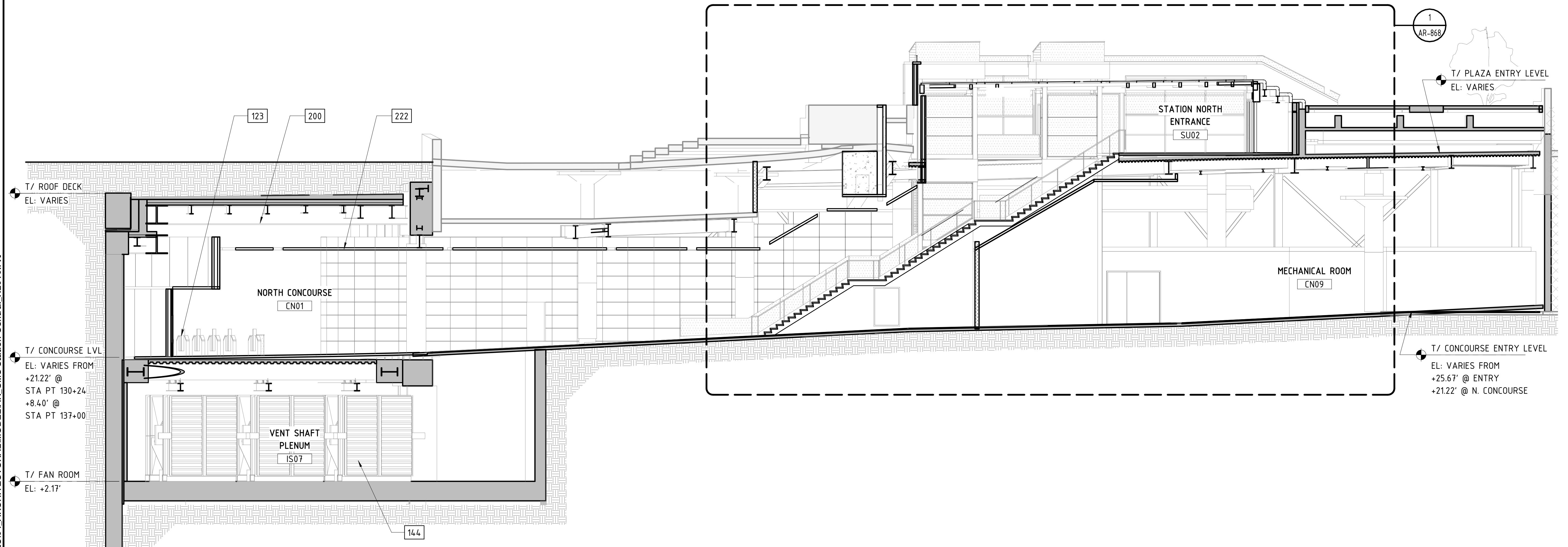
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SHEET NO.	

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KEYNOTES

- 123 STD FAREGATES + (1) ACCESSIBLE FAREGATE ARRAY, TYP
- 144 EMERGENCY FAN UNITS (3), SEE 'MV' DRAWINGS, TYP
- 200 STEEL ROOF SUPPORT BEAMS W/ 1HR FIREPROOFING PROTECTION, SEE 'ST' DWGS AND SEE AR-011 FOR FIREPROOFING REQUIREMENTS AT STRUCTURAL FRAME AND METAL DECK, TYP
- 222 SUSPENDED PLASTER CEILING SYSTEM, TYP



TRANSVERSE (SOUTH) @ GEARY STREET

1
AR-334

SECTION

1/8" = 1'-0" VIEW ALIGNED TO STAIR/ESCALATOR 5° OFF STRUCTURAL GRID

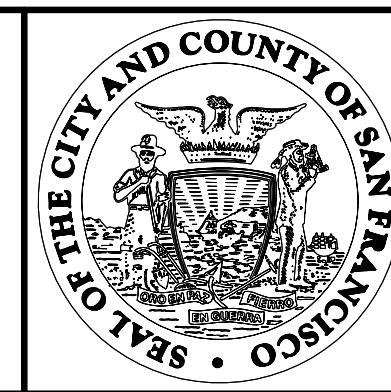
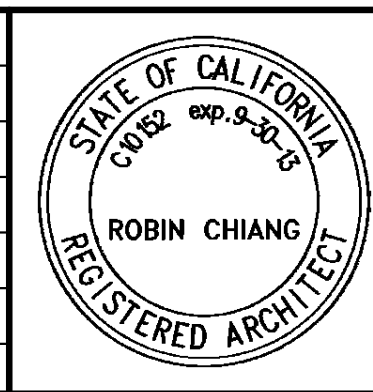


DATE	ISSUED FOR BID	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID		0			

central subway design group

Robin Chiang & Company

DESIGNED
R. CHIANG
DRAWN
K. BIGOSINSKI
CHECKED
D. FUNG
REVIEWED
R. CHIANG
RECOMMENDED
A. READ
APPROVED
R. EDWARDS
DATE
02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

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DIRECTOR OF TRANSPORTATION

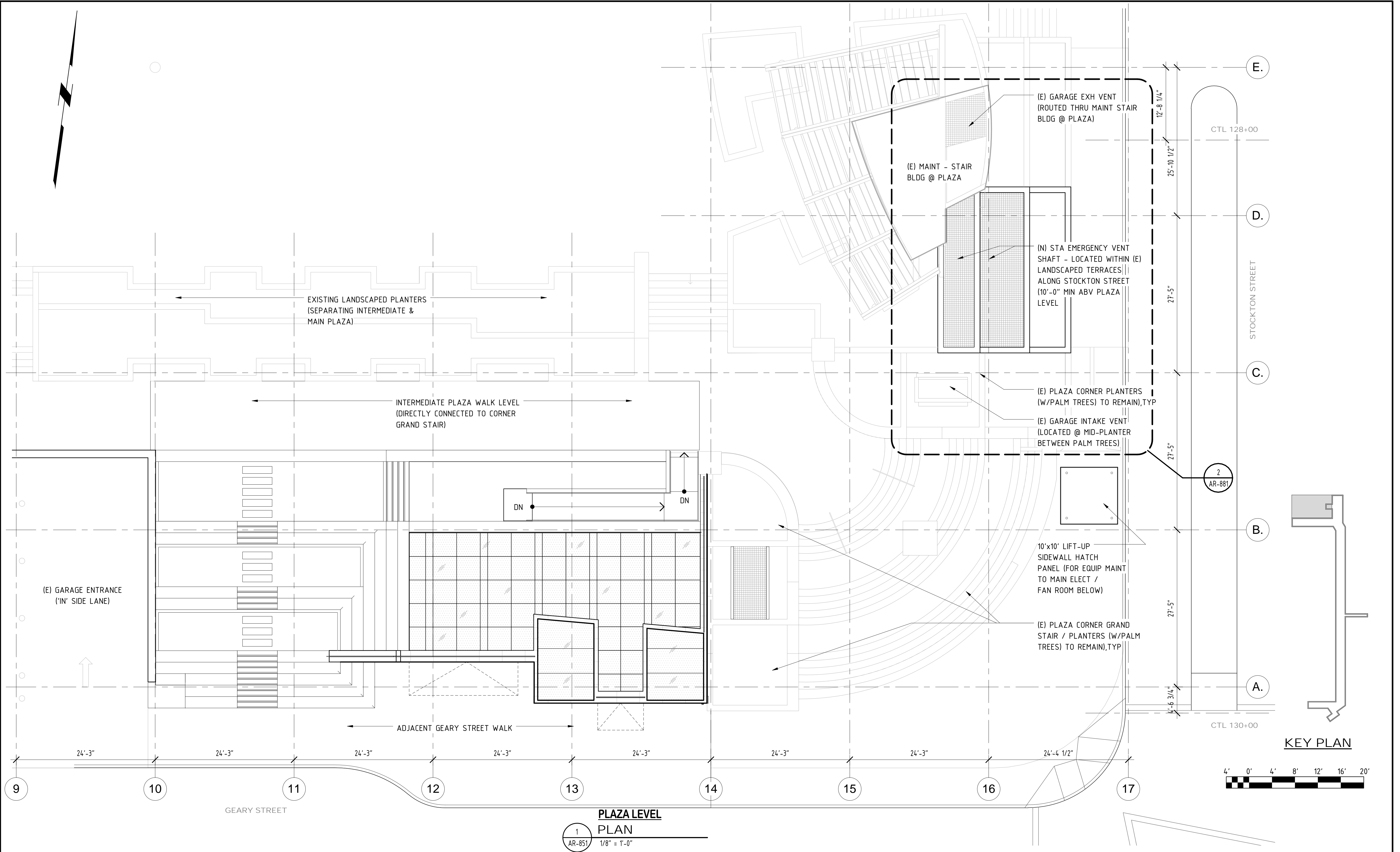
THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE / MARKET STREET STATION

ARCHITECTURAL
SECTION - TRANSVERSE (SOUTH)
UNION SQUARE ENTRANCE

CONTRACT NO. 1253	REVISION
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SHEET NO.	

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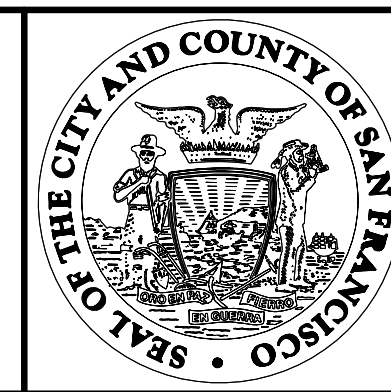
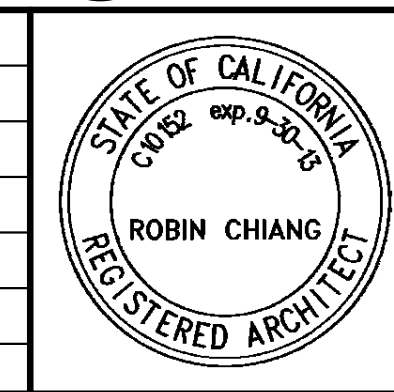
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AR-851
PLAZA LEVEL
PLAN
1/8" = 1'-0"

DATE	ISSUED FOR BID	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID		0			

central subway design group

Robin Chiang & Company

DESIGNED R. CHIANG
DRAWN J. GAINES
CHECKED D. FUNG
REVIEWED R. CHIANG
RECOMMENDED A. READ
APPROVED R. EDWARDS
DATE 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE / MARKET STREET STATION

ARCHITECTURAL
FLOOR PLAN - SURFACE LEVEL
UNION SQUARE PLAZA / GARAGE

CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18875
DRAWING NO. AR-851
REVISION 0

GENERAL SHEET NOTES

- 1 FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
- 2 PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
- 3 REF 'MP' & 'MD' DWGS FOR AREA DRAIN LOCATIONS IN RECONFIGURED TERRACE LAWN AND PAVEMENT AREAS, TYP

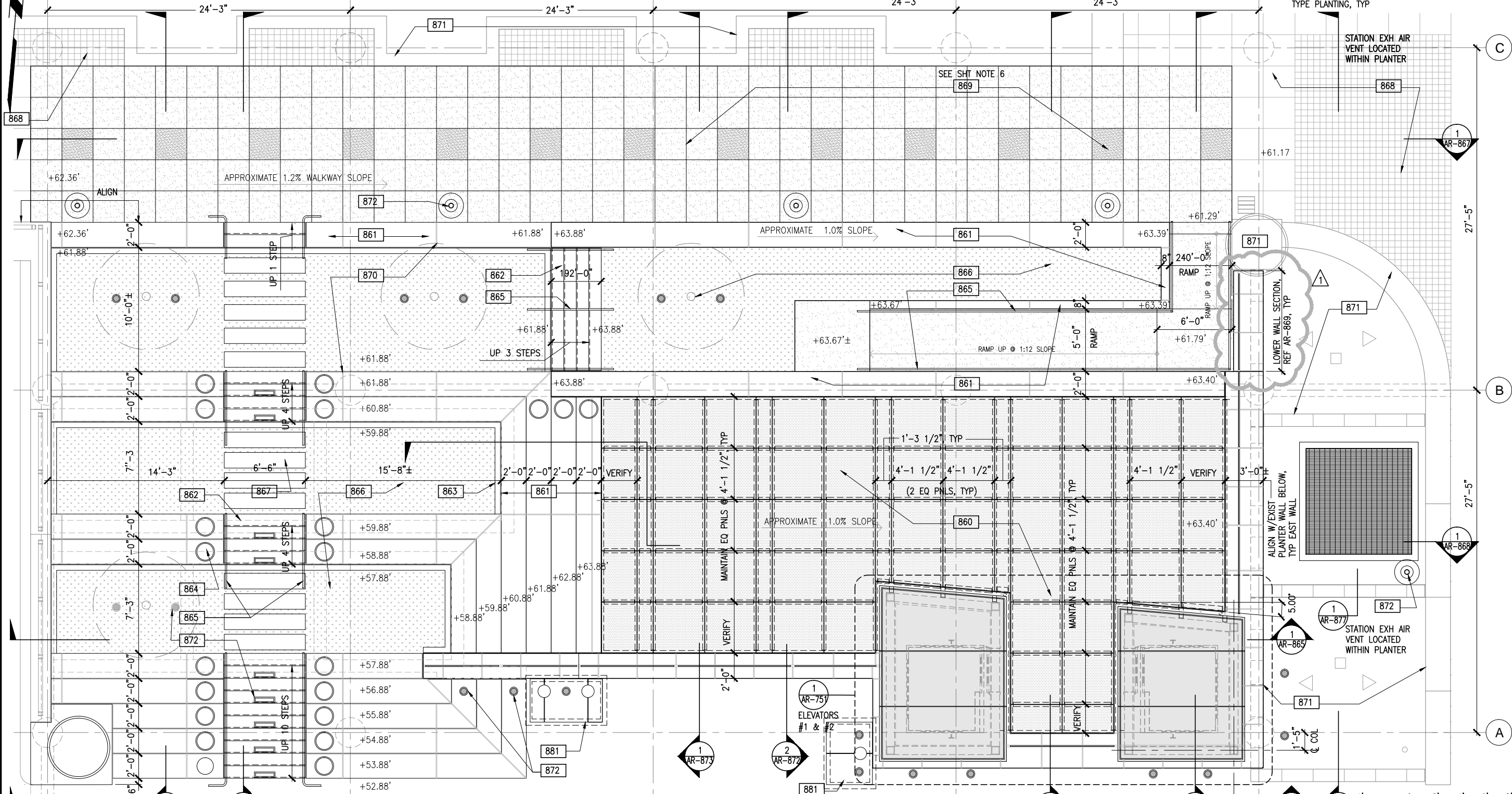
4. COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP
5. COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP
6. PAVEMENT COLORS ARE 'SHADOW SLATE' & 'SOMBRERO BUFF' (ACCENT) W/LIGHT SAND BLAST FINISH TO MATCH, TYP

KEYNOTES

- 860 1 1/2-HR FIRE RATED GLASS WALK ROOF DECK SYSTEM, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
- 862 PRE-CAST CONC STAIR STEPS, TYP
- 863 PRE-CAST CONC MOW STRIP, TYP
- 864 PRE-CAST CONC PLANTER POTS (EXISTING LARGE / SMALL) - SEE 'LA' DWGS FOR PLANTING, TYP
- 865 36" H S. ST. STAIR / RAMP HANDRAILS, TYP

- 866 LAWN TERRACE W/TREE PLANTING - SEE 'LA' DWGS, TYP
- 867 PRE-CAST CONC PAVER STEPS, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP
- 869 RECONSTRUCT INTERMEDIATE PLAZA WALKWAY - ALIGN JOINTS W/EXIST & NEW PRE-CAST JTS AS INDICATED (WALK COLORS TO MATCH EXIST INCL 'ACCENT' SQUARE - SHOWN DARKER), TYP
- 870 PRE-CAST CONC JOINT, TYP

- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP
- 872 LIGHT STD W/LIGHT, IN-GROUND ACCENT LIGHTING @ PLANTING & HARDSCAPED AREAS AND RECESSED STEP LIGHTING AS INDICATED, SEE 'EL' DWGS, TYP
- 881 BELOW SIDEWALK PLANTER ADJACENT WALL W/REMOVABLE PRE-CAST COVER (FLUSH W/SIDEWALK), APPROX 3'-0" WIDE x 5'-6" LG x 30" DEEP, SEE 'LA' DWGS FOR VINE TYPE PLANTING, TYP



1 NORTH ENTRANCE PLAN @ TERRACE LEVEL
SCALE: 1/4"=1'-0"

FOR ORIGINAL SIGNATURES, SEE CL-18880, REV. 0.

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DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

central subway design group

Robyn Chiang & Company

DESIGNED R. CHIANG
DRAWN J. GAINES
CHECKED D. FUNG
REVIEWED R. CHIANG
RECOMMENDED A. READ
APPROVED R. EDWARDS
DATE 02/15/2012

REV. 0
SEALED BY
R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
NORTH ENTRANCE
ENLARGED PLAN - TERRACE LEVEL

CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18880
DRAWING NO. AR-861
SHEET NO. 1

SENSITIVE SECURITY INFORMATION

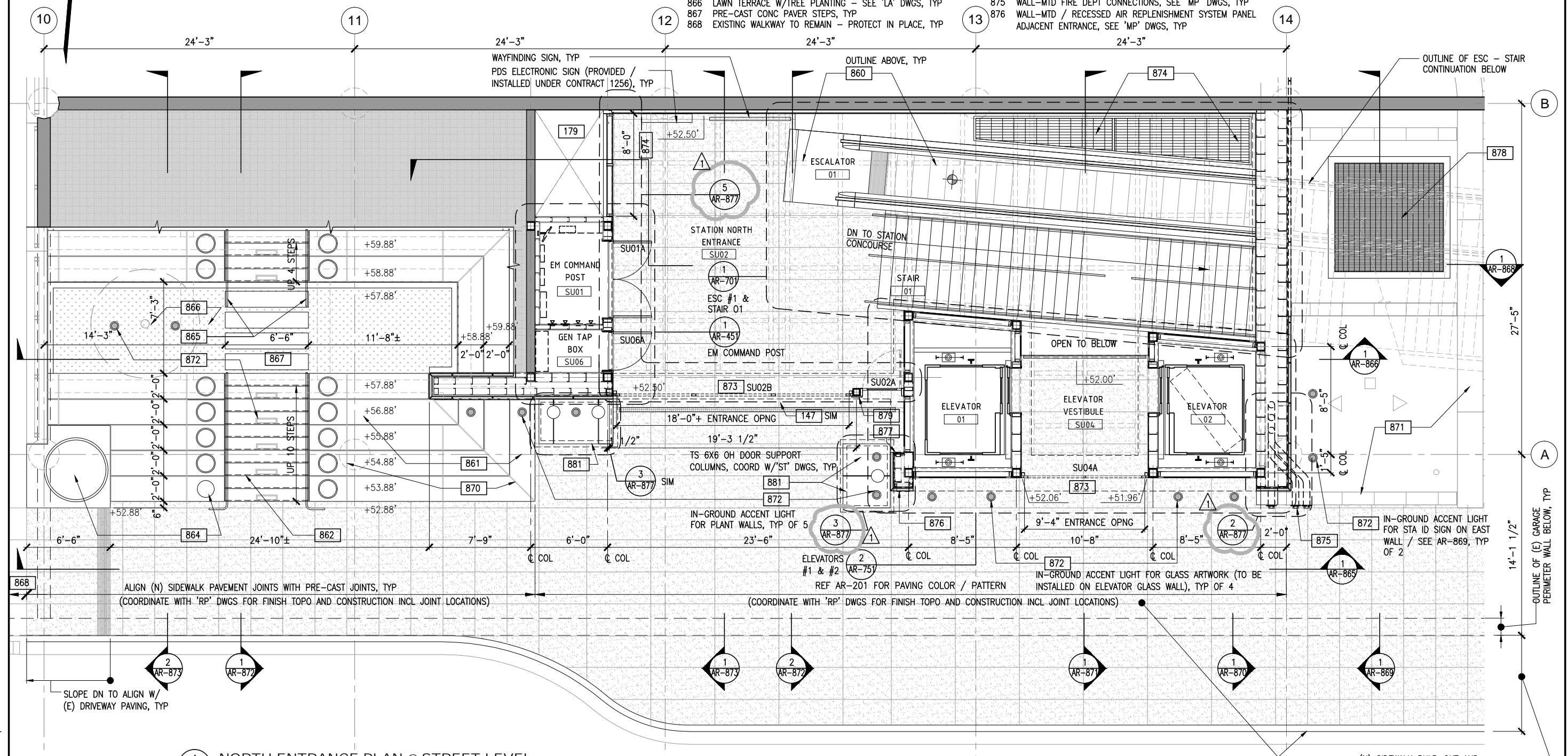
GENERAL SHEET NOTES

- 1 FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
- 2 PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
3. REF 'MP' & 'MD' DWGS FOR AREA DRAIN LOCATIONS IN RECONFIGURED TERRACE LAWN AND PAVEMENT AREAS, TYP
4. COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP

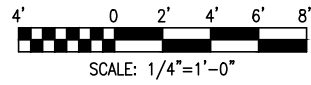
5. COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP

KEYNOTES

- 147 CONTINUOUS ST. STL GRATING OVER TRENCH DRAIN, REF 'MP' DWGS, SEE DETAIL 3/AR-814, TYP
- 179 FLOOR OPNGS FOR MECHANICAL HVAC DUCTS, SEE 'ST' & 'ME' DWGS, TYP
- 860 1 1/2-HR FIRE RATED GLASS WALK ROOF DECK SYSTEM, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
- 862 PRE-CAST CONC STAIR STEPS, TYP
- 863 PRE-CAST CONC MOW STRIP, TYP
- 864 PRE-CAST CONC PLANTER POTS (EXISTING LARGE / SMALL) - SEE 'LA' DWGS FOR PLANTING, TYP
- 865 36" H S. ST. STAIR / RAMP HANDRAILS, TYP
- 866 LAWN TERRACE W/TREE PLANTING - SEE 'LA' DWGS, TYP
- 867 PRE-CAST CONC PAVER STEPS, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP
- 870 PRE-CAST CONC JOINT, TYP
- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP
- 872 LIGHT STD W/LIGHT, IN-GROUND ACCENT LIGHTING @ PLANTING & HARDSCAPED AREAS AND RECESSED STEP LIGHTING AS INDICATED, SEE 'EL' DWGS, TYP
- 873 OH BI-FOLD STATION ENTRANCE DOOR SYSTEM, TYP
- 874 STATION AIR-INTAKE VENT LOUVERS (2 LOCATIONS - HORIZONTALLY-MTD BWTN COL LINE '13' - '14' & VERTICAL WALL LOUVERS (REF 'ME' FOR HVAC CONNECTION BELOW), TYP
- 875 WALL-MTD FIRE DEPT CONNECTIONS, SEE 'MP' DWGS, TYP
- 876 WALL-MTD / RECESSED AIR REPLENISHMENT SYSTEM PANEL ADJACENT ENTRANCE, SEE 'MP' DWGS, TYP
- 877 WALL-MTD / RECESSED SFFD KEY LOCK BOX ADJACENT ENTRANCE, TYP
- 878 STATION EXH AIR VENT STRUCTURE W/STL GRATING LOCATED WITHIN (E) MIDDLE PLANTER - SEE 'ST' DWGS, TYP
- 879 STATION ENTRANCE NAME ID SIGN PLAQUE IN BRILLE, SEE AR-900 SIGNAGE DWGS
- 881 BELOW SIDEWALK PLANTER ADJACENT WALL W/REMOVABLE PRE-CAST COVER (FLUSH W/SIDEWALK), APPRX 3'-0" WIDE x 5'-6" LG x 30" DEEP, SEE 'LA' DWGS FOR VINE TYPE PLANTING, TYP



1 NORTH ENTRANCE PLAN @ STREET LEVEL
SCALE: 1/4"=1'-0"



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 Janglim Tue May 15, 2012 3:34 pm

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0
 SEALED BY
 R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

**ARCHITECTURAL
 NORTH ENTRANCE
 ENLARGED PLAN - STREET LEVEL**

CONTRACT NO. 1253	REVISION 1
SFMTA CONTROL NO. CL-18881	
DRAWING NO. AR-862	
SHEET NO.	

GENERAL SHEET NOTES

- 1 REF AR-121 FOR FULL PLAN CONFIGURATION AND T/FLOOR TOPO, TYP
- 2 PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
- 3 COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE EXISTING GARAGE COLUMNS, TYP
- 4 COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP
- 5 REFERENCE 'MP' & 'MD' DWGS FOR DRAIN PIPING COMING DOWN FROM TERRACE AREA DRAINS, SIDEWALK DRAINS, PLANTER DRAINS ABOVE, TYP
- 6 REFERENCE 'ME' DWGS FOR MECHANICAL EQUIPMENT / DUCTS LOCATED WITHIN MECHANICAL ROOM CN09, TYP

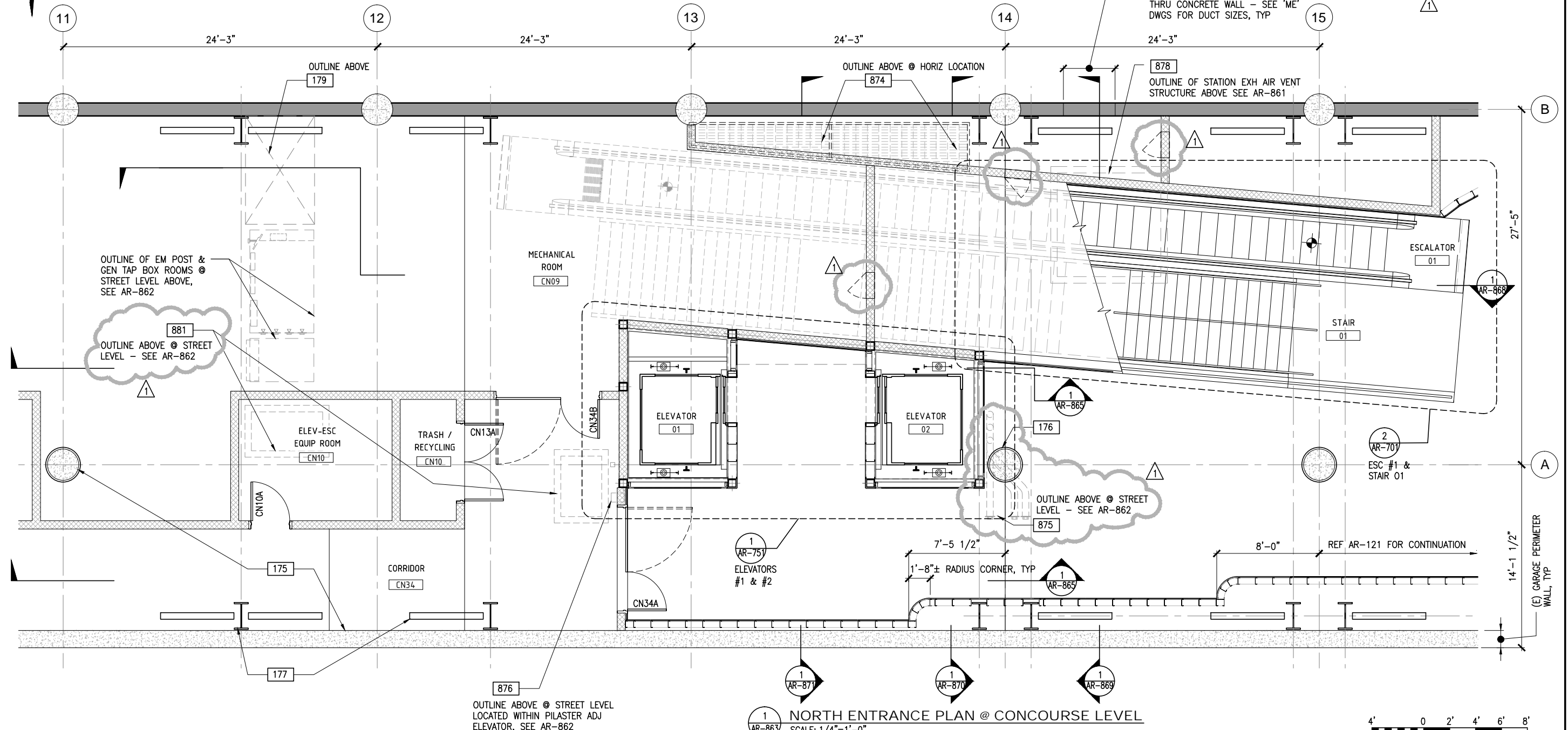
KEYNOTES

- 175 EXISTING UNION SQUARE GARAGE PERIMETER WALL AND INTERIOR SUPPORT COLUMNS TO REMAIN, TYP
- 176 EXISTING UNION SQUARE GARAGE SUPPORT COLUMNS TO RECEIVE STEEL JACKET, SEE 'ST' DWGS, TYP
- 177 STEEL SUPPORT COLUMNS AND BRACE FRAME, SEE 'ST' DWGS, TYP
- 179 FLOOR OPNGS FOR MECHANICAL HVAC DUCTS, SEE 'ST' & 'ME' DWGS, TYP
- 874 STATION AIR-INTAKE VENT LOUVERS (2 LOCATIONS - HORIZONTALLY-MTD BWTN COL LINE '13' - '14' & VERTICAL WALL LOUVERS (REF 'ME' FOR HVAC CONNECTION BELOW), TYP
- 875 WALL-MTD FIRE DEPT CONNECTIONS, SEE 'MP' DWGS, TYP
- 876 WALL-MTD / RECESSED AIR REPLENISHMENT SYSTEM PANEL ADJACENT ENTRANCE, SEE 'MP' DWGS, TYP

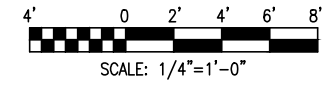
- 878 STATION EXH AIR VENT STRUCTURE W/STL GRATING LOCATED WITHIN (E) MIDDLE PLANTER - SEE 'ST' DWGS, TYP
- 881 BELOW SIDEWALK PLANTER ADJACENT WALL W/REMOVABLE PRE-CAST COVER (FLUSH W/SIDEWALK), APPRX 3'-0" WIDE x 5'-6" LG x 30" DEEP, SEE 'LA' DWGS FOR VINE TYPE PLANTING, TYP

LEGEND

- (N) 8" CMU WALL, TYP
- (N) MTL STUD WALL, TYP
- (N) STRUCTURAL CONC WALL, SEE 'ST' DWGS, TYP
- METAL ACCESS PANEL LOCATED IN CMU WALLS / MIN 27"x27" CLR OPNG / MATCH WALL FIRE-RATING, COORDINATE & VERIFY FINAL LOCATION IN FIELD, TYP



1 NORTH ENTRANCE PLAN @ CONCOURSE LEVEL
SCALE: 1/4"=1'-0"



FOR ORIGINAL SIGNATURES, SEE CL-18882, REV. 0.

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 Janglim Tue May 15, 2012 3:34 pm NORTH ENTRANCE

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED BY: R. CHIANG
 DRAWN BY: J. GAINES
 CHECKED BY: D. FUNG
 REVIEWED BY: R. CHIANG
 RECOMMENDED BY: A. READ
 APPROVED BY: R. EDWARDS
 DATE: 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0
 SEALED BY
 R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

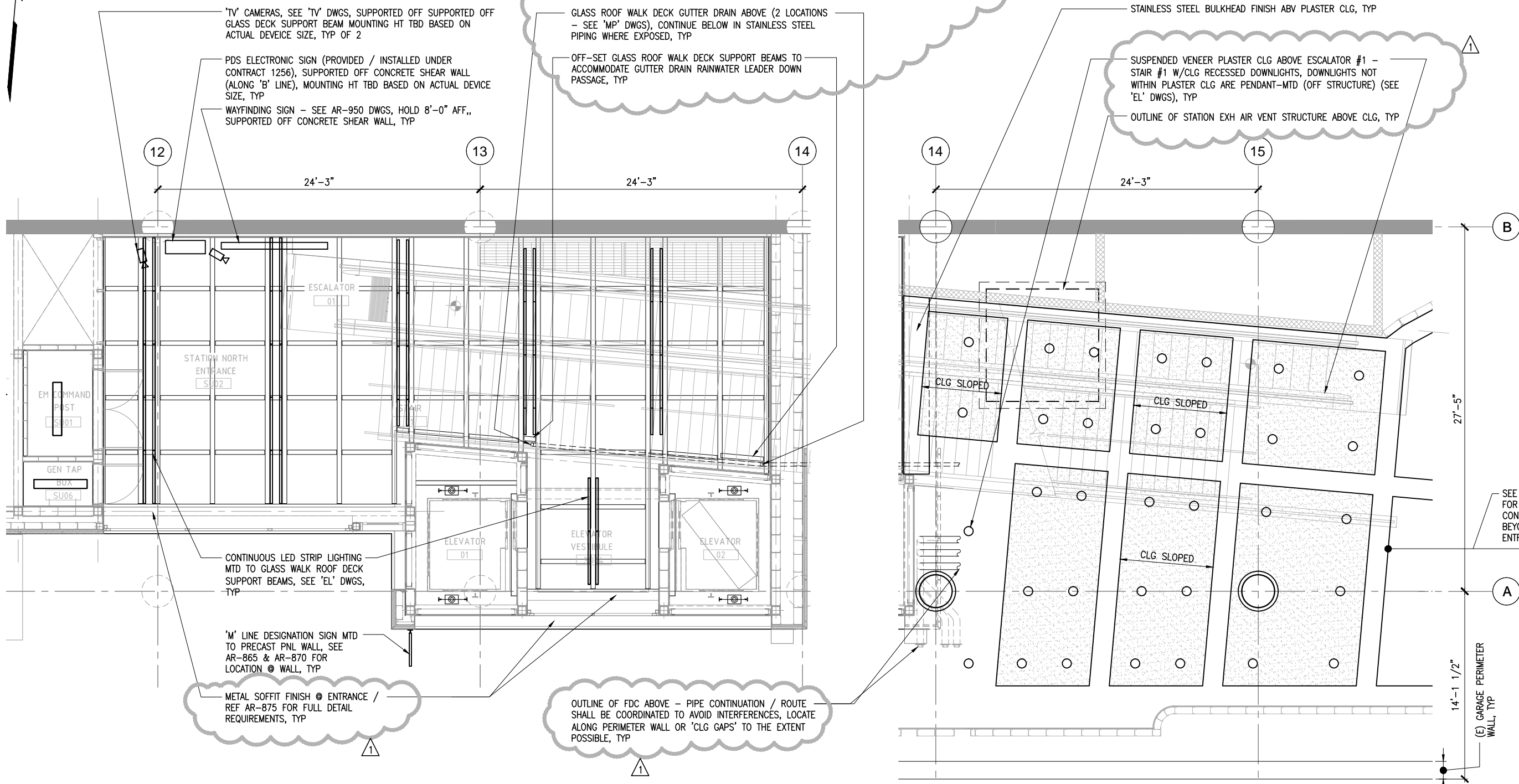
ARCHITECTURAL
 NORTH ENTRANCE
 ENLARGED PLAN - CONCOURSE LEVEL

CONTRACT NO. 1253	REVISION
SFMTA CONTROL NO. CL-18882	
DRAWING NO. AR-863	1
SHEET NO.	

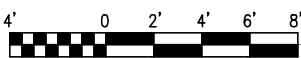
SENSITIVE SECURITY INFORMATION

GENERAL SHEET NOTES

1. PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
2. COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP
3. COORDINATE WITH GLASS DECK MFER FOR ALLOWABLE & APPROPRIATE ANCHORAGE TO GLASS DECK SUPPORT BEAMS AS NOT TO COMPROMISE THE INTUMESCENT FIRE-PAINT PROTECTION O/BEAMS FOR SUPPORT OF CLG-MTD EQUIPMENT (CAMERAS, SIGNS, LIGHTS, ETC), TYP
4. REF 'FP' DWGS FOR FIRE-SPRINKLER DESIGN REQUIREMENTS LOCATED BELOW GLASS WALK ROOF DECK - SPRINKLERS 'ADD' ADDITIONAL 1/2 HOUR FIRE-RATED PROTECTION COMBINED WITH THE 1 1/2 HOUR FIRE-RATED GLASS DECK ACHIEVE / OBTAIN AN OVERALL 2-HOUR EQUIVALENT GLASS ROOF WALK DECK FIRE-RATED PROTECTION SYSTEM, TYP



2 REFLECTED CEILING PLAN @ STREET LEVEL
SCALE: 1/4"=1'-0"



1 REFLECTED CEILING PLAN @ CONCOURSE LEVEL
SCALE: 1/4"=1'-0"

FOR ORIGINAL SIGNATURES, SEE CL-18883, REV. 0.

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 JAnglim Tue May 15, 2012 3:35 pm NORTH ENTRANCE

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0
 SEALED BY
 R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 NORTH ENTRANCE
 ENLARGED REFLECTED CEILING PLANS

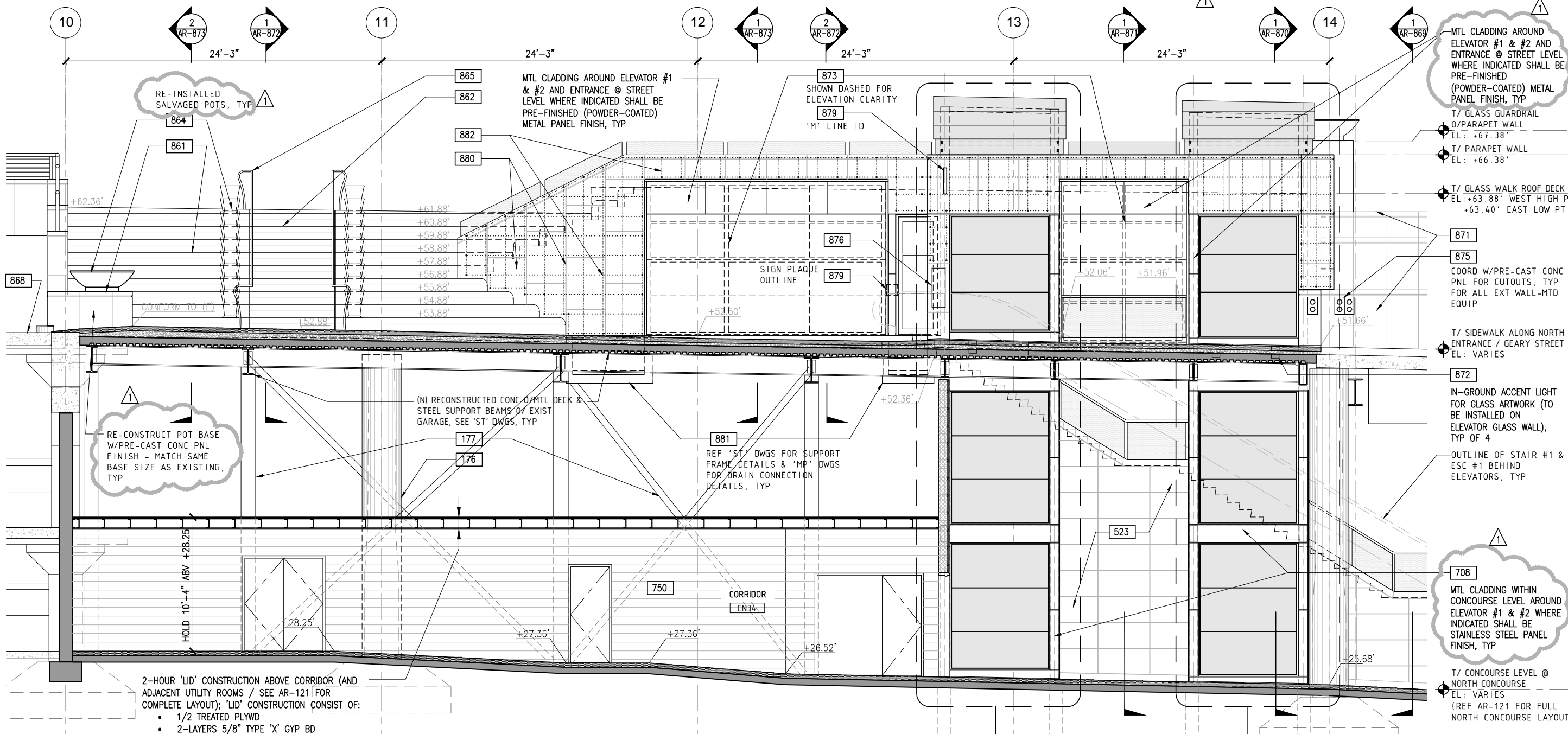
CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18883
DRAWING NO. AR-864
SHEET NO. 1

GENERAL SHEET NOTES

- 1 FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
- 2 PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
- 3 REF 'MP' & 'MD' DWGS FOR AREA DRAIN LOCATIONS IN RECONFIGURED TERRACE LAWN AND PAVEMENT AREAS, TYP
- 4 COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP
5. COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP

KEYNOTES

- 176 EXISTING UNION SQUARE GARAGE SUPPORT COLUMNS TO RECEIVE STEEL JACKET, SEE 'ST' DWGS, TYP
- 177 STEEL SUPPORT COLUMNS AND BRACE FRAME, SEE 'ST' DWGS, TYP
- 523 CRYSTALLIZED GLASS WALL PANEL FINISH, TYP
- 708 STAINLESS STEEL WALL PANEL, TYP
- 750 CMU WALL, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
- 862 PRE-CAST CONC STAIR STEPS, TYP
- 864 PRE-CAST CONC PLANTER POTS (EXISTING LARGE / SMALL) - SEE 'LA' DWGS FOR PLANTING, TYP
- 865 36" H S. ST. STAIR / RAMP HANDRAILS, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP
- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP
- 872 LIGHT STD W/LIGHT, IN-GROUND ACCENT LIGHTING @ PLANTING & HARDSCAPED AREAS AND RECESSED STEP LIGHTING AS INDICATED, SEE 'EL' DWGS, TYP
- 873 OH BI-FOLD STATION ENTRANCE DOOR SYSTEM, TYP
- 875 WALL-MTD FIRE DEPT CONNECTIONS, SEE 'MP' DWGS, TYP
- 876 WALL-MTD / RECESSED AIR REPLENISHMENT SYSTEM PANEL ADJACENT ENTRANCE, SEE 'MP' DWGS, TYP
- 877 WALL-MTD / RECESSED SFFD KEY LOCK BOX ADJACENT ENTRANCE, TYP
- 879 STATION ENTRANCE ID SIGNAGE W/'BRAILLE' TEXT, SEE AR-900 SIGNAGE DWGS FOR DESIGN DETAILS, TYP
- 880 PRE-CAST CONCRETE WALL PANEL FINISH, TYP
- 881 BELOW SIDEWALK PLANTER ADJACENT WALL W/REMOVABLE PRE-CAST COVER (FLUSH W/SIDEWALK), APPROX 3'-0" WIDE x 5'-6" LG x 30" DEEP, SEE 'LA' DWGS FOR VINE TYPE PLANTING, TYP
- 882 VINE SUPPORT CABLE ATTACHED TO PRE-CAST CONC WALL FACADE, SEE 'LA' DWGS FOR WIRE SUPPORT CABLE DETAIL, TYP



2-HOUR 'LID' CONSTRUCTION ABOVE CORRIDOR (AND ADJACENT UTILITY ROOMS / SEE AR-121 FOR COMPLETE LAYOUT); 'LID' CONSTRUCTION CONSIST OF:

- 1/2 TREATED PLYWD
- 2-LAYERS 5/8" TYPE 'X' GYP BD
- 8"x16 GA 'C' CHANNELS CLG JOISTS @ 16" OC SUPPORTED O/CMU WALL
- 2-LAYERS 5/8" TYPE 'X' GYP BD W/VENEERED PLASTER FINISH

LOOKING NORTH TOWARD ENTRANCE ALONG GEARY STREET

SECTION AR-865 SCALE: 1/4"=1'-0"

FOR ORIGINAL SIGNATURES, SEE CL-18884, REV. 0.

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 Janglim Tue May 15, 2012 3:35 pm NORTH ENTRANCE - SOUTH ELEVATION

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	0			
02/15/2012	ISSUED FOR BID	1			

central subway design group

Robyn Chiang & Company

DESIGNED R. CHIANG
DRAWN J. GAINES
CHECKED D. FUNG
REVIEWED R. CHIANG
RECOMMENDED A. READ
APPROVED R. EDWARDS
DATE 02/15/2012

REV. 0
SEALED BY
R. CHIANG

CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
NORTH ENTRANCE - SOUTH ELEVATION
LONGITUDINAL SECTION (NORTH)

CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18884
DRAWING NO. AR-865
SHEET NO. 1

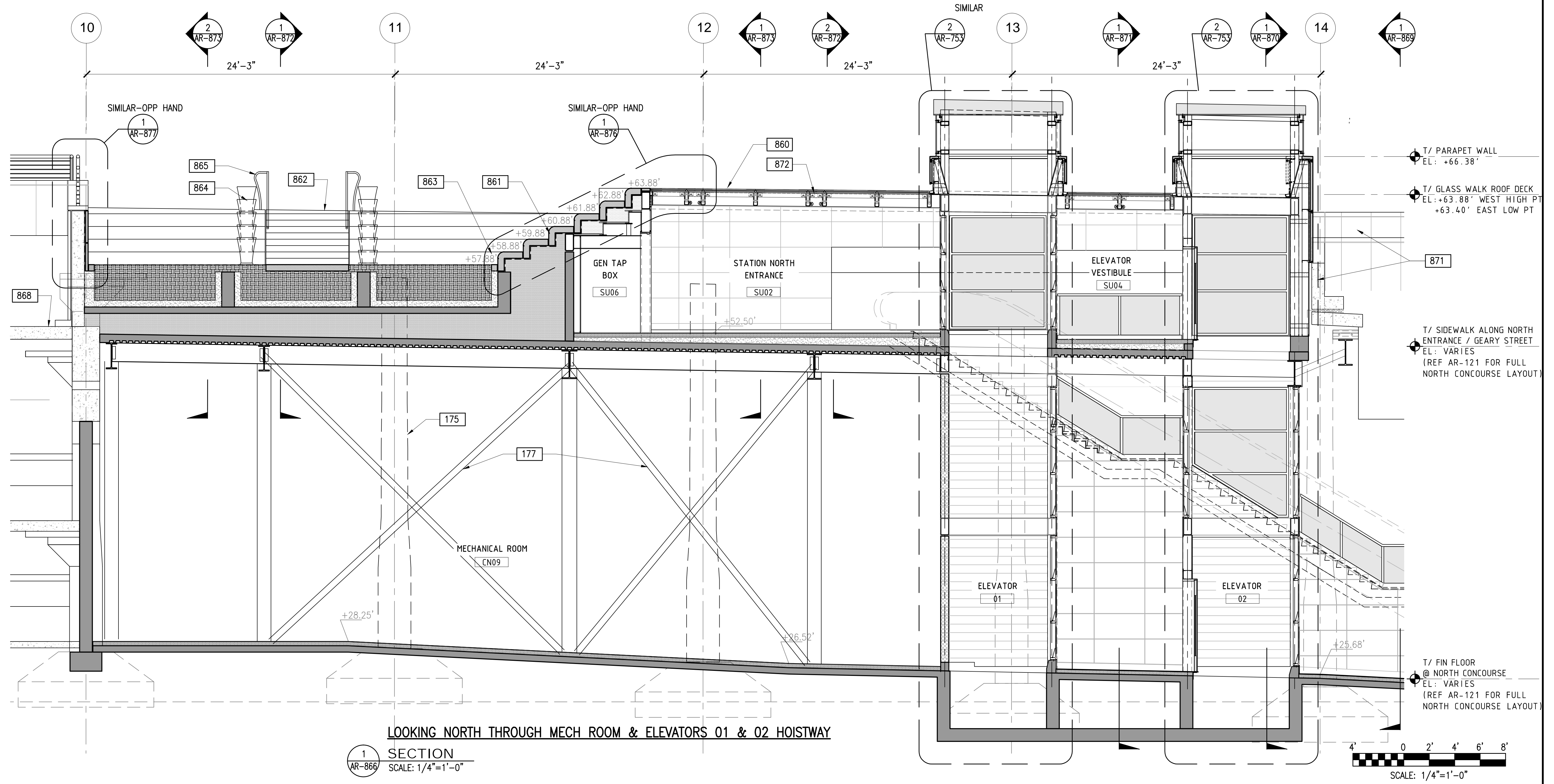
GENERAL SHEET NOTES

1. FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
2. PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
3. REF 'MP' FOR AREA DRAIN LOCATIONS IN RECONFIGURED TERRACE LAWN AND PAVEMENT AREAS, TYP
4. COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP

5. COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP

KEYNOTES

- 175 EXISTING UNION SQUARE GARAGE PERIMETER WALL AND INTERIOR SUPPORT COLUMNS TO REMAIN, TYP
- 177 STEEL SUPPORT COLUMNS AND BRACE FRAME, SEE 'ST' DWGS, TYP
- 860 2-HOURS GLASS WALK ROOF DECK SYSTEM, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
- 862 PRE-CAST CONC STAIR STEPS, TYP
- 863 PRE-CAST CONC MOW STRIP, TYP
- 864 PRE-CAST CONC PLANTER POTS (EXISTING LARGE / SMALL) - SEE 'LA' DWGS FOR PLANTING, TYP
- 865 36" H S. ST. STAIR / RAMP HANDRAILS, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP
- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP
- 872 LIGHT & LIGHT POLE / IN-GROUND TREE ACCENT LIGHTING / RECESSED STEP LIGHTING, SEE 'EL' DWGS, TYP



LOOKING NORTH THROUGH MECH ROOM & ELEVATORS 01 & 02 HOISTWAY

SECTION 1
SCALE: 1/4"=1'-0"

SCALE: 1/4"=1'-0"

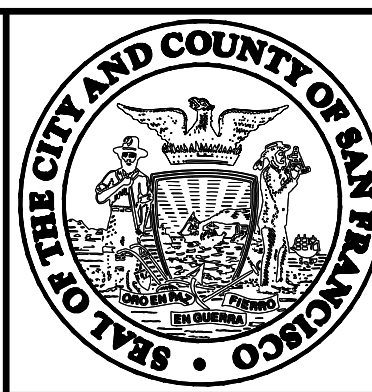
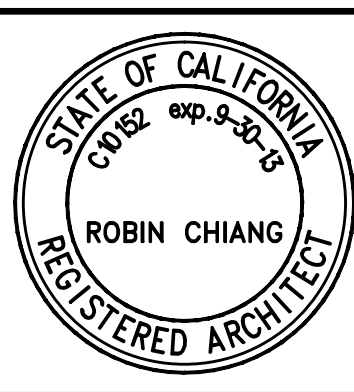
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 janglim Fri Mar 30,2012 7:22 pm AR-866 NORTH ENTRANCE

DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
02/15/2012	ISSUED FOR BID	0			

central subway design group

Robinson Chiang & Company

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 NORTH ENTRANCE
 LONGITUDINAL SECTION (NORTH)

CONTRACT NO.	1253
SFMTA CONTROL NO.	CL-18885
DRAWING NO.	AR-866
SHEET NO.	0

GENERAL SHEET NOTES

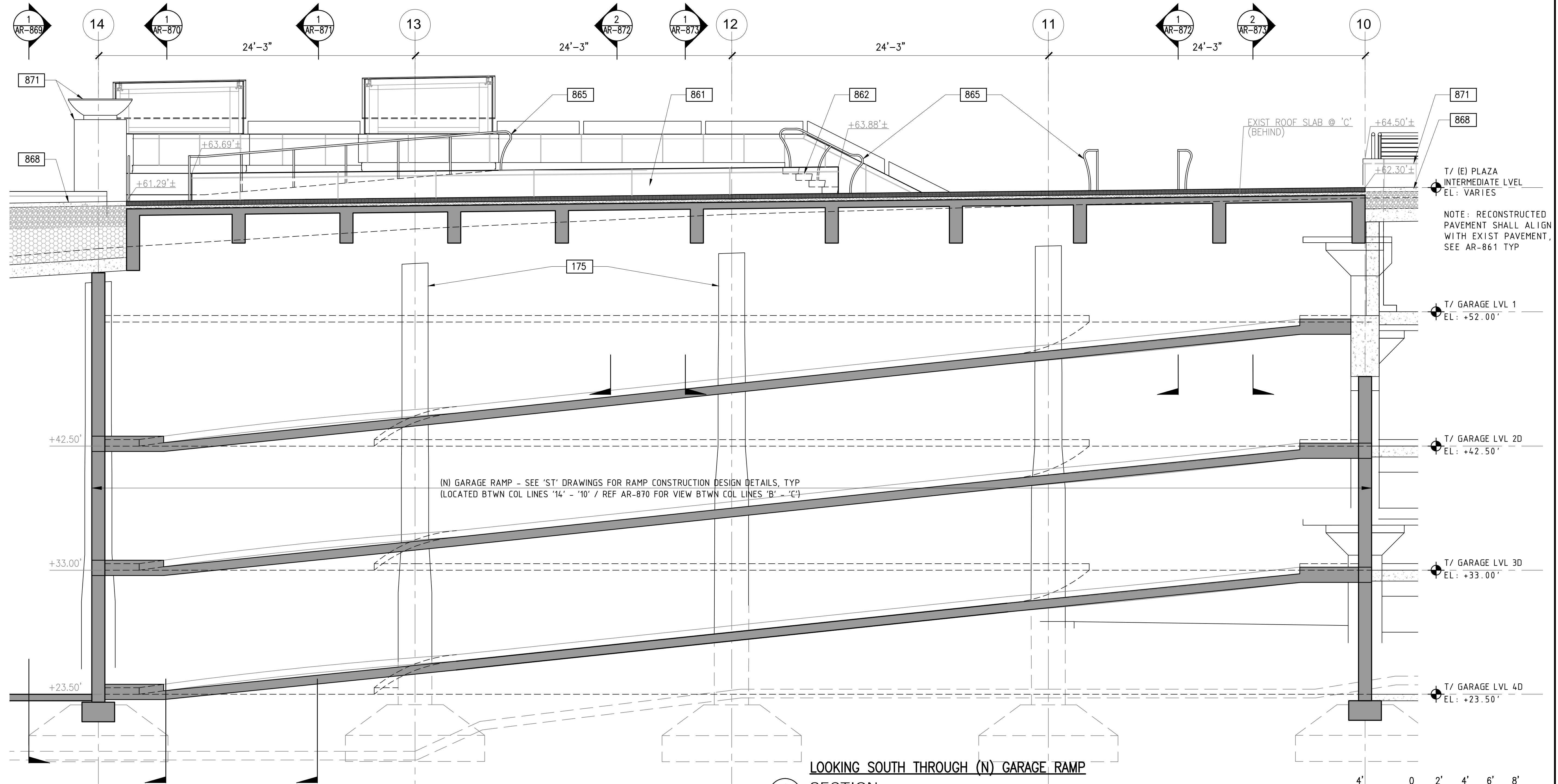
- 1 FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
- 2 PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
3. REF 'MP' FOR AREA DRAIN LOCATIONS IN RECONFIGURED TERRACE LAWN AND PAVEMENT AREAS, TYP
4. COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP

5. COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP

KEYNOTES

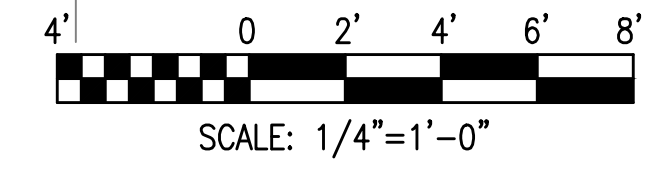
- 175 EXISTING UNION SQUARE GARAGE PERIMETER WALL AND INTERIOR SUPPORT COLUMNS TO REMAIN, TYP
- 177 STEEL SUPPORT COLUMNS AND BRACE FRAME, SEE 'ST' DWGS, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
- 862 PRE-CAST CONC STAIR STEPS, TYP
- 865 36" H S. ST. STAIR / RAMP HANDRAILS, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP

- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP



LOOKING SOUTH THROUGH (N) GARAGE RAMP

1 SECTION
SCALE: 1/4"=1'-0"



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 janglim Fri Mar 30,2012 - 7:23 pm AR-867 NORTH ENTRANCE - NORTH ELEVATION

02/15/2012 ISSUED FOR BID DATE DESCRIPTION REV NO BY CHECKED APPROVED		DESIGNED: R. CHIANG DRAWN: J. GAINES CHECKED: D. FUNG REVIEWED: R. CHIANG RECOMMENDED: A. READ APPROVED: R. EDWARDS DATE: 02/15/2012				CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED DIRECTOR OF TRANSPORTATION		THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION ARCHITECTURAL NORTH ENTRANCE - NORTH ELEVATION LONGITUDINAL SECTION (SOUTH)		CONTRACT NO. 1253 SFMTA CONTROL NO. CL-18886 DRAWING NO. AR-867 SHEET NO. 0	
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GENERAL SHEET NOTES

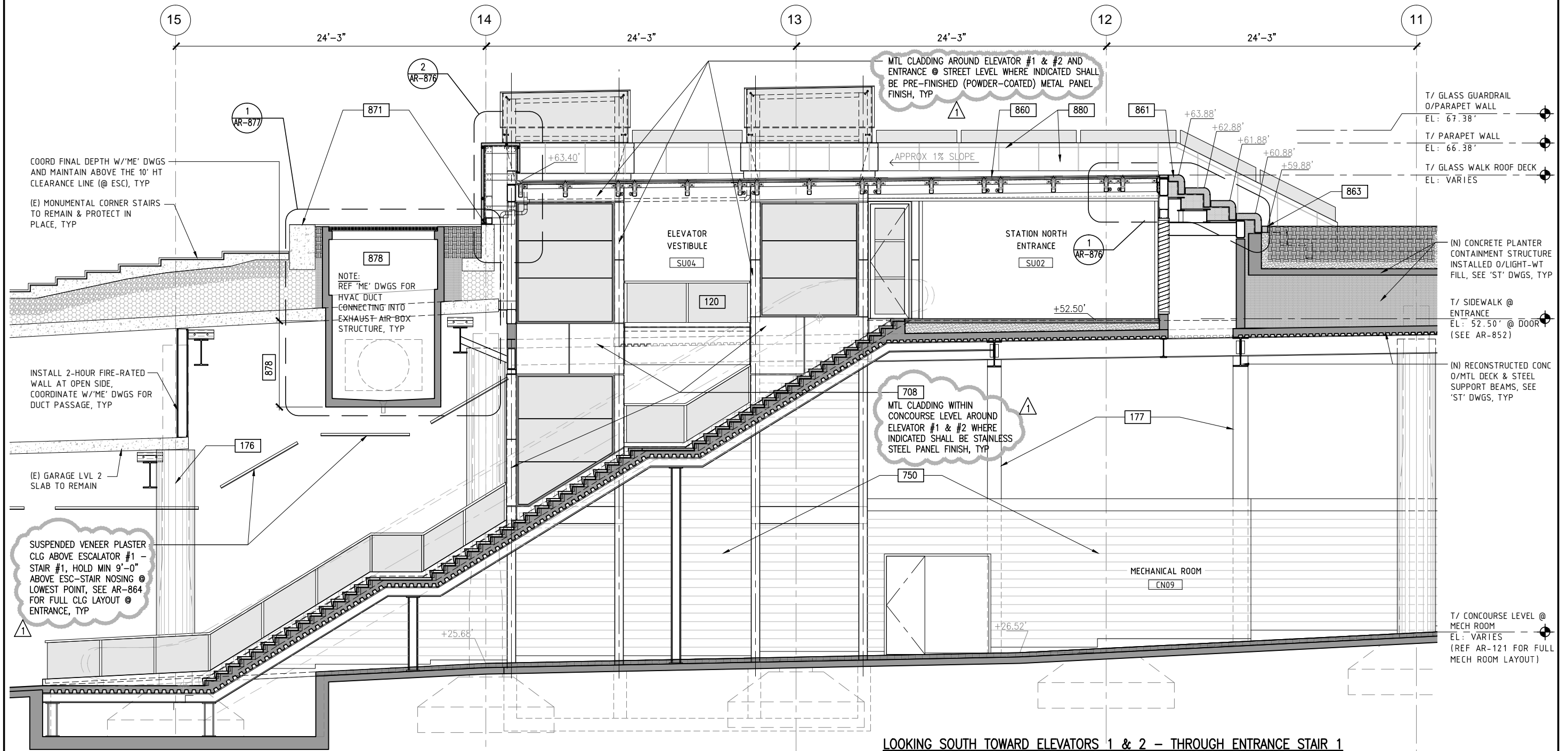
- 1 FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
- 2 PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
3. COORDINATE WITH AND REF 'MP' & 'MD' DWGS FOR AREA DRAINS LOCATED WITHIN RECONFIGURED TERRACE LAWN PLANTER STRUCTURES AND PAVEMENT AREAS, TYP
4. COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP

5. COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP

KEYNOTES

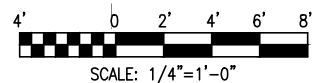
- 120 42" FLOOR-MT GLASS RAIL BARRIER, TYP
- 176 EXISTING UNION SQUARE GARAGE SUPPORT COLUMNS TO RECEIVE STEEL JACKET, SEE 'ST' DWGS, TYP
- 177 STEEL SUPPORT COLUMNS AND BRACE FRAME, SEE 'ST' DWGS, TYP
- 708 STAINLESS STEEL WALL PANEL, TYP
- 750 CMU WALL, TYP
- 860 1 1/2-HOURS GLASS WALK ROOF DECK SYSTEM, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
- 863 PRE-CAST CONC MOW STRIP, TYP

- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP
- 878 STATION EXH AIR VENT STRUCTURE W/STL GRATING LOCATED WITHIN (E) MIDDLE PLANTER - SEE 'ST' DWGS, TYP
- 880 PRE-CAST CONCRETE WALL PANEL FINISH, TYP



LOOKING SOUTH TOWARD ELEVATORS 1 & 2 - THROUGH ENTRANCE STAIR 1

1 SECTION
AR-868 SCALE: 1/4"=1'-0"



FOR ORIGINAL SIGNATURES, SEE CL-18887, REV. 0.

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 Janglim Tue May 15, 2012 - 3:35 pm NORTH ENTRANCE

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05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED: R. CHIANG
 DRAWN: H. IRFANI
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0
 SEALED BY
 R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 NORTH ENTRANCE
 LONGITUDINAL SECTION (SOUTH)

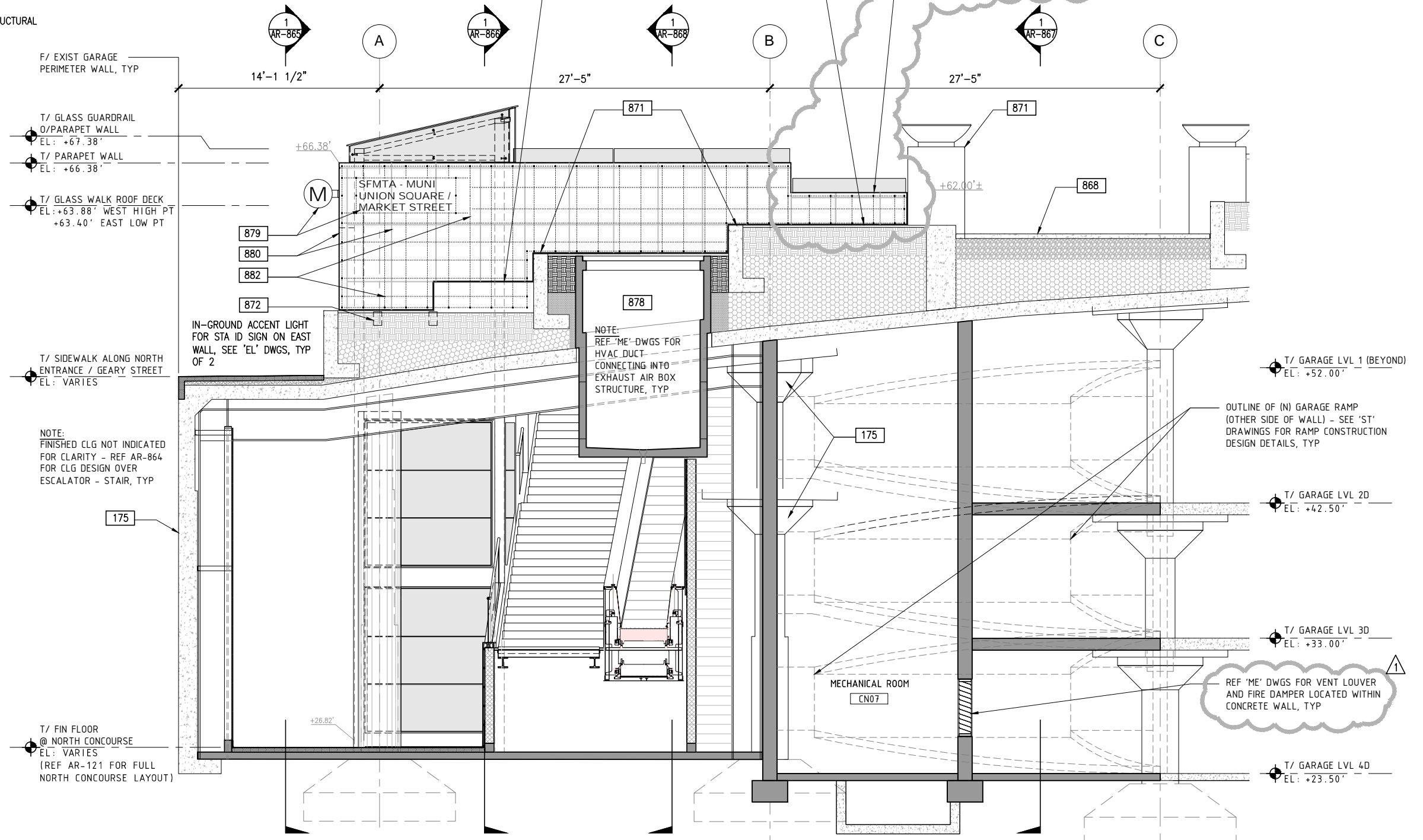
CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18887
DRAWING NO. AR-868
SHEET NO. 1

GENERAL SHEET NOTES

- 1 FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
- 2 PROTECT ADJOINING EXISTING CONDITIONS FOR DURATION OF CONSTRUCTION, TYP
3. REF 'MP' & 'MD' DWGS FOR AREA DRAIN CONNECTION IN MECHANICAL EXHAUST VENT STRUCTURE, TYP
4. COLUMN GRID LINES ARE OF THE EXISTING UNION SQUARE GARAGE BELOW - EXISTING GARAGE COLUMNS INDICATED (IN DASHED LINES), TYP
5. COORDINATE WITH AND REF 'ST' DWGS FOR GARAGE STRUCTURAL MODIFICATIONS / RETROFIT DESIGN WORK REQUIRED TO ACCOMMODATE (N) NORTH ENTRANCE, TYP

KEYNOTES

- 175 EXISTING UNION SQUARE GARAGE PERIMETER WALL AND INTERIOR SUPPORT COLUMNS TO REMAIN, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP
- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP
- 872 LIGHT STD W/LIGHT, IN-GROUND ACCENT LIGHTING @ PLANTING & HARDSCAPED AREAS AND RECESSED STEP LIGHTING AS INDICATED, SEE 'EL' DWGS, TYP
- 878 STATION EXH AIR VENT STRUCTURE W/STL GRATING LOCATED WITHIN (E) MIDDLE PLANTER - SEE 'ST' DWGS, TYP
- 879 STATION ENTRANCE ID SIGNAGE, SEE AR-900 SIGNAGE DWGS FOR DESIGN DETAILS, TYP
- 880 PRE-CAST CONCRETE WALL PANEL FINISH, TYP
- 882 VINE SUPPORT CABLE ATTACHED TO PRE-CAST CONC WALL FACADE, SEE 'LA' DWGS FOR WIRE SUPPORT CABLE DETAIL, TYP



LOOKING WEST THROUGH (N) EXHAUST AIR VENT STRUCTURE

1 SECTION
AR-869 SCALE: 1/4"=1'-0"

4' 0 2' 4' 6' 8'
SCALE: 1/4"=1'-0"

FOR ORIGINAL SIGNATURES, SEE CL-18888, REV. 0.

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 JAnglim Tue May 15, 2012 - 3:35 pm NORTH ENTRANCE - EAST ELEVATION

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

central subway design group

Robin Chiang & Company

DESIGNED R. CHIANG
DRAWN H. IRFANI
CHECKED D. FUNG
REVIEWED R. CHIANG
RECOMMENDED A. READ
APPROVED R. EDWARDS
DATE 02/15/2012

REV. 0
SEALED BY
R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
NORTH ENTRANCE - EAST ELEVATION
TRANSVERSE SECTION (WEST)

CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18888
DRAWING NO. AR-869
SHEET NO. 1

SENSITIVE SECURITY INFORMATION

GENERAL SHEET NOTES

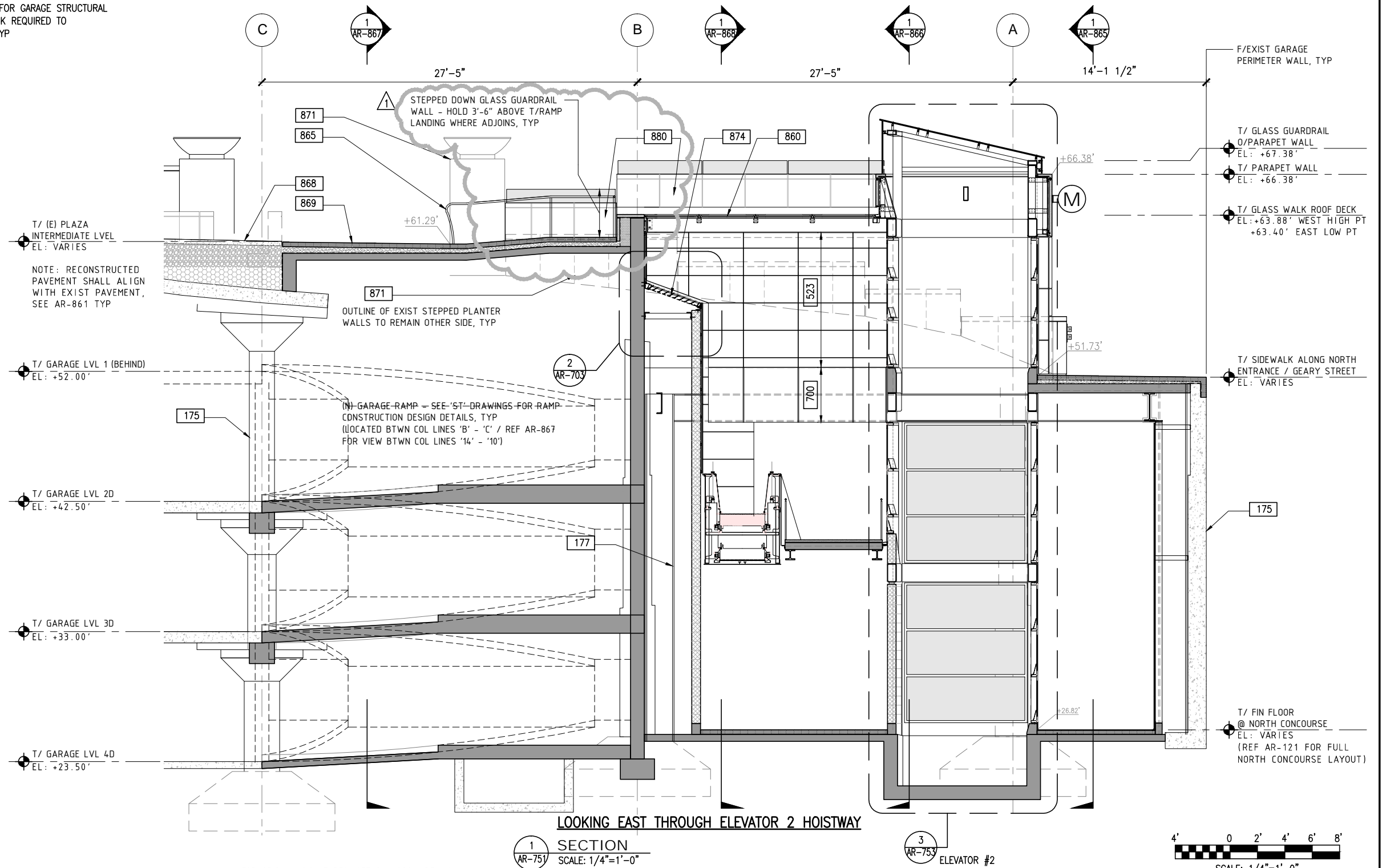
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KEYNOTES

- 175 EXISTING UNION SQUARE GARAGE PERIMETER WALL AND INTERIOR SUPPORT COLUMNS TO REMAIN, TYP
- 177 STEEL SUPPORT COLUMNS AND BRACE FRAME, SEE 'ST' DWGS, TYP
- 523 CRYSTALLIZED GLASS PANEL WALL FINISH, TYP
- 700 STAINLESS STEEL WALL PANEL, TYP
- 860 1-1/2-HOURS GLASS WALK ROOF DECK SYSTEM, TYP
- 865 36" H S. ST. STAIR / RAMP HANDRAILS, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP

- 869 RECONSTRUCT INTERMEDIATE PLAZA WALKWAY - ALIGN JOINTS W/EXIST & NEW PRE-CAST JTS AS INDICATED (WALK COLORS TO MATCH EXIST INCL 'ACCENT' SQUARE - SHOWN DARKER), TYP
- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP

- 874 STATION AIR-INTAKE VENT LOUVERS (2 LOCATIONS - HORIZONTALLY-MTD BWTN COL LINE '13' - '14' & VERTICAL WALL LOUVERS (REF 'ME' FOR HVAC CONNECTION BELOW), TYP
- 880 PRE-CAST CONCRETE WALL PANEL FINISH, TYP



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 JAnglim Tue May 15, 2012 - 3:35 pm NORTH ENTRANCE - WEST ELEVATION

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED BY R. CHIANG
 DRAWN BY J. GAINES
 CHECKED BY D. FUNG
 REVIEWED BY R. CHIANG
 RECOMMENDED BY A. READ
 APPROVED BY R. EDWARDS
 DATE 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0
 SEALED BY
 R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 NORTH ENTRANCE - WEST ELEVATION
 TRANSVERSE SECTION (EAST) - SHEET 1 OF 2

CONTRACT NO. 1253	REVISION 1
SPMTA CONTROL NO. CL-18889	
DRAWING NO. AR-870	
SHEET NO.	

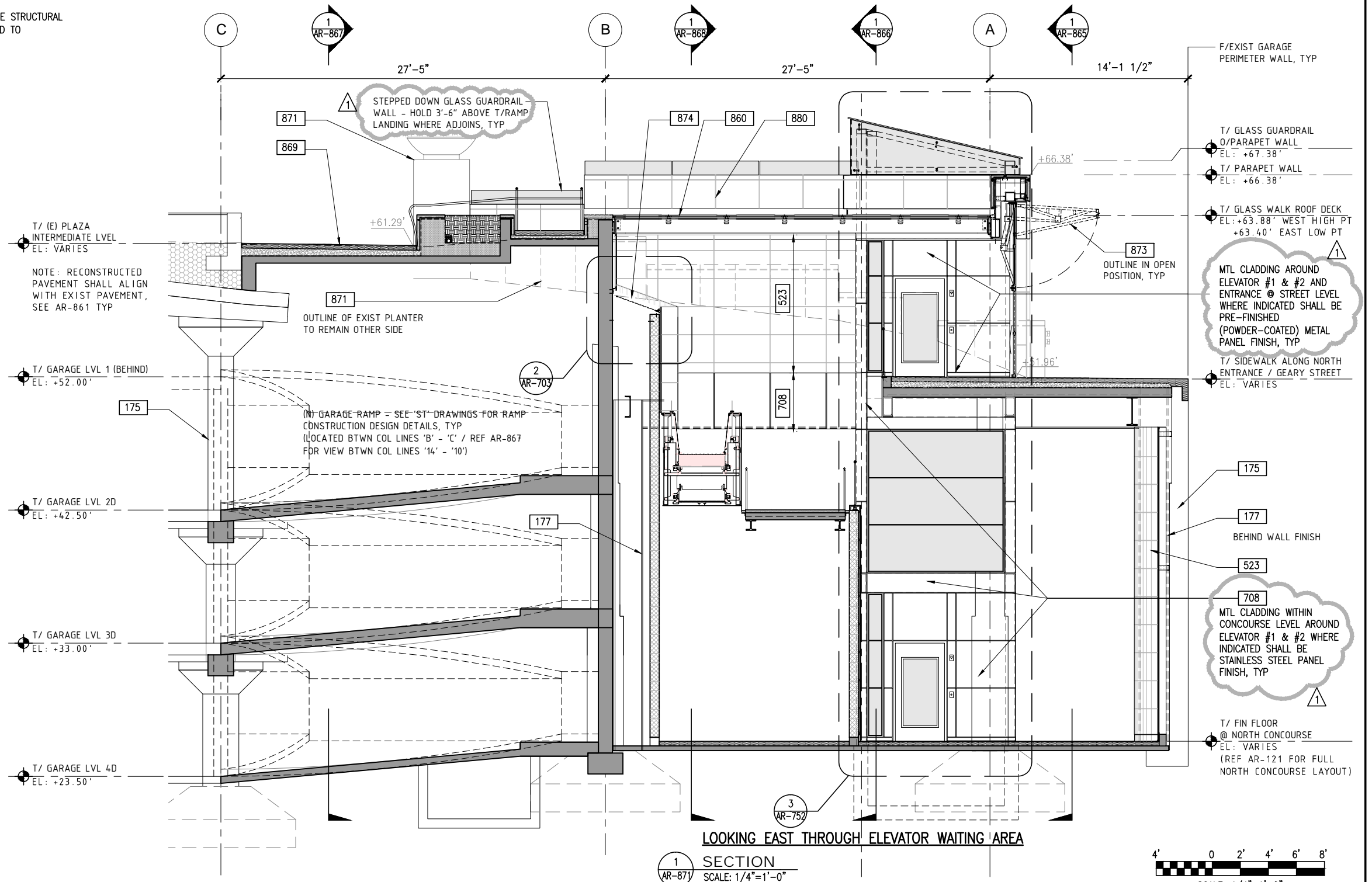
SENSITIVE SECURITY INFORMATION

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KEYNOTES

- | | | | | | |
|-----|---|-----|---|-----|---|
| 175 | EXISTING UNION SQUARE GARAGE PERIMETER WALL AND INTERIOR SUPPORT COLUMNS TO REMAIN, TYP | 869 | RECONSTRUCT INTERMEDIATE PLAZA WALKWAY - ALIGN JOINTS W/EXIST & NEW PRE-CAST JTS AS INDICATED (WALK COLORS TO MATCH EXIST INCL 'ACCENT' SQUARE - SHOWN DARKER), TYP | 874 | STATION AIR-INTAKE VENT LOUVERS (2 LOCATIONS - HORIZONTALLY-MTD BWTN COL LINE '13' - '14' & VERTICAL WALL LOUVERS (REF 'ME' FOR HVAC CONNECTION BELOW), TYP |
| 177 | STEEL SUPPORT COLUMNS AND BRACE FRAME, SEE 'ST' DWGS, TYP | 871 | EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP | 880 | PRE-CAST CONCRETE WALL PANEL FINISH, TYP |
| 523 | CRYSTALLIZED GLASS PANEL WALL FINISH, TYP | 873 | OH BI-FOLD STATION ENTRANCE DOOR SYSTEM, TYP | | |
| 708 | STAINLESS STEEL WALL PANEL, TYP | | | | |
| 860 | 1-1/2-HOURS GLASS WALK ROOF DECK SYSTEM, TYP | | | | |
| 865 | 36" H S. ST. STAIR / RAMP HANDRAILS, TYP | | | | |
| 868 | EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP | | | | |



LOOKING EAST THROUGH ELEVATOR WAITING AREA

1 SECTION
SCALE: 1/4"=1'-0"

4' 0 2' 4' 6' 8'
SCALE: 1/4"=1'-0"

FOR ORIGINAL SIGNATURES, SEE CL-18890, REV. 0.

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED BY: R. CHIANG
 DRAWN BY: J. GAINES
 CHECKED BY: D. FUNG
 REVIEWED BY: R. CHIANG
 RECOMMENDED BY: A. READ
 APPROVED BY: R. EDWARDS
 DATE: 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0
SEALED BY
R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
NORTH ENTRANCE - WEST ELEVATION
TRANSVERSE SECTION (EAST) - SHEET 2 OF 2

CONTRACT NO. 1253	SPM/C CONTROL NO. CL-18890	DRAWING NO. AR-871	REVISION 1
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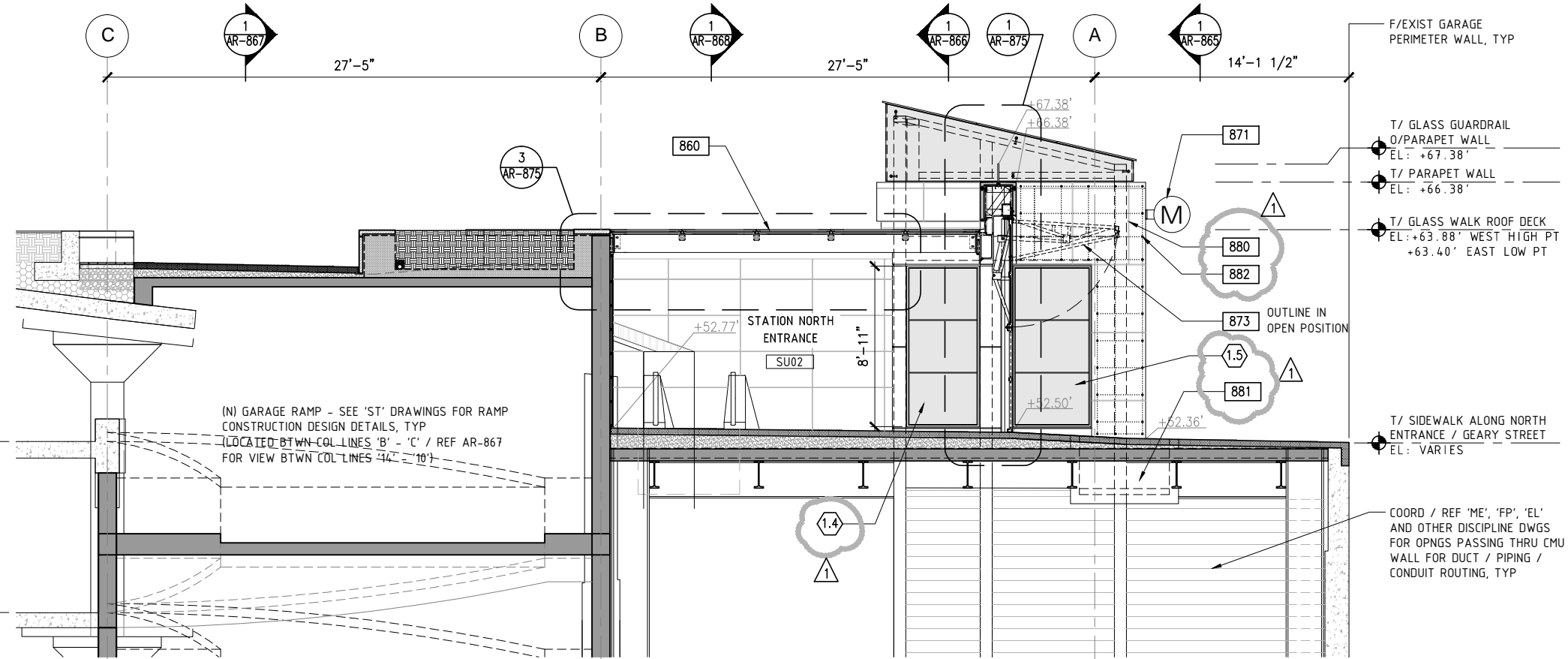
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 JAnglim Tue May 15, 2012 3:35 pm NORTH ENTRANCE - WEST ELEVATION

GENERAL SHEET NOTES

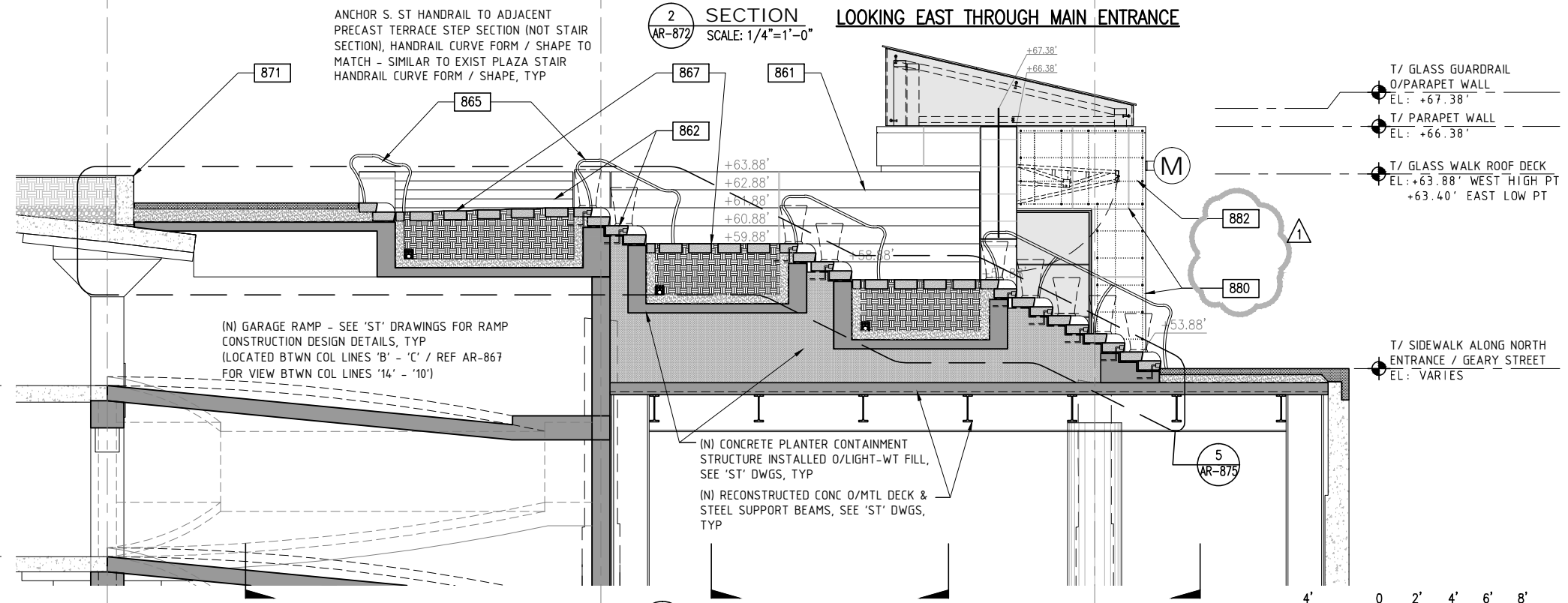
1. FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
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KEYNOTES

- 860 2-HR FIRE RATED GLASS WALK ROOF DECK SYSTEM, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
- 862 PRE-CAST CONC STAIR STEPS, TYP
- 864 PRE-CAST CONC PLANTER POTS (EXISTING LARGE / SMALL) - SEE 'LA' DWGS FOR PLANTING, TYP
- 865 36" H S. ST. STAIR / RAMP HANDRAILS, TYP
- 867 PRE-CAST CONC PAVER STEPS, TYP
- 868 EXISTING WALKWAY TO REMAIN - PROTECT IN PLACE, TYP
- 871 EXISTING PLANTER WALLS, STONE PILASTER TO REMAIN - PROTECT IN PLACE, TYP
- 873 OH BI-FOLD STATION ENTRANCE DOOR SYSTEM, TYP
- 879 STATION ENTRANCE ID SIGNAGE, SEE AR-900 SIGNAGE DWGS FOR DESIGN DETAILS, TYP
- 880 PRE-CAST CONCRETE WALL PANEL FINISH, TYP
- 881 BELOW SIDEWALK PLANTER ADJACENT WALL W/REMOVABLE PRE-CAST COVER (FLUSH W/SIDEWALK), APPROX 3'-0" WIDE x 5'-6" LG x 30" DEEP, SEE 'LA' DWGS FOR VINE TYPE PLANTING, TYP
- 882 VINE SUPPORT CABLE ATTACHED TO PRE-CAST CONC WALL FACADE, SEE 'LA' DWGS FOR WIRE SUPPORT CABLE DETAIL, TYP



2 SECTION AR-872 SCALE: 1/4"=1'-0" LOOKING EAST THROUGH MAIN ENTRANCE



1 SECTION AR-872 SCALE: 1/4"=1'-0" LOOKING EAST THROUGH TERRACE STAIR

FOR ORIGINAL SIGNATURES, SEE CL-18891, REV. 0.

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 JAnglim Tue May 15, 2012 3:36 pm NORTH ENTRANCE

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0
 SEALED BY
 R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 NORTH ENTRANCE
 TRANSVERSE SECTIONS (EAST)

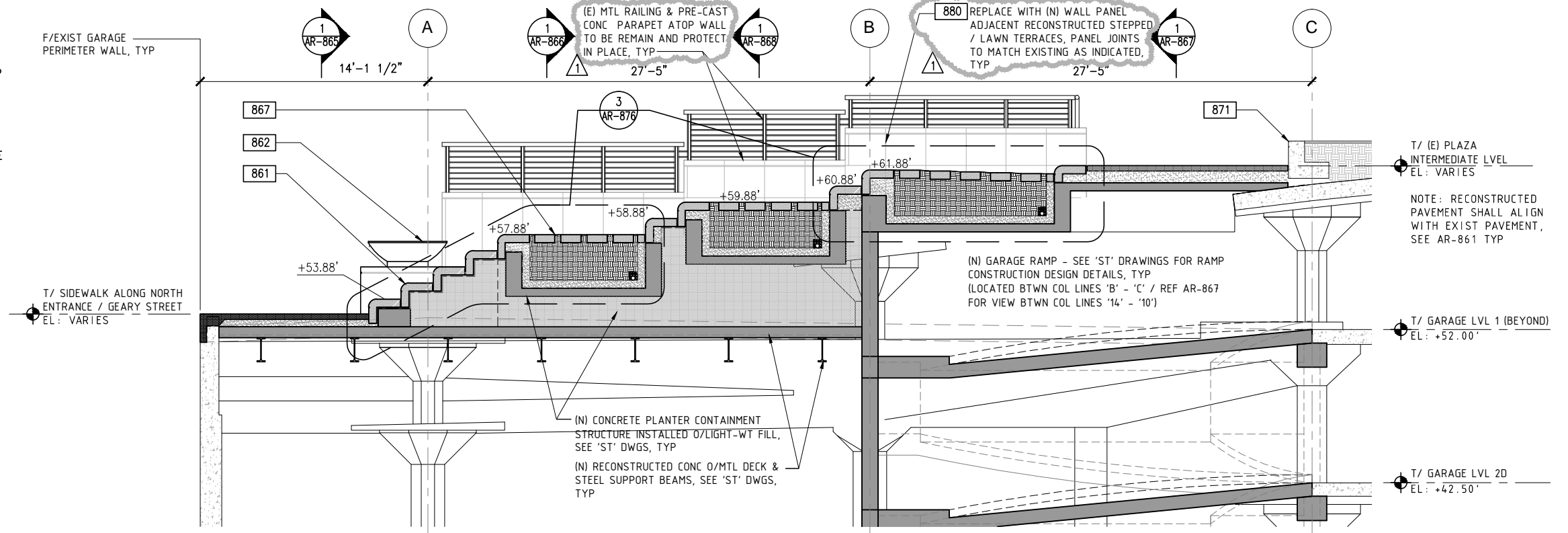
CONTRACT NO. 1253	REVISION 1
SFMTA CONTROL NO. CL-18891	
DRAWING NO. AR-872	
SHEET NO.	

GENERAL SHEET NOTES

1. FINISHED TOPO INDICATED ARE APPROXIMATE AND SHALL BE VERIFY IN FIELD AND ADJUSTED (TO ACCOUNT FOR EXIST CONDITIONS IF REQ'D) BEFORE PROCEEDING TO FINAL LAYOUT, TYP
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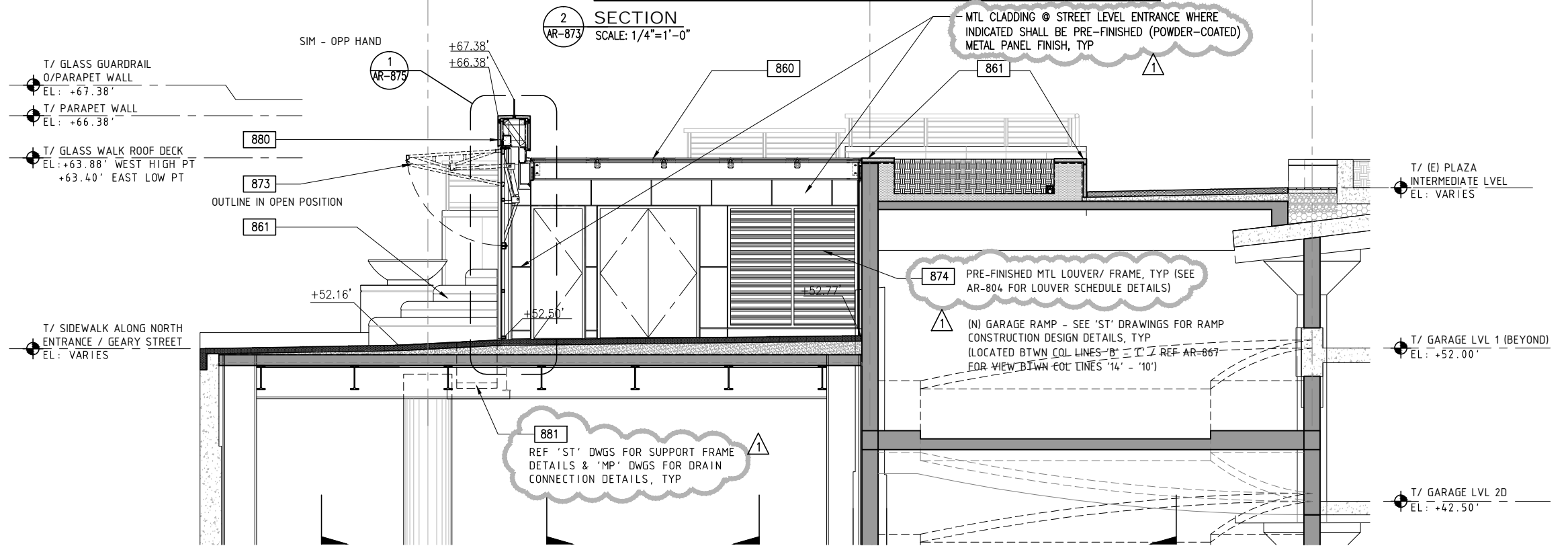
KEYNOTES

- 860 2-HR FIRE RATED GLASS WALK ROOF DECK SYSTEM, TYP
- 861 PRE-CAST CONC TERRACE STEPS / FLAT PNL, TYP
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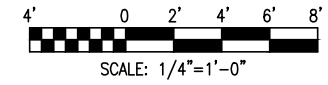
LOOKING WEST THROUGH TERRACE SEATS TOWARD GARAGE ENTRANCE WALL

SECTION 2 SCALE: 1/4"=1'-0"



LOOKING WEST - INSIDE ENTRANCE EAST ELEVATION @ VENT / EM POST DOORS)

SECTION 1 SCALE: 1/4"=1'-0"

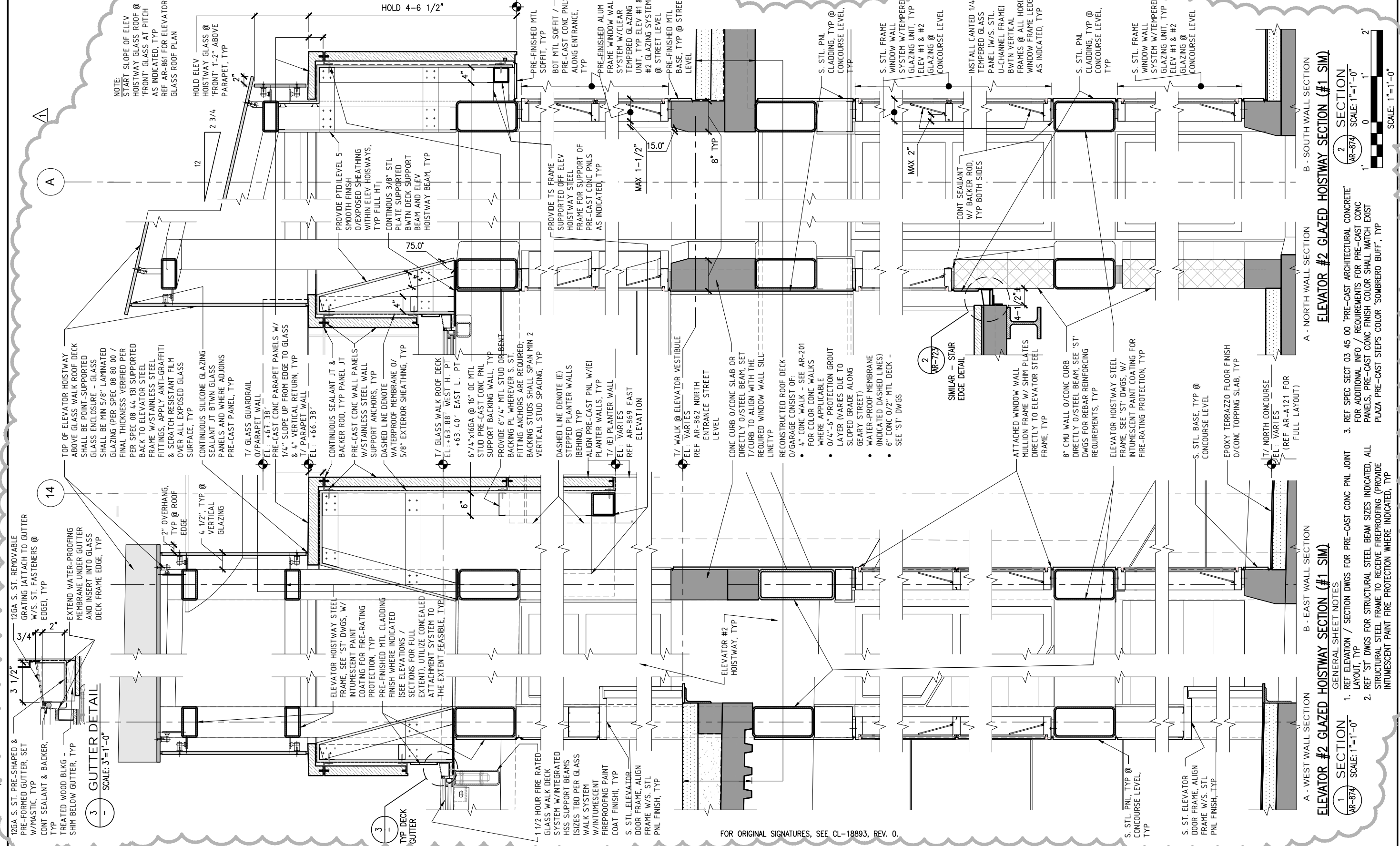


FOR ORIGINAL SIGNATURES, SEE CL-18892, REV. 0.

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 JAnglim Tue May 15, 2012 3:36 pm NORTH ENTRANCE

DESIGNED: R. CHIANG DRAWN: J. GAINES CHECKED: D. FUNG REVIEWED: R. CHIANG RECOMMENDED: A. READ APPROVED: R. EDWARDS DATE: 02/15/2012		REV. 0 SEALED BY R. CHIANG		CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED EDWARD D. REISKIN DIRECTOR OF TRANSPORTATION		THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION		CONTRACT NO. 1253 SFMTA CONTROL NO. CL-18892	
05/15/2012 ISSUED FOR ADDENDUM NO. 1 02/15/2012 ISSUED FOR BID		1 0		Robin Chiang & Company		ARCHITECTURAL NORTH ENTRANCE TRANSVERSE SECTIONS (WEST)		DRAWING NO. AR-873 SHEET NO. 1	

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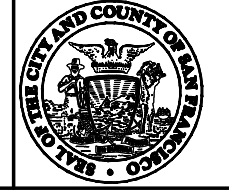
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DESIGNED BY R. CHIANG
 DRAWN BY J. GAINES
 CHECKED BY D. FUNG
 REVIEWED BY R. CHIANG
 RECOMMENDED BY A. READ
 APPROVED BY R. EDWARDS
 DATE 02/15/2012

central subway design group

Robbin Chiang & Company

REV. 0
 SEALED BY
 R. CHIANG



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
 NORTH ENTRANCE
 WALL SECTIONS AND DETAILS - SHEET 1 OF 4

CONTRACT NO. 1253	
SFMTA CONTROL NO. CL-18893	
DRAWING NO. AR-874	REVISION
SHEET NO.	1

GENERAL SHEET NOTES

- COORDINATE WITH & REF 'EL' DWGS FOR LIGHT FIXTURES INSTALLATION INSET / FLUSH MOUNTED WITHIN PRE-CAST PANELS @ RAMP AND STAIR STEPS, TYP
- REF 'ST' DWGS FOR STRUCTURAL STEEL BEAM SIZES INDICATED, ALL STRUCTURAL STEEL FRAME TO RECEIVE FIREPROOFING (PROVIDE INTUMESCENT PAINT FIRE PROTECTION WHERE INDICATED, TYP)
- REF SPEC SECT 03 45 00 'PRE-CAST ARCHITECTURAL CONCRETE' FOR ADDITIONAL INFO / REQUIREMENTS FOR PRE-CAST CONC PANELS & STEPS, PRE-CAST CONC FINISH COLOR SHALL MATCH EXIST PLAZA PRE-CAST STEPS COLOR 'SOMBRERO BUFF', TYP

2 1/2" THK PRE-CAST CONC FLAT PANELS, INSTALLED DIRECTLY O/ LT WT FILL OR STRUCT CONC FRAME, REF AR-861 FOR FULL EXTENT AND JT DIVISIONS, TYP

2 1/2" THK PRE-CAST CONC VERTICAL PANELS, INSTALLED DIRECTLY O/ LT WT FILL OR STRUCT CONC FRAME, ALIGN JT DIVISIONS W/FLAT PNLS, PNL HT VARIES PER PLAZA / RAMP ELEVATION, TYP

4" VERTICAL RETURN EDGE, TYP

4" CONC WALK O/ 4"-8" GROUT PROTECTION SUBBASE, COORD W/ & REF 'ST' DWGS, REF AR-861 FOR WALK JTS AND ACCENT WALK COLOR LOCATIONS, TYP

LAWN PLANTING - SEE 'LA' DWGS

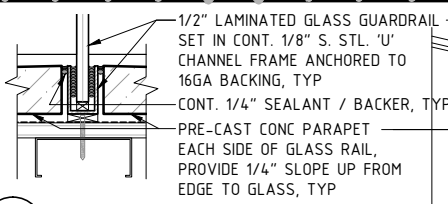
1 1/2" DIA S. STL RAMP HANDRAIL, TYP

HOLD 3'-0" TYP

5'-0" CLR RAMP

DASHED LINE DENOTE CONTINUOUS/ UNBROKEN WATER-PROOFING MEMBRANE AGAINST LT-WT CONC FILL / STRUCTURAL CONC FRAME, TYP

4 SECTION AR-875 SCALE: 1"=1'-0"



6 GLASS RAIL DETAIL SCALE: 3"=1'-0"

PROVIDE 4x4 TS SUPPORTED OFF STRUCTURAL FRAME FOR PRE-CAST PNL SUPPORT, TYP

4"x16GA @ 16" OC MTL STUD PARAPET SUPPORT WALL, TYP

DASHED LINE DENOTE WATERPROOF MEMBRANE O/ 5/8" EXTERIOR SHEATHING, TYP

EXTEND WATERPROOF MEMBRANE INTO GLASS DECK FRAME SIM. TO DECK GUTTER CONDITION, TYP

3 SECTION AR-875 SCALE: 1"=1'-0"

1 1/2 HOUR FIRE RATED GLASS WALK DECK SYSTEM W/INTEGRATED HSS SUPPORT BEAMS (SIZES TBD PER GLASS WALK SYSTEM W/INTUMESCENT FIREPROOFING PAINT COAT FINISH), TYP

3/8" CRYSTALLIZED WALL PNL FINISH O/ 5/8" CEMENT BD O/ 2-1/2" MTL STUD FURRED-OUT WALL FRMG, TYP @ ENTRANCE NORTH WALL

12" CONCRETE SHEAR WALL ALONG LINE 'B', SEE 'ST' DWGS, TYP

2 SECTION AR-875 SCALE: 1"=1'-0"

PRE-CAST CONC MOW STRIP EDGE AND TERRACE STEPPING STEPS, REF AR-861 FOR ALL LOCATIONS, TYP

1 SECTION AR-875 SCALE: 1"=1'-0"

1 1/2" DIA S. STL HANDRAIL, ANCHOR TO ADJACENT PRE-CAST SEAT STEP (NOT THE STAIR STEP) W/1-1/2" CURVED BALUSTRADES (HANDRAIL EA SIDE TO BE ALIGN W/SHALL EDGE), HANDRAIL CURVES INDICATED SHALL BE SIMILAR TO THE EXISTING PLAZA 'EXPRESSIVE' STAIR RAILING CURVES / SHAPES, TYP

PRE-CAST CONC STAIR STEPS INSTALLED O/ LT-WT FILL (OR CONC STOOP - SHAPED TO MATCH STAIR PROFILE), REF SPEC SECT 03 45 00 'PRE-CAST ARCHITECTURAL CONCRETE' FOR STAIR CONTRASTING NOSING @ EA STEP REQUIREMENT, TYP

ADJACENT PRE-CAST CONC TERRACE SEAT STEPS, REF 3/AR-876 FOR DETAILS, TYP

5 SECTION AR-875 SCALE: 1"=1'-0"

3" DIA PLANTER DRAIN TILE W/GRAVEL, REF 'MP' DWGS FOR DRAIN OUTLET CONNECTION, TYP EA PLANTER

6" GROUT PROTECTION LAYER, REF 'ST' DWGS, TYP ALL SIDES

SHADED FLOORS / WALLS DENOTE (N) STRUCTURAL CONCRETE FRAME FORMING RECONSTRUCTED PLAZA WALK & PLANTING AREAS O/GARAGE BWTN COL LINE 'B' & 'C', COORD W/ & REF 'ST' DWGS, TYP

TYPICAL TERRACE STAIR STEPS & PLANTERS

FOR ORIGINAL SIGNATURES, SEE CL-18894, REV. 0.

SCALE: 1"=1'-0"

EXIST PLAZA PLANTER WALLS, PAVING, LANDSCAPING AND IRRIGATION SYSTEM WITHIN PLANTERS TO REMAIN AND PROTECTED IN PLACE FOR DURATION OF CONSTRUCTION PERIOD, TYP

T/ PLAZA INTERMEDIATE LEVEL EL: VARIES SEE AR-861

T/ PLAZA INTERMEDIATE LEVEL EL: VARIES SEE AR-861

EXIST GARAGE CONC COLUMN, COLUMN CAP AND ROOF SLAB ALONG LINE 'C' TO REMAIN - SEE GARAGE DEMO DWGS FOR EXTENT OF DEMOLITION, TYP

12" CONCRETE SHEAR WALL ALONG LINE 'B', SEE 'ST' DWGS, TYP

T/ WALK EL: VARIES SEE AR-862

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Janglim Tue May 15, 2012 3:37 pm NORTH ENTRANCE

DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

DESIGNED: R. CHIANG
DRAWN: J. GAINES
CHECKED: D. FUNG
REVIEWED: R. CHIANG
RECOMMENDED: A. READ
APPROVED: R. EDWARDS
DATE: 02/15/2012

central subway design group

Robin Chiang & Company

REV. 0 SEALED BY R. CHIANG

CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

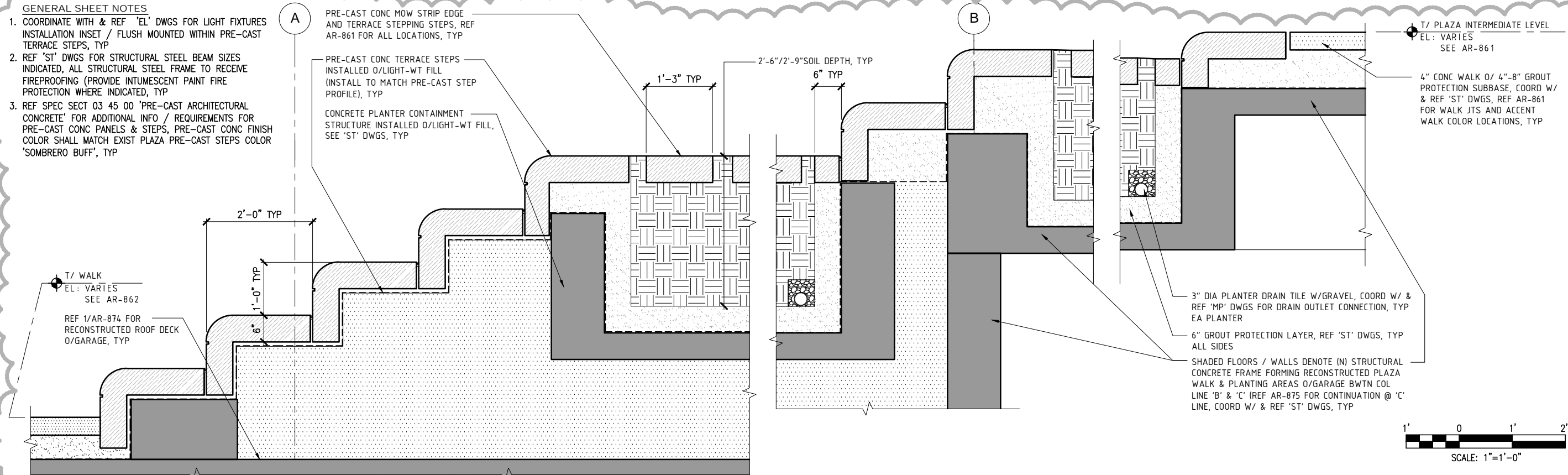
THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL NORTH ENTRANCE
WALL SECTIONS AND DETAILS - SHEET 2 OF 4

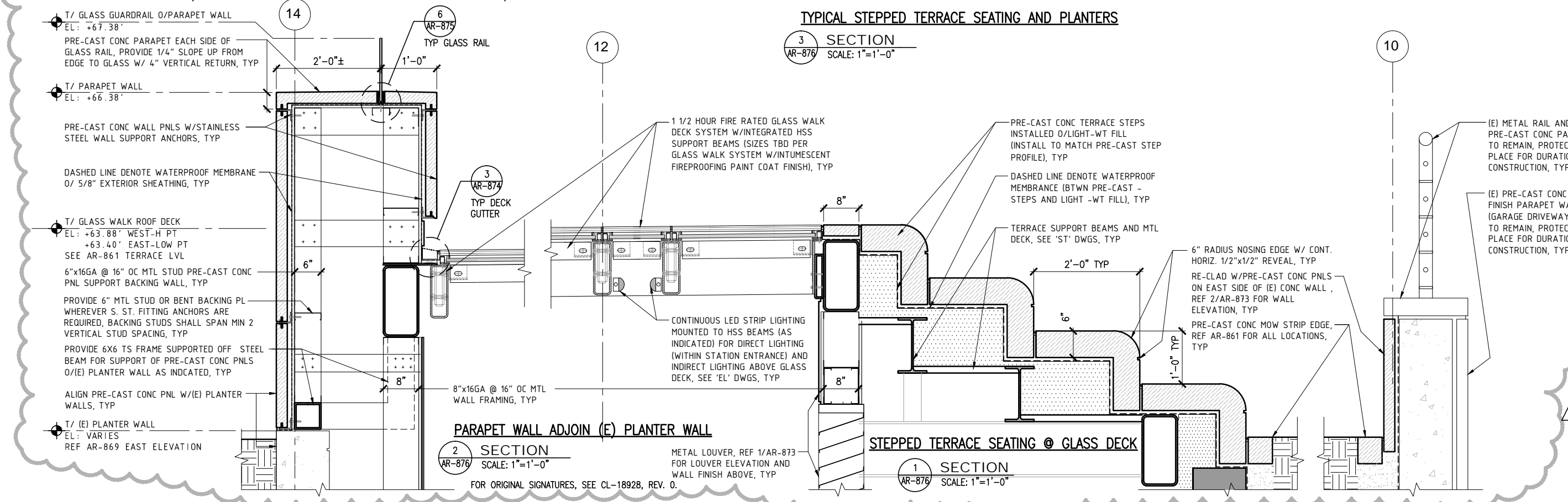
CONTRACT NO. 1253	SPMTA CONTROL NO. CL-18894
DRAWING NO. AR-875	REVISION 1

GENERAL SHEET NOTES

- COORDINATE WITH & REF 'EL' DWGS FOR LIGHT FIXTURES INSTALLATION INSET / FLUSH MOUNTED WITHIN PRE-CAST TERRACE STEPS, TYP
- REF 'ST' DWGS FOR STRUCTURAL STEEL BEAM SIZES INDICATED, ALL STRUCTURAL STEEL FRAME TO RECEIVE FIREPROOFING (PROVIDE INTUMESCENT PAINT FIRE PROTECTION WHERE INDICATED, TYP
- REF SPEC SECT 03 45 00 'PRE-CAST ARCHITECTURAL CONCRETE' FOR ADDITIONAL INFO / REQUIREMENTS FOR PRE-CAST CONC PANELS & STEPS, PRE-CAST CONC FINISH COLOR SHALL MATCH EXIST PLAZA PRE-CAST STEPS COLOR 'SOMBRERO BUFF', TYP



TYPICAL STEPPED TERRACE SEATING AND PLANTERS



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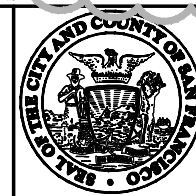
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05/15/2012	ISSUED FOR ADDENDUM NO. 1	1			
02/15/2012	ISSUED FOR BID	0			

central subway design group

Robinson Chiang & Company

DESIGNED R. CHIANG
DRAWN H. IRFANI
CHECKED D. FUNG
REVIEWED R. CHIANG
RECOMMENDED A. READ
APPROVED R. EDWARDS
DATE 02/15/2012

REV. 0
SEALED BY
R. CHIANG



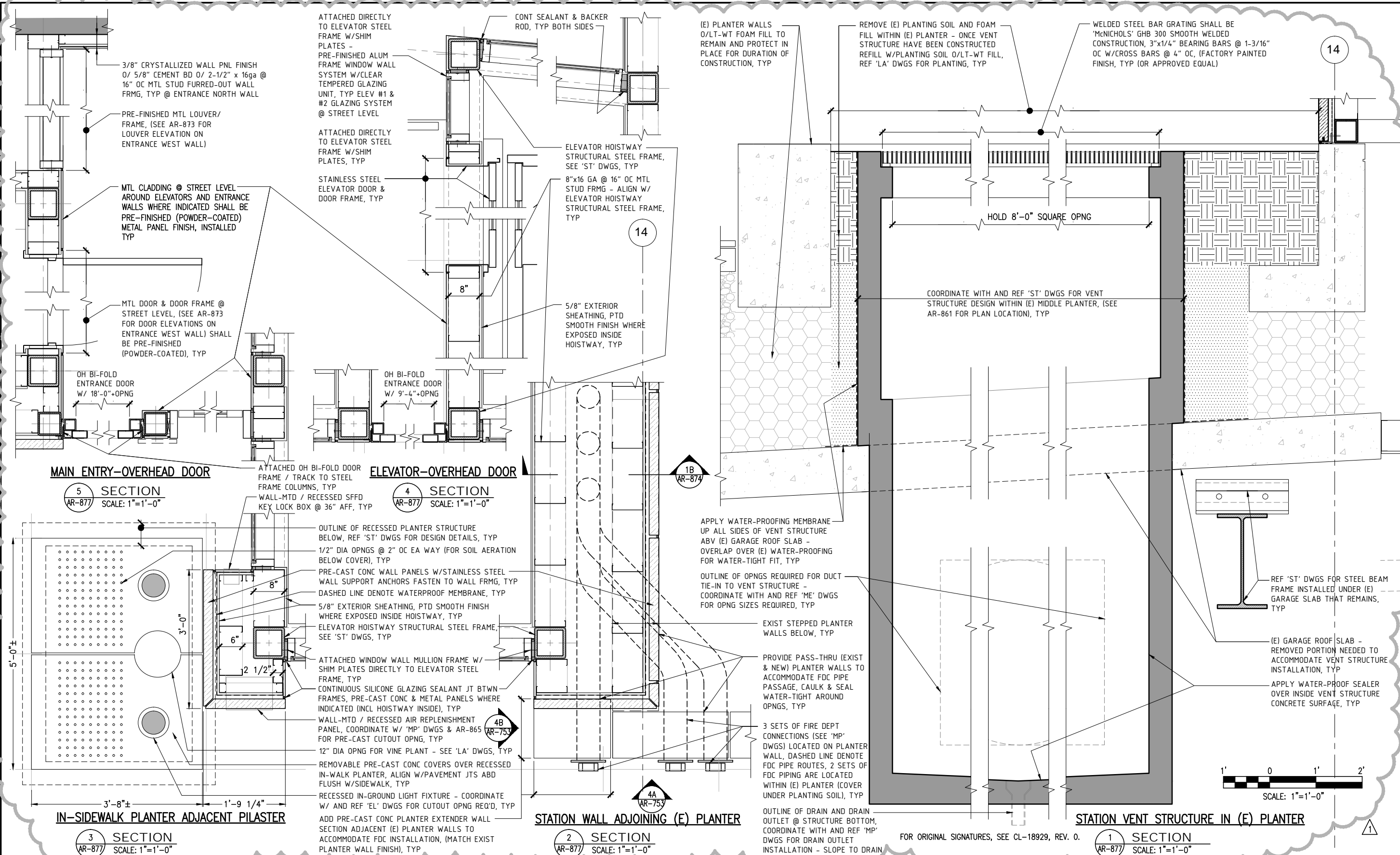
CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
UNION SQUARE/MARKET STREET STATION

ARCHITECTURAL
NORTH ENTRANCE
WALL SECTIONS AND DETAILS - SHEET 3 OF 4

CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18928
DRAWING NO. AR-876
SHEET NO. 1



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02/15/2012	ISSUED FOR BID	0			

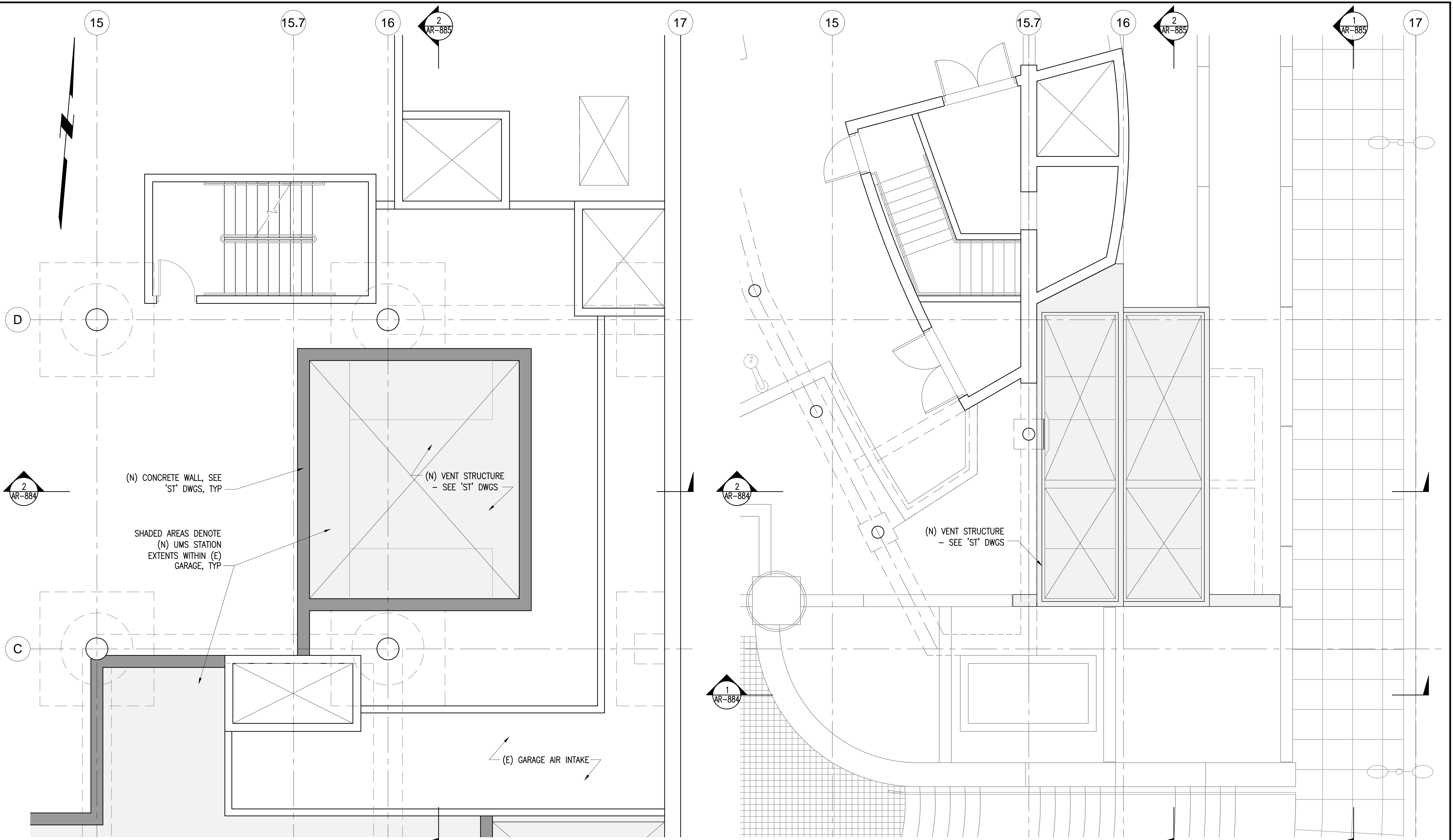
central subway design group
 Robin Chiang & Company
 DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012

REV. 0
 SEALED BY
 R. CHIANG

CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION
ARCHITECTURAL
NORTH ENTRANCE
WALL SECTIONS AND DETAILS - SHEET 4 OF 4

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SPMTA CONTROL NO.	CL-18929
DRAWING NO.	AR-877
SHEET NO.	1

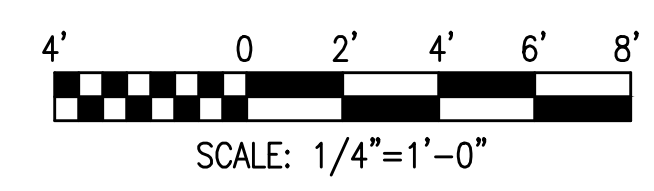


VENT STRUCTURE AT GARAGE LEVEL 2

PLAN

VENT STRUCTURE AT SURFACE LEVEL

PLAN

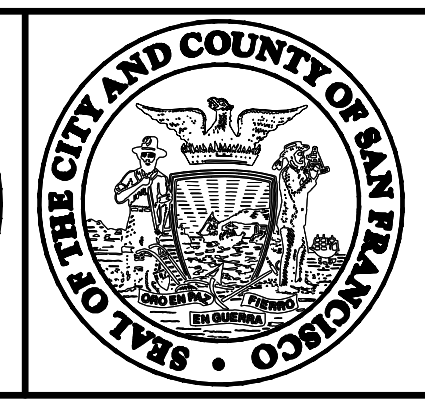
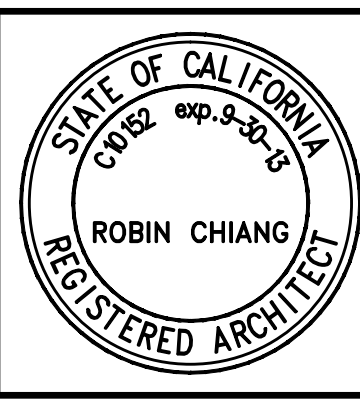


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02/15/2012	ISSUED FOR BID	0			
DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED

central subway design group
 Robin Chiang & Company

DESIGNED	R. CHIANG
DRAWN	J. GAINES
CHECKED	D. FUNG
REVIEWED	R. CHIANG
RECOMMENDED	A. READ
APPROVED	R. EDWARDS
DATE	02/15/2012

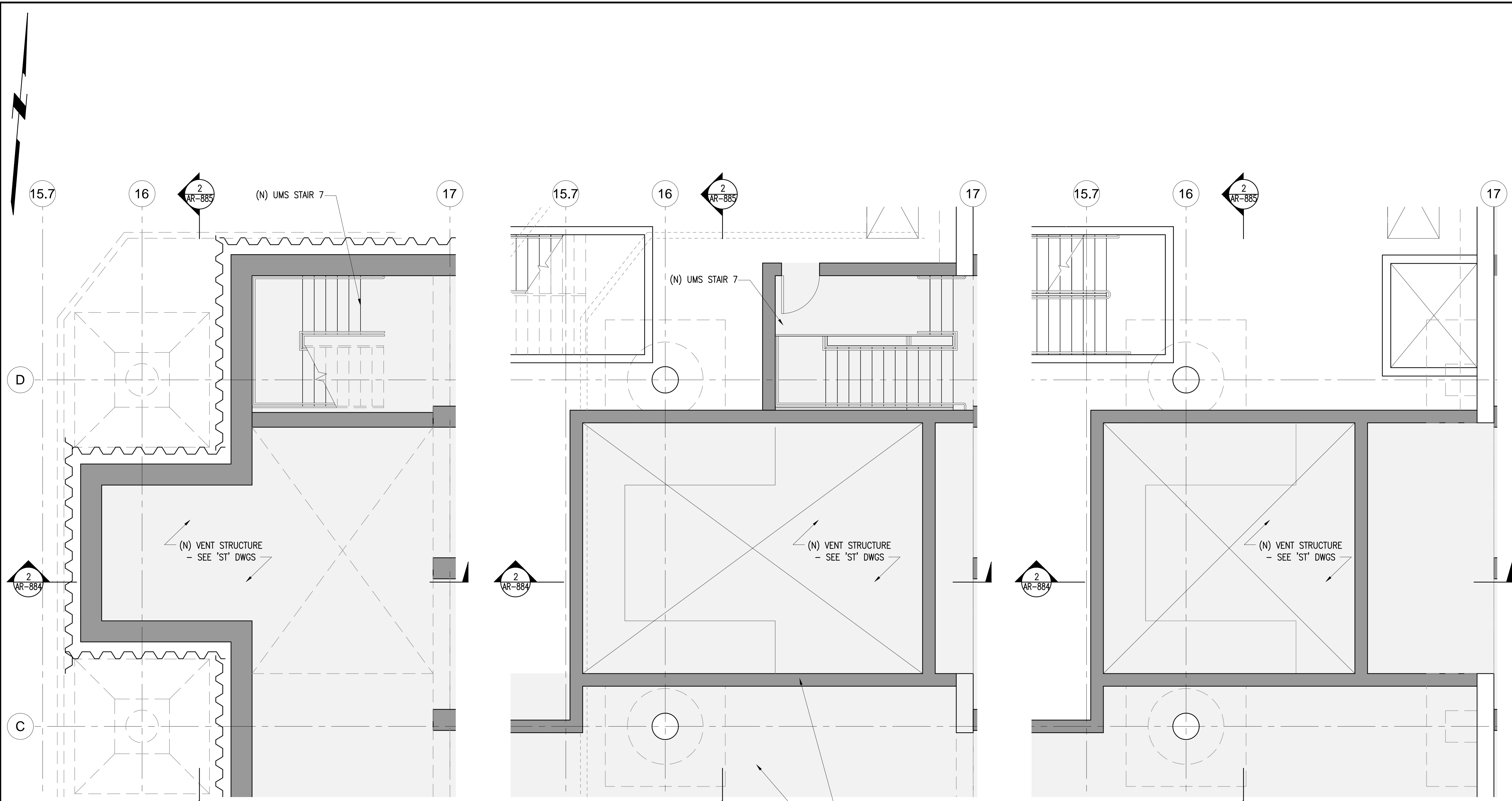


CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
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 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION
**ARCHITECTURAL
 EMERGENCY VENTILATION SHAFT
 ENLARGED PLANS - SHEET 1 OF 2**

CONTRACT NO.	1253
SFMTA CONTROL NO.	CL-18895
DRAWING NO.	AR-881
SHEET NO.	0

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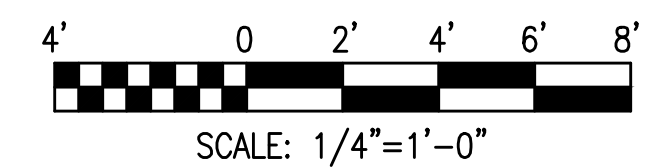


VENT STRUCTURE AT INTERMEDIATE STRUT LEVEL PLAN

VENT STRUCTURE AT GARAGE LEVEL 4 PLAN

VENT STRUCTURE AT GARAGE LEVEL 3 PLAN

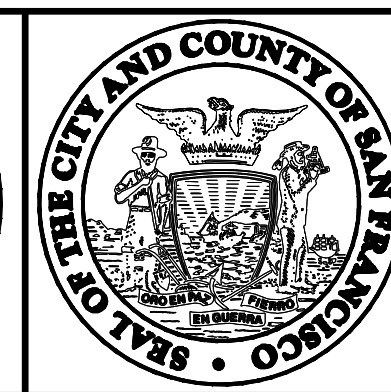
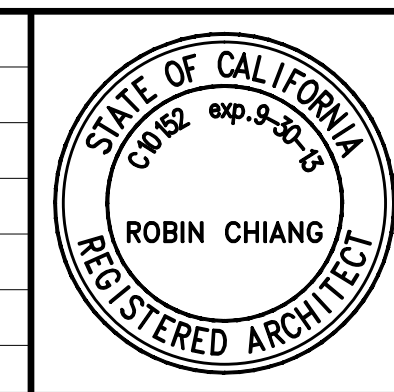
(N) CONCRETE WALL, SEE 'ST' DWGS, TYP
 SHADED AREAS DENOTE (N) UMS STATION EXTENTS WITHIN (E) GARAGE, TYP



02/15/2012	ISSUED FOR BID	0			
DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED

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 Robin Chiang & Company

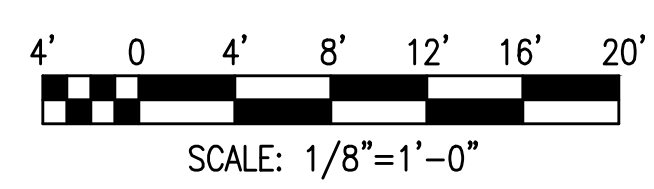
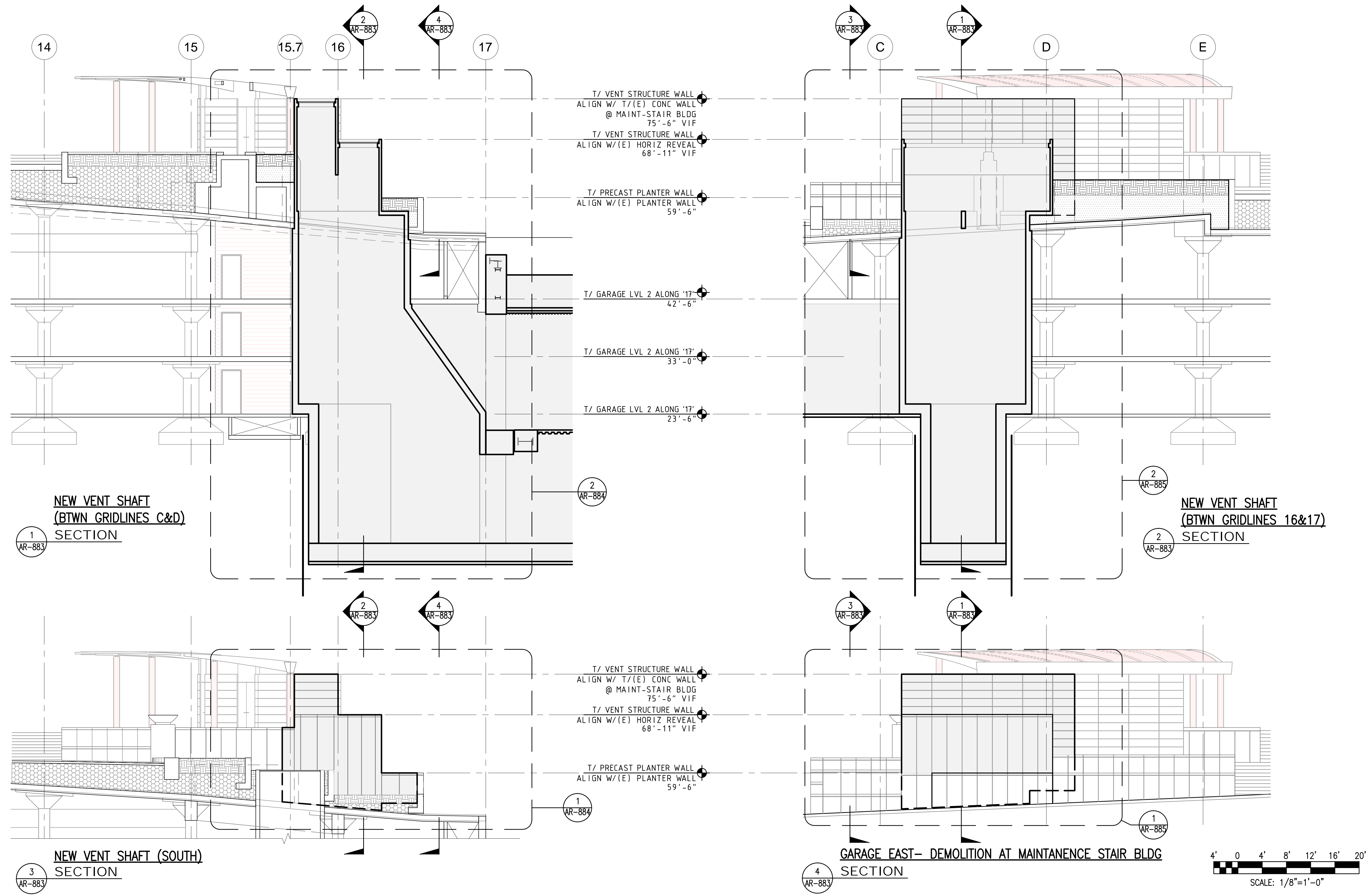
DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION
**ARCHITECTURAL
 EMERGENCY VENTILATION SHAFT
 ENLARGED PLANS - SHEET 2 OF 2**

CONTRACT NO.	1253
SFMTA CONTROL NO.	CL-18896
DRAWING NO.	AR-882
SHEET NO.	0

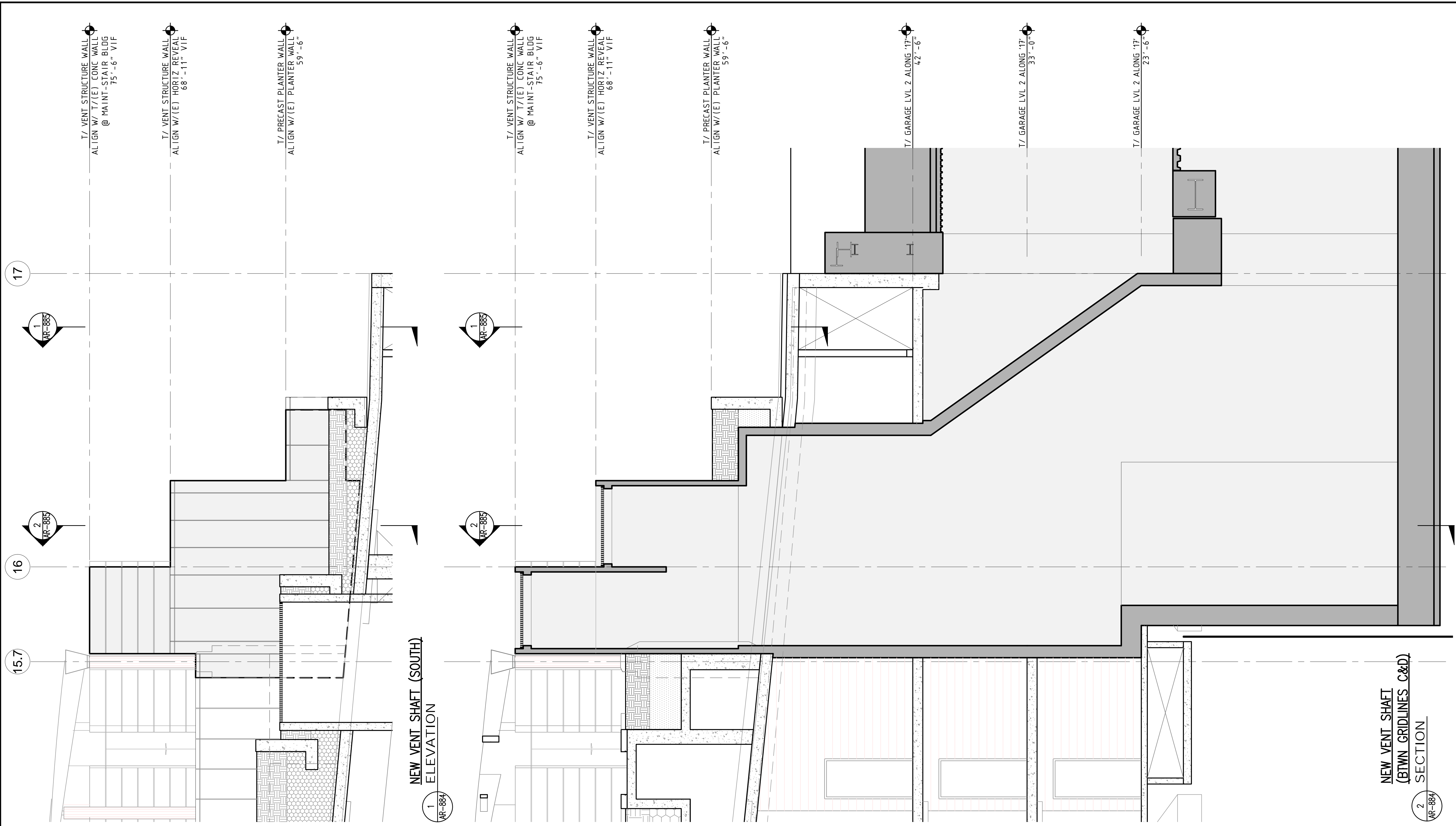


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 janglim Tue Mar 06 2012 2:31 pm AR-883 EMERGENCY VENTILATION SHAFT

02/15/2012 ISSUED FOR BID DATE DESCRIPTION REV NO BY CHECKED APPROVED		DESIGNED: R. CHIANG DRAWN: J. GAINES CHECKED: D. FUNG REVIEWED: R. CHIANG RECOMMENDED: A. READ APPROVED: R. EDWARDS DATE: 02/15/2012				CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED DIRECTOR OF TRANSPORTATION		THIRD STREET LIGHT RAIL PROGRAM PHASE 2 – CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION ARCHITECTURAL EMERGENCY VENTILATION SHAFT SECTIONS		CONTRACT NO. 1253 SFMTA CONTROL NO. CL-18897 DRAWING NO. AR-883 SHEET NO. 0	
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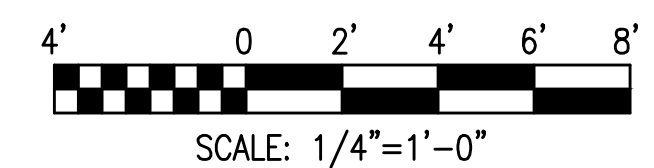
BORDER REVISED 11/04/2011
 WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 49 CFR PARTS 15 AND 1520. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSON WITHOUT A "NEED TO KNOW", AS DEFINED IN 49 CFR PARTS 15 AND 1520, EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION.

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 janglim Tue Mar 06 2012 2:28 pm AR-884 EMERGENCY VENTILATION SHAFT



NEW VENT SHAFT (SOUTH)
ELEVATION

NEW VENT SHAFT
(BTWN GRIDLINES C&D)
SECTION

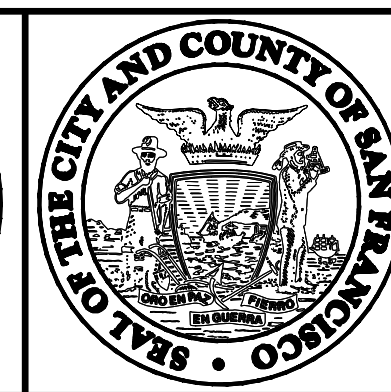
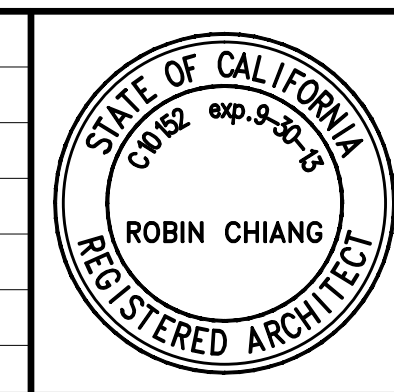


02/15/2012	ISSUED FOR BID		0		
DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED

central subway design group

Robin Chiang & Company

DESIGNED R. CHIANG
DRAWN J. GAINES
CHECKED D. FUNG
REVIEWED R. CHIANG
RECOMMENDED A. READ
APPROVED R. EDWARDS
DATE 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

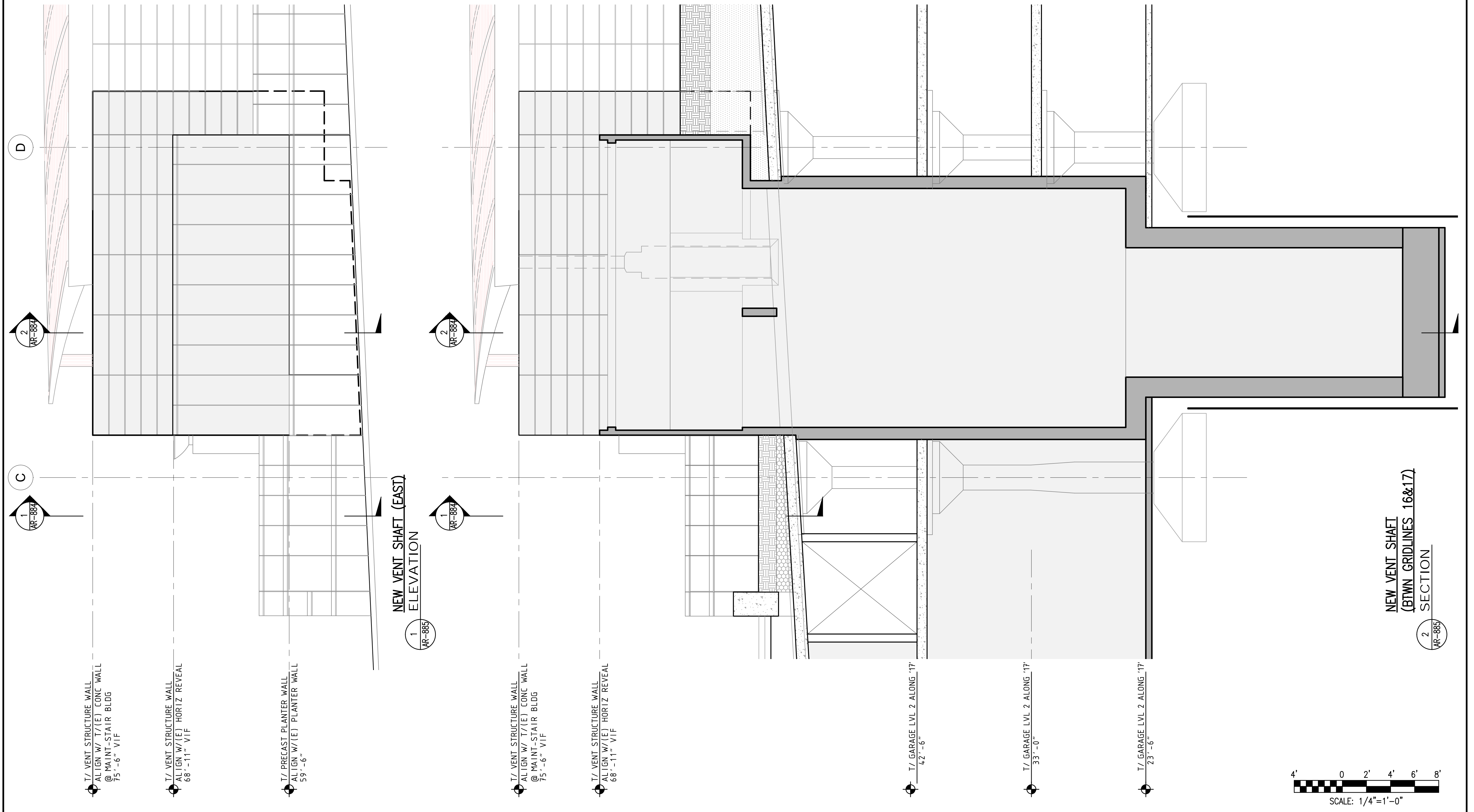
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 – CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

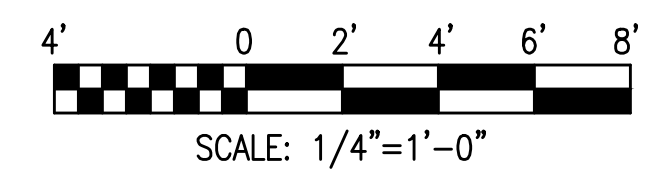
**ARCHITECTURAL
 EMERGENCY VENTILATION SHAFT
 ENLARGED SECTIONS – SHEET 1 OF 2**

CONTRACT NO. 1253
SFMTA CONTROL NO. CL-18898
DRAWING NO. AR-884
SHEET NO. 0
REVISION

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 jgaines Fri Feb 24, 2012 11:35 am AR-885 EMERGENCY VENTILATION SHAFT



NEW VENT SHAFT
 (BTWN GRIDLINES 16&17)
 SECTION
 2
 AR-885

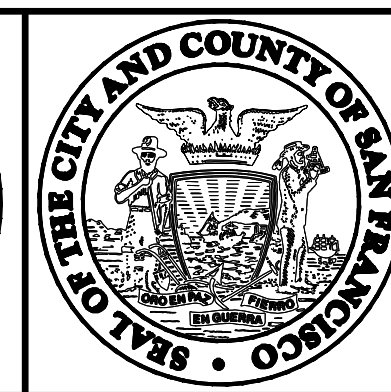
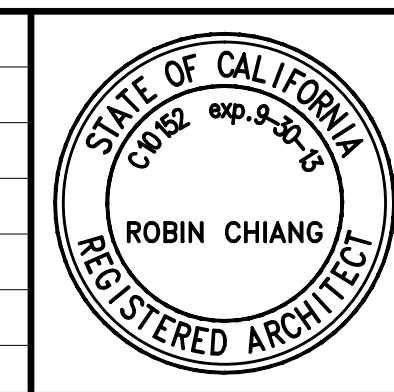


02/15/2012	ISSUED FOR BID	0			
DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED


 central subway design group

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DRAWN J. GAINES
CHECKED D. FUNG
REVIEWED R. CHIANG
RECOMMENDED A. READ
APPROVED R. EDWARDS
DATE 02/15/2012

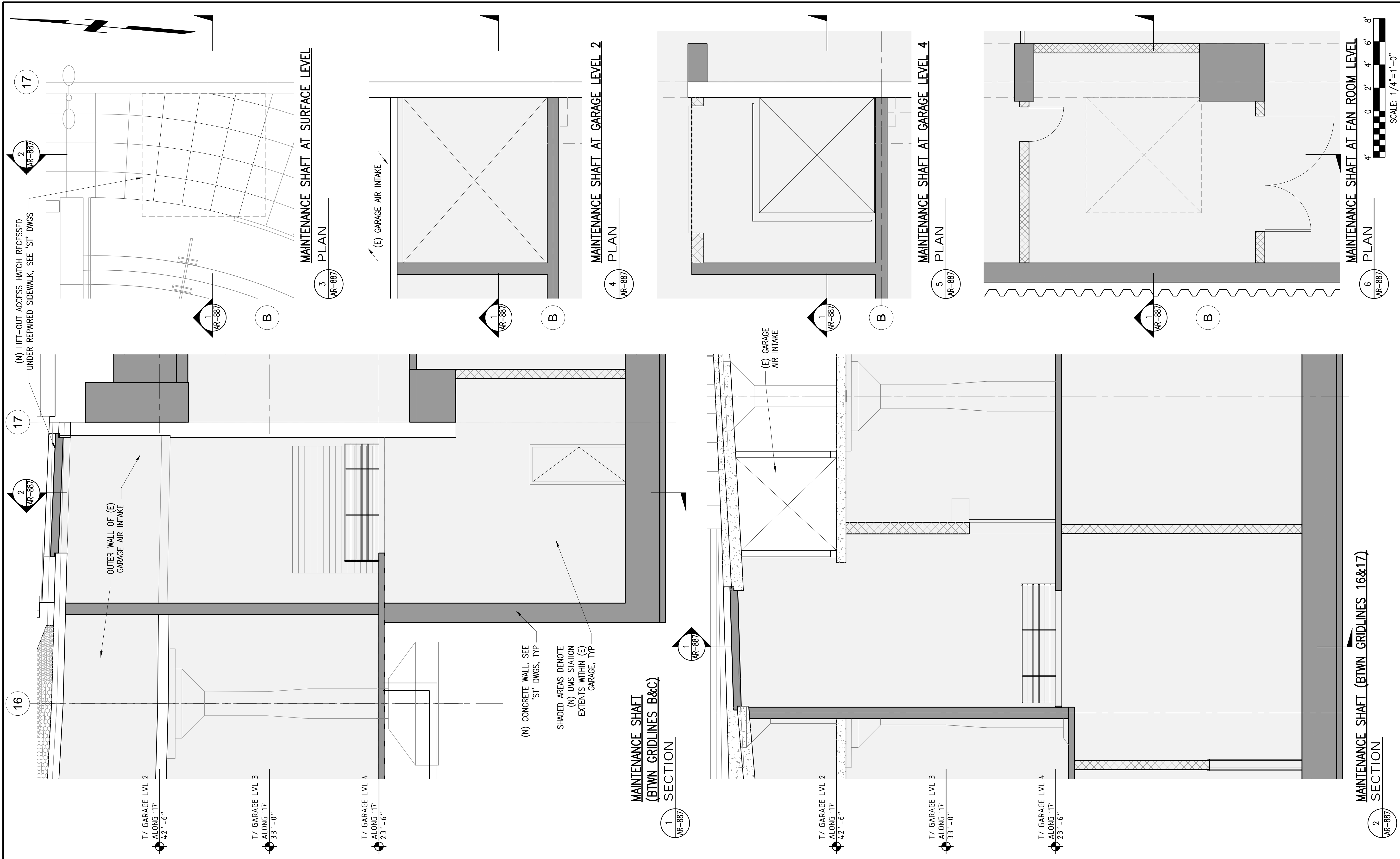


CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
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 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION
**ARCHITECTURAL
 EMERGENCY VENTILATION SHAFT
 ENLARGED SECTIONS - SHEET 2 OF 2**

CONTRACT NO. 1253
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SHEET NO.
REVISION 0

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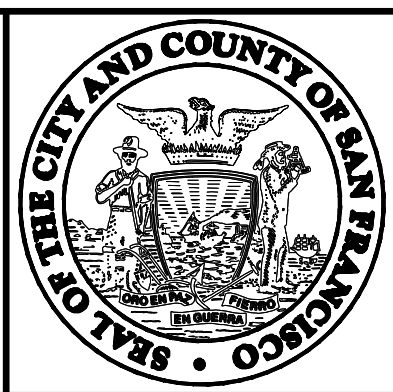
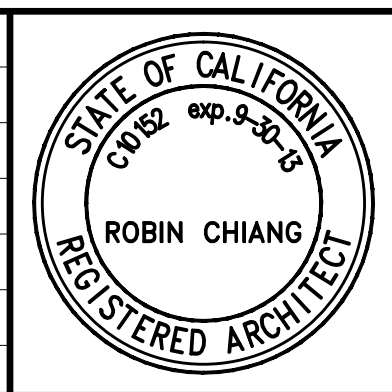


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DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED

central subway design group

Robyn Chiang & Company

DESIGNED: R. CHIANG
 DRAWN: J. GAINES
 CHECKED: D. FUNG
 REVIEWED: R. CHIANG
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 02/15/2012



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

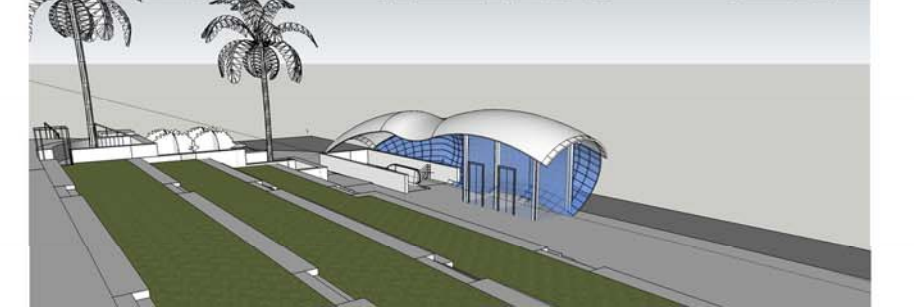
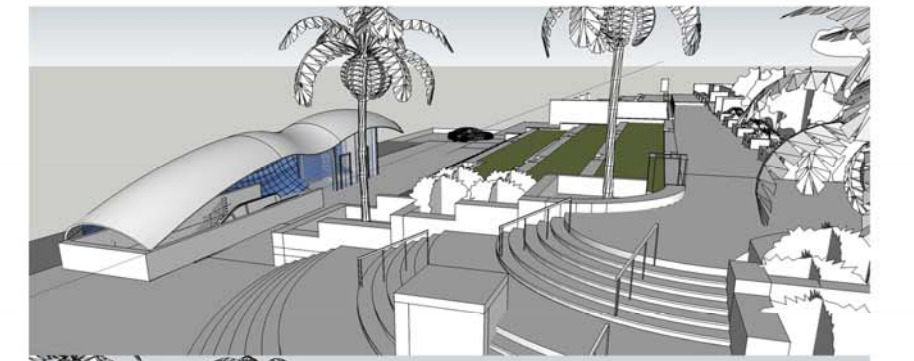
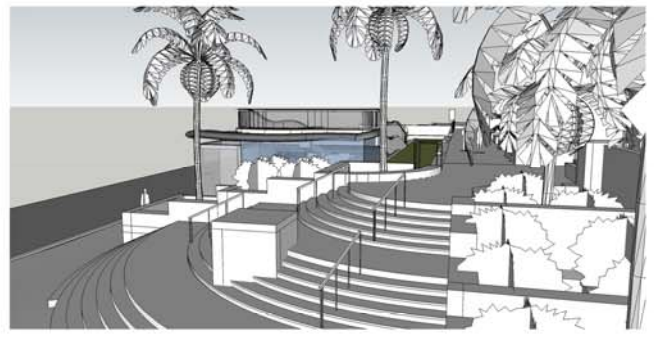
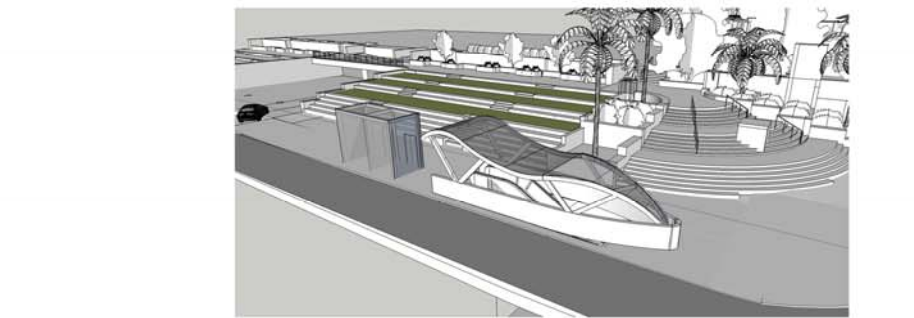
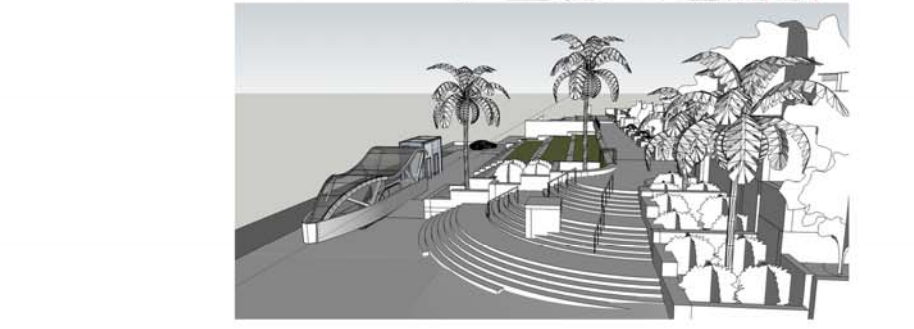
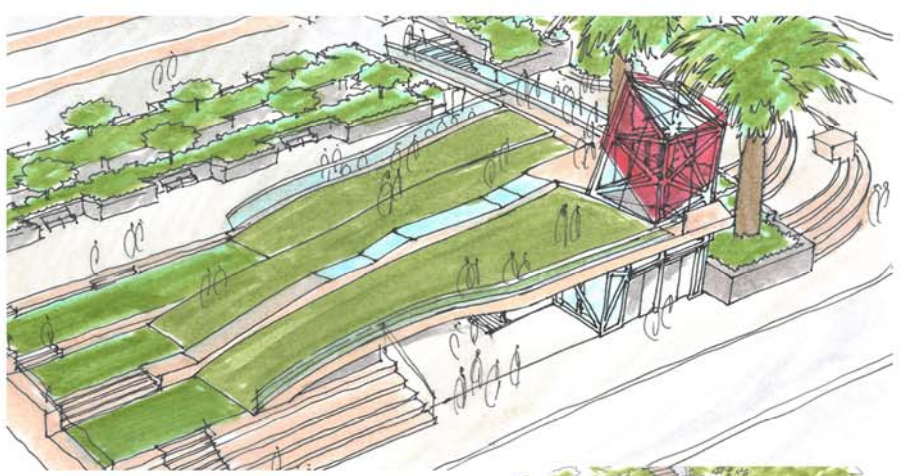
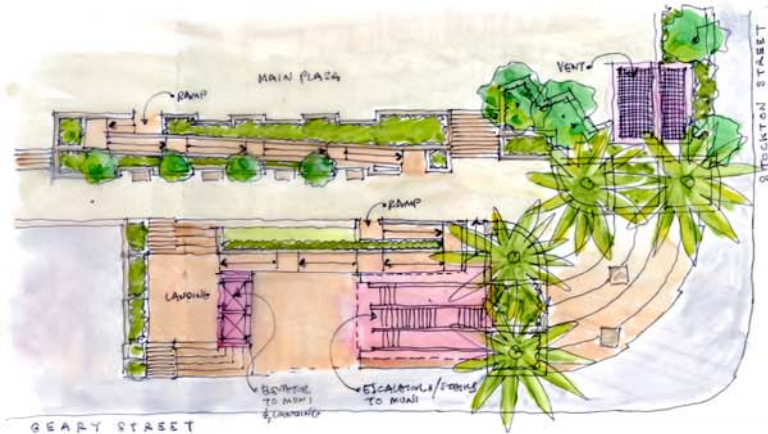
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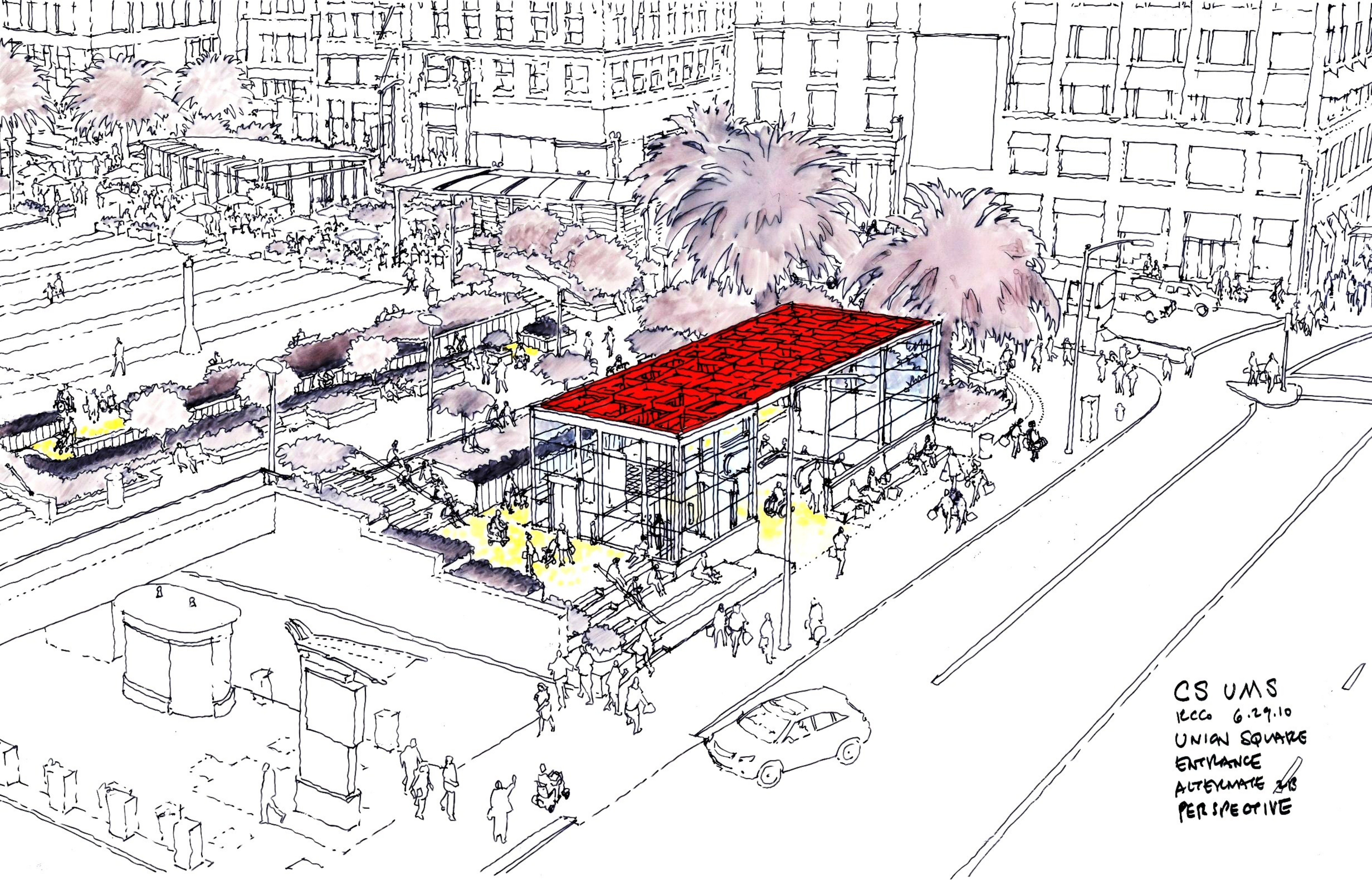
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 – CENTRAL SUBWAY
 UNION SQUARE/MARKET STREET STATION

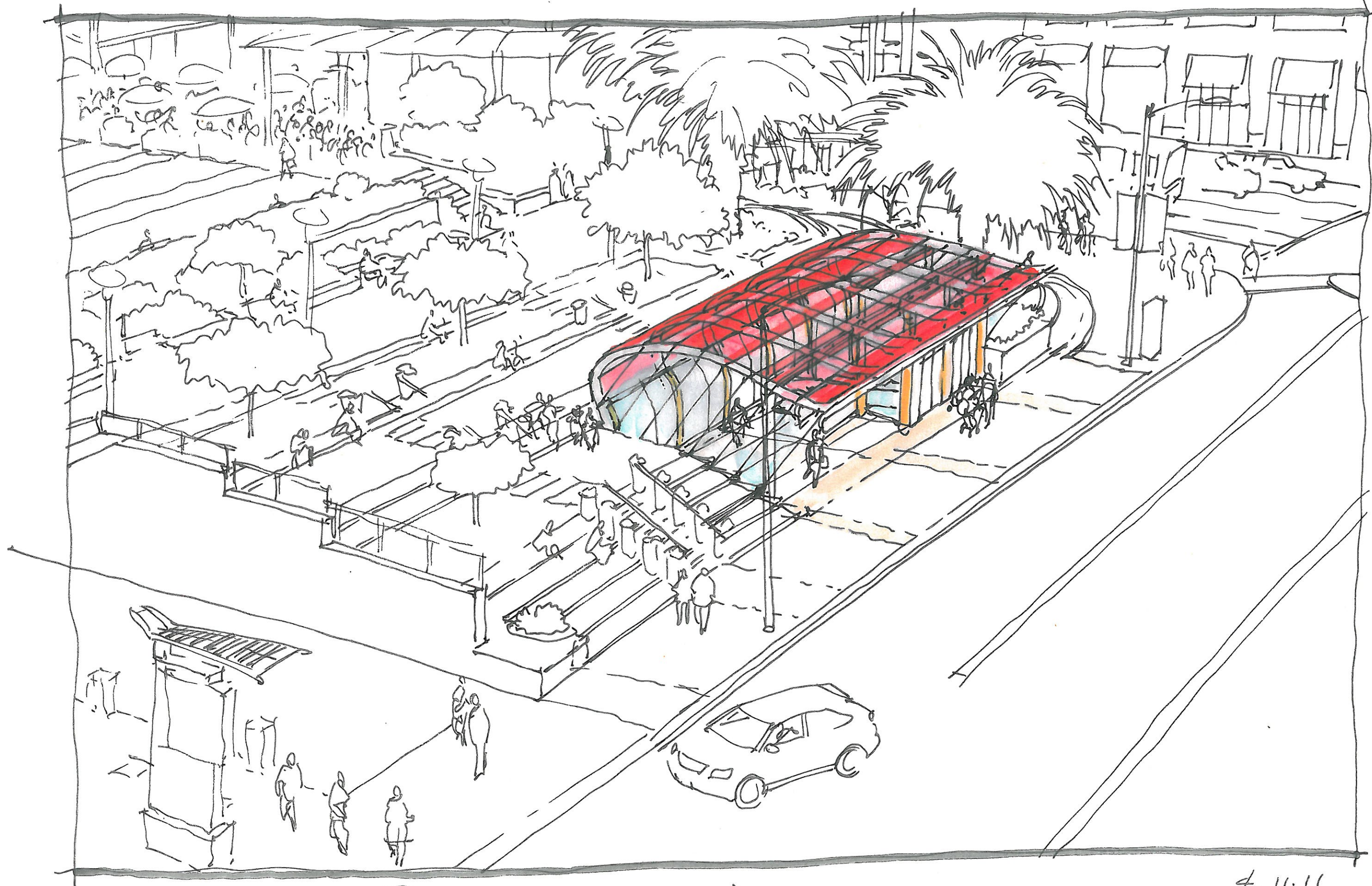
**ARCHITECTURAL
 MAINTENANCE ACCESS SHAFT
 ENLARGED PLANS AND SECTIONS**

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SFMTA CONTROL NO. CL-19432	
DRAWING NO. AR-887	REVISION 0
SHEET NO.	



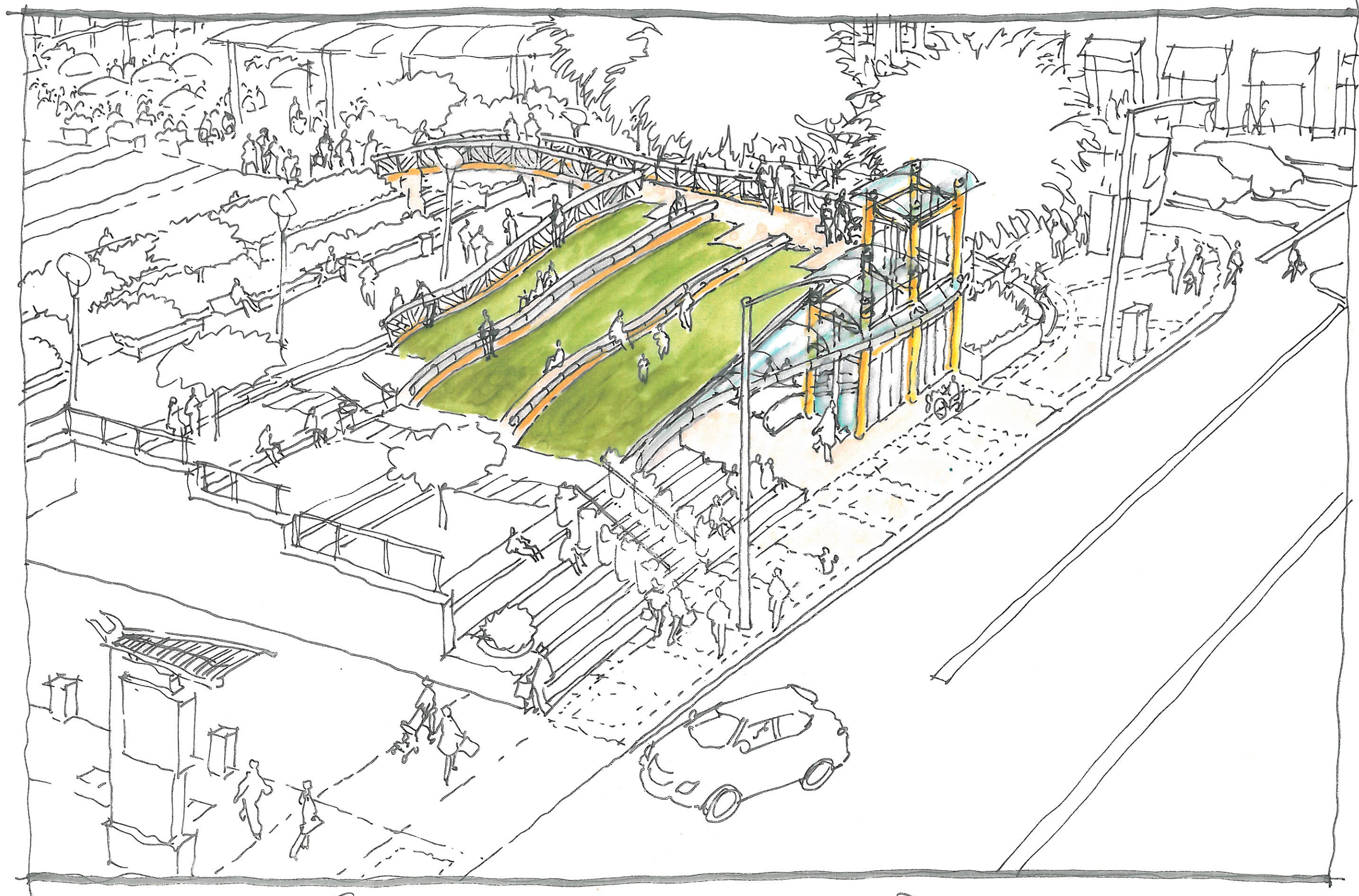


CS UMS
12CC 6.29.10
UNION SQUARE
ENTRANCE
ALTERNATE 2/3
PERSPECTIVE



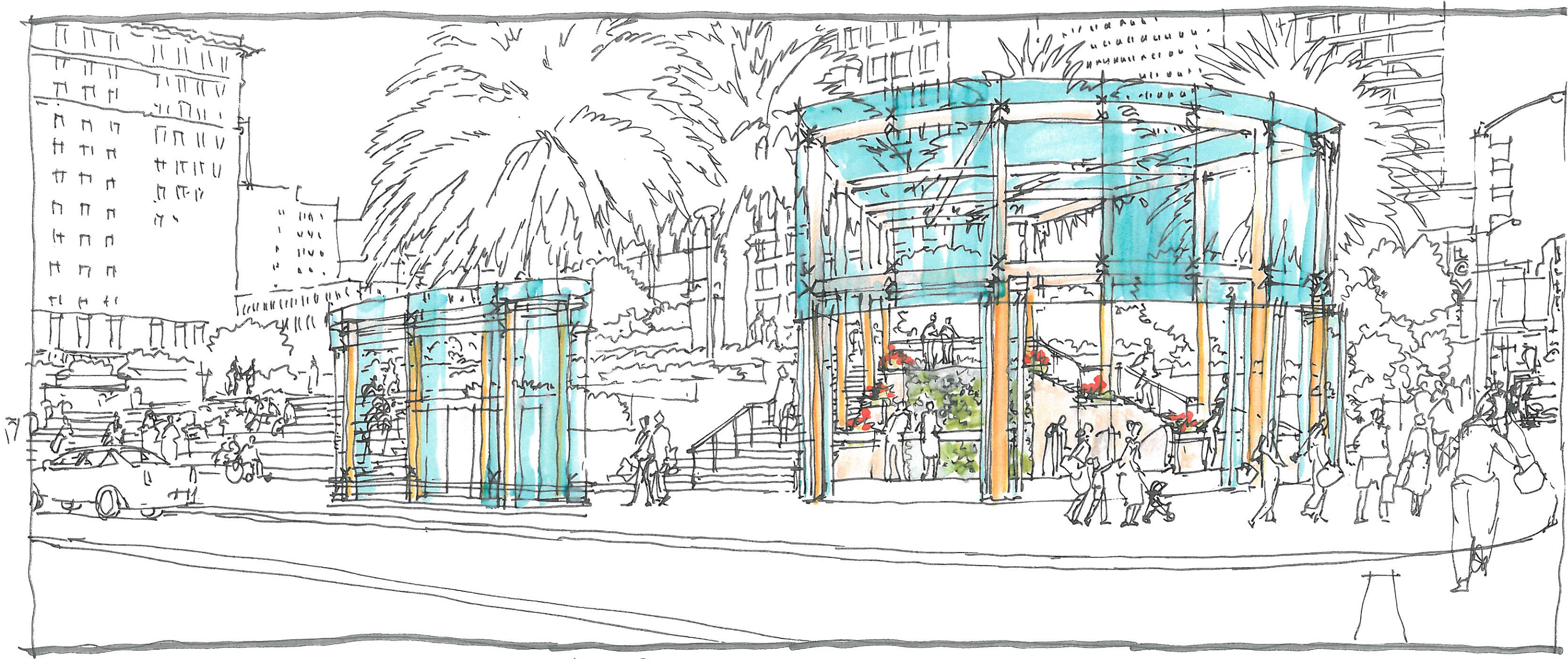
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4.11.11



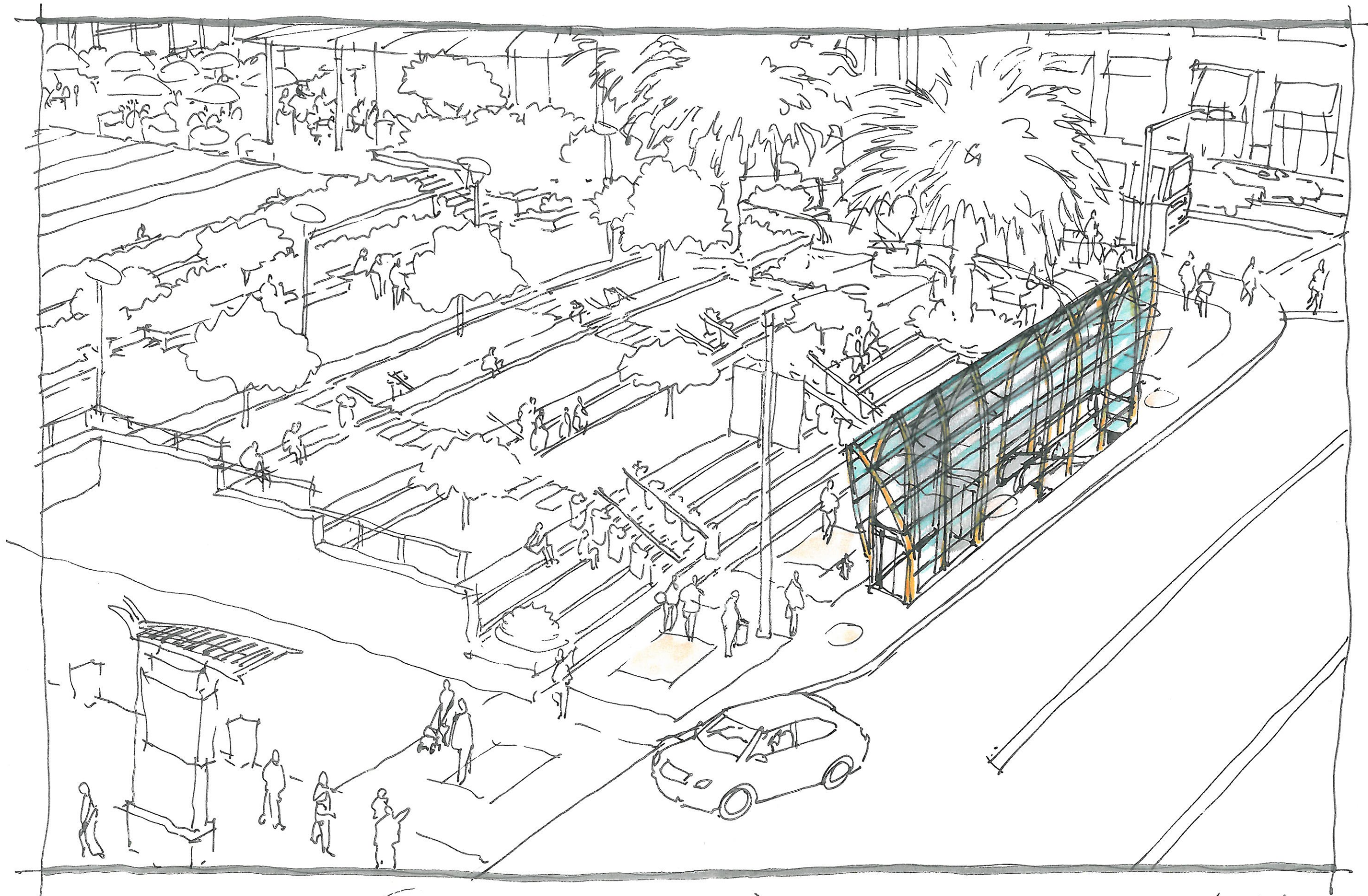
● NORTH ENTRANCE (BIO/MAD CONFIGURATION w/ PARK ACCESS)

4.11.11



WORLD ENTRANCE (CORNER CONFIGURATION)

4.11.11



● NORTH ENTRANCE (BULLS OUT CONFIGURATION)

4.11.11