



SAN FRANCISCO PLANNING DEPARTMENT

Executive Summary Large Project Authorization

HEARING DATE: OCTOBER 16, 2014
CONTINUED FROM OCTOBER 9, 2014

Date: October 9, 2014
Case No.: **2013.0220X**
Project Address: **815-825 TENNESSEE STREET**
Zoning: UMU (Urban Mixed-Use) Zoning District
58-X Height and Bulk District
Block/Lot: 4059/001A and 001B
Project Sponsor: Mark Pilarczyk and Jonathan Emami, ROEM Development Corp.
1650 Lafayette Street
Santa Clara, CA 95050
Staff Contact: Richard Sucre – (415) 575-9108
richard.sucre@sfgov.org

Recommendation: **Approval with Conditions**

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

PROJECT DESCRIPTION

The proposed project includes demolition of the two existing one-to-two-story industrial buildings (collectively measuring approximately 32,000 gross square feet) on the subject lot, and new construction of a five-story, residential building (approximately 93,938 gross square feet) with 69 dwelling units, 48 off-street parking spaces (including one car-share parking space and two accessibility parking spaces), 70 Class 1 bicycle parking spaces, and 5 Class 2 bicycle parking spaces. The project includes a dwelling unit mix consisting of five three-bedroom units, 38 two-bedroom units, and 26 one-bedroom units. The proposed project includes common open space (approximately 10,552 square feet) via a public entry court at the corner of 19th and Tennessee Streets, a rear courtyard and a roof deck. The entry to the below-grade garage is located off of 19th Street.

SITE DESCRIPTION AND PRESENT USE

The proposed project is located on a rectangular parcel at the southeast corner of 19th and Tennessee Streets. The subject parcel has a length of 100-ft along 19th Street and 200-ft along Tennessee Street. Currently, the subject lot contains two vacant industrial buildings—a two-story brick industrial building and a one-story corrugated aluminum shed.

SURROUNDING PROPERTIES AND NEIGHBORHOOD

The project site is located in the UMU Zoning District along a transitioning industrial corridor within the Central Waterfront Area Plan. The immediate neighborhood includes a two-story live/work building

directly across Tennessee Street and several smaller-scale light industrial properties to the north across 19th Street. The project site is located one block away from 3rd Street, which is a transit corridor for the Muni T-Line. Along this portion of 3rd Street are a series of newer, three-to-five-story residential properties. Along Tennessee Street to the south are a number of smaller-scale commercial and industrial properties, as well as several residential properties, which start mid-block past 20th Street and demarcate the Dogpatch Landmark District. Other zoning districts in the vicinity of the project site include: P (Public); RH-2 (Residential, House, Two-Family); RH-3 (Residential, House, Three-Family) and, PDR-1-G (Production, Distribution and Repair-General).

ENVIRONMENTAL REVIEW

Pursuant to the Guidelines of the State Secretary of Resources for the implementation of the California Environmental Quality Act (CEQA), on September 8, 2014, the Planning Department of the City and County of San Francisco determined that the proposed application was exempt from further environmental review under Section 15183 of the CEQA Guidelines and California Public Resources Code Section 21083.3. The Project is consistent with the adopted zoning controls in the Eastern Neighborhoods Area Plan and was encompassed within the analysis contained in the Eastern Neighborhoods Area Plan Final EIR. Since the Final EIR was finalized, there have been no substantial changes to the Eastern Neighborhoods Area Plan and no substantial changes in circumstances that would require major revisions to the Final EIR due to the involvement of new significant environmental effects or an increase in the severity of previously identified significant impacts, and there is no new information of substantial importance that would change the conclusions set forth in the Final EIR.

HEARING NOTIFICATION

TYPE	REQUIRED PERIOD	REQUIRED NOTICE DATE	ACTUAL NOTICE DATE	ACTUAL PERIOD
Classified News Ad	20 days	September 26, 2014	September 19, 2014	27 days
Posted Notice	20 days	September 26, 2014	September 26, 2014	20 days
Mailed Notice	20 days	September 26, 2014	September 26, 2014	20 days

The proposal requires a Section 312 Neighborhood notification, which was conducted in conjunction with the required hearing notification for the Large Project Authorization.

PUBLIC COMMENT

As of October 9, 2014, the Department has not received any public correspondence expressing either support or opposition to the proposed project.

ISSUES AND OTHER CONSIDERATIONS

- Large Project Authorization Modifications: As part of the Large Project Authorization (LPA), the Commission may grant modifications from certain Planning Code requirements for projects that exhibit outstanding overall design and are complementary to the design and values of the surrounding area. The proposed project requests modifications from: 1) rear yard (Planning Code Section 134); 2) open space (Planning Code Section 135); and, 3) permitted obstructions

over the street, setback, yard or useable open space (Planning Code Section 136). Department staff is generally in agreement with the proposed modifications given the overall project and its outstanding and unique design.

- Project Updates: Since publication of the hearing notification, the Project Sponsor has updated the project plans to meet the street frontage requirements, as outlined in Planning Code Section 145.1. The Project provides direct access, walk-up residential units along 19th and Tennessee Streets, thus meeting the active use requirements of Planning Code Section 145.1.
- Inclusionary Affordable Housing: The Project has elected the on-site affordable housing alternative, identified in Planning Code Section 415.6 and 419.3. The project site is located within the UMU Zoning District, and is subject to the Tier A Affordable Housing Program Requirements, which requires 14.4% of the total number of units to be designated as part of the inclusionary affordable housing program. The Project contains 69 units and the Project Sponsor will fulfill this requirement by providing the 10 affordable units on-site, which will be available for ownership.
- Development Impact Fees: The Project would be subject to the following development impact fees, which are estimated as follows:

FEE TYPE	PLANNING CODE SECTION/FEE	AMOUNT
Eastern Neighborhoods Impact Fee (32,000 sq ft – Tier 1; Change in Use from PDR to Residential)	423 (@ \$5.78)	\$184,960
Eastern Neighborhoods Impact Fee (61,938 sq ft – Tier 1; New Residential)	423 (@ \$9.25)	\$572,927
	TOTAL	\$757,887

Please note that these fees are subject to change between Planning Commission approval and approval of the associated Building Permit Application, as based upon the annual updates managed by the Development Impact Fee Unit of the Department of Building Inspection.

REQUIRED COMMISSION ACTION

In order for the project to proceed, the Commission must grant a Large Project Authorization pursuant to Planning Code Section 329 to allow the new construction of a five-story residential building with 69 dwelling units and 48 off-street parking spaces, and to allow modifications to the requirements for rear yard (Planning Code Section 134), open space (Planning Code Section 135), and permitted obstructions over the street, setback, yard or useable open space (Planning Code Section 136).

BASIS FOR RECOMMENDATION

The Department believes this project is approvable for the following reasons:

- The Project complies with the applicable requirements of the Planning Code.
- The Project is consistent with the objectives and policies of the General Plan.

- The Project is located in a zoning district where residential use is principally permitted.
- The Project produces a new residential development with significant site updates, including landscaping and common open space.
- The Project is consistent with and respects the existing neighborhood character, and provides an appropriate massing and scale for a large block.
- The Project complies with the First Source Hiring Program.
- The Project adds 69 new dwelling units to the City's housing stock.
- The Project proposes a parking ratio of approximately .69 spaces per dwelling unit, or 48 off-street parking spaces, which is below the maximum permitted ratio of .75, or 52 off-street parking spaces.
- The Project will fully utilize the Eastern Neighborhoods Area Plan controls, and will pay the appropriate development impact fees.

RECOMMENDATION: Approval with Conditions
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Attachments:

Draft Motion-Large Project Authorization
Parcel Map
Sanborn Map
Aerial Photograph
Zoning Map
Major Projects Map
Architectural Drawings
Public Correspondence
Community Plan Exemption

Attachment Checklist

- | | |
|---|--|
| <input checked="" type="checkbox"/> Executive Summary | <input checked="" type="checkbox"/> Project Sponsor Submittal |
| <input checked="" type="checkbox"/> Draft Motion | Drawings: <u>Existing Conditions</u> |
| <input checked="" type="checkbox"/> Environmental Determination | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Zoning District Map | Drawings: <u>Proposed Project</u> |
| <input checked="" type="checkbox"/> Height & Bulk Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Parcel Map | <input type="checkbox"/> Health Dept. Review of RF levels |
| <input checked="" type="checkbox"/> Sanborn Map | <input type="checkbox"/> RF Report |
| <input checked="" type="checkbox"/> Aerial Photo | <input type="checkbox"/> Community Meeting Notice |
| <input checked="" type="checkbox"/> Context Photos | <input checked="" type="checkbox"/> Inclusionary Affordable Housing Program:
Affidavit for Compliance |
| <input checked="" type="checkbox"/> Site Photos | |

Exhibits above marked with an "X" are included in this packet

_____ RS _____
Planner's Initials

RS: G:\Documents\Large Project Authorization\2013.0220X 815-825 Tennessee St\ExecutiveSummary_815-825 Tennessee St.doc



SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- | | |
|---|---|
| <input checked="" type="checkbox"/> Affordable Housing (Sec. 415) | <input checked="" type="checkbox"/> First Source Hiring (Admin. Code) |
| <input type="checkbox"/> Jobs Housing Linkage Program (Sec. 413) | <input type="checkbox"/> Child Care Requirement (Sec. 414) |
| <input type="checkbox"/> Downtown Park Fee (Sec. 412) | <input checked="" type="checkbox"/> Other (EN Impact Fees) |

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

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415.558.6378

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Planning Commission Draft Motion

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ADOPTING FINDINGS RELATING TO A LARGE PROJECT AUTHORIZATION PURSUANT TO PLANNING CODE SECTION 329, TO ALLOW EXCEPTIONS TO 1) REAR YARD PURSUANT TO PLANNING CODE SECTION 134, 2) OPEN SPACE PURSUANT TO PLANNING CODE SECTION 135 AND 3) PERMITTED OBSTRUCTIONS OVER THE STREET, SETBACK, YARD OR USEABLE OPEN SPACE PURSUANT TO PLANNING CODE SECTION 136, TO ALLOW CONSTRUCTION OF A NEW FIVE-STORY RESIDENTIAL BUILDING (APPROXIMATELY 93,938 GSF) WITH 69 DWELLING UNITS (CONSISTING OF 26 1-BEDROOM UNITS, 38 2-BEDROOM UNITS, AND 5 3-BEDROOM UNITS), LOCATED AT 815-825 TENNESSEE STREET, LOT 001A AND 001B IN ASSESSOR'S BLOCK 4059, WITHIN THE UMU (URBAN MIXED-USE) ZONING DISTRICT AND A 58-X HEIGHT AND BULK DISTRICT, AND ADOPTING FINDINGS UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT.

PREAMBLE

On December 6, 2013, Mark Pilarczyk and Jonathan Emami, ROEM Development Corp. (hereinafter "Project Sponsor") filed Application No. 2013.0220X (hereinafter "Application") with the Planning Department (hereinafter "Department") for a Large Project Authorization to construct a new five-story residential building with 69 dwelling units at 815-825 Tennessee Street (Block 4059 Lots 001A and 001B) in San Francisco, California.

The environmental effects of the Project were determined by the San Francisco Planning Department to have been fully reviewed under the Eastern Neighborhoods Area Plan Environmental Impact Report

(hereinafter "EIR"). The EIR was prepared, circulated for public review and comment, and, at a public hearing on August 7, 2008, by Motion No. 17661, certified by the Commission as complying with the California Environmental Quality Act (Cal. Pub. Res. Code Section 21000 et seq., (hereinafter "CEQA"). The Commission has reviewed the Final EIR, which has been available for this Commission's review as well as public review.

The Eastern Neighborhoods EIR is a Program EIR. Pursuant to CEQA Guideline 15168(c)(2), if the lead agency finds that no new effects could occur or no new mitigation measures would be required of a proposed project, the agency may approve the project as being within the scope of the project covered by the program EIR, and no additional or new environmental review is required. In approving the Eastern Neighborhoods Plan, the Commission adopted CEQA Findings in its Motion No. 17661 and hereby incorporates such Findings by reference.

Additionally, State CEQA Guidelines Section 15183 provides a streamlined environmental review for projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified, except as might be necessary to examine whether there are project-specific effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that (a) are peculiar to the project or parcel on which the project would be located, (b) were not analyzed as significant effects in a prior EIR on the zoning action, general plan or community plan with which the project is consistent, (c) are potentially significant off-site and cumulative impacts which were not discussed in the underlying EIR, or (d) are previously identified in the EIR, but which are determined to have a more severe adverse impact than that discussed in the underlying EIR. Section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for that project solely on the basis of that impact.

On September 8, 2014, the Department determined that the proposed application did not require further environmental review under Section 15183 of the CEQA Guidelines and Public Resources Code Section 21083.3. The Project is consistent with the adopted zoning controls in the Eastern Neighborhoods Area Plan and was encompassed within the analysis contained in the Eastern Neighborhoods Final EIR. Since the Eastern Neighborhoods Final EIR was finalized, there have been no substantial changes to the Eastern Neighborhoods Area Plan and no substantial changes in circumstances that would require major revisions to the Final EIR due to the involvement of new significant environmental effects or an increase in the severity of previously identified significant impacts, and there is no new information of substantial importance that would change the conclusions set forth in the Final EIR. The file for this project, including the Eastern Neighborhoods Final EIR and the Community Plan Exemption certificate, is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, San Francisco, California.

Planning Department staff prepared a Mitigation Monitoring and Reporting Program (MMRP) setting forth mitigation measures that were identified in the Eastern Neighborhoods Plan EIR that are applicable to the project. These mitigation measures are set forth in their entirety in the MMRP attached to the draft Motion as Exhibit C.

On October 16, 2014, the Planning Commission (“Commission”) conducted a duly noticed public hearing at a regularly scheduled meeting on Large Project Authorization Application No. 2013.0220X.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Large Project Authorization requested in Application No. 2013.0220X, subject to the conditions contained in “EXHIBIT A” of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The proposed project is located on a rectangular parcel at the southeast corner of 19th and Tennessee Streets. The subject parcel has a length of 100-ft along 19th Street and 200-ft along Tennessee Street. Currently, the subject lot contains two vacant industrial buildings—a two-story brick industrial building and a one-story corrugated aluminum shed.
3. **Surrounding Properties and Neighborhood.** The project site is located in the UMU Zoning District along a transitioning industrial corridor within the Central Waterfront Area Plan. The immediate neighborhood includes a two-story live/work building directly across Tennessee Street and several smaller-scale light industrial properties to the north across 19th Street. The project site is located one block away from 3rd Street, which is a transit corridor for the Muni T-Line. Along this portion of 3rd Street are a series of newer, three-to-five-story residential properties. Along Tennessee Street to the south are a number of smaller-scale commercial and industrial properties, as well as several residential properties, which start mid-block past 20th Street and demarcate the Dogpatch Landmark District. Other zoning districts in the vicinity of the project site include: P (Public); RH-2 (Residential, House, Two-Family); RH-3 (Residential, House, Three-Family) and, PDR-1-G (Production, Distribution and Repair-General).
4. **Project Description.** The proposed project includes demolition of the two existing one-to-two-story industrial buildings (collectively measuring approximately 32,000 gross square feet) on the subject lot, and new construction of a five-story, residential building (approximately 93,938 gross square feet) with 69 dwelling units, 48 off-street parking spaces (including one car-share parking space and two accessibility parking spaces), 70 Class 1 bicycle parking spaces, and 5 Class 2 bicycle parking spaces. The project includes a dwelling unit mix consisting of five three-bedroom units, 38 two-bedroom units, and 26 one-bedroom units. The proposed project includes common open space (approximately 10,552 square feet) via a public entry court at the corner of 19th and

Tennessee Streets, a rear courtyard and a roof deck. The entry to the below-grade garage is located off of 19th Street.

5. **Public Comment.** The Department has not received any public correspondence expressing either support or opposition to the proposed project.
6. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:

- A. **Permitted Uses in UMU Zoning Districts.** Planning Code Section 843.20 states that residential use is principally permitted use within the UMU Zoning District.

The proposed project would construct a new residential use within the UMU Zoning District; therefore, the proposed project complies with Planning Code Sections 843.20.

- B. **Rear Yard.** Planning Code Section 134 requires a minimum rear yard equal to 25 percent of the total lot depth of the lot to be provided at every residential level. Therefore, the Project would have to provide a rear yard, which measures approximately 5,000 sq ft, located along the rear property line.

Currently, the Project features a rear yard on the ground floor level, which measures approximately 35-ft wide by 100-ft long. The Project provides open space through a series of private balconies, a public entry court, a rear courtyard, and a roof deck. The Project provides a total of 10,552 sq ft of open space (includes compliant and non-complaint open space). Thus, the total amount of open space, which would have been provided through the required rear yard, is exceeded. The Project is seeking a modification of the rear yard requirement as part of the Large Project Authorization, since the proposed rear yard (or rear courtyard) does not extend the entire length of the subject lot (See Below). Both front and rear courtyards total 4,952 sq ft, which is a comparable area.

The Project occupies a corner lot at the intersection of 19th and Tennessee Streets. Since the surrounding area is primarily industrial, the subject block does not possess a pattern of mid-block open space. By using a courtyard design at the front along with the former brick industrial building, the Project maintains the street wall along 19th and Tennessee Streets, while also providing an urban intervention which more closely resembles a traditional mid-block open space pattern at the rear.

- C. **Useable Open Space.** Planning Code Section 135 requires a minimum of 80 sq ft of open space per dwelling unit, if not publically accessible, or 54 sq ft of open space per dwelling unit, if publically accessible. Private useable open space shall have a minimum horizontal dimension of six feet and a minimum area of 36 sq ft is located on a deck, balcony, porch or roof, and shall have a minimum horizontal dimension of 10 feet and a minimum area of 100 sq ft if located on open ground, a terrace or the surface of an inner or outer court. Common useable open space shall be at least 15 feet in every horizontal dimension and shall be a minimum of 300 sq ft. Further, inner courts may be credited as common useable open space if the enclosed space is not less than 20 feet in every horizontal dimension and 400 sq ft in

area, and if the height of the walls and projections above the court on at least three sides is such that no point on any such wall or projection is higher than one foot for each foot that such point is horizontally distant from the opposite side of the clear space in the court.

For the proposed 69 dwelling units, the Project is required to provide 5,520 sq ft of open space, either private or common.

In total, the Project exceeds the amount of open space by constructing a total of 10,552 sq ft of useable open space via a ground floor entry courtyard, a rear court, and a roof deck, as well as through a series of private balconies, which face onto the rear court. However, the proposed rear court does not meet the dimensional requirements of Planning Code Section 135; therefore, the Project is seeking a modification of the open space requirement through the Large Project Authorization (See Below).

- D. Streetscape and Pedestrian Improvements.** Planning Code Section 138.1 requires one new street tree for every 20 feet of street frontage for projects proposing new construction, as well as a streetscape plan, which includes elements from the Better Streets Plan.

The Project includes the new construction of a five-story residential building on a lot with approximately 100-ft of frontage along 19th Street and 200-ft of frontage along Tennessee Street. Therefore, the Project is required to provide a total of five street trees along 19th Street, and ten street trees along Tennessee Street.

Currently, the Project meets the street tree requirement by providing five street trees along 19th Street and ten street trees along Tennessee Street. Depending on consultation with the Department of Public Works (DPW), the Project shall seek a waiver from the Zoning Administrator and pay an in-lieu fee for any street tree not provided along the street.

In addition, the Project includes streetscape elements, including a bulb-out at the corner of 19th and Tennessee Streets, as well as new sidewalk plantings, bicycle parking and street furniture.

Therefore, the proposed project complies with Planning Code Section 138.1.

- E. Bird Safety.** Planning Code Section 139 outlines the standards for bird-safe buildings, including the requirements for location-related and feature-related hazards.

The subject lot is not located in close proximity to an Urban Bird Refuge. The proposed project meets the requirements of feature-related standards and does not include any unbroken glazed segments 24-sq ft and larger in size; therefore, the proposed project complies with Planning Code Section 139.

- F. Dwelling Unit Exposure.** Planning Code Section 140 requires that at least one room of all dwelling units face onto a public street, rear yard or other open area that meets minimum requirements for area and horizontal dimensions. To meet exposure requirements, a public street, public alley, side yard or rear yard must be at least 25 ft in width, or an open area

(inner court) must be no less than 25 ft in every horizontal dimension for the floor at which the dwelling unit is located.

The Project organizes the dwelling units to have exposure either on 19th or Tennessee Streets, or along the inner court. Currently, the inner court meets the dimensional requirements of the Planning Code, since it is a minimum of 25-ft wide in every horizontal dimension for the floor of the lowest dwelling units and the floor above, and increases in horizontal dimension on each subsequent floor. Therefore, the Project meets the exposure requirements of the Planning Code.

- G. Street Frontage in Mixed Use Districts.** Planning Code Section 145.1 requires off-street parking at street grade on a development lot to be set back at least 25 feet on the ground floor; that no more than one-third of the width or 20 feet, whichever is less, of any given street frontage of a new structure parallel to and facing a street shall be devoted to parking and loading ingress or egress; that space for active uses be provided within the first 25 feet of building depth on the ground floor; that non-residential uses have a minimum floor-to-floor height of 14 feet; that the floors of street-fronting interior spaces housing non-residential active uses and lobbies be as close as possible to the level of the adjacent sidewalk at the principal entrance to these spaces; and that frontages with active uses that are not residential or PDR be fenestrated with transparent windows and doorways for no less than 60 percent of the street frontage at the ground level.

The Project meets the requirements of Planning Code Section 145.1. At grade, the off-street parking is setback by more than 25-ft from the street. The Project has only one twelve-ft wide garage entrance to the off-street parking located along 19th Street. The Project features active uses on the ground floor with walk-up dwelling units with direct, individual pedestrian access to a public sidewalk. Finally, the Project features appropriate street-facing ground level spaces, as well as the ground level transparency and fenestration requirements.

- H. Off-Street Parking.** Planning Section 151.1 of the Planning Code allows off-street parking at a maximum ratio of .75 per dwelling unit.

For the 69 dwelling units, the Project is allowed to have a maximum of 52 off-street parking spaces. Currently, the Project provides 48 below-grade off-street parking spaces. Of these 48 off-street parking spaces, two handicap parking spaces have been identified. Therefore, the proposed project complies with Planning Code Section 151.1.

- I. Bicycle Parking.** Planning Section 155.2 of the Planning Code requires at least one Class 1 bicycle parking spaces for each dwelling unit and one Class 2 bicycle parking spaces for every 20 dwelling units.

The Project includes 69 dwelling units; therefore, the Project is required to provide 69 Class 1 bicycle parking spaces and three Class 2 bicycle parking spaces. The Project will provide 70 Class 1 bicycle parking spaces and five Class 2 bicycle parking spaces, thus exceeding the requirement. Therefore, the proposed project complies with Planning Code Section 155.2.

- J. **Car Share Requirements.** Planning Code Section 166 requires one car-share parking space for projects constructing 50 to 200 dwelling units.

Since the Project includes 69 dwelling units, it is required to provide a minimum of one car-share parking space. The Project provides one car-share parking space; therefore, the proposed project complies with Planning Code Section 166.

- K. **Unbundled Parking.** Planning Code Section 167 requires that all off-street parking spaces accessory to residential uses in new structures of 10 dwelling units or more be leased or sold separately from the rental or purchase fees for dwelling units for the life of the dwelling units.

The Project is providing off-street parking that is accessory to the dwelling units. These spaces will be unbundled and sold and/or leased separately from the dwelling units; therefore, the Project meets this requirement.

- L. **Dwelling Unit Mix.** Planning Code Section 207.6 requires that no less than 40 percent of the total number of proposed dwelling units contain at least two bedrooms, or no less than 30 percent of the total number of proposed dwelling units contain at least three bedrooms.

For the 69 dwelling units, the Project is required to provide at least 28 two-bedroom units or 21 three-bedroom units. The Project provides 26 one-bedroom units, 28 two-bedroom units, and 5 three-bedroom units. Therefore, the Project meets and exceeds the requirements for dwelling unit mix.

- M. **Shadow.** Planning Code Section 295 restricts net new shadow, cast by structures exceeding a height of 40 feet, upon property under the jurisdiction of the Recreation and Park Commission. Any project in excess of 40 feet in height and found to cast net new shadow must be found by the Planning Commission, with comment from the General Manager of the Recreation and Parks Department, in consultation with the Recreation and Park Commission, to have no adverse impact upon the property under the jurisdiction of the Recreation and Park Commission.

Based upon a detail shadow analysis, the Project does not cast any net new shadow upon property under the jurisdiction of the Recreation and Parks Commission.

- N. **Inclusionary Affordable Housing Program.** Planning Code Section 415 sets forth the requirements and procedures for the Inclusionary Affordable Housing Program. Since the subject property is located within the UMU Zoning District, the Project is subject to the inclusionary affordable housing requirements identified in Planning Code Section 419. The subject property has been designated as Tier A, thus a minimum of 14.4 percent of the total units constructed shall be considered affordable.

The Project Sponsor has submitted an 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to satisfy the requirements of the Inclusionary Affordable Housing Program by providing the affordable housing on-site instead of through payment of the Affordable Housing Fee. Based upon the Affidavit dated July 3, 2014, the Project Sponsor has elected the On-Site Affordable Housing Alternative. Therefore, for 69 total dwelling units, the Project shall provide 10 affordable dwelling units for ownership.

If the Project becomes ineligible to meet its Inclusionary Affordable Housing Program obligation through the On-Site Affordable Housing Alternative, it must pay the Affordable Housing Fee with interest, if applicable.

- O. **Eastern Neighborhood Infrastructure Impact Fees.** Planning Code Section 423 is applicable to any development project within the UMU (Urban Mixed-Use) Zoning District that results in the addition of gross square feet of non-residential space.

The proposed project includes approximately 93,938 gross square feet of new residential development. These uses are subject to Eastern Neighborhood Infrastructure Impact Fees, as outlined in Planning Code Section 423. These fees must be paid prior to the issuance of the building permit application.

- 7. **Large Project Authorization in Eastern Neighborhoods Mixed Use District.** Planning Code Section 329(c) lists nine aspects of design review in which a project must comply; the Planning Commission finds that the project is compliant with these nine aspects as follows:

- A. Overall building mass and scale.

The Project's mass and scale are appropriate for a large corner lot and the surrounding context, which includes smaller-scale industrial properties and larger-scale residential buildings that create a varied street wall. In particular, the proposed project is consistent with the mass and scale of nearby industrial properties, including those larger-scale four-story industrial properties located within the nearby Dogpatch Landmark District. The Project divides the massing into two distinct elements, which addresses the adjacent street frontage along 19th and Tennessee Streets. The Project is further enhanced by the incorporation of the existing brick industrial buildings, which acts as a folly and defines the entry courtyard. This feature helps to scale down the Project's overall mass, while relating to the surrounding varied industrial context. Thus, the project is appropriate and consistent with the mass and scale of the surrounding neighborhood.

- B. Architectural treatments, facade design and building materials:

The proposed project's architectural treatments, facade design and building materials include composite metal panels, brick veneer, cast stone panels and aluminum windows. The Project provides for a unique expression along the street, which merges well with the existing two-story brick industrial building at the corner. Along Tennessee Street, the building features angled bay windows and includes framing elements, which assist in breaking down the overall scale, while providing for visual interest. Along 19th Street, the building continues the pattern established along Tennessee

Street, while appropriately addressing the steep topography. Overall, the Project offers a high quality architectural treatment, which provides for unique and expressive architectural design that is consistent and compatible with the surrounding neighborhood.

- C. The design of lower floors, including building setback areas, commercial space, townhouses, entries, utilities, and the design and siting of rear yards, parking and loading access;

Along the lower floors, the Project provides for four walk-up dwelling units with individual pedestrian access on 19th and Tennessee Streets. These dwelling units provide for activity along the street. At the corner of 19th and Tennessee Street, the Project provides a publically-accessible entry courtyard, which is defined by a portion of the former brick industrial building. This feature provides activity at the corner, while also providing a design feature that harkens to the area's industrial heritage.

- D. The provision of required open space, both on- and off-site. In the case of off-site publicly accessible open space, the design, location, access, size, and equivalence in quality with that otherwise required on-site;

The Project provides the required open space for the 69 dwelling units through private balconies and common open space via an entry courtyard, rear court, and roof deck. In total, the Project provides 10,552 sq ft of open space, which far exceeds the required amount for the dwelling units.

- E. The provision of mid-block alleys and pathways on frontages between 200 and 300 linear feet per the criteria of Section 270, and the design of mid-block alleys and pathways as required by and pursuant to the criteria set forth in Section 270.2;

The Project is not subject to the mid-block alley requirements of Planning Code Section 270.2.

- F. Streetscape and other public improvements, including tree planting, street furniture, and lighting.

In compliance with Planning Code Section 138.1, the Project provides 15 new street trees along the street frontages on 19th and Tennessee Streets, and would pay an in-lieu fee for any required street trees not provided due to proximity of underground utilities, etc., as specified by the Department of Public Works. In addition, the Project includes streetscape elements, including a corner bulb out, sidewalk planters and site furnishings. The Department finds that these improvements would improve the public realm.

- G. Circulation, including streets, alleys and mid-block pedestrian pathways;

The Project provides ample circulation in and around the project site through the sidewalk improvement and a publically-accessible entry courtyard. The primary focal point for the residents would occur at the corner of 19th and Tennessee Street. Automobile access is limited to the one entry/exit (measuring 12-ft wide) on the Tennessee Street façade.

H. Bulk limits;

The Project is within an 'X' Bulk District, which does not restrict bulk.

I. Other changes necessary to bring a project into conformance with any relevant design guidelines, Area Plan or Element of the General Plan;

The Project, on balance, meets the Objectives and Policies of the General Plan. See Below.

8. **Large Project Authorization Exceptions.** Proposed Planning Code Section 329 allows exceptions for Large Projects in the Eastern Neighborhoods Mixed Use Districts:

A. Exception from residential useable open space requirements. In circumstances where such exception is granted a fee shall be required pursuant to the standards in Sections 135(j), pursuant to the criteria of Section 305(c).

The Project is required to provide 5,520 sq ft of open space for the proposed 69 dwelling units. The Project provides 10,552 sq ft of residential open space, thus exceeding the required amount of residential useable open space; however, the proposed rear court (measuring approximately 3,808 sq ft) does not meet the dimensional requirements of the Planning Code. Given the overall amount of open space and the quality of the Project, the modification to the dimensional requirements would not severely impact the usability and quality of the proposed rear court.

B. Exception for rear yards, pursuant to the requirements of Section 134(f);

Modification of Requirements in the Eastern Neighborhoods Mixed Use Districts. The rear yard requirement in Eastern Neighborhoods Mixed Use Districts may be modified or waived by the Planning Commission pursuant to Section 329. The rear yard requirement in Eastern Neighborhoods Mixed Use Districts may be modified by the Zoning Administrator pursuant to the procedures set forth in Section 307(h) for other projects, provided that:

(1) A comparable, but not necessarily equal amount of square footage as would be created in a code conforming rear yard is provided elsewhere within the development;

The Project provides for a comparable amount of open space, in lieu of the required rear yard. Overall, the project site is 20,000 sq ft in size, and would be required to provide a rear yard measuring 5,000 sq ft. The Project provides 10,552 sq ft of open space through private balconies, a ground floor entry courtyard, a rear court and a roof deck, thus exceeding the amount of space, which would have been provided in a code-conforming rear yard. Further, the amount of ground floor open space is comparable to the amount of open space, which would have been via the required rear yard.

(2) The proposed new or expanding structure will not significantly impede the access to light and air from adjacent properties or adversely affect the interior block open space formed by the rear yards of adjacent properties; and

The Project does not impede access to light and air for the adjacent properties. The adjacent properties are not residential in nature. In addition, the subject block does not have a pattern of rear yard open space.

(3) The modification request is not combined with any other residential open space modification or exposure variance for the project, except exposure modifications in designated landmark buildings under Section 307(h)(1).

The Project is not seeking a modification to the exposure requirements; however, the Project is seeking a modification to the open space requirement since the Project's rear court does not conform to the dimensional requirements of the Planning Code. Overall, the Project meets the intent of open space requirements defined in Planning Code Section 135.

- C. Where not specified elsewhere in Planning Code Section 329(d), modification of other Code requirements which could otherwise be modified as a Planned Unit Development (as set forth in Section 304), irrespective of the zoning district in which the property is located;

In addition to the modification of the requirements for rear yard and open space, the proposed project is seeking modifications of the requirements for permitted obstructions over streets, setbacks, yards, and useable open space (Planning Code Section 136).

Under Planning Code Section 136, rectangular bay windows are limited to 9-ft wide, and 3-ft deep over a street, alley or setback. The Project proposes bay windows and deck projections over the street, which exceeds the dimensions and spacing outlined within Planning Code Section 136. Given the overall design and composition, the Commission finds this modification is warranted, due to the project's quality of design and integration with the existing brick industrial building, which is a strong urban design element that relates to the area's industrial heritage.

8. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

HOUSING

Objectives and Policies

OBJECTIVE 1

IDENTIFY AND MAKE AVAILABLE FOR DEVELOPMENT ADEQUATE SITES TO MEET THE CITY'S HOUSING NEEDS, ESPECIALLY PERMANENTLY AFFORDABLE HOUSING.

Policy 1.1

Plan for the full range of housing needs in the City and County of San Francisco, especially affordable housing.

The Project is a higher density residential development in a transitioning industrial area. The Project site is an ideal infill site that is vacant. The project site was rezoned to UMU as part of a long range planning goal to create a cohesive, higher density residential and mixed-use neighborhood. The project includes ten on-site affordable housing units, which complies with the UMU District's goal to provide a higher level of affordability.

OBJECTIVE 4

FOSTER A HOUSING STOCK THAT MEETS THE NEEDS OF ALL RESIDENTS ACROSS LIFECYCLES.

The Project meets the affordable housing requirements for the UMU Zoning District, and provides ten on-site affordable units.

OBJECTIVE 11

SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORHOODS.

Policy 11.1

Promote the construction and rehabilitation of well-designed housing that emphasizes beauty, flexibility, and innovative design, and respects existing neighborhood character.

Policy 11.2

Ensure implementation of accepted design standards in project approvals.

Policy 11.3

Ensure growth is accommodated without substantially and adversely impacting existing residential neighborhood character.

Policy 11.4

Continue to utilize zoning districts which conform to a generalized residential land use and density plan and the General Plan.

Policy 11.6

Foster a sense of community through architectural design, using features that promote community interaction.

Policy 11.8

Consider a neighborhood's character when integrating new uses, and minimize disruption caused by expansion of institutions into residential areas.

The architecture of this Project responds to the site's location as a transition between industrial zones and the contemporary and traditional architecture of residential zones. The Project's facades provide a unique

expression not commonly found within the surrounding area, while providing for a material palette, which draws from the surrounding industrial context. The exterior is designed with modern materials including composite metal panels and brick.

RECREATION AND OPEN SPACE ELEMENT

Objectives and Policies

OBJECTIVE 4:

PROVIDE OPPORTUNITIES FOR RECREATION AND THE ENJOYMENT OF OPEN SPACE IN EVERY SAN FRANCISCO NEIGHBORHOOD.

Policy 4.5:

Require private usable outdoor open space in new residential development.

Policy 4.6:

Assure the provision of adequate public open space to serve new residential development.

The Project will create private and common open space areas in a new residential mixed-use development through private balconies, an entry courtyard, a rear court and a roof deck. The project will not cast shadows over any open spaces under the jurisdiction of the Recreation and Park Department.

TRANSPORTATION ELEMENT

Objectives and Policies

OBJECTIVE 24:

IMPROVE THE AMBIENCE OF THE PEDESTRIAN ENVIRONMENT.

Policy 24.2:

Maintain and expand the planting of street trees and the infrastructure to support them.

Policy 24.3:

Install pedestrian-serving street furniture where appropriate.

Policy 24.4:

Preserve pedestrian-oriented building frontages.

The Project will install new street trees along 19th and Tennessee Street. Further, the Project will provide a new corner bulb-out, street plantings, and new site furnishings. Frontages are designed with active spaces oriented at the pedestrian level.

OBJECTIVE 28:

PROVIDE SECURE AND CONVENIENT PARKING FACILITIES FOR BICYCLES.

Policy 28.1:

Provide secure bicycle parking in new governmental, commercial, and residential developments.

Policy 28.3:

Provide parking facilities which are safe, secure, and convenient.

The Project includes 70 Class 1 bicycle parking spaces and 5 Class 2 bicycle parking spaces in secure, convenient locations.

OBJECTIVE 34:

RELATE THE AMOUNT OF PARKING IN RESIDENTIAL AREAS AND NEIGHBORHOOD COMMERCIAL DISTRICTS TO THE CAPACITY OF THE CITY'S STREET SYSTEM AND LAND USE PATTERNS.

Policy 34.1:

Regulate off-street parking in new housing so as to guarantee needed spaces without requiring excesses and to encourage low auto ownership in neighborhoods that are well served by transit and are convenient to neighborhood shopping.

Policy 34.3:

Permit minimal or reduced off-street parking supply for new buildings in residential and commercial areas adjacent to transit centers and along transit preferential streets.

Policy 34.5:

Minimize the construction of new curb cuts in areas where on-street parking is in short supply and locate them in a manner such that they retain or minimally diminish the number of existing on-street parking spaces.

The Project has a parking to dwelling unit ratio of .69 space per unit, which is below the permitted ratio of .75 per unit. The parking spaces are accessed by one ingress/egress point measuring 12-ft wide from 19th Street. Parking is adequate for the project and complies with maximums prescribed by the Planning Code.

URBAN DESIGN ELEMENT

Objectives and Policies

OBJECTIVE 1:

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

Policy 1.7:

Recognize the natural boundaries of districts, and promote connections between districts.

OBJECTIVE 2:

CONSERVATION OF RESOURCES WHICH PROVIDE A SENSE OF NATURE, CONTINUITY WITH THE PAST, AND FREEDOM FROM OVERCROWDING.

Policy 2.6:

Respect the character of older development nearby in the design of new buildings.

The Project is located within the Central Waterfront Area and Dogpatch neighborhood, which is characterized by the mix of residential and industrial uses. As such, the Project provides expressive street façades, which respond to form, scale and material palette of the existing neighborhood, while also providing a new contemporary architectural vocabulary.

OBJECTIVE 4:

IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY.

Policy 4.5:

Design walkways and parking facilities to minimize danger to pedestrians.

Policy 4.13:

Improve pedestrian areas by providing human scale and interest.

Although the project site has two street frontages, it only provides one vehicular access point for the entire project, limiting conflicts with pedestrians and bicyclists. Numerous street trees will be planted on each street. Along the project site, the pedestrian experience will be greatly improved.

CENTRAL WATERFRONT AREA PLAN

Objectives and Policies

Land Use

OBJECTIVE 1.1

ENCOURAGE THE TRANSITION OF PORTIONS OF THE CENTRAL WATERFRONT TO A MORE MIXED-USE CHARACTER, WHILE PROTECTING THE NEIGHBORHOOD'S CORE OF PDR USES AS WELL AS THE HISTORIC DOGPATCH NEIGHBORHOOD.

Policy 1.1.6

Permit and encourage small and moderate size retail establishments in neighborhood commercial areas of Central Waterfront, while allowing larger retail in the new Urban Mixed Use districts only when part of a mixed-use development.

Policy 1.1.9

Permit and encourage greater retail uses on the ground floor on parcels that front 3rd Street to take advantage of transit service and encourage more mixed uses, while protecting against the wholesale displacement of PDR uses.

OBJECTIVE 1.2

IN AREAS OF THE CENTRAL WATERFRONT WHERE HOUSING AND MIXED-USE IS ENCOURAGED, MAXIMIZE DEVELOPMENT POTENTIAL IN KEEPING WITH NEIGHBORHOOD CHARACTER.

Policy 1.2.3

In general, where residential development is permitted, control residential density through building height and bulk guidelines and bedroom mix requirements.

Housing

OBJECTIVE 2.3

REQUIRE THAT A SIGNIFICANT NUMBER OF UNITS IN NEW DEVELOPMENTS HAVE TWO OR MORE BEDROOMS EXCEPT SENIOR HOUSING AND SRO DEVELOPMENTS UNLESS ALL BELOW MARKET RATE UNITS ARE TWO OR MORE BEDROOM UNITS.

Policy 2.3.3

Require that a significant number of units in new developments have two or more bedrooms, except Senior Housing and SRO developments.

Policy 2.3.6

Establish an impact fee to be allocated towards an Eastern Neighborhoods Public Benefit Fund to mitigate the impacts of new development on transit, pedestrian, bicycle, and street improvements, park and recreational facilities, and community facilities such as libraries, child care and other neighborhood services in the area.

Urban Form

OBJECTIVE 3.1

PROMOTE AN URBAN FORM THAT REINFORCES THE CENTRAL WATERFRONT'S DISTINCTIVE PLACE IN THE CITY'S LARGER FORM AND STRENGTHENS ITS PHYSICAL FABRIC AND CHARACTER.

Policy 3.1.6

New buildings should epitomize the best in contemporary architecture, but should do so with full awareness of, and respect for, the height, mass, articulation and materials of the best of the older buildings that surrounds them.

OBJECTIVE 3.2

PROMOTE AN URBAN FORM AND ARCHITECTURAL CHARACTER THAT SUPPORTS WALKING AND SUSTAINS A DIVERSE, ACTIVE AND SAFE PUBLIC REALM.

Policy 3.2.1

Require high quality design of street-facing building exteriors.

Policy 3.2.5

Building form should celebrate corner locations.

The Project is residential in use and provides the mix of uses encouraged by the Area Plan for this location. In addition, the Project is located within the prescribed height and bulk guidelines, and includes the appropriate dwelling unit mix, since approximately 62.2% or 43 units are two- or three-bedroom dwellings. The Project introduces a contemporary architectural vocabulary, which is sensitive to the prevailing scale and neighborhood fabric. The Project provides for a high quality designed exterior, which features a variety of materials, colors and textures, including composite metal panels, brick veneer, and aluminum windows. The Project will also pay the appropriate development impact fees, including the Eastern Neighborhoods Impact Fees.

9. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project does comply with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

Currently, the project site does not contain any existing neighborhood-serving uses. The Project improves the urban form of the neighborhood by vacant buildings. The Project would add new residents, visitors, and employees to the neighborhood, which would assist in strengthening nearby retail uses.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

No housing exists on the project site. The project will provide up to 69 new dwelling units, thus resulting in an increase in the neighborhood housing stock. The Project is expressive in design, and relates to the scale and form of the surrounding neighborhood by providing relationships to the smaller-scale industrial properties as well as the newer, larger-scale nearby residential properties. For these reasons, the proposed project would protect and preserve the cultural and economic diversity of the neighborhood.

- C. That the City's supply of affordable housing be preserved and enhanced.

The Project will not displace any affordable housing because there is currently no housing on the site. The Project will comply with the City's Inclusionary Housing Program, therefore increasing the stock of affordable housing units in the City.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The project site is well-served by public transportation. The Project is located within one block of the MUNI T-Line Station. In addition, the Project is located within the vicinity of the 22nd Street Caltrain Station. Future residents would be afforded close proximity to bus or rail transit. The Project also provides sufficient off-street parking at a ratio of .69 per dwelling unit, and sufficient bicycle parking for residents and their guests.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The Project is consistent with the Central Waterfront Area Plan, which provides for a balance between industrial and residential development. The Project does not displace the City's industrial and services sectors for commercial office development. The Project provides new housing, which is a top priority in the City.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The project will be designed and will be constructed to conform to the structural and seismic safety requirements of the Building Code. This proposal will not impact the property's ability to withstand an earthquake.

- G. That landmarks and historic buildings be preserved.

Currently, the project site does not contain any City Landmarks. Although the project would remove an individually-eligible historic resource, the Project would retain a portion of the existing two-story brick industrial building and incorporate it into the Project, thus maintaining a connection to the site's past history.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The Project will not affect the City's parks or open space or their access to sunlight and vistas. A shadow study was completed and concluded that the Project will not cast shadows on any property under the jurisdiction of, or designated for acquisition by, the Recreation and Park Commission.

9. **First Source Hiring.** The Project is subject to the requirements of the First Source Hiring Program as they apply to permits for residential development (Section 83.4(m) of the Administrative Code), and the Project Sponsor shall comply with the requirements of this Program as to all construction work and on-going employment required for the Project. Prior to the issuance of any building permit to construct or a First Addendum to the Site Permit, the Project Sponsor

shall have a First Source Hiring Construction and Employment Program approved by the First Source Hiring Administrator, and evidenced in writing. In the event that both the Director of Planning and the First Source Hiring Administrator agree, the approval of the Employment Program may be delayed as needed.

The Project Sponsor submitted a First Source Hiring Affidavit and prior to issuance of a building permit will execute a First Source Hiring Memorandum of Understanding and a First Source Hiring Agreement with the City's First Source Hiring Administration.

10. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
11. The Commission hereby finds that approval of the Large Project Authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Large Project Authorization Application No. 2013.0220X** under Planning Code Section 329 to allow the new construction of a five-story residential building with 69 dwelling units, and a modification to the requirements for: 1) rear yard (Planning Code Section 134); 2) open space (Planning Code Section 135); and, 3) permitted obstructions over the street, setback, yard or useable open space (Planning Code Section 136), within the UMU (Urban Mixed-Use) Zoning District, and a 58-X Height and Bulk District. The project is subject to the following conditions attached hereto as "EXHIBIT A" in general conformance with plans on file, dated September 25, 2014, and stamped "EXHIBIT B", which is incorporated herein by reference as though fully set forth.

The Planning Commission hereby adopts the MMRP attached hereto as Exhibit C and incorporated herein as part of this Motion by this reference thereto. All required mitigation measures identified in the Eastern Neighborhoods Plan EIR and contained in the MMRP are included as conditions of approval.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Section 329 Large Project Authorization to the Board of Appeals within fifteen (15) days after the date of this Motion. The effective date of this Motion shall be the date of adoption of this Motion if not appealed (after the 15-day period has expired) OR the date of the decision of the Board of Appeals if appealed to the Board of Appeals. For further information, please contact the Board of Appeals at (415) 575-6880, 1660 Mission, Room 3036, San Francisco, CA 94103.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission's adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator's Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission ADOPTED the foregoing Motion on October 16, 2014.

Draft Motion
October 16, 2014

CASE NO. 2013.0220X
815-825 Tennessee Street

Jonas P. Ionin
Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED: October 16, 2014

EXHIBIT A

AUTHORIZATION

This authorization is for a Large Project Authorization to allow for the new construction of a five-story residential building with 69 dwelling units, and a modification to the requirements for rear yard, open space, and permitted obstructions over the street, setback, yard or useable open space, located at 815-825 Tennessee Street, Lot 001A and 001B in Assessor's Block 4059 pursuant to Planning Code Section 329 within the UMU (Urban Mixed-Use) Zoning District, and a 58-X Height and Bulk District; in general conformance with plans, dated September 25, 2014, and stamped "EXHIBIT B" included in the docket for Case No. 2013.0220X and subject to conditions of approval reviewed and approved by the Commission on October 16, 2014 under Motion No. XXXXXX. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on October 16, 2014 under Motion No. XXXXXX.

PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. XXXXXX shall be reproduced on the Index Sheet of construction plans submitted with the Site or Building permit application for the Project. The Index Sheet of the construction plans shall reference to the Office Development Authorization and any subsequent amendments or modifications.

SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new authorization.

Conditions of Approval, Compliance, Monitoring, and Reporting

PERFORMANCE

Validity. The authorization and right vested by virtue of this action is valid for three (3) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this three-year period.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Expiration and Renewal. Should a Building or Site Permit be sought after the three (3) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Diligent Pursuit. Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than three (3) years have passed since this Authorization was approved.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Extension. All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Conformity with Current Law. No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Mitigation Measures. Mitigation measures described in the MMRP for the Eastern Neighborhoods Plan EIR (Case No. 2013.0220E) attached as Exhibit C are necessary to avoid potential significant effects of the proposed project and have been agreed to by the project sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

DESIGN – COMPLIANCE AT PLAN STAGE

Final Materials. The Project Sponsor shall continue to work with Planning Department on the building design. Final materials, glazing, color, texture, landscaping, and detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

Street Trees. Pursuant to Planning Code Section 138.1 (formerly 143), the Project Sponsor shall submit a site plan to the Planning Department prior to Planning approval of the building permit application indicating that street trees, at a ratio of one street tree of an approved species for every 20 feet of street frontage along public or private streets bounding the Project, with any remaining fraction of 10 feet or more of frontage requiring an extra tree, shall be provided. Therefore, the Project shall provide at least five street trees along 19th Street and ten street trees along Tennessee Street. The street trees shall be evenly spaced along the street frontage except where proposed driveways or other street obstructions do not permit. The exact location, size and species of tree shall be as approved by the Department of Public Works (DPW). In any case in which DPW cannot grant approval for installation of a tree in the public right-of-way, on the basis of inadequate sidewalk width, interference with utilities or other reasons regarding the public welfare, and where installation of such tree on the lot itself is also impractical, the requirements of this Section 428 may be modified or waived by the Zoning Administrator to the extent necessary.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

Garbage, Composting and Recycling Storage. Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the architectural addenda. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided at the ground level of the buildings.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

Rooftop Mechanical Equipment. Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application for each building. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

PARKING AND TRAFFIC

Unbundled Parking. All off-street parking spaces shall be made available to Project residents only as a separate “add-on” option for purchase or rent and shall not be bundled with any Project dwelling unit for the life of the dwelling units. The required parking spaces may be made available to residents within a quarter mile of the project. All affordable dwelling units pursuant to Planning Code Section 415 shall have equal access to use of the parking as the market rate units, with parking spaces priced commensurate with the affordability of the dwelling unit. Each unit within the Project shall have the first right of refusal to rent or purchase a parking space until the number of residential parking spaces are no longer available. No conditions may be placed on the purchase or rental of dwelling units, nor may homeowner’s rules be established, which prevent or preclude the separation of parking spaces from dwelling units.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Parking Maximum. Pursuant to Planning Code Section 151.1, the Project shall provide no more than 48 off-street parking spaces for the 69 dwelling units (or .69 off-street parking spaces for each dwelling unit) contained therein.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Car Share. Pursuant to Planning Code Section 166, no fewer than one (1) car share space shall be made available, at no cost, to a certified car share organization for the purposes of providing car share services for its service subscribers.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Bicycle Parking. Pursuant to Planning Code Sections 155.1, 155.4, and 155.5, the Project shall provide no fewer than **70** Class 1 bicycle parking spaces and **5** Class 2 bicycle parking spaces.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Managing Traffic During Construction. The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

PROVISIONS

First Source Hiring. The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415-581-2335, www.onestopSF.org

Eastern Neighborhoods Infrastructure Impact Fee. Pursuant to Planning Code Section 423 (formerly 327), the Project Sponsor shall comply with the Eastern Neighborhoods Public Benefit Fund provisions through payment of an Impact Fee pursuant to Article 4.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

MONITORING

Enforcement. Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Revocation Due to Violation of Conditions. Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

OPERATION

Garbage, Recycling, and Composting Receptacles. Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-.5810, <http://sfdpw.org>

Sidewalk Maintenance. The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org>

Community Liaison. Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Lighting. All Project lighting shall be directed onto the Project site and immediately surrounding sidewalk area only, and designed and managed so as not to be a nuisance to adjacent residents. Nighttime lighting shall be the minimum necessary to ensure safety, but shall in no case be directed so as to constitute a nuisance to any surrounding property.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

INCLUSIONARY HOUSING

Eastern Neighborhoods Affordable Housing Requirements for UMU. Pursuant to Planning Code Section 419.3, Project Sponsor shall meet the requirements set forth in Planning Code Section 419.3 in addition to the requirements set forth in the Affordable Housing Program, per Planning Code Section 415. Prior to issuance of first construction document, the Project Sponsor shall select one of the options described in Section 419.3 or the alternatives described in Planning Code Section 419.5 to fulfill the affordable housing requirements and notify the Department of their choice. Any fee required by Section 419.1 et seq. shall be paid to the Development Fee Collection Unit at DBI prior to issuance of the first construction document an option for the project sponsor to defer payment to prior to issuance of the first certificate of occupancy upon agreeing to pay a deferral surcharge in accordance with Section 107A.13.3 of the San Francisco Building Code.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

Affordable Units

1. **Number of Required Units.** Pursuant to Planning Code Section 415.6 and 419.3, the Project is required to provide 14.4% of the proposed dwelling units as affordable to qualifying households. The Project contains 69 units; therefore, 10 affordable units are required. The Project Sponsor will fulfill this requirement by providing the 10 affordable units on-site. If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.
2. **Unit Mix.** The Project contains 26 one-bedroom, 38 two-bedroom units, and 5 three-bedroom units; therefore, the required affordable unit mix is 4 one-bedroom units, 5 two-bedroom units, and 1 three-bedroom unit. If the market-rate unit mix changes, the affordable unit mix will be modified accordingly with written approval from Planning Department staff in consultation with MOH.
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.
3. **Unit Location.** The affordable units shall be designated on a reduced set of plans recorded as a Notice of Special Restrictions on the property prior to the issuance of the first construction permit.
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.
4. **Phasing.** If any building permit is issued for partial phasing of the Project, the Project Sponsor shall have designated not less than 14.4 percent of the each phase's total number of dwelling units as on-site affordable units.
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.
5. **Duration.** Under Planning Code Section 415.8, all units constructed pursuant to Section 415.6, must remain affordable to qualifying households for the life of the project.
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.
6. **Other Conditions.** The Project is subject to the requirements of the Inclusionary Affordable Housing Program under Section 415 et seq. of the Planning Code and City and County of San

Francisco Inclusionary Affordable Housing Program Monitoring and Procedures Manual ("Procedures Manual"). The Procedures Manual, as amended from time to time, is incorporated herein by reference, as published and adopted by the Planning Commission, and as required by Planning Code Section 415. Terms used in these conditions of approval and not otherwise defined shall have the meanings set forth in the Procedures Manual. A copy of the Procedures Manual can be obtained at the MOHCD at 1 South Van Ness Avenue or on the Planning Department or Mayor's Office of Housing and Community Development's websites, including on the internet at:

<http://sf-planning.org/Modules/ShowDocument.aspx?documentid=4451>.

As provided in the Inclusionary Affordable Housing Program, the applicable Procedures Manual is the manual in effect at the time the subject units are made available for sale.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.

- a. The affordable unit(s) shall be designated on the building plans prior to the issuance of the first construction permit by the Department of Building Inspection ("DBI"). The affordable unit(s) shall (1) reflect the unit size mix in number of bedrooms of the market rate units, (2) be constructed, completed, ready for occupancy and marketed no later than the market rate units, and (3) be evenly distributed throughout the building; and (4) be of comparable overall quality, construction and exterior appearance as the market rate units in the principal project. The interior features in affordable units should be generally the same as those of the market units in the principal project, but need not be the same make, model or type of such item as long they are of good and new quality and are consistent with then-current standards for new housing. Other specific standards for on-site units are outlined in the Procedures Manual.
- b. If the units in the building are offered for sale, the affordable unit(s) shall be sold to first time home buyer households, as defined in the Procedures Manual, whose gross annual income, adjusted for household size, does not exceed an average of ninety (90) percent of Area Median Income under the income table called "Maximum Income by Household Size derived from the Unadjusted Area Median Income for HUD Metro Fair Market Rent Area that contains San Francisco." The initial sales price of such units shall be calculated according to the Procedures Manual. Limitations on (i) reselling; (ii) renting; (iii) recouping capital improvements; (iv) refinancing; and (v) procedures for inheritance apply and are set forth in the Inclusionary Affordable Housing Program and the Procedures Manual.
- c. The Project Sponsor is responsible for following the marketing, reporting, and monitoring requirements and procedures as set forth in the Procedures Manual. MOH shall be responsible for overseeing and monitoring the marketing of affordable units. The Project Sponsor must contact MOHCD at least six months prior to the beginning of marketing for any unit in the building.

- d. Required parking spaces shall be made available to initial buyers or renters of affordable units according to the Procedures Manual.
- e. Prior to the issuance of the first construction permit by DBI for the Project, the Project Sponsor shall record a Notice of Special Restriction on the property that contains these conditions of approval and a reduced set of plans that identify the affordable units satisfying the requirements of this approval. The Project Sponsor shall promptly provide a copy of the recorded Notice of Special Restriction to the Department and to MOHCD or its successor.
- f. The Project Sponsor has demonstrated that it is eligible for the On-Site Affordable Housing Alternative under Planning Code Section 415.6 instead of payment of the Affordable Housing Fee, and has submitted the *Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415* to the Planning Department stating that any affordable units designated as on-site units shall be sold as ownership units and will remain as ownership units for the life of the Project.
- g. If the Project Sponsor fails to comply with the Inclusionary Affordable Housing Program requirement, the Director of DBI shall deny any and all site or building permits or certificates of occupancy for the development project until the Planning Department notifies the Director of compliance. A Project Sponsor's failure to comply with the requirements of Planning Code Section 415 et seq. shall constitute cause for the City to record a lien against the development project and to pursue any and all available remedies at law.
- h. If the Project becomes ineligible at any time for the On-Site Affordable Housing Alternative, the Project Sponsor or its successor shall pay the Affordable Housing Fee prior to issuance of the first construction permit or may seek a fee deferral as permitted under Ordinances 0107-10 and 0108-10. If the Project becomes ineligible after issuance of its first construction permit, the Project Sponsor shall notify the Department and MOHCD and pay interest on the Affordable Housing Fee.

EXHIBIT B

Attachment A: MITIGATION MONITORING AND REPORTING PROGRAM (Includes Text for Adopted Mitigation Measures)				
MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
MITIGATION MEASURES FROM THE EASTERN NEIGHBORHOODS AREA PLAN EIR				
<p>PMM-1 – Construction Noise (Mitigation Measure F-2 of the Eastern Neighborhoods PEIR). Where environmental review of a development project undertaken subsequent to the adoption of the proposed zoning controls determines that construction noise controls are necessary due to the nature of planned construction practices and the sensitivity of proximate uses, the Planning Director shall require that the sponsors of the subsequent development project develop a set of site-specific noise attenuation measures under the supervision of a qualified acoustical consultant. Prior to commencing construction, a plan for such measures shall be submitted to the Department of Building Inspection to ensure that maximum feasible noise attenuation will be achieved. These attenuation measures shall include as many of the following control strategies as feasible:</p> <ul style="list-style-type: none"> • Erect temporary plywood noise barriers around a construction site, particularly where a site adjoins noise-sensitive uses. • Utilize noise control blankets on a building structure as the building is erected to reduce noise emission from the site. • Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings housing sensitive uses. • Monitor the effectiveness of noise attenuation measures by taking noise measurements. • Post signs on-site pertaining to permitted construction days and hours and complaint procedures and who to notify in the event of a problem, with telephone numbers listed. 	Project sponsor	Prior to and during construction.	Project sponsor, contractor(s), shall provide Department of Building Inspection and the Planning Department with monthly reports during construction period.	Considered complete upon receipt of final monitoring report at completion of construction.

**Attachment A:
 MITIGATION MONITORING AND REPORTING PROGRAM
 (Includes Text for Adopted Mitigation Measures)**

MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>PMM-2 – Open Space in Noisy Environments (Mitigation Measure F-6 of the Eastern Neighborhoods PEIR). To minimize effects on development in noisy areas, for new development including noise-sensitive uses, the Planning Department shall, through its building permit review process, in conjunction with noise analysis required pursuant to Eastern Neighborhoods Mitigation Measure F-4, require that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design.</p>	Project sponsor	Prior to construction	Department of Building Inspection and Planning Department.	Considered complete upon approval of final construction drawing set.
<p>PMM-3 – Hazardous Building Materials (Mitigation Measure L-1 of the Eastern Neighborhoods PEIR). The project sponsor shall ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.</p>	Project sponsor/ contractor	Prior to demolition of structures.	Project Sponsor/contractor shall submit a monitoring report to the Department of Public Health and Planning.	Considered complete upon receipt of final monitoring report.

**Attachment A:
 MITIGATION MONITORING AND REPORTING PROGRAM
 (Includes Text for Adopted Mitigation Measures)**

MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
IMPROVEMENT MEASURES FROM COMMUNITY PLAN EXEMPTION FOR 815-825 TENNESSEE STREET				
<p>PIM-1 – Monitoring and Abatement of Queues. As an improvement measure to reduce the potential for queuing of vehicles accessing the project site, it shall be the responsibility of the project sponsor/property owner to ensure that recurring vehicle queues do not occur on 19th or Tennessee Streets adjacent to the site. A vehicle queue is defined as one or more vehicles (destined to the underground parking garage) blocking any portion of the 19th Street sidewalk or travel lane on 19th Street, or along Tennessee Street for a consecutive period of three minutes or longer on a daily and/or weekly basis. Because the proposed project would include a new off-street parking facility with more than 20 parking spaces (excluding loading and car-share spaces), the project is subject to conditions of approval set forth by the San Francisco Planning Department to address the monitoring and abatement of queues.</p>	<p>Project sponsor/property owner</p>	<p>Ongoing during building operations.</p>	<p>Planning Department</p>	<p>Ongoing during building operations.</p>
<p>It shall be the responsibility of the owner/operator of any off-street parking facility with more than 20 parking spaces (excluding loading and car-share spaces) to ensure that recurring vehicle queues do not occur on the public right-of-way. A vehicle queue is defined as one or more vehicles (destined to the parking facility) blocking any portion of any public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis. If a recurring queue occurs, the owner/operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable).</p>				

**Attachment A:
 MITIGATION MONITORING AND REPORTING PROGRAM
 (Includes Text for Adopted Mitigation Measures)**

MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as additional bicycle parking, customer shuttles, delivery services; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.</p> <p>If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Department shall notify the property owner in writing. Upon request, the owner/operator shall hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant shall prepare a monitoring report to be submitted to the Department for review. If the Department determines that a recurring queue does exist, the facility owner/operator shall have 90 days from the date of the written determination to abate the queue.</p>				
<p>PIM-2 – Installation of Visual/Audible Devices at Underground Garage Driveway. As an improvement measure to reduce potential conflicts between vehicles entering and exiting the underground garage and pedestrians traveling along the south side sidewalk of 19th Street, the Project Sponsor shall install visual and/or audible notifications (alarms) to alert pedestrians of vehicles traveling in and out of the underground parking garage.</p>	Project Sponsor	Design measures to be incorporated into project design; prior to issuance of a building permit.	Planning Department	Considered complete upon installation.

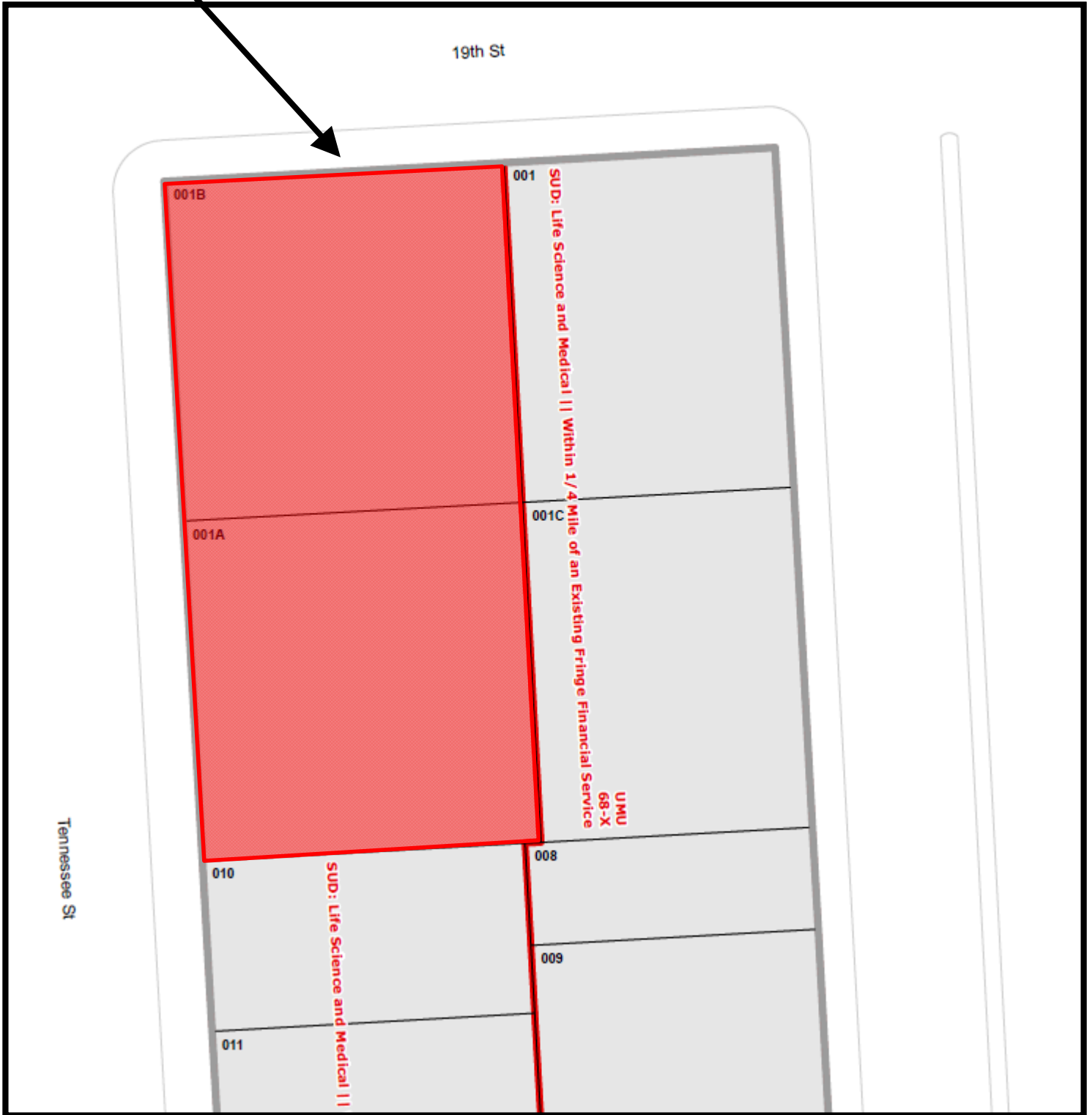
**Attachment A:
MITIGATION MONITORING AND REPORTING PROGRAM
(Includes Text for Adopted Mitigation Measures)**

MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>PIM-3 – Installation of Crosswalks along 19th Street and Tennessee Street. As an improvement measure to reduce potential conflicts between pedestrians and other users of 19th and Tennessee Streets (e.g., autos, bus transit vehicles, freight/delivery vehicles, bicyclists, etc.), the project sponsor shall provide crosswalks to enhance pedestrian circulation and safe passage of pedestrians walking to and from the project site and reduce and/or eliminate any vehicle-pedestrian conflicts. The project sponsor shall provide two high-visibility crosswalks (e.g., continental, transverse, and/or ladder marking pattern) at the intersection of 19th Street and Tennessee Street, specifically along the east leg of the intersection (along 19th Street) and the south leg of the intersection (along Tennessee Street). Installation of crosswalks will provide enhanced pedestrian circulation, connectivity between the project site and adjacent uses, and enhance driver awareness of pedestrian activity at these locations.</p> <p>The crosswalks shall be constructed in accordance with the <i>California Manual on Uniform Traffic Control Devices</i> (MUTCD). These new crosswalks may require approvals by the San Francisco Planning Department, Department of Public Works, and SFMTA’s Livable Streets Subdivision, as appropriate.</p>	Project Sponsor	Design measures to be incorporated into project design; prior to issuance of a building permit.	Planning Department; SFMTA; Public Works	Considered complete upon installation.
<p>PIM-4 – Coordination of Move-in/Move-Out Operations and Large Deliveries. To reduce the potential for parking of delivery vehicles within the travel lane adjacent to the curb lane on 19th Street (in the event that the on-street loading is occupied), residential move-in and move-out activities and larger deliveries shall be scheduled and coordinated through building management. Appropriate move-in/move-out procedures shall be enforced to avoid any blockages of 19th Street over an extended period of time and reduce any potential conflicts between movers and pedestrians walking along 19th Street. Curb parking on 19th Street shall be reserved through SFMTA or by directly contacting the local 311 service.</p>	Project sponsor/ property owner	Ongoing during building operations.	Planning Department	Ongoing during building operations.

Attachment A: MITIGATION MONITORING AND REPORTING PROGRAM (Includes Text for Adopted Mitigation Measures)				
MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>PIM-5 – Construction Management Plan. In addition to items required in the Construction Management Plan, the project sponsor shall include the following:</p> <ul style="list-style-type: none"> • <i>Carpool and Transit Access for Construction Workers</i> – As an improvement measure to minimize parking demand and vehicle trips associated with construction workers, the construction contractor shall include methods to encourage carpooling and transit use to the project site by construction workers in the Construction Management Plan contracts. • <i>Project Construction Updates</i> – As an improvement measure to minimize construction impacts on nearby businesses, the project sponsor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction inquiries or concerns. 	Project sponsor/ project contractor(s)	Prior to and during construction.	Project sponsor, contractor(s)	Considered complete upon completion of construction.

Parcel Map

SUBJECT PROPERTY



Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

Sanborn Map*

SUBJECT PROPERTY

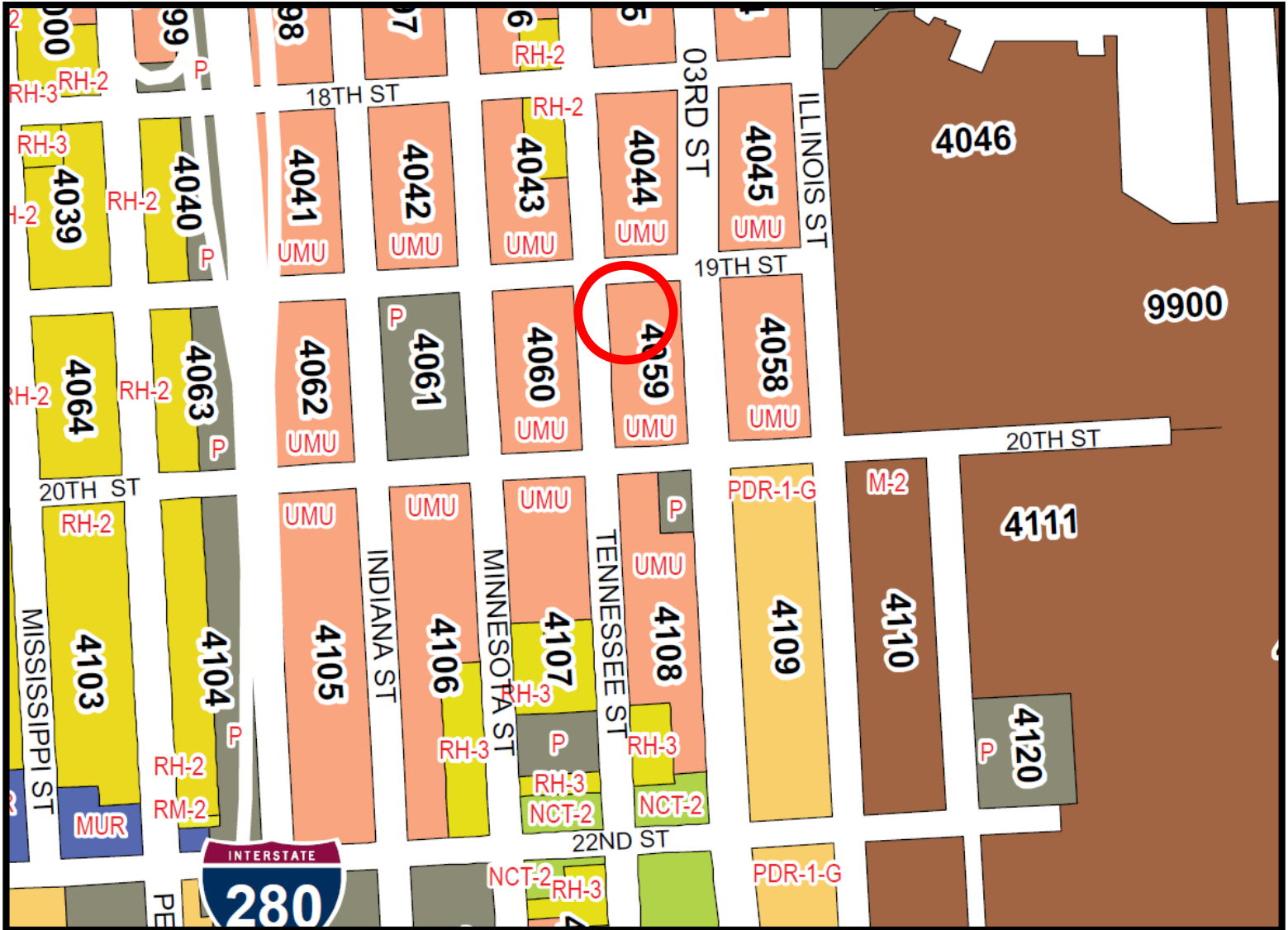


*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



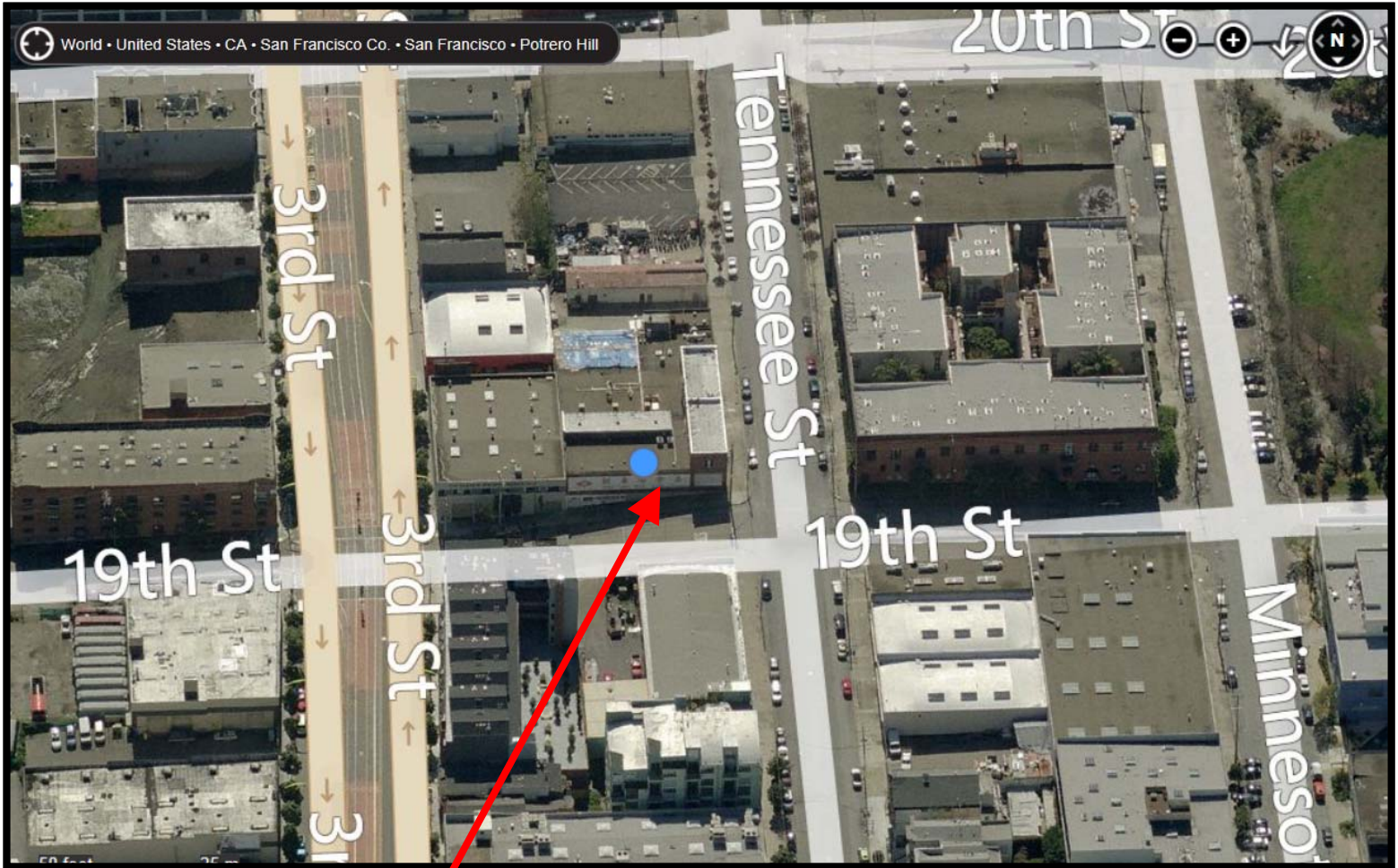
Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

Zoning Map



Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

Aerial Photo

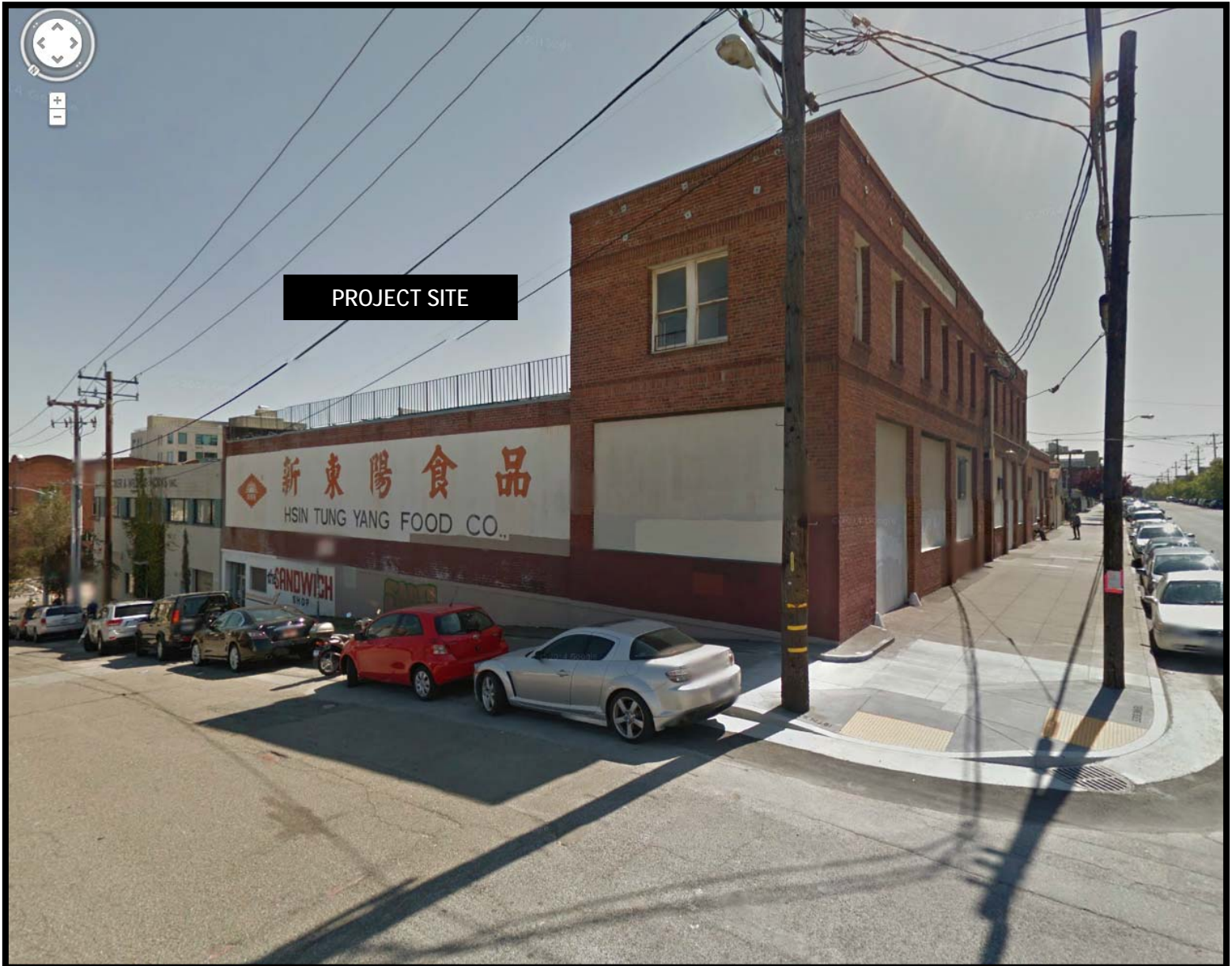


PROJECT SITE



Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

Site Photo



815-825 Tennessee Street, View along 19th Street

Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

Site Photo



815-825 Tennessee Street, View along Tennessee Street

Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

Site Photo



815-825 Tennessee Street, View along Tennessee Street

Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

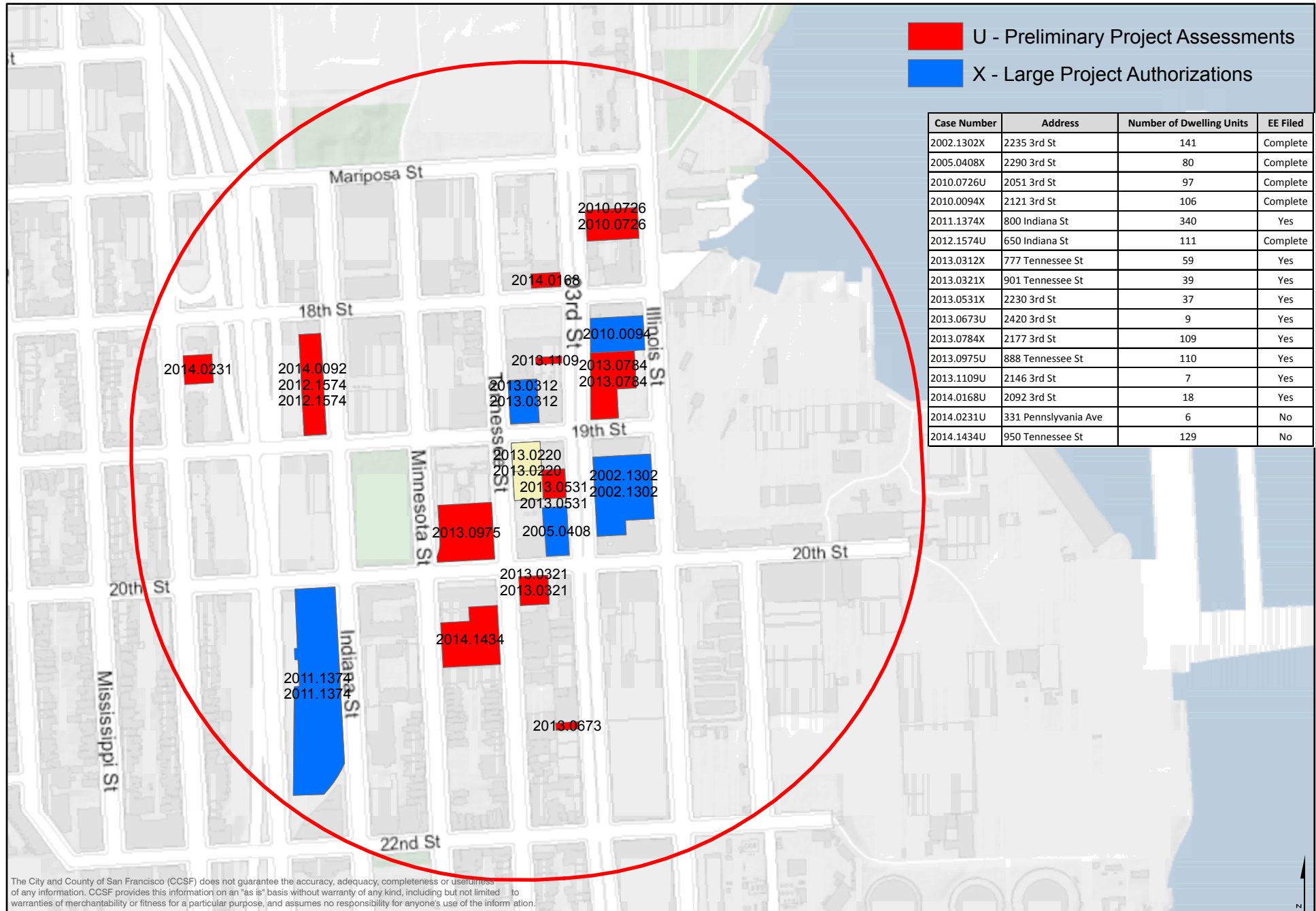
Site Photo



815-825 Tennessee Street, View along Tennessee Street

Large Project Authorization Hearing
Case Number 2013.0220X
815-825 Tennessee Street

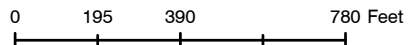
Major Projects Within .25 Mile Radius of 815-825 Tennessee St



- U - Preliminary Project Assessments
- X - Large Project Authorizations

Case Number	Address	Number of Dwelling Units	EE Filed
2002.1302X	2235 3rd St	141	Complete
2005.0408X	2290 3rd St	80	Complete
2010.0726U	2051 3rd St	97	Complete
2010.0094X	2121 3rd St	106	Complete
2011.1374X	800 Indiana St	340	Yes
2012.1574U	650 Indiana St	111	Complete
2013.0312X	777 Tennessee St	59	Yes
2013.0321X	901 Tennessee St	39	Yes
2013.0531X	2230 3rd St	37	Yes
2013.0673U	2420 3rd St	9	Yes
2013.0784X	2177 3rd St	109	Yes
2013.0975U	888 Tennessee St	110	Yes
2013.1109U	2146 3rd St	7	Yes
2014.0168U	2092 3rd St	18	Yes
2014.0231U	331 Pennsylvania Ave	6	No
2014.1434U	950 Tennessee St	129	No

The City and County of San Francisco (CCSF) does not guarantee the accuracy, adequacy, completeness or usefulness of any information. CCSF provides this information on an "as is" basis without warranty of any kind, including but not limited to warranties of merchantability or fitness for a particular purpose, and assumes no responsibility for anyone's use of the information.



MULTI-FAMILY DEVELOPMENT

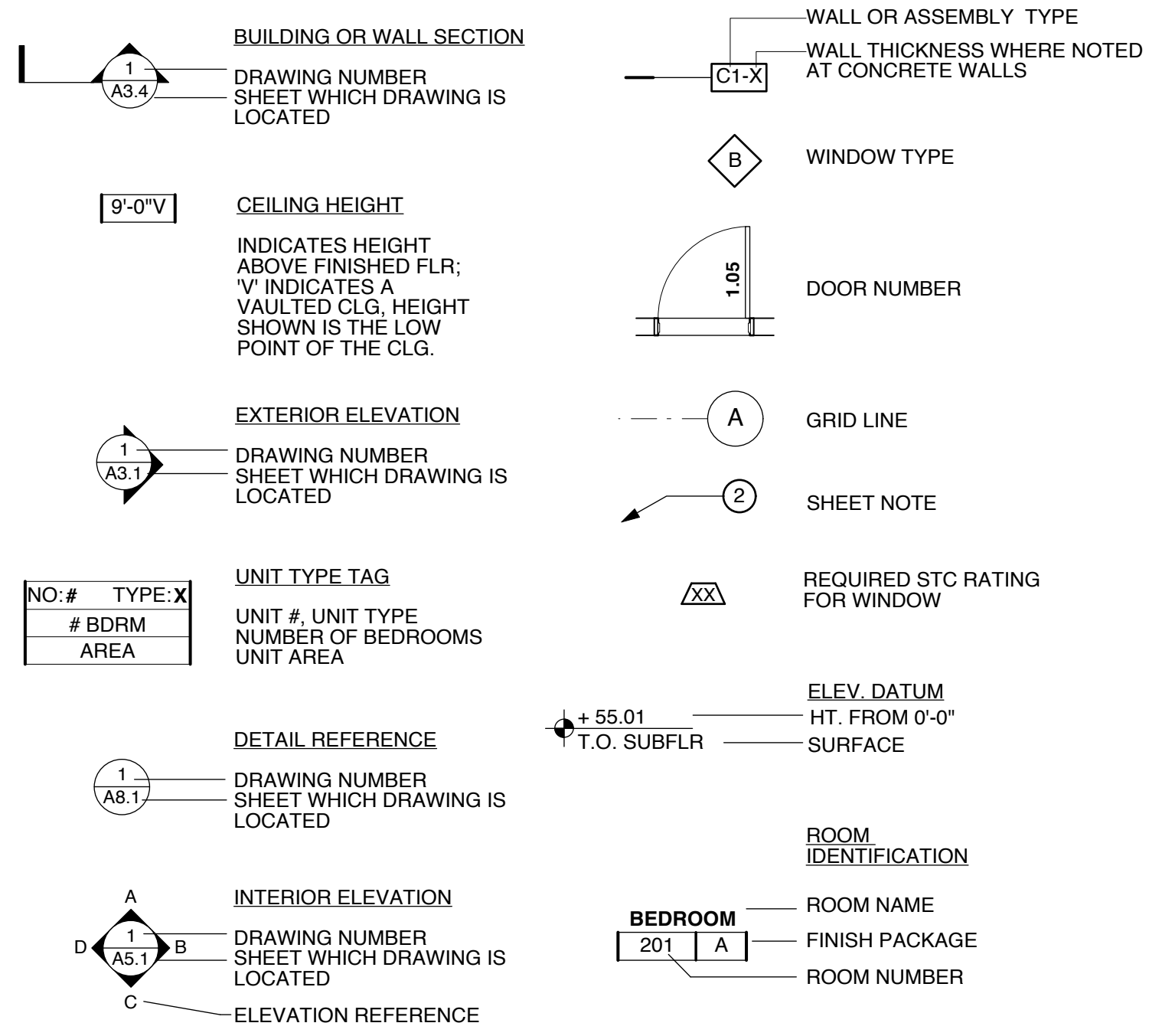
815 TENNESSEE STREET
SAN FRANCISCO, CA

LPA RESUBMIT 3 SEPTEMBER 25, 2014



TENNESSEE STREET
815-825 TENNESSEE ST.
SAN FRANCISCO, CA

ISSUE	DATE
PP APPLICATION	09.30.2013
REVIEW	11.06.2013
HRE SUBMITTAL	11.27.2013
LPA SUBMITTAL	12.05.2013
LPA RESUBMITTAL	03.28.2014
PRE APP 2	04.25.2014
SITE PERMIT	05.02.2014
LPA RESUBMIT 2	07.15.2014
LPA RESUBMIT 3	09.25.2014



LEGEND 7
 1/4"=1'-0"

ABV. Above	GCMU Glazed Conc Masonry Units	RFL Reflected
AFF Above Finished Floor	GD Grading	REFR Refrigerator
AP Access Panel	GRN Granite	REG Register
ACC Acoustical	GVL Gravel	RE Reinforce (d), (ing)
ADD Addendum	GT Grout	REM Remove
ADH Adhesive	GWB Gypsum Wall Board	RES Resilient
ADJ Adjacent	GYP Gypsum	RET Return
ADJT Adjustable		RA Return Air
A/C Air Conditioning	HDW Hardware	REV Revision (s), Revised
ALT Alternate	HMTL Hollow Metal	R Riser
AL Aluminum	HWD Hardwood	RD Roof Drain
AB Anchor Bolt	HDR Header	RFH Roof Hatch
ANOD Anodized	HTG Heating	RFG Roofing
APX Approximate	HVAC Heating/Ventilation/	RM Room
ARCH Architect (ural)	Air Conditioning	RO Rough Opening
AD Area Drain	HT Height	RB Rubber Base
	HC Hollow Core	RBT Rubber Tile
BSMT Basement	HOR Horizontal	RWL Rain Water Leader
BVL Beveled	HB Hose Bib	
BIT Bituminous	HWH Hot Water Heater	
BLK Block		SAC Suspended Acoustical Clg.
BLKG Blocking	INCL Include (d), (ing)	SAM Self Adhesive Sht Membrane
BD Board	ID Inside Diameter	SFGL Safety Glass
B.O. Bottom Of	INS Insulate (d), (ing)	SCD See Civil (Eng) Drawings
BRK Brick	INT Interior	SCHD Schedule
BLDG Building	INV Invert	SCN Screen
BUR Built Up Roofing		SLNT Seatant
	JT Joint	SEC Section
CAB Cabinet	JF Joint Filler	SSK Service Sink
CK Calk (ing) Caulk (ing)	KPL Kickplate	SHTH Sheathing
OPT Carpet (ed)	KIT Kitchen	SHT Sheet
CSMT Casement	KOC Knockout	SF Square Feet
CST Cast Stone	HOR Horizontal	SG Sheet Glass
CLG Ceiling	LBL Label	SIM Similar
CT Ceiling Height	LB Lag Bolt	SD See Landscape Drawings
ER Ceramic	LAM Laminate (d)	SC Solid Core
CT Ceramic Tile	LAV Lavatory	SMD See Mechanical Drawings
CLR Clear (ance)	LW Lightweight	SP Soundproof
OCL Column	LWC Lightweight Concrete	SPC Spacer
CCNC Concrete	LMS Limestone	SPD See Plumbing Drawings
CMU Concrete Masonry Unit	LVR Louver	SPK Speaker
CONST Construction	LPT Low Point	SPEC Specification (s)
CONT Continuous or Continue		SQ Square
CJ Control Joint	M Mirrored	SS Stainless Steel
CG Corner Guard	MB Machine Bolt	STD See Structural Drawings
	MDF Medium Density Fiberboard	ST Standard
DP Dampproofing	MH Manhole	STA Station
DEM Demolish, Demolition	MFR Manufacture (r)	ST Steel
DTL Detail	MAS Masonry	STOR Storage
DIAG Diagonal	MO Masonry Opening	SD Storm Drain
DIA Diameter	MAX Maximum	STR Structural
D Drain	MECH Mechanic (al)	SUS Suspended
DM Dimension	MED Medicine Cabinet	SYM Symmetry (ical)
DNV Division	MET Metal	SV Sheet Vinyl
DH Double Hung	MTRF Metal Furring	STC Sound Transmission
DN Down	MWK Milkwork	COEFFICIENT
DS Downspout	MIN Minimum	
	MIR Mirror	TEL Telephone
DWG Drawing	MISC Miscellaneous	TMP Tempered
	MLD Molding, Moulding	TV Television
EF Each Face		THR Threshold
ELEC Electric (al)	NR Noise Reduction	TPD Toilet Paper Dispenser
EP Electrical Panelboard	NOM Nominal	TR Trash
EL Elevation	NIC Not In Contact	TOP Top of Plate
ELEV Elevator	NTS Not to Scale	TOS Top Of Slab
EMER Emergency	NO (#) Number	TST Top Of Steel
EPDM Ethylene Propylene Diene Monomer		TW Top Of Wall
EQ Equal		TB Towel Bar
EOP Equipment	O Over	T Tread
EST Estimate	OBS Obscure	TYP Typical
EXH Exhaust	OC On Center (s)	
EB Expansion Bolt	OCF Over Flow Drain	
EJ Expansion Joint	OFS Overflow Scupper	UNF Unfinished
EXP Exposed	OP Opaque	UON Unless Otherwise Noted
(E) Existing	OPG Opening	UR Urinal
EXT Exterior	OPP Opposite	
	OD Outside Diameter	V Vaulted
FC Fiberglass Clad	OA Overall	VAR Varies
FOC Face Of Concrete	OH Overhead	VB Vapor Barrier
FOF Face Of Finish		VNR Veneer
FOM Face Of Masonry	PTR Paper Towel Receptor	VERT Vertical
FOS Face Of Studs	PB Particle Board	VG Vertical Grain
FF Finish Floor	PTD Painted	VIN Vinyl
FIN Finish (ed)	PTN Partition	VAT Vinyl Acoustical Tile
FFE Finished Floor Elevation	PBD Particle Board	VB Vinyl Base
FA Fire Alarm	PERF Perforate (d)	VCT Vinyl Composite Tile
FP Fireproof	PERI Perimeter	
FR Fire-resistant	PLAM Plastic Laminate	W/ With
FRT Fire-retardant	PG Plate Glass	WCST Wainscot
FL Finish Floor (ing)	PWY Plywood	WTW Wall To Wall
FLS Flashing	PT Part	WT Wall Hung
FD Floor Drain	PVC Polyvinyl Chloride	WC Water Closet
FLUR Fluorescent	PSF Pounds Per Square Foot	WP Waterproofing, Waterproof
FJT Flush Joint	PSI Pounds Per Square Inch	WPM Water Proof Membrane
FTG Footing	PCC Precast Concrete	WR Water Repellant
FND Foundation	PFB Prefabricate (d)	WRB Weather Resistant Barrier
FUR Furred (ing)	PFN Prefinished	WS Waterstop
	PIF Performed	WWF Welded Wire Fabric
	PL Property Line	WDW Window
	PT Pressure Treated	WG Wired Glass
GA Gage, Gauge	RAD Radius	WM Wire Mesh
GV Galvanized	RWL Rainwater Leader	WO Without
GC General Contractor	REC Recessed	WD Wood
GL Glass, Glazing	REF Reference	WB Wood Base
GLB Glass Block		WI Wrought Iron
GSM Galvanized Sheet Metal		

ABBREVIATIONS 6
 NONE

PLANNING INFORMATION

PROJECT LOCATION: 815 TENNESSEE STREET
 SAN FRANCISCO, CA 94147

ASSESSOR'S PARCEL NUMBER: LOTS 001A AND 001B, BLOCK 4059

PROJECT DESCRIPTION: 100% PRIVATELY FUNDED
 CONDOMINIUM DEVELOPMENT
 69 RESIDENTIAL UNITS (14% OF UNITS AFFORDABLE)

MAXIMUM BUILDING HEIGHT: ±58'-0"

PROPOSED SETBACKS: REAR: ±XX'
 RIGHT SIDE: ±XX'
 LEFT SIDE: ±XX'
 FRONT - CURB TO WALK: XX'
 FRONT - WALK TO PL: XX'
 FRONT - PL TO BUILDING: ±XX'

DENSITY: 31.05 DU/AC

BUILDING CODE INFORMATION

OCCUPANCY GROUP: R2

CONSTRUCTION TYPE: TYPE VA OVER TYPE IA, SPRINKLERED PER NFPA 13

NUMBER OF STORIES: 4 OVER 1

ACCESSIBILITY: 100% OF UNITS SHALL BE ADAPTABLE,
 PER CBC 2013 CHAPTER 11A
 ALL COMMON USE AREAS SHALL BE ACCESSIBLE
 PER CBC CHAPTER 11A
 ALL PUBLIC AREAS SHALL BE ACCESSIBLE
 PER CBC 2013 CHAPTER 11B

PROJECT INFORMATION 5
 NONE

Date: 03/18/2014

CONSTRUCTION TYPE:		TYPE VA OVER TYPE I	
FLOORS:		4 WOOD OVER 2 CONCRETE	
UNIT TYPE	Unit	Remainable	Floor Levels
	SF		1ST 2ND 3RD 4TH 5TH
1 BEDROOM	A1.1 1BED/1BATH	708	0 3 0 0 0 0
	A1.2 1BED/1BATH	531	0 2 0 0 0 0
	A1.3 1BED/1BATH	584	0 0 2 2 2 0
	A1.4 1BED/1BATH	561	0 0 0 0 0 2
	A1.5 1BED/1BATH	708	0 0 3 3 0 0
	A1.6 1BED/1BATH	673	0 0 0 0 3 0
	A1.7 1BED/1BATH	562	0 0 0 0 0 3
	A2.1 1BED/1BATH	573	0 1 0 0 0 0
			26 38%
2 BEDROOM	B1.1 2BED/2BATH	842	0 1 0 0 0 0
	B1.2 2BED/2BATH	805	0 1 0 0 0 0
	B1.3 2BED/2BATH	859	0 0 2 2 2 0
	B1.4 2BED/2BATH	859	0 1 1 1 0 0
	B1.5 2BED/2BATH	837	0 0 0 0 0 2
	B1.6 2BED/2BATH	837	0 0 0 0 0 1
	B2.1 2BED/2BATH	910	0 0 1 1 1 1
	B2.2 2BED/2BATH	912	0 0 1 1 1 1
	B3.1 2BED/2BATH	988	0 1 0 0 0 0
	B3.2 2BED/2BATH	988	0 0 1 1 1 1
	B4.1 2BED/2BATH	1,047	0 1 0 0 0 0
	B4.2 2BED/2BATH	1,047	0 0 1 1 1 1
	B5.1 2BED/2BATH	1,002	0 0 0 0 0 1
	B5.2 2BED/2BATH	1,024	0 0 0 1 1 0
	B5.3 2BED/2BATH	1,002	0 0 0 0 0 1
	B6.1 2BED/2BATH	836	0 2 0 0 0 0
			38 55%
3 BEDROOM	C1.1 3BED/3BATH	1,183	0 1 0 0 0 0
	C1.2 3BED/3BATH	1,218	0 0 1 1 1 0
	C1.3 3BED/3BATH	1,172	0 0 0 0 0 1
			5 7%
TOTAL UNITS		832	0 13 14 14 14 14
			69 100%
			57,423

Remainable Residential by floor (not incl. decks)

	0	10,296	11,946	11,968	11,863	11,350
Total						57,423

Gross Residential (lobby, corridors, stairs, etc.) (not incl. decks)

	0	4,364	3,143	3,143	3,143	3,143
Total Gross		19,579	-	-	-	-
		19,579	14,660	15,089	15,111	15,006
						14,493

PARKING

GARAGE	45 *
CAR SHARE	1
ADA	2
Total	48
ratio	0.70
ALLOWED	62.5
* Parking is regular parking stalls (NO Stackers proposed)	

BIKE PARKING

CLASS 1 (RESIDENTIAL) BIKE PARKING PROVIDED	
1 : 1 STALLS FOR 1ST 100 UNITS	70
1 : 4 STALLS FOR ADD. UNITS	0
Total	70
CLASS 2 (SIDEWALK) BIKE PARKING PROVIDED	
Total	5

OPEN SPACE*

For each rentable unit 80 sq ft of private open space is required.
 When private open space is not provided, the 80 sq ft may be common open space.
 Example: 1 Rental Unit/Private OS x 80 sq ft/Private OS = 80 sq ft Common OS
 When private open space is provided for a unit but the required 80 sq ft is not met, the remaining open space must be provided as common open space.
 Example: 80 sq ft (1 Rental Unit/Private OS x 60 sq ft/Private OS) = 20 sq ft Common OS
 When a rental unit's private open space exceeds the required 80 sq ft, the excess area can not be counted towards units that do not meet the required 80 sq ft

REQUIRED OPEN SPACE

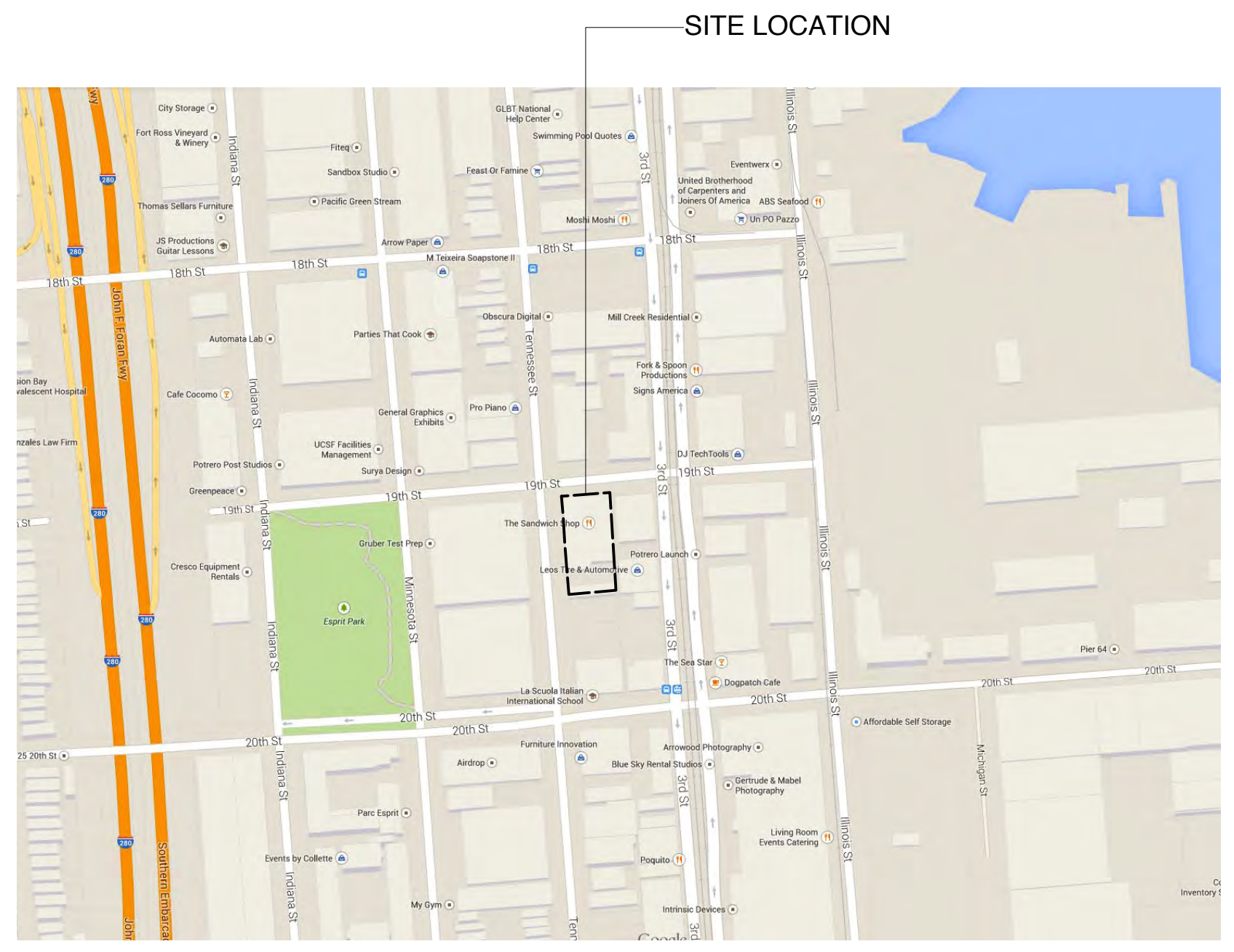
69 DU	80 SF/DU	=	69 X 80	5,520
ALL OPEN SPACE				
STREET LEVEL	(DU x 106 sq ft/DU =			0
PODIUM PATIOS	(DU x 80 sq ft/DU =			0
REAR COURTYARD				3,808
PUBLIC COURTYARD				1,144
ROOF TERRACE**				5,600
Total				10,552

DU = Deck Units
 OS = Open space
 * In compliance with San Francisco Required Open Space: Section 843.11

UNIT AND AREA SUMMARY 4



RENDERED ELEVATION - TENNESSEE STREET



SITE LOCATION

VICINITY MAP 3
 NONE

PROJECT TEAM

CLIENT:
 ROEM DEVELOPMENT CORPORATION
 1650 LAFAYETTE STREET
 SANTA CLARA, CA 95050
 P: 408.984.5600X22
 CONTACT: JONATHAN EMANI

ARCHITECT:
 BDE ARCHITECTURE
 465 CALIFORNIA STREET, SUITE 1200
 SAN FRANCISCO, CA 94104
 P: 415.677.0966
 F: 415.677.0964
 CONTACT: JON ENNIS

CIVIL:
 HOHBACH-LEWIN, INC.
 260 SHERIDAN AVENUE
 PALO ALTO, CA 94306
 P: 650.617.5930
 CONTACT: VERONICA ADAMSON

LANDSCAPE:
 FLETCHER STUDIO
 2339 3RD STREET, SUITE 48R
 SAN FRANCISCO, CA 94107
 P: 415.431.7878
 CONTACT: DAVID FLETCHER

PROJECT TEAM 2

PROJECT INFORMATION

- A0.0 PROJECT INFORMATION
- A0.1 PHOTOGRAPHS
- A0.2 RADIUS MAP
- CIVIL
- C6.0 TOPOGRAPHIC SURVEY
- C7.0 PRELIMINARY DRAINAGE STUDY

LANDSCAPE

- L1.0 LANDSCAPE STREETScape
- L1.1 LANDSCAPE COURTYARD
- L1.2 LANDSCAPE ROOFDECK
- L1.3 LANDSCAPE DIMENSION PLAN

ARCHITECTURAL

- A1.0 PLOT PLAN
- A2.0 BUILDING BASEMENT PLAN
- A2.1 BUILDING PLAN FLOOR 1
- A2.2 BUILDING PLAN FLOOR 2
- A2.3 BUILDING PLAN FLOOR 3
- A2.4 BUILDING PLAN FLOOR 4
- A2.5 BUILDING PLAN FLOOR 5
- A2.6 BUILDING PLAN ROOF
- A3.0 ELEVATIONS
- A3.1 ELEVATIONS
- A3.2 ELEVATIONS
- A3.3 SECTIONS



950 HOWARD STREET
 SAN FRANCISCO
 CA 94103
 P: (415) 677-0966
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CONTRACT NO.

TENNESSEE STREET
815-825 TENNESSEE ST.
SAN FRANCISCO, CA

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PROJECT INFORMATION

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 Project No. 1330
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 Sheet Size: 30 x 42

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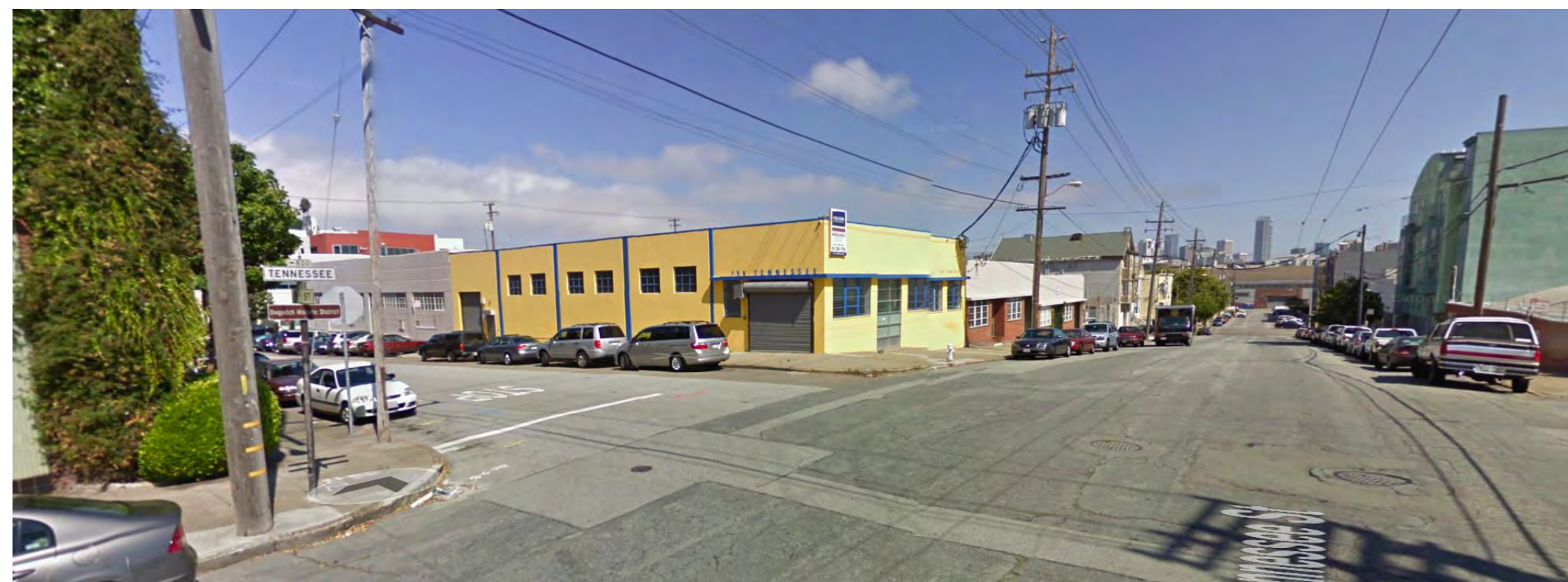
SHEET INDEX 1



ADJACENT PROPERTY 12



ADJACENT PROPERTY 11



ADJACENT PROPERTY 10



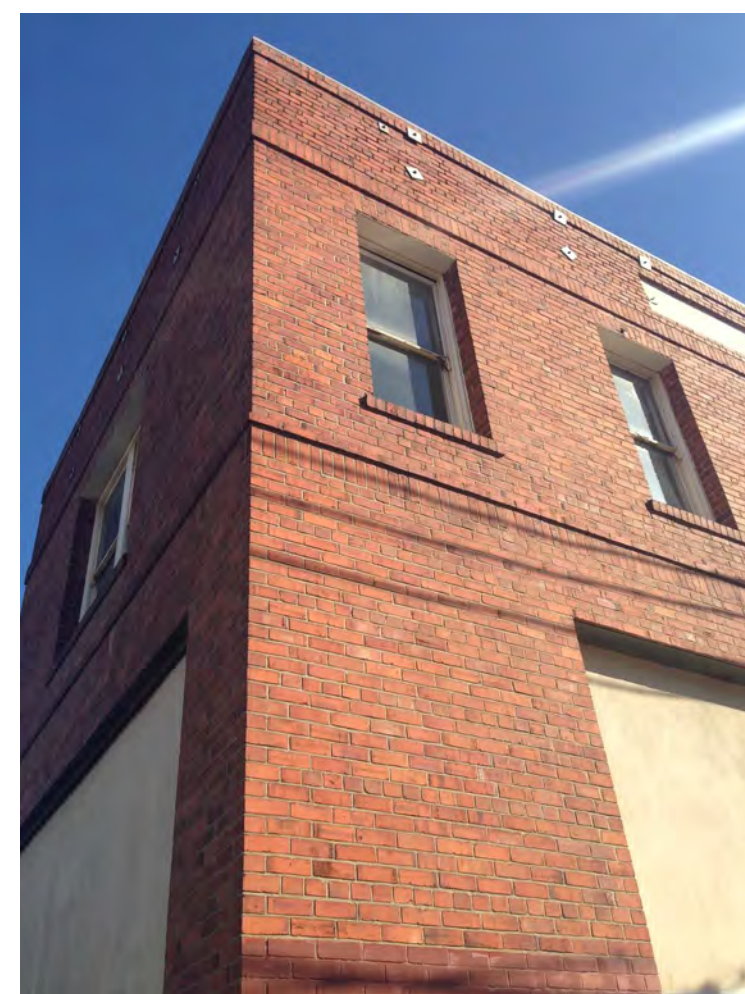
ADJACENT PROPERTY 9



ADJACENT PROPERTY 8



SITE DETAIL 7



SITE DETAIL 6



SITE DETAIL 5



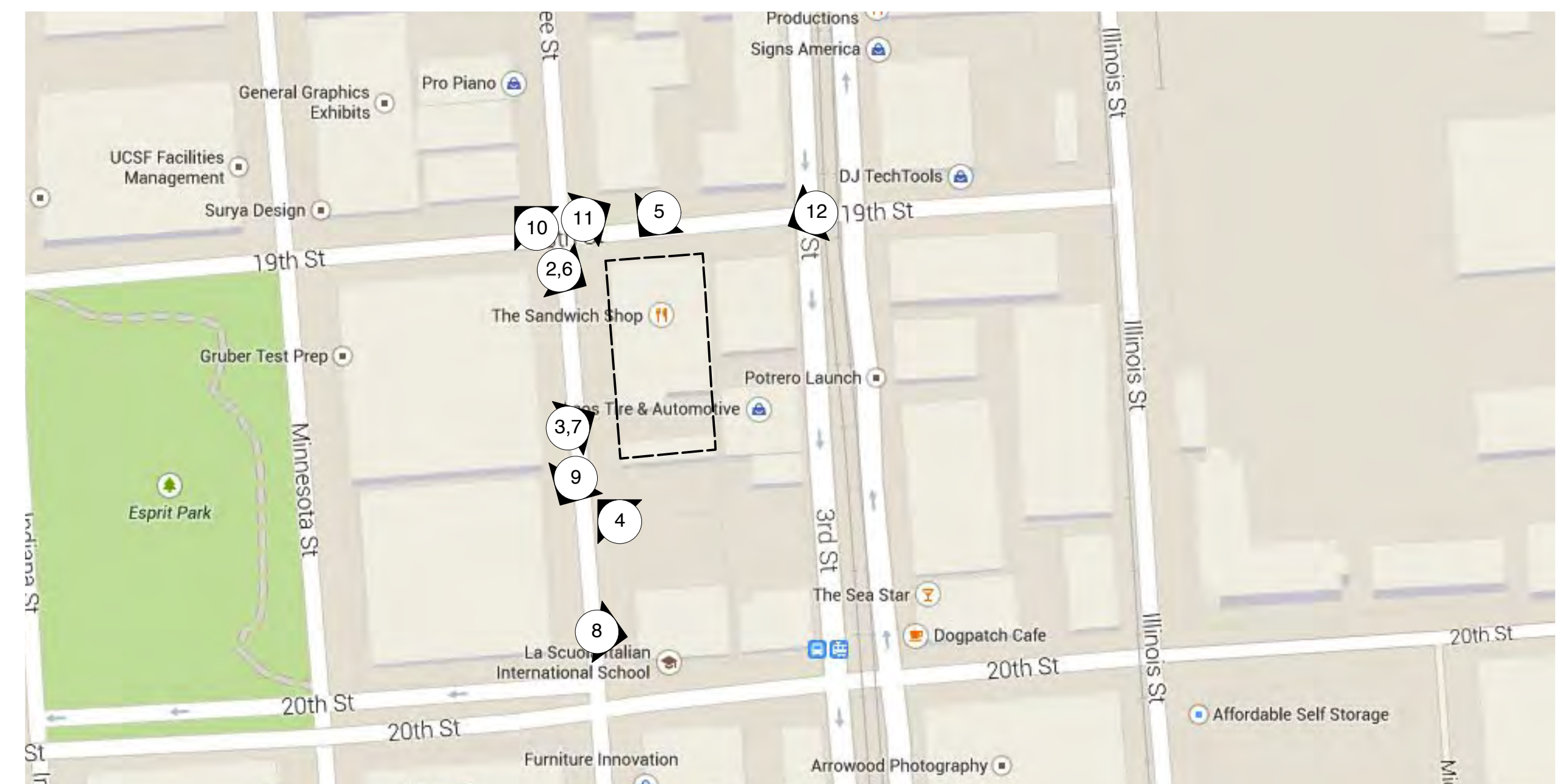
RESIDENTIAL PROPERTY ACROSS TENNESSEE 4



SITE 3

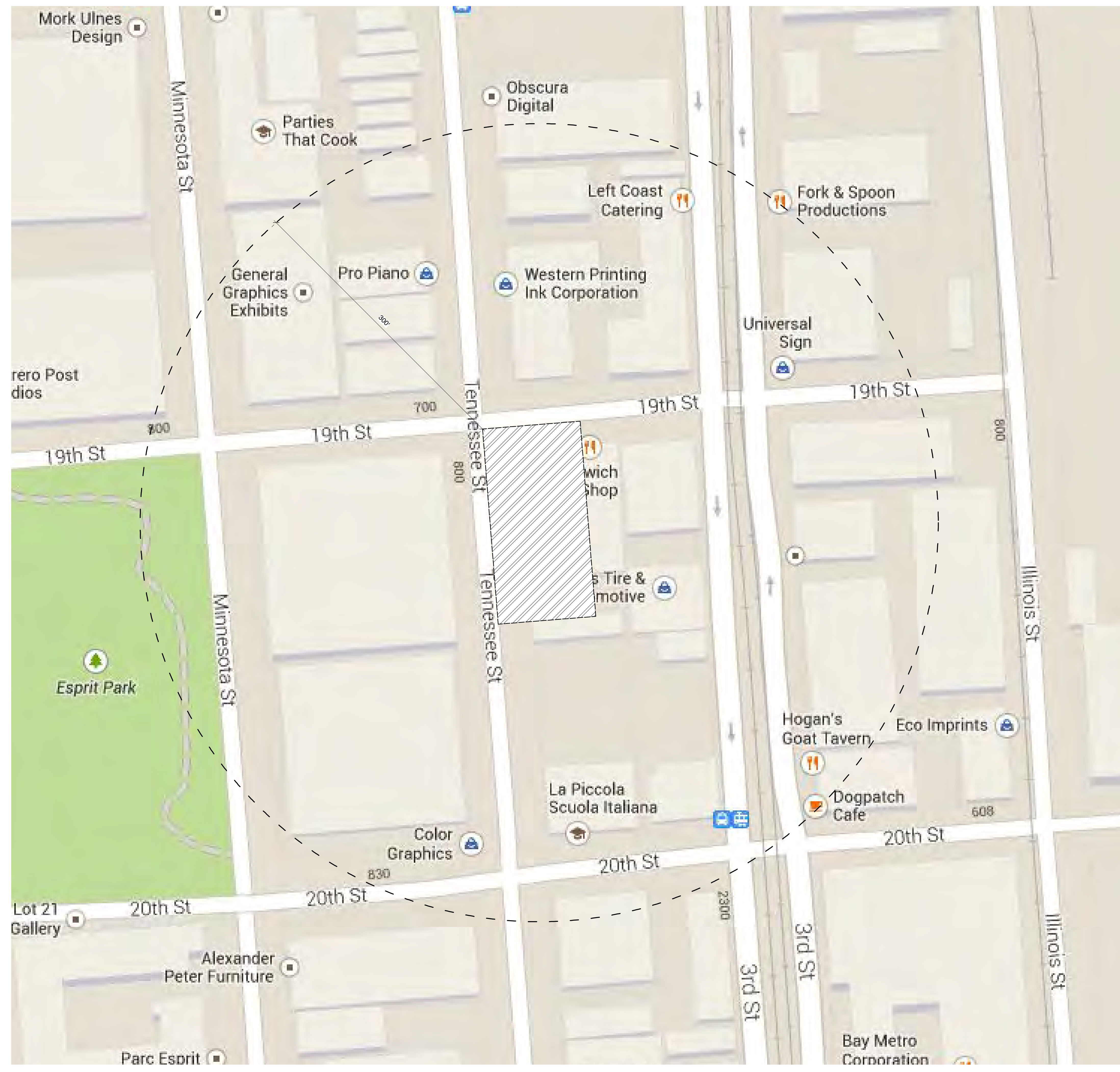


SITE 2



SITE REFERENCE 1
NTS

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RADIUS MAP 1
1" = 50'

TENNESSEE STREET
815-825 TENNESSEE ST.
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AND CONTROL SHALL BE THE RESPONSIBILITY...
OF THE ARCHITECT.

**300' RADIUS
MAP**

Sheet Title: _____
Scale: 1" = 50'
Project No: 1330 Drawn by: NS
Sheet Size: 30" x 42"

GENERAL NOTES

A. REFER TO SITE SURVEY FOR EXISTING:
 -CURB CUTS
 -CURB LINES
 -SITE AND VICINITY GRADING
 -ADJACENT PROPERTY LINES
 -EXISTING UTILITIES

B. REFER TO LANDSCAPE ARCHITECTURE PLANS FOR PROPOSED:
 -CURB CUTS
 -CURB LINES
 -DIMENSIONS

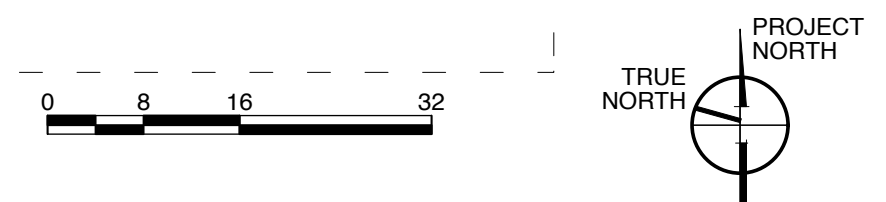
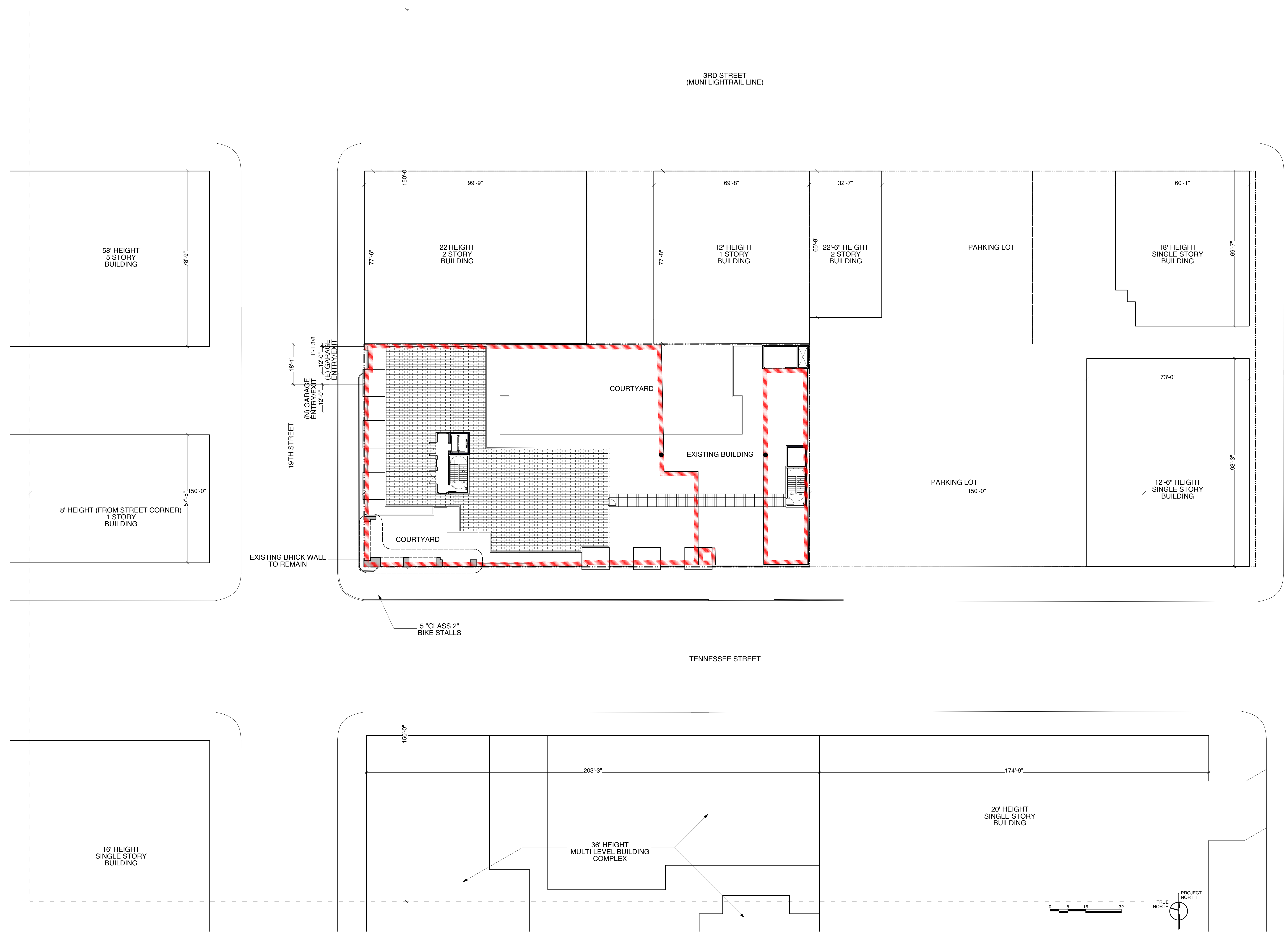
C. REFER TO A3.3 FOR COMPLIANCE OF SAN FRANCISCO PLANNING:
 -REAR YARD SET BACK
 -WINDOW EXPOSURE

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ALL DIMENSIONS AND MATERIAL SPECIFICATIONS ARE TO BE VERIFIED BY THE ARCHITECT AND SHALL BE SUBJECT TO THE WRITTEN CONSENT OF THE ARCHITECT.

PLOT PLAN

Scale: 1/16"=1'-0"
 Project No. 1330
 Drawing by: NS
 Sheet Size: 30 x 42

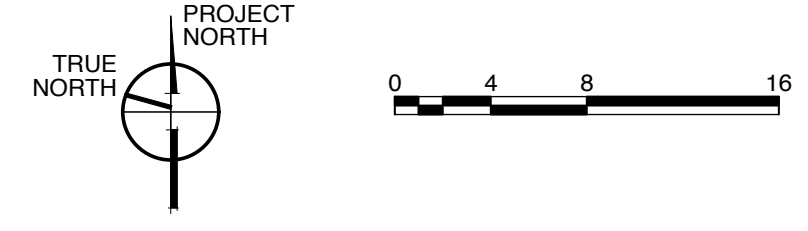
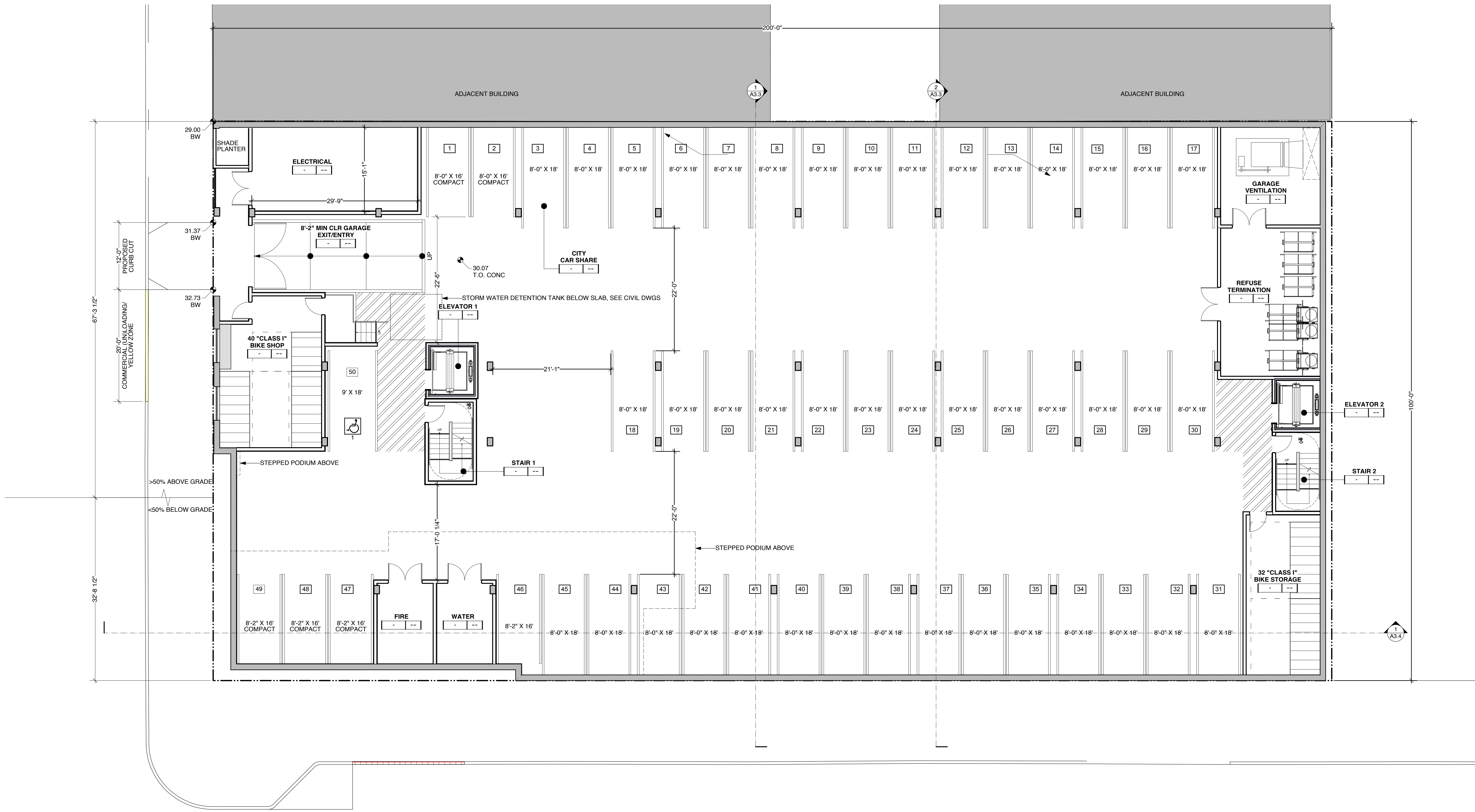


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**BUILDING
PLAN
BASEMENT**

Sheet Title
Scale: 1/8"=1'
Project No. 1330
Drawn By: NS
Sheet Size: 30" x 42"

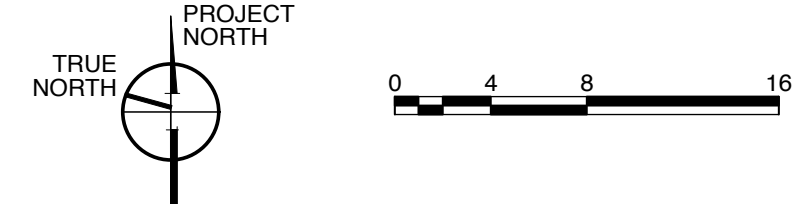
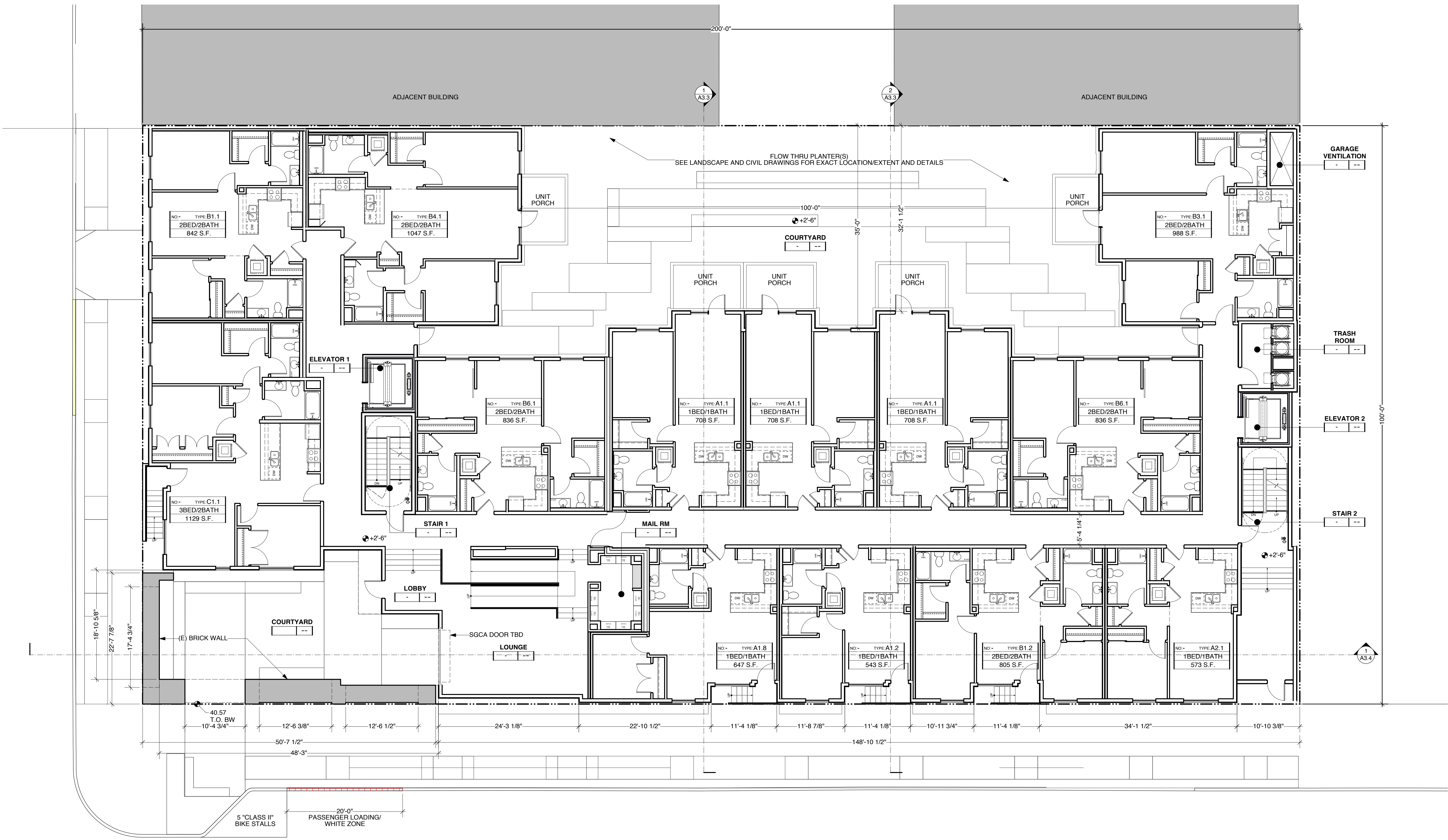


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AND CONTRACTS MADE BY THE ARCHITECT
AND NOT TO BE CONSIDERED, UNLESS THE
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**BUILDING
PLAN
FLOOR 1**

Sheet Title
Scale: 1/8"=1'
Project No. 1330 Drawn By: NS
Sheet Size: 30" x 42"

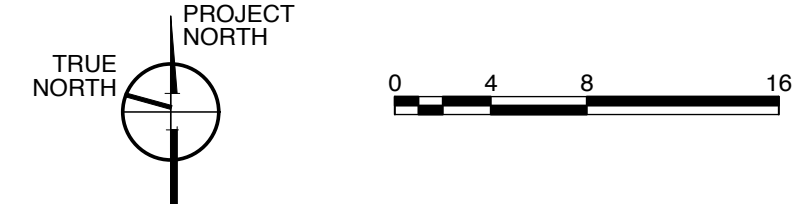
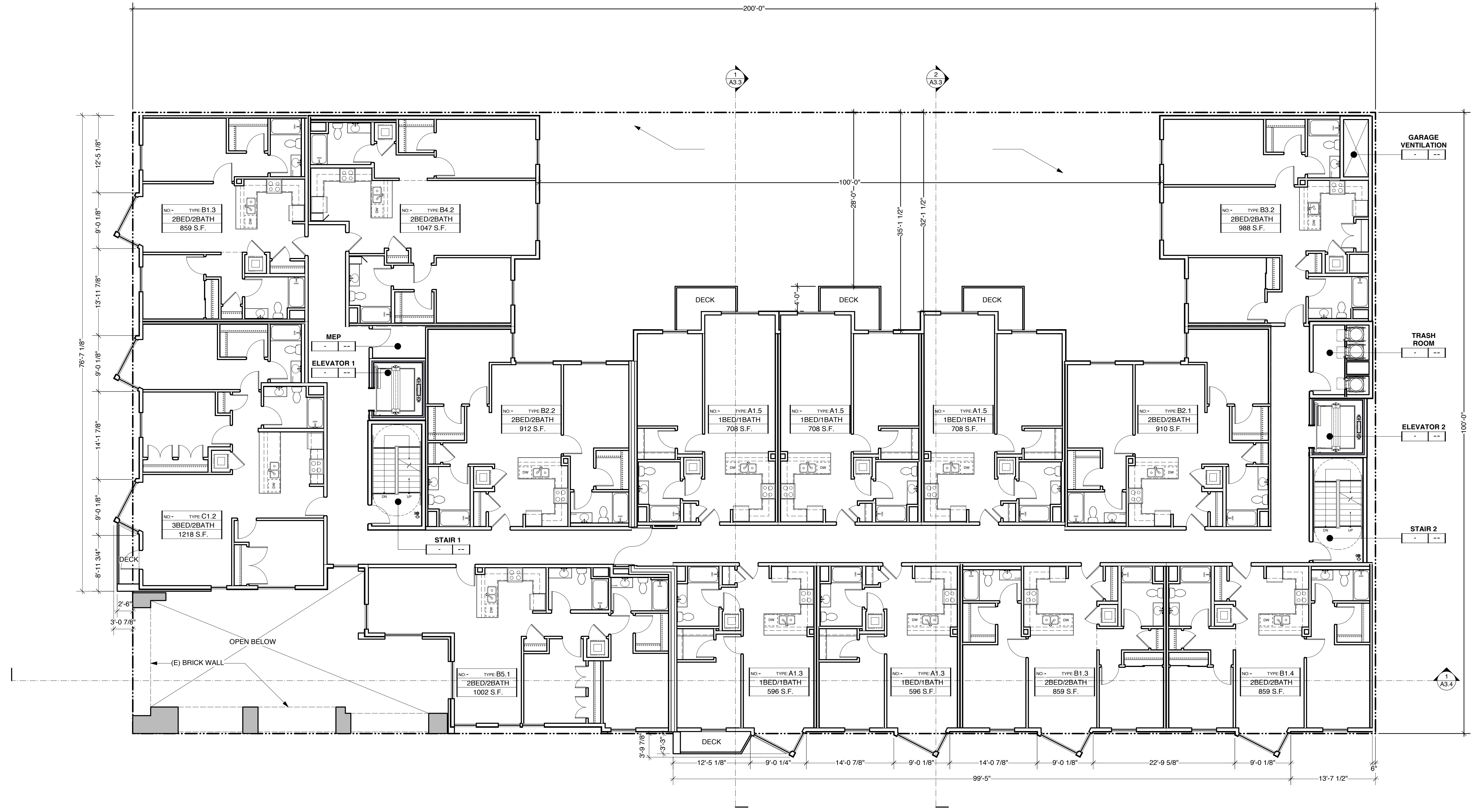


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**BUILDING
PLAN
FLOOR 2**

Sheet Title
Scale: 1/8"=1'
Project No. 1330 Drawn by: NS
Sheet Size: 30" x 42"

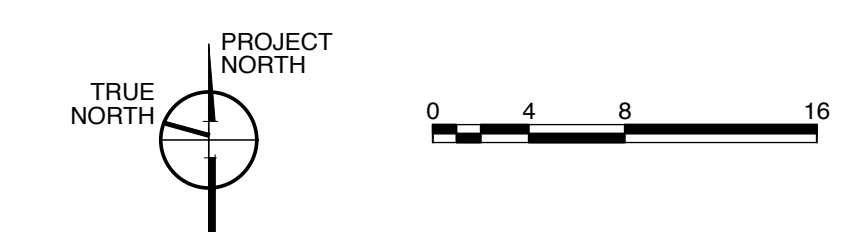
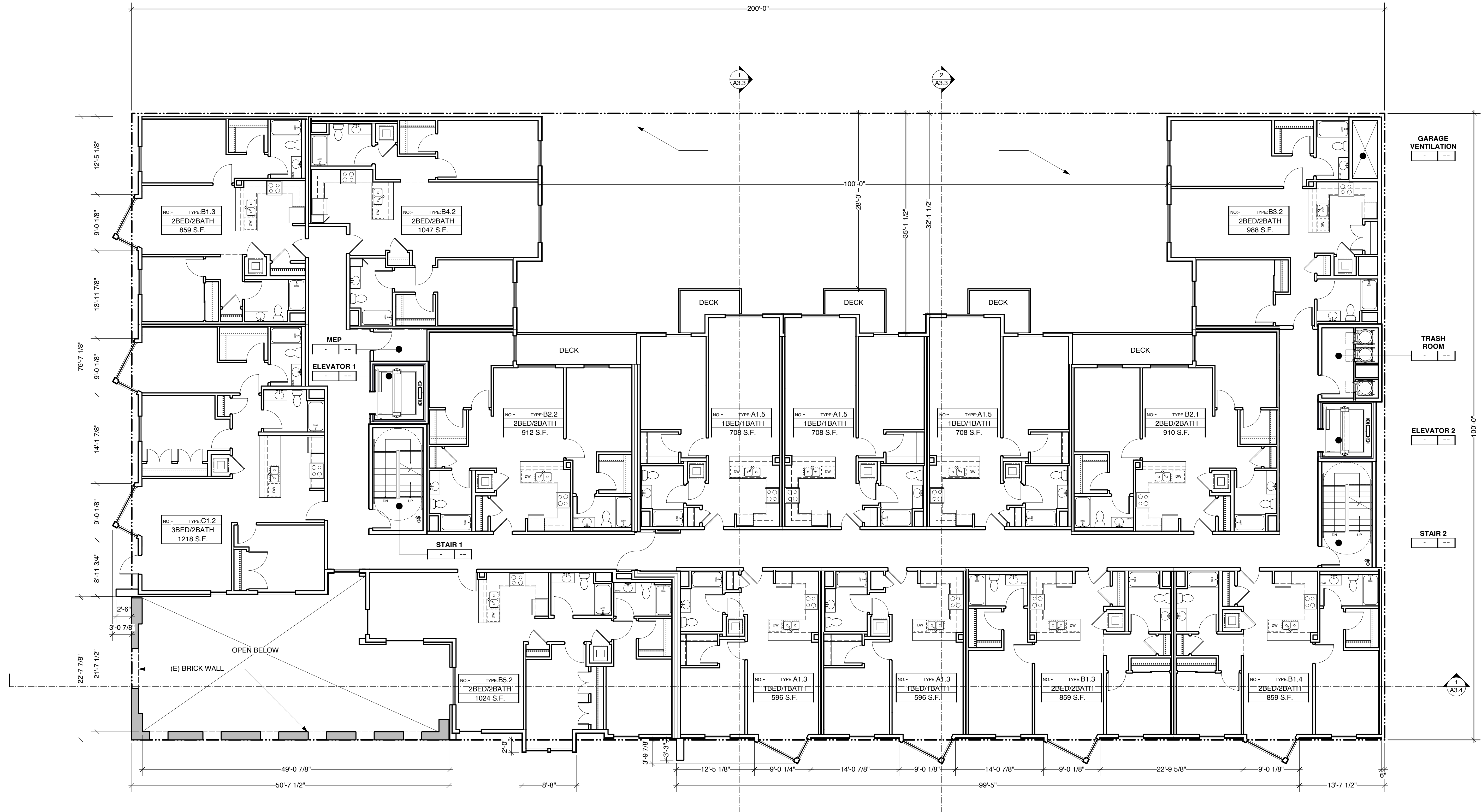


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**BUILDING
PLAN
FLOOR 3**

Sheet Title
Scale: 1/8"=1'
Project No. 1330 Drawn By: NS
Sheet Size: 30" x 42"

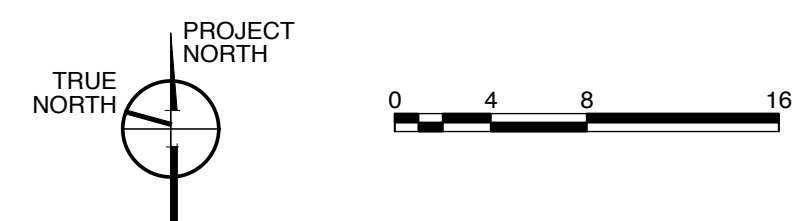
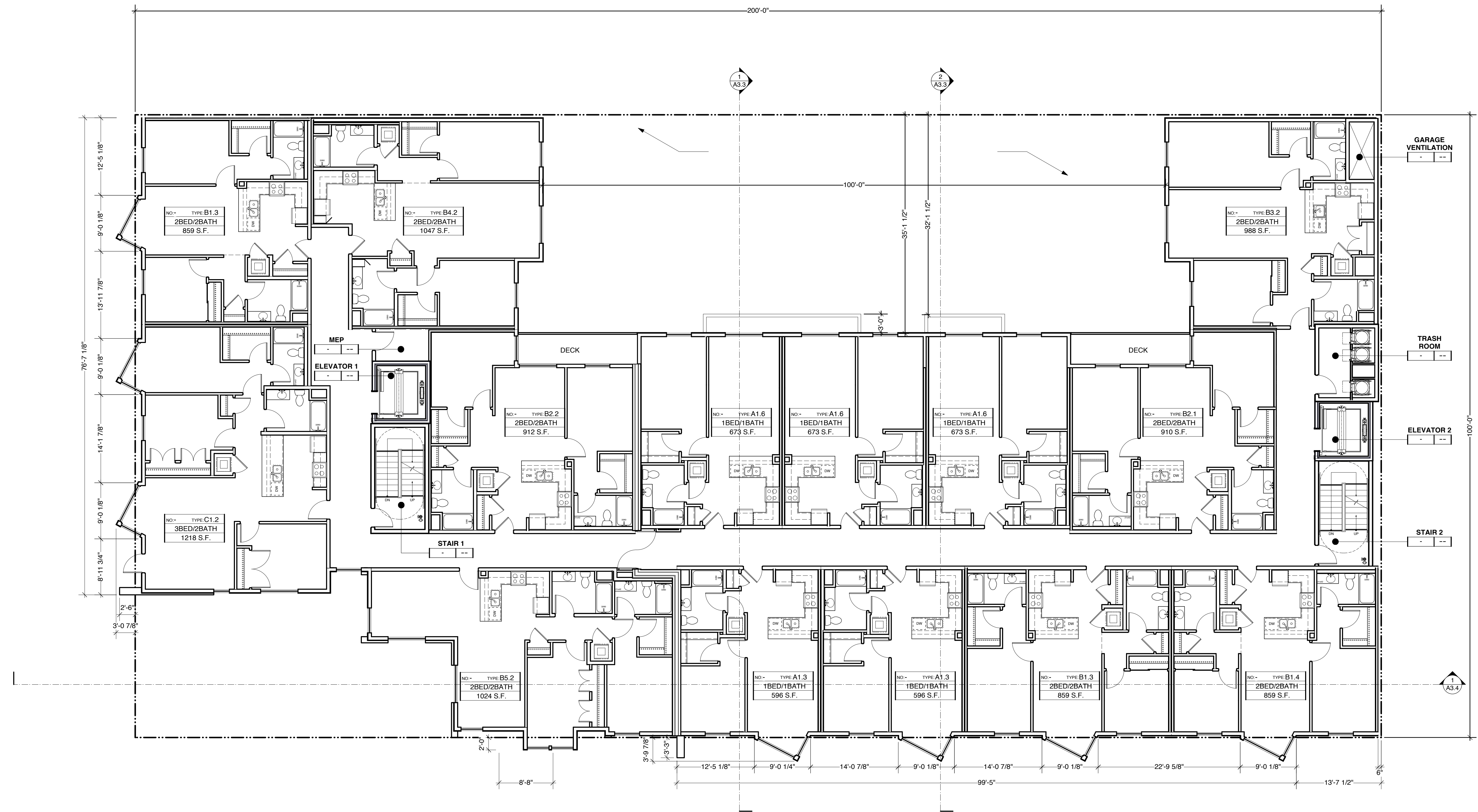


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**BUILDING
PLAN
FLOOR 4**

Sheet Title
Scale: 1/8"=1'
Project No. 1330 Drawn by: NS
Sheet Size: 30" x 42"

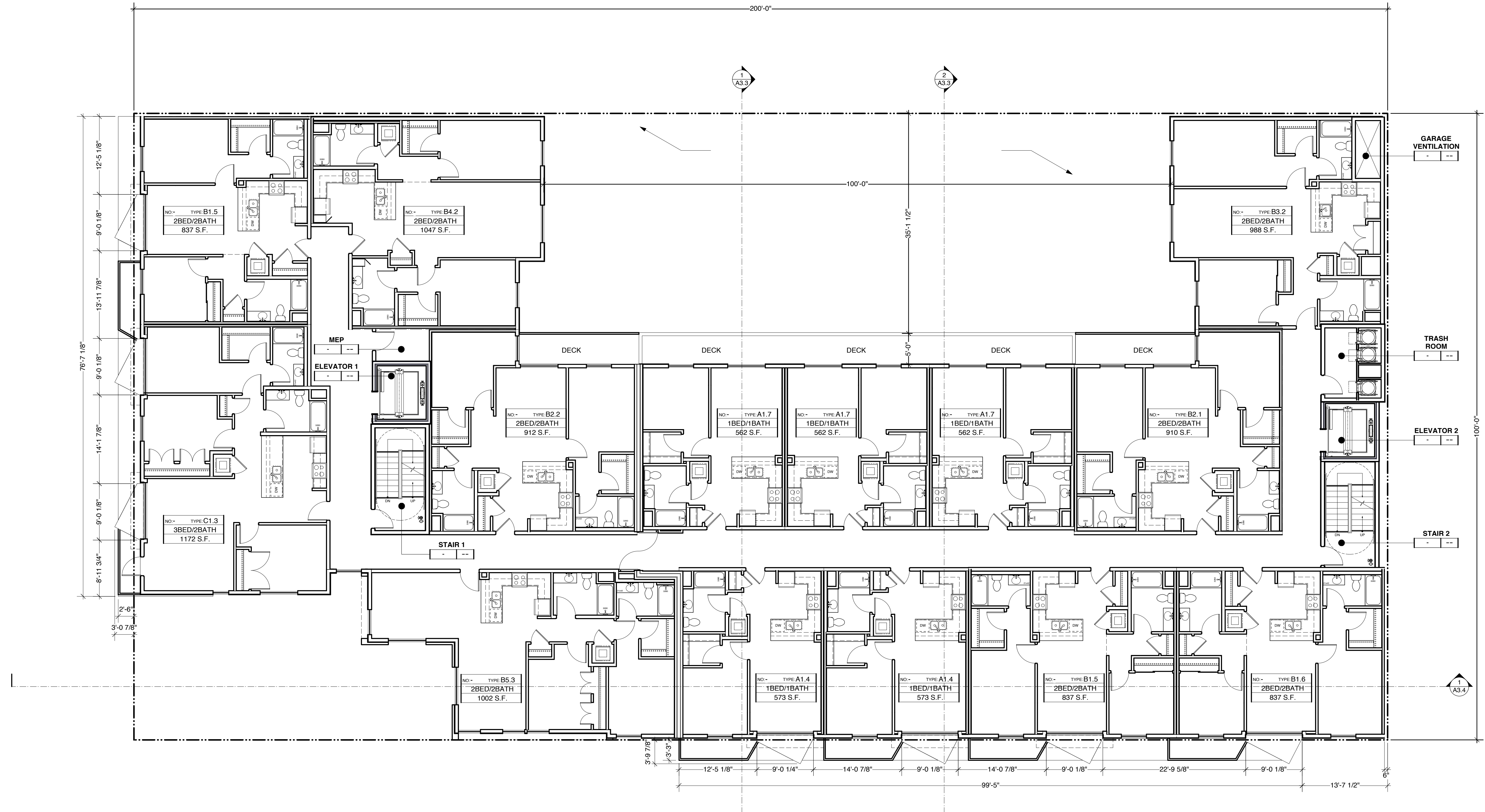


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**BUILDING
PLAN
FLOOR 5**

Sheet Title
Scale: 1/8"=1'
Project No. 1330 Drawn by: NS
Sheet Size: 30" x 42"

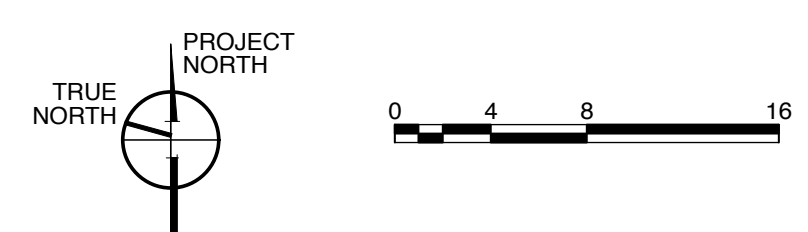
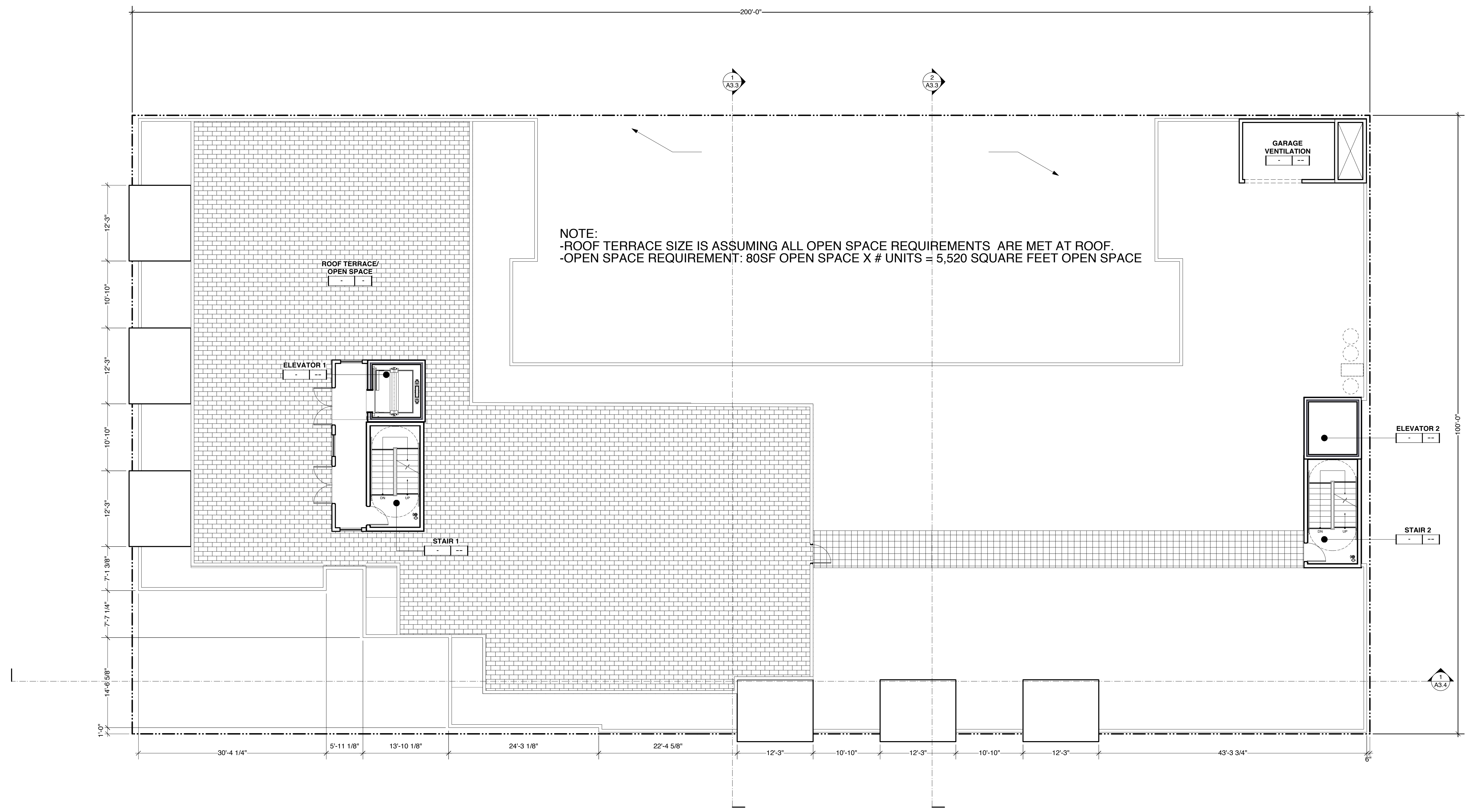


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**BUILDING
PLAN
ROOF**

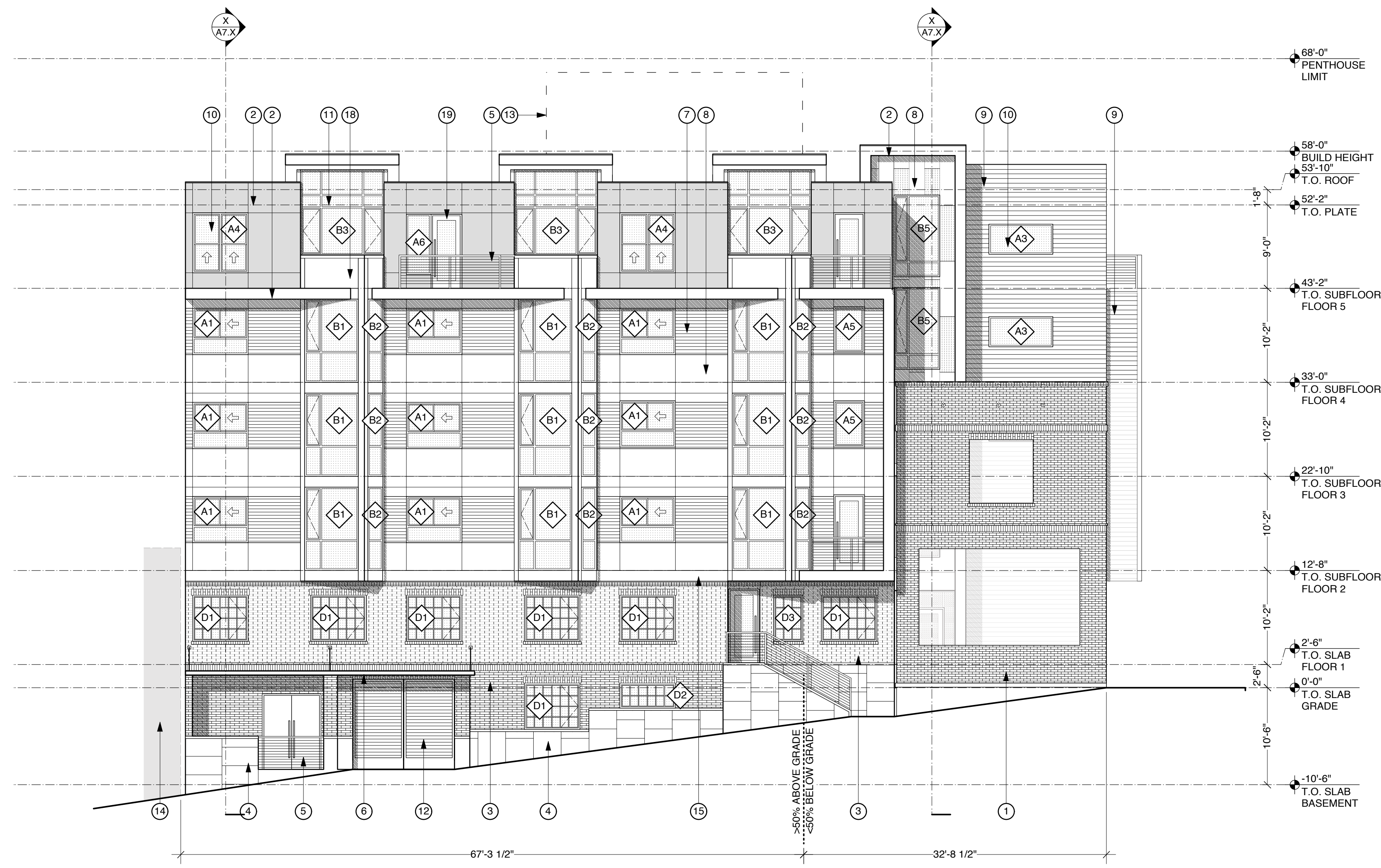
Sheet Title
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Project No. 1330 Drawn by: NS
Sheet Size: 30" x 42"



SHEET NOTES

NOTE: NOT ALL NOTES ARE USED ON EVERY SHEET

- 1 EXISTING BRICK STRUCTURE
- 2 FIRESTONE COMPOSITE METAL PANEL ROUTE AND RETRUN, DRY SYSTEM: CHAMPAGNE METALLIC
- 3 FIRESTONE COMPOSITE METAL PANEL ROUTE AND RETRUN, DRY SYSTEM: SILVER METALLIC
- 4 NORMAN THIN BRICK VENEER O/ PLASTER W/ GROUT AND WRAPPER BRICKS: CAROB OR TBD
- 5 STONE OR CAST STONE PANELS: COLOR/MATERIAL TBD
- 6 HOT-DIP GALVANIZED, PAINTED DECK RAILING
- 7 HOT-DIP GALVANIZED, PAINTED TRELLIS/CANOPY
- 8 HARDI PLANK SIDING: "WHITE" CLW 1033W
- 9 HARDI REVEAL 2 PANEL SYSTEM: "WHITE" CLW 1033W
- 10 HARDI REVEAL 2 PANEL SYSTEM: "YELLOW" CL 1695A
- 11 HARDI REVEAL 2 PANEL SYSTEM: "GREEN" CL 1915A
- 12 HARDI REVEAL 2 PANEL SYSTEM: "DK GREEN" CL 2997N
- 13 PRODEMA WOOD PLANK SIDING: LIGHT BROWN OR RUSTIK
- 14 ALUMINUM OR VINYL NAIL FIN WINDOW
- 15 WINDOW WALL
- 16 GARAGE METAL COILING DOOR
- 17 STAIR/ELEVATOR PENTHOUSE
- 18 ADJACENT BUILDING
- 19 6" EXTRUDED PT SLAB W/ COMPOSITE METAL PANEL WRAP
- 20 PROPERTY LINE WINDOWS
- 21 PRIVACY WALL
- 22 BAY WINDOW
- 23 PATIO TERRACE DOORS



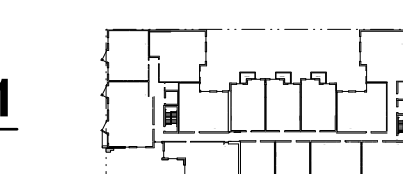
ELEVATION - 19TH ST 2

1/8" = 1'-0"



ELEVATION - TENNESSEE ST 1

1/8" = 1'-0"

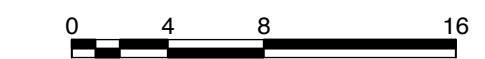


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LPA RESUBMITTAL	03.28.2014
PRE APP 2	04.25.2014
SITE PERMIT	05.02.2014
LPA RESUBMIT 2	07.03.2014
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ALL DIMENSIONS AND WRITTEN MATERIAL APPROVED UNDER PROFESSIONAL SEAL AND LICENSED UNDER THE ARCHITECTURE AND ENGINEERING BOARD OF THE STATE OF CALIFORNIA. THESE ARE DELIVERED WITHOUT THE WRITTEN CONSENT OF THE ARCHITECT.

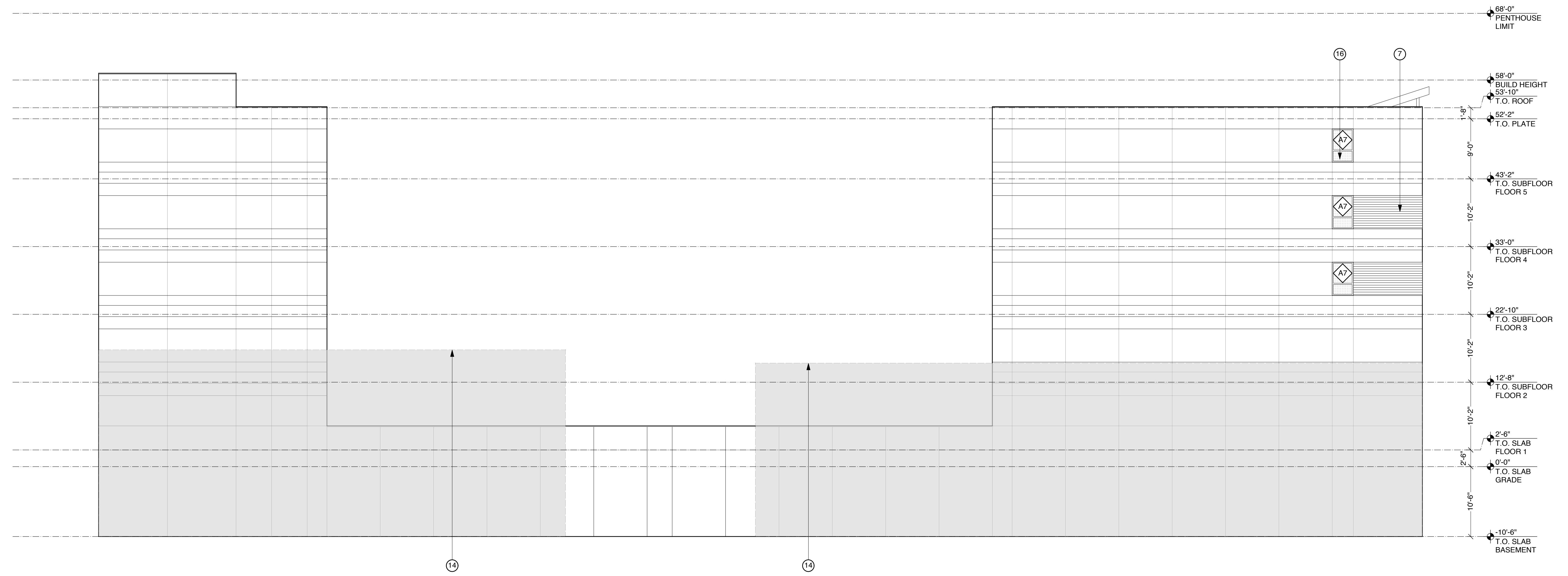
BUILDING ELEVATIONS

Sheet Title
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Project No. 1330 Drawn by: NS
Sheet Size: 30" x 42"

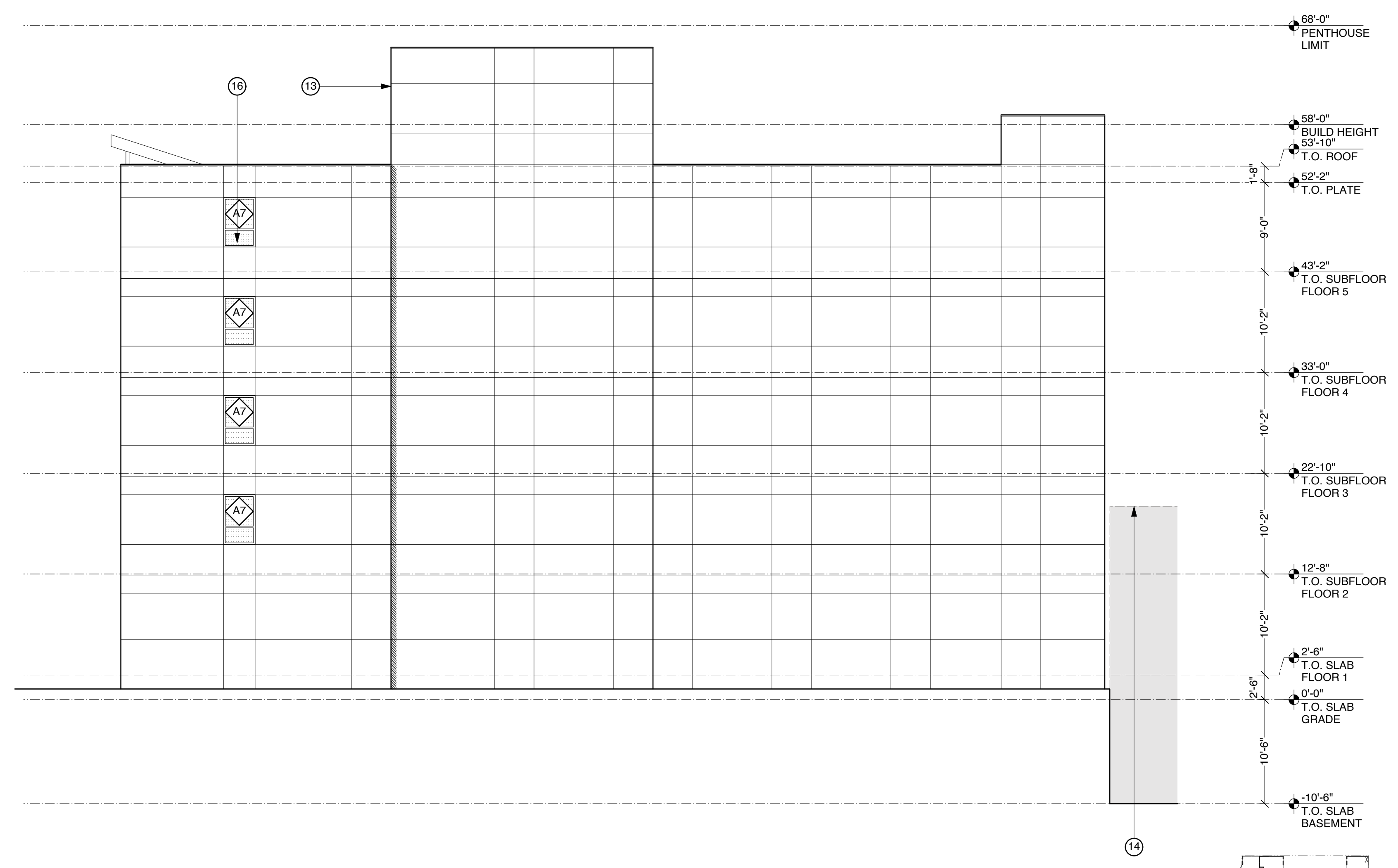


SHEET NOTES

- NOTE: NOT ALL NOTES ARE USED ON EVERY SHEET
- 1 EXISTING BRICK STRUCTURE
 - 2 FIRESTONE COMPOSITE METAL PANEL ROUTE AND RETRUN, DRY SYSTEM: CHAMPAGNE METALLIC
 - 3 FIRESTONE COMPOSITE METAL PANEL ROUTE AND RETRUN, DRY SYSTEM: SILVER METALLIC
 - 4 NORMAN THIN BRICK VENEER O/ PLASTER W/ GROUT AND WRAPPER BRICKS: CAROB OR TBD
 - 5 STONE OR CAST STONE PANELS: COLOR/MATERIAL TBD
 - 6 HOT-DIP GALVANIZED, PAINTED DECK RAILING
 - 7 HOT-DIP GALVANIZED, PAINTED TRELLIS/CANOPY
 - 8 HARDI PLANK SIDING: "WHITE" CLW 1033W
 - 9 HARDI REVEAL 2 PANEL SYSTEM: "WHITE" CLW 1033W
 - 10 HARDI REVEAL 2 PANEL SYSTEM: "YELLOW" CL 1895A
 - 11 HARDI REVEAL 2 PANEL SYSTEM: "GREEN" CL 1915A
 - 12 HARDI REVEAL 2 PANEL SYSTEM: "DK GREEN" CL 2997N
 - 13 PRODEMA WOOD PLANK SIDING: LIGHT BROWN OR RUSTIK
 - 14 ALUMINUM OR VINYL NAIL FIN WINDOW
 - 15 WINDOW WALL
 - 16 GARAGE METAL COILING DOOR
 - 17 STAIR/ELEVATOR PENTHOUSE
 - 18 ADJACENT BUILDING
 - 19 6" EXTRUDED PT SLAB W/ COMPOSITE METAL PANEL WRAP
 - 20 PROPERTY LINE WINDOWS
 - 21 PRIVACY WALL
 - 22 BAY WINDOW
 - 23 PATIO TERRACE DOORS



ELEVATION - NORTH (3RD STREET) 2
1/8" = 1'-0"



ELEVATION - EAST 1
1/8" = 1'-0"



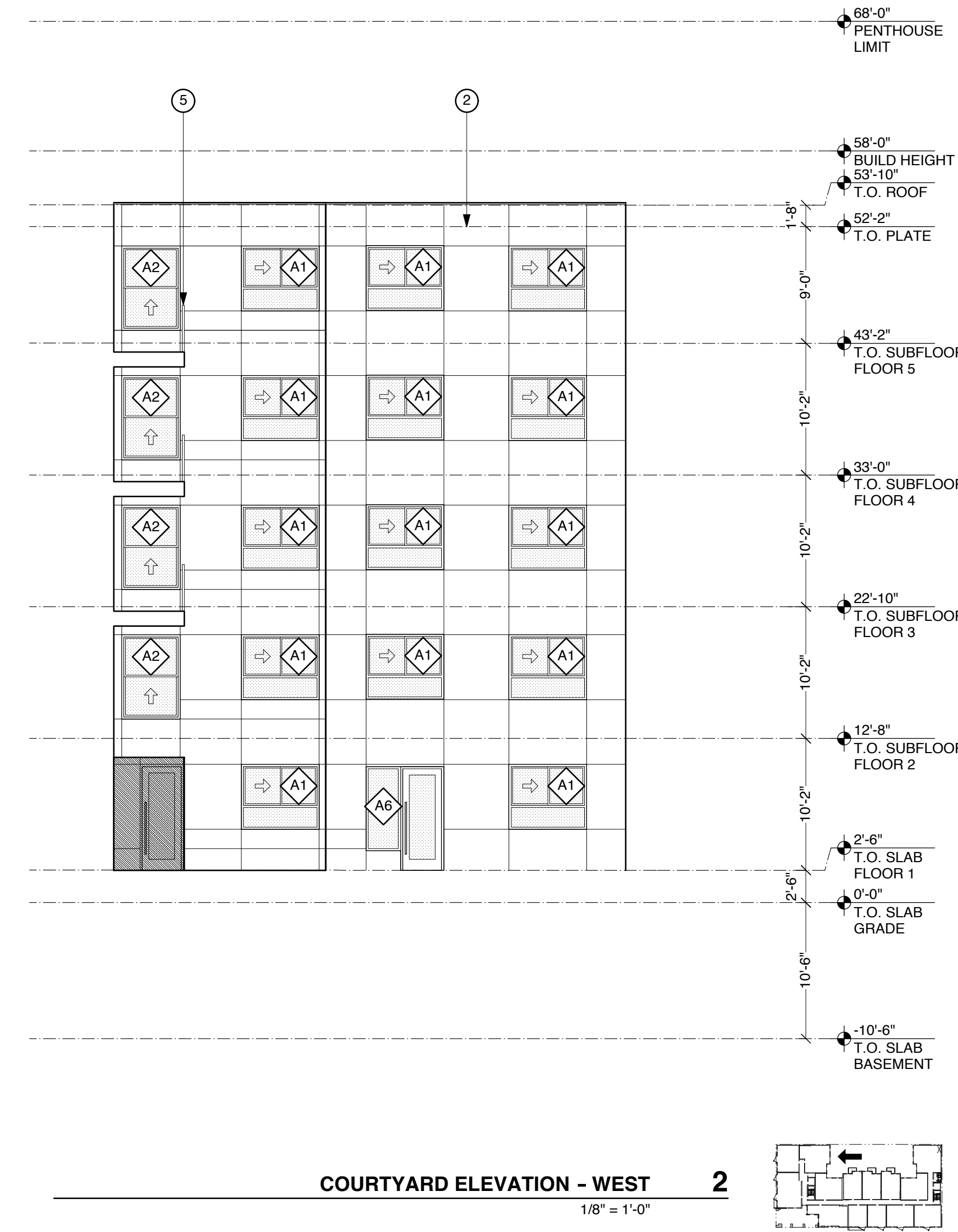
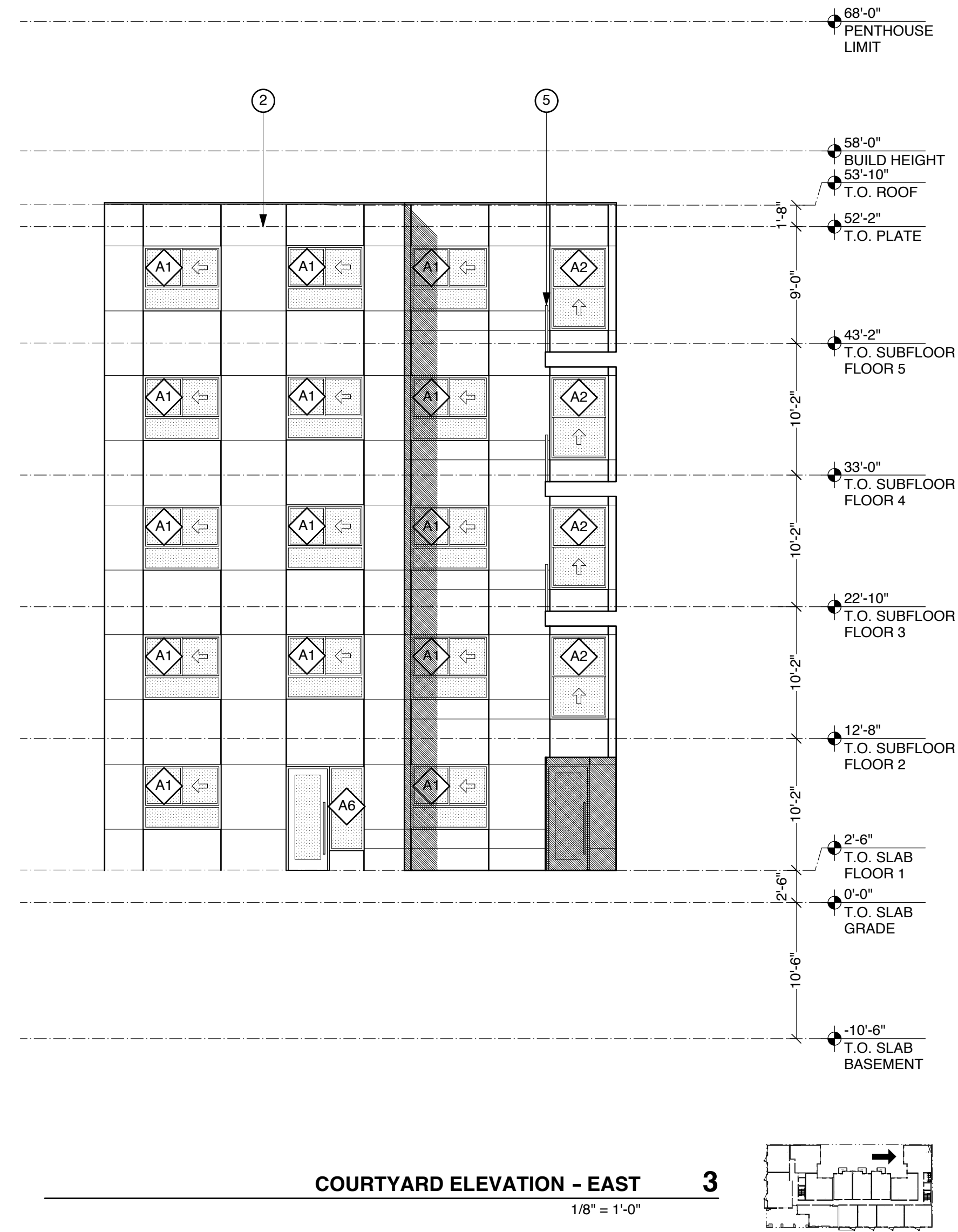
ISSUE	DATE
PP APPLICATION	09.30.2013
REVIEW	11.06.2013
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LPA RESUBMITTAL	03.28.2014
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BUILDING ELEVATIONS

Sheet Title
Scale 1/8" = 1'-0"
Product No. 1330 Drawn By: NS
Sheet Size: 30 x 42

SHEET NOTES

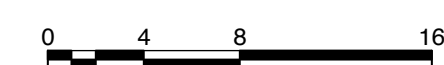
- NOTE: NOT ALL NOTES ARE USED ON EVERY SHEET
- 1 EXISTING BRICK STRUCTURE
 - 2 FIRESTONE COMPOSITE METAL PANEL ROUTE AND RETRUN, DRY SYSTEM: CHAMPAGNE METALLIC
 - 3 FIRESTONE COMPOSITE METAL PANEL ROUTE AND RETRUN, DRY SYSTEM: SILVER METALLIC
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 - 21 BAY WINDOW
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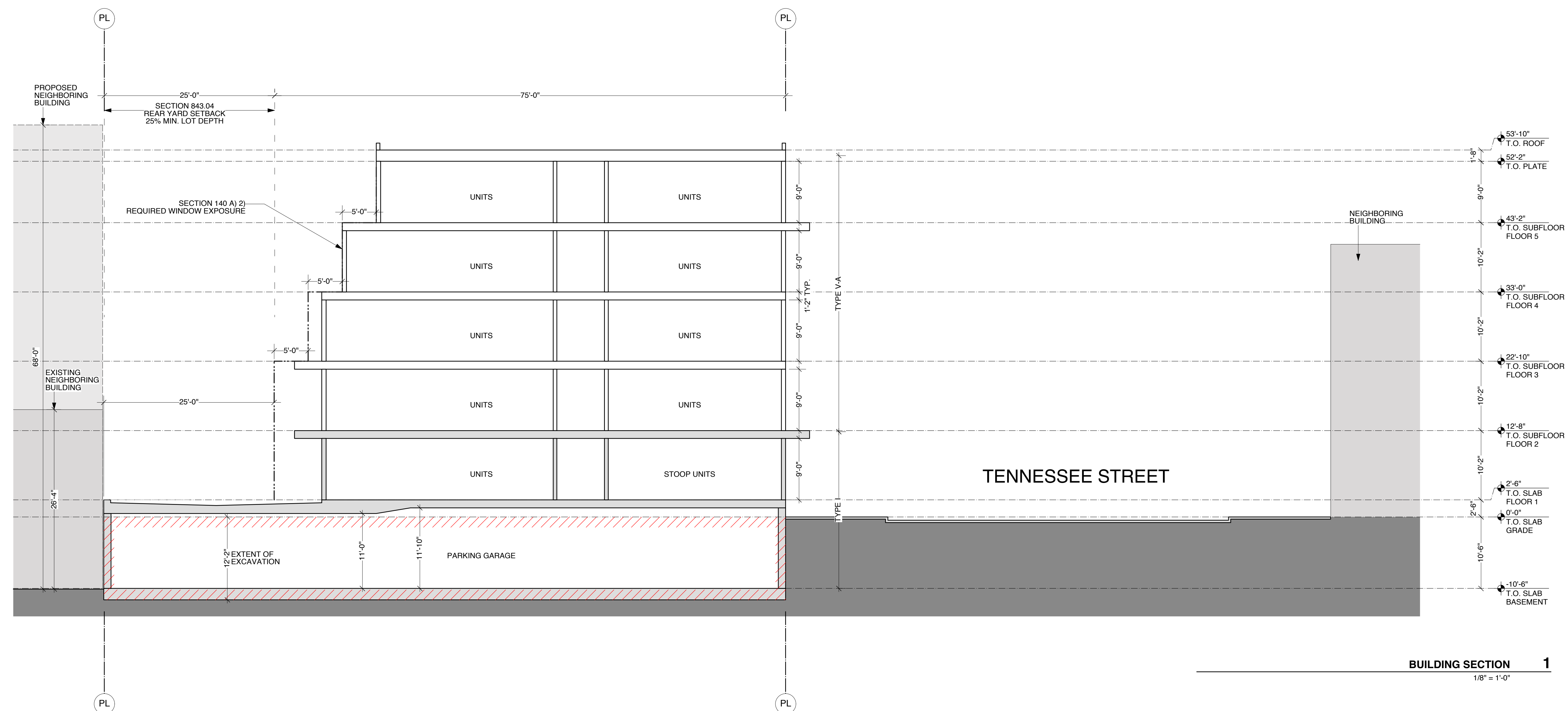
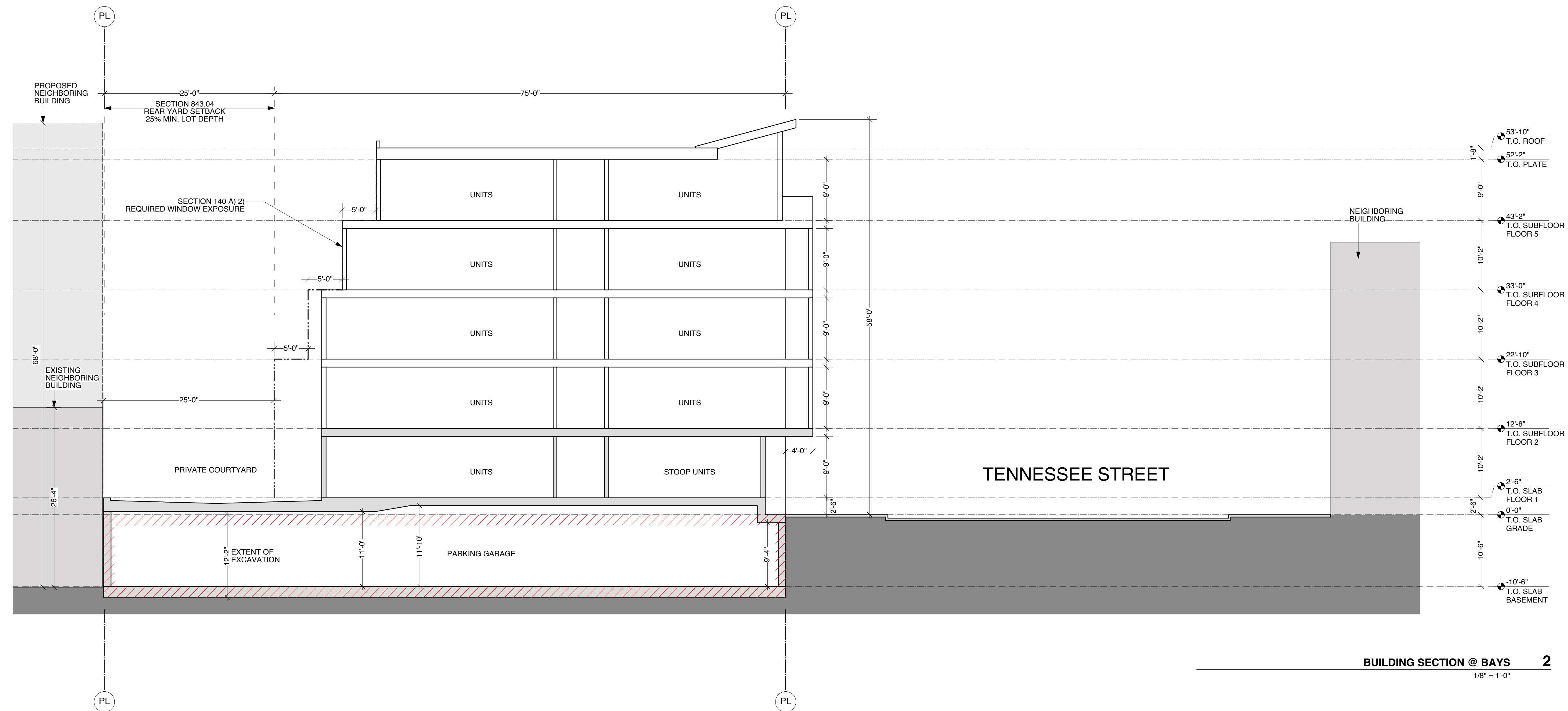


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LPA RESUBMITTAL	03.28.2014
PRE APP 2	04.25.2014
SITE PERMIT	05.02.2014
LPA RESUBMIT 2	07.03.2014
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BUILDING COURTYARD ELEVATIONS

Scale: 1/8" = 1'-0"
Product No. 1330
Sheet Size: 30" x 42"



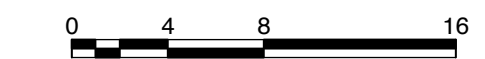


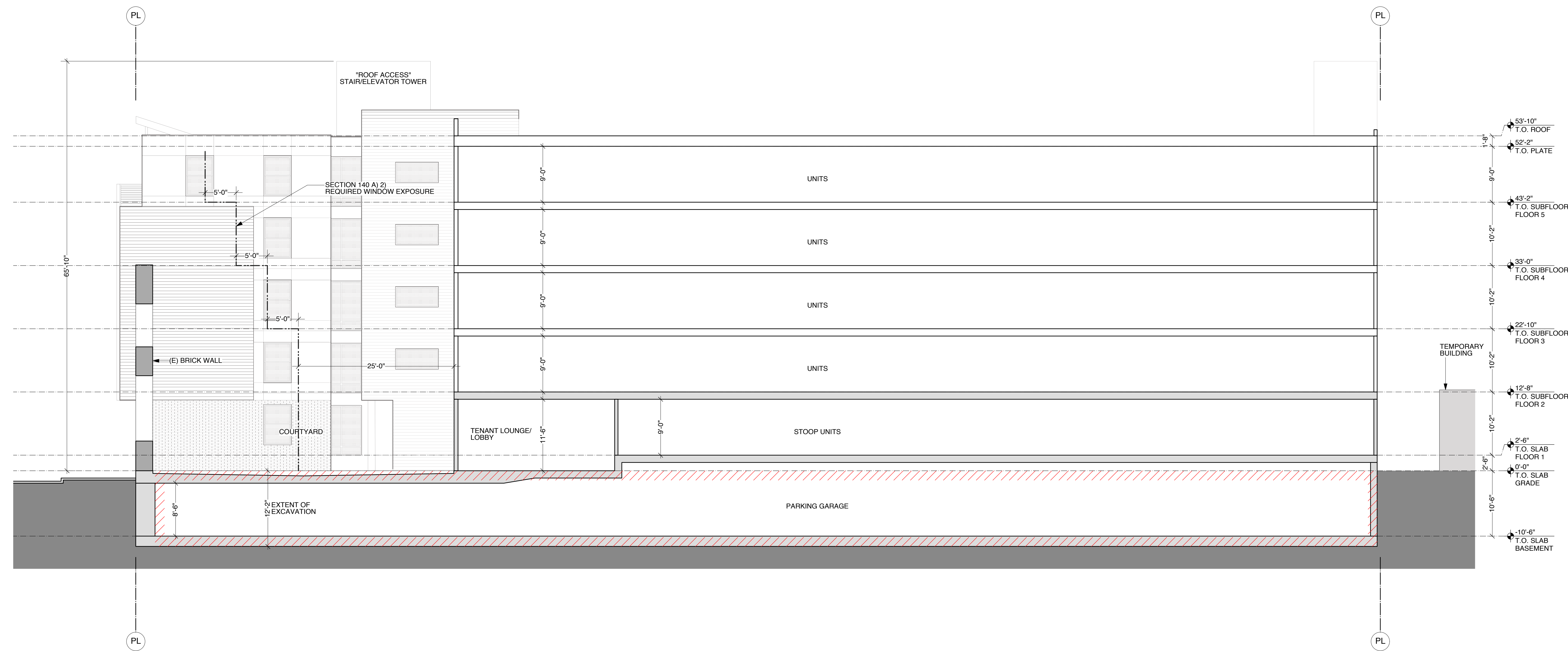
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LPA SUBMITTAL	12.05.2013
LPA RESUBMITTAL	03.28.2014
PRE APP 2	04.25.2014
SITE PERMIT	05.02.2014
LPA RESUBMIT 2	07.15.2014
LPA RESUBMIT 3	09.25.2014
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ALL DIMENSIONS AND WRITTEN MATERIAL APPROVED BY THE ARCHITECT AND CONTRACTOR SHALL BE THE AUTHORITY AND SHALL NOT BE SUPERSEDED, CHANGED OR DISREGARDED WITHOUT THE WRITTEN CONSENT OF THE ARCHITECT.

BUILDING SECTION

Sheet Title
Scale: 1/8" = 1'-0"
Project No. 1330
Drawn by: NS
Sheet Size: 30" x 42"





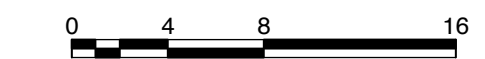
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LPA RESUBMIT 3	09.25.2014

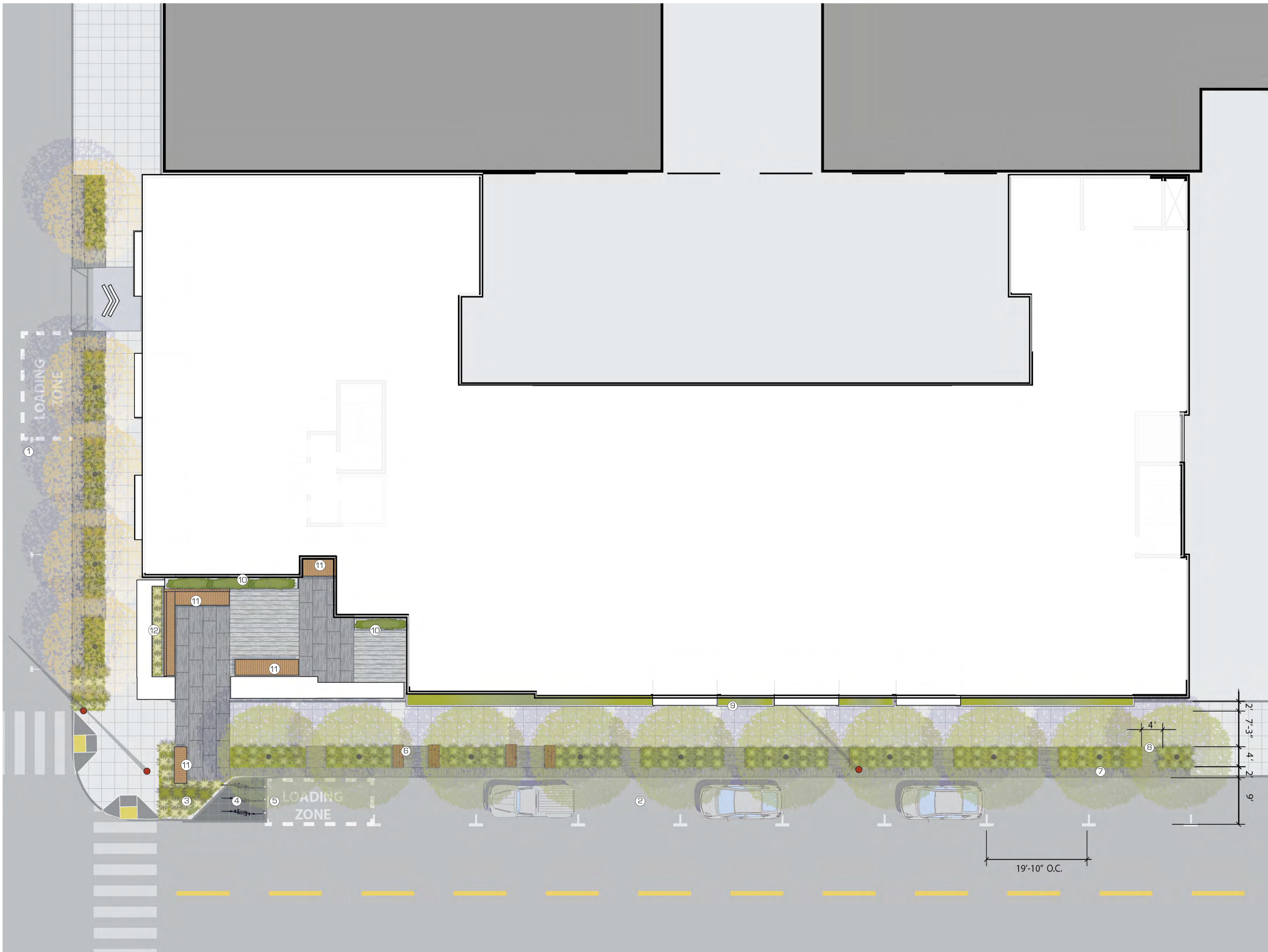
ALL DIMENSIONS AND WRITTEN MATERIAL
APPROVED HEREIN CONSTITUTE CONTRACTUAL
AND UNLAWFUL IN WHOLE OR IN PART, UNLESS
AND UNLESS NOTED BY SUPPLEMENTAL NOTES OR
DISCLOSED WITHOUT THE WRITTEN CONSENT
OF THE ARCHITECT.

BUILDING SECTION

Sheet Title
Scale: 1/8" = 1'-0"
Product No. 1330
Sheet Size: 30" x 42"

BUILDING SECTION 1
1/8" = 1'-0"





NARRATIVE

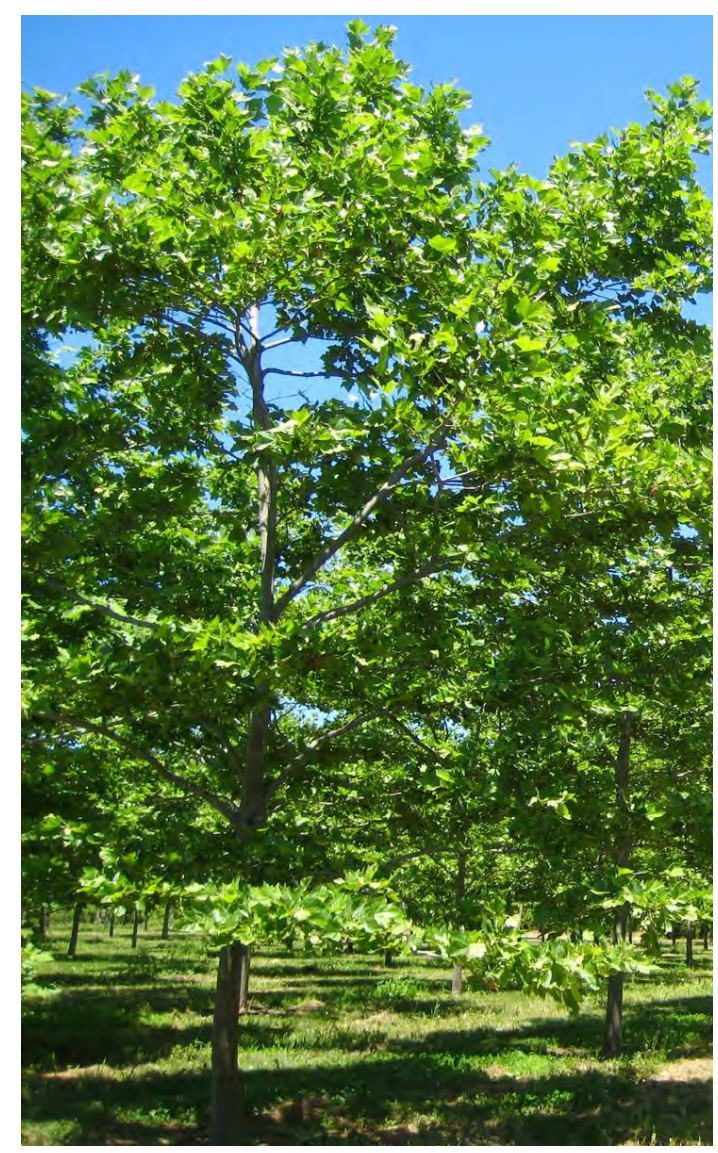
815 TENNESSEE STREETScape IMPROVEMENTS INCLUDE THE ADDITION OF A BULBOUT AT THE SOUTH EAST CORNER OF 19TH STREET AND TENNESSEE STREET INTERSECTION. THE BULBOUT WILL ACCOMMODATE A NEW PLANTING AND SEATING AREA AND PROTECT A NEW BIKE PARKING ZONE ON TENNESSEE. CURRENTLY THERE ARE NO STREET TREES OR PLANTING ALONG TENNESSEE AND 19TH STREET. THE PLAN INCLUDED THE ADDITION OF 15 STREET TREES AND CONTINUOUS PLANTING ALONG BOTH STREETS. THE STREETScape SIDEWALK WILL INCLUDES A NEW COURTESY STRIP, NEW ACCESS PATHS PLACED BETWEEN THE PLANTING AND NEW PERMANENT STREET FURNITURE IN THE FURNISHING ZONE NEAR THE NEW COURTYARD. THE EXISTING PARALLEL PARKING ALONG BOTH STREETS IS TO REMAIN.

COURTYARD AMENITIES

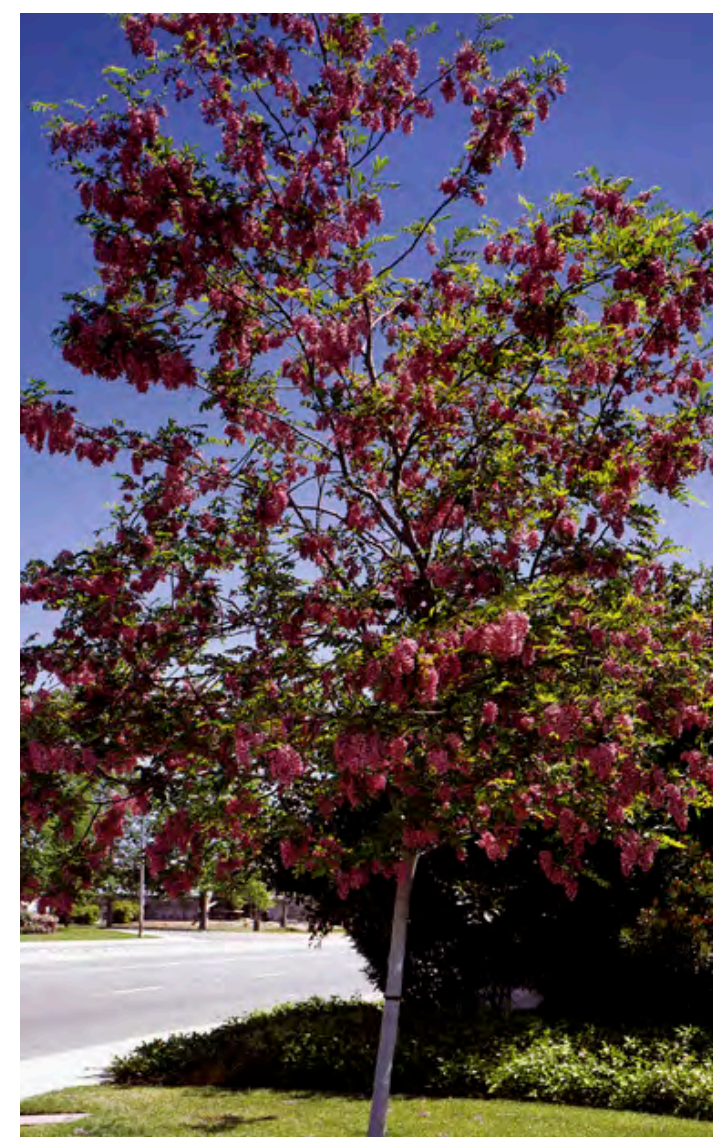
- ⑩ BAMBOO SCREEN
- ⑪ CUSTOM WOOD SEATING
- ⑫ RAISED PLANTING

STREET AMENITIES

- ③ BULBOUT WITH PLANTING
- ④ BIKE PARKING WITH SPECIAL PAVING
- ⑤ BOLLARDS
- ⑥ STREET FURNISHING
- ⑦ PERMEABLE PAVING COURTESY STRIP
- ⑧ PARKING ACCESS PATH
- ⑨ PLANTING IN FRONTAGE ZONE



① PLATANUS ACERIFOLIA 'COLUMBIA'



② ROBINIA 'PURPLE ROBE'



CEANOTHUS SPP.



DIANELLA CAERULEA

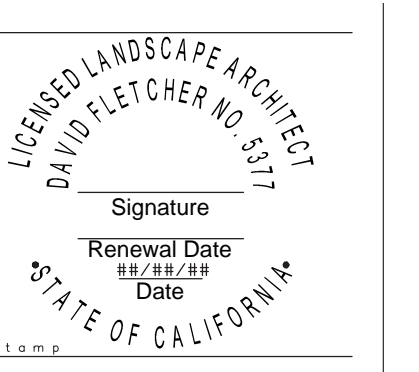


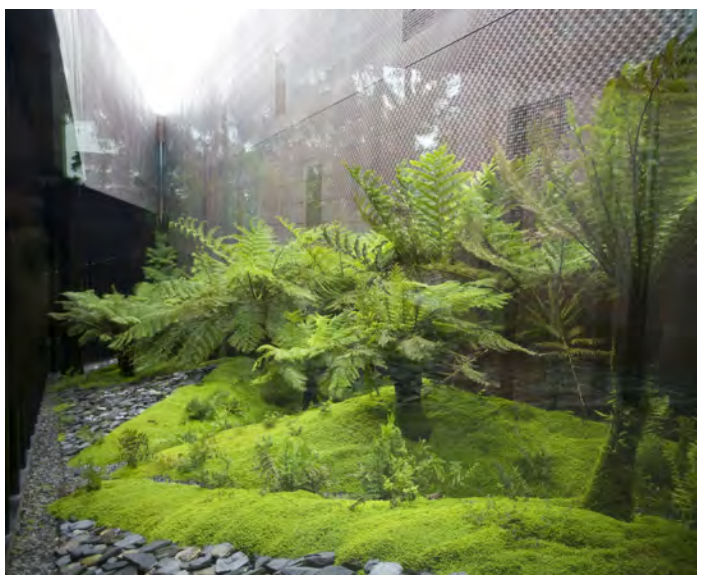
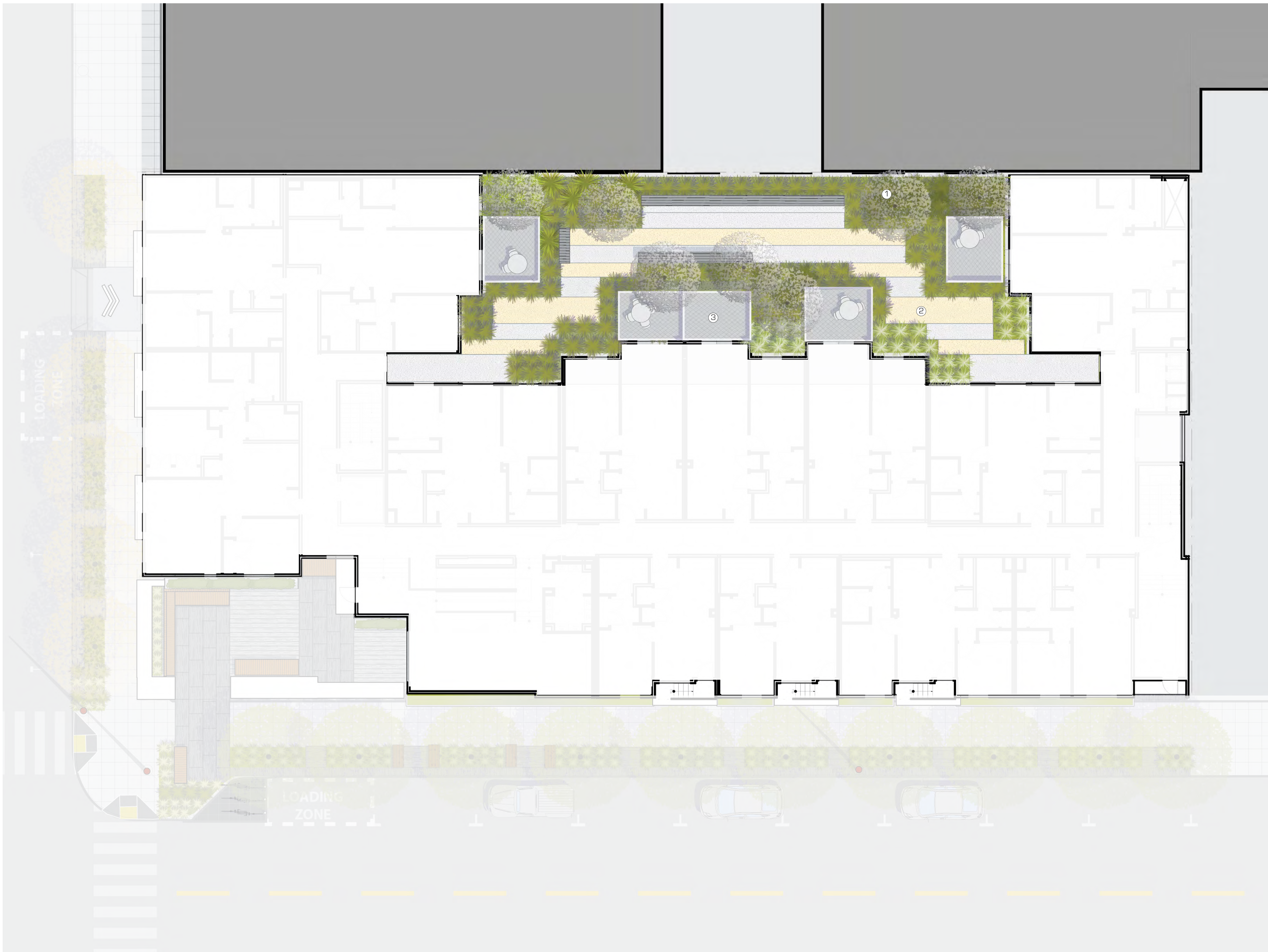
PENNISETUM MESSIACUM

48" BOX PLANTED IN CONTINUOUS REMEDIATED SOIL TRENCH

48" BOX PLANTED IN CONTINUOUS REMEDIATED SOIL TRENCH

Issue	Date
REVIEW	09.25.2013
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SITE PERMIT	05.02.2014
LPA SUBMITTAL	07.01.2014
LPA SUBMITTAL	07.15.2014





PLANTING

PLANTING PALETTE
 DICKSONIA
 BLECHNUM GIBBUM
 ASPLENIUM BULBIFERUM
 OPHIOPOGON JAPONICUS



① TREES (TYP)

PLANTING PALETTE
 MAGNOLIA STELLATA



SEAT WALLS

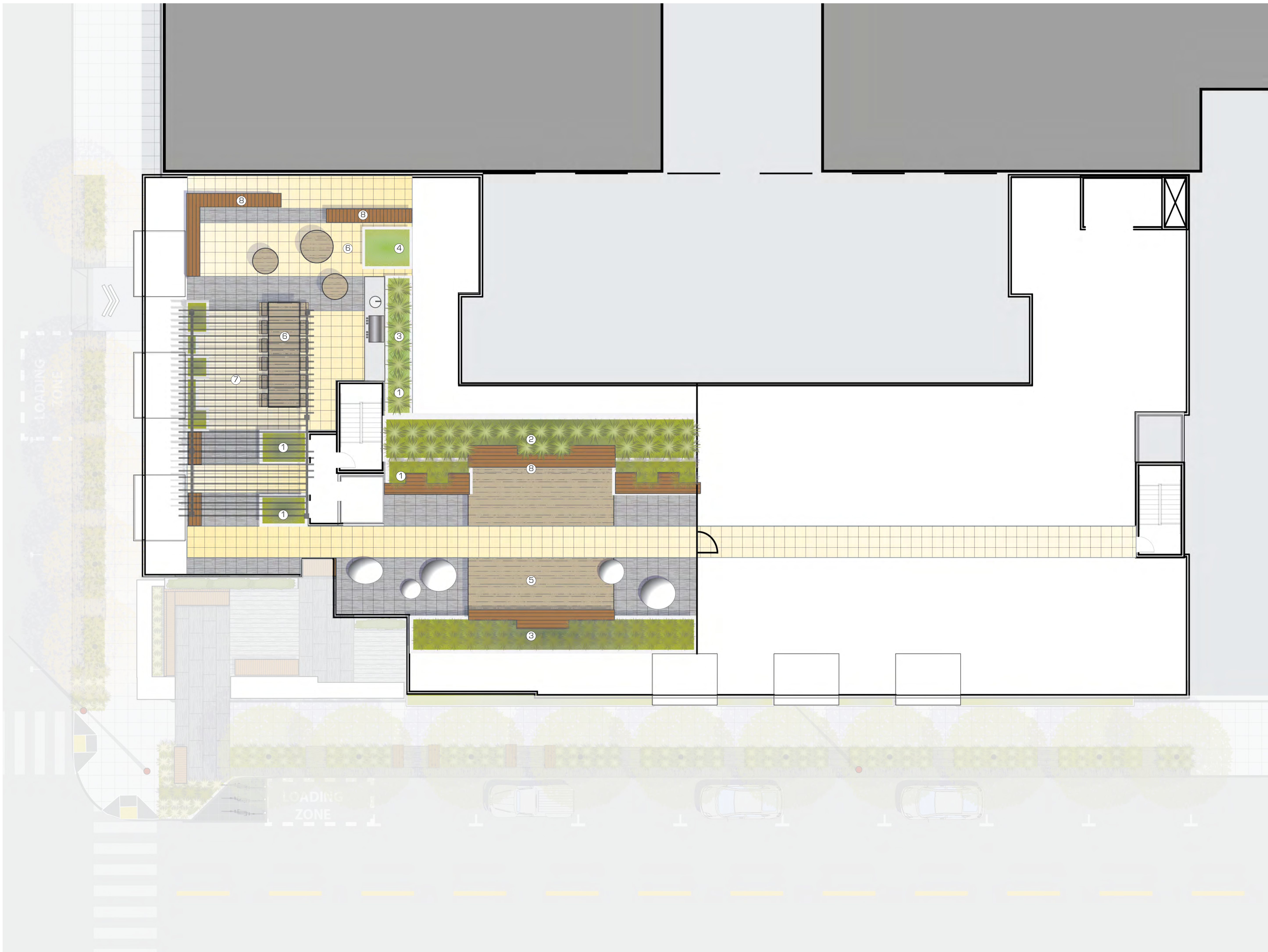


② PAVING



③ COURTYARD

Rev	Date
REVIEW	09.25.2013
REVIEW	09.26.2013
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LPA SUBMITTAL	07.01.2014
LPA SUBMITTAL	07.15.2014



① PLANTERS

PLANTING PALETTE
 FESTUCA GLAUCA
 CALAMAGROSTIS
 NUTKAENSIS
 VERBENA BONARIENSIS



② BUTTERFLY HABITAT

PLANTING PALETTE
 ERIOGONIUM LATIFOLIUM
 PHACELIA CALIFORNICA
 SIDALCEA SPP.
 CEANOTHUS SPP.
 LIMONIUM VULGARE
 ERIODICTYON
 TEMENTOSUM



③ HUMMINGBIRD HABITAT

PLANTING PALETTE
 PENSTEMON SPP.
 EPILOBIUM CANUM
 SALVIA CLEVELANDII
 SEDUM SPATHULIFOLIUM



④ HERB GARDEN



⑤ SITE FURNISHING



⑥ COMMUNITY TABLE



⑦ ARBOR



⑧ BENCH

Issue	Date
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LPA SUBMITTAL	07.15.2014

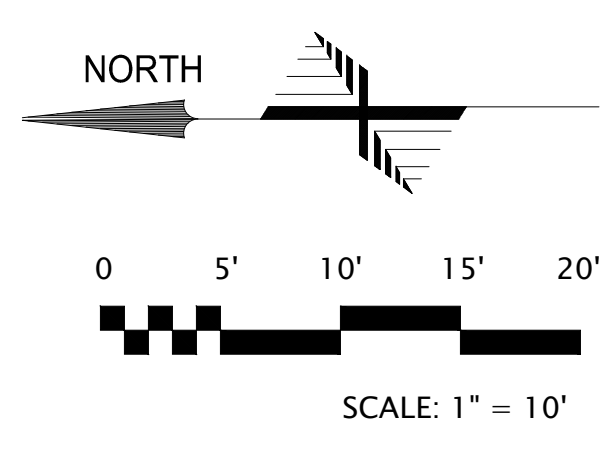


HOHBACH-LEWIN, INC.
STRUCTURAL & CIVIL ENGINEERS
280 SHAWMUT PARKWAY, Suite 150
Palo Alto, CA 94306
(650) 617-5930, Fax (650) 617-5932

815 TENNESSEE STREET
SAN FRANCISCO, CALIFORNIA

Issue	Date
LPA	2014-07-17
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OWNER:
ROEM CORPORATION
Contact: Mark Pilarczyk
1650 Lafayette St.
Santa Clara, CA 95050
(408) 984-5600

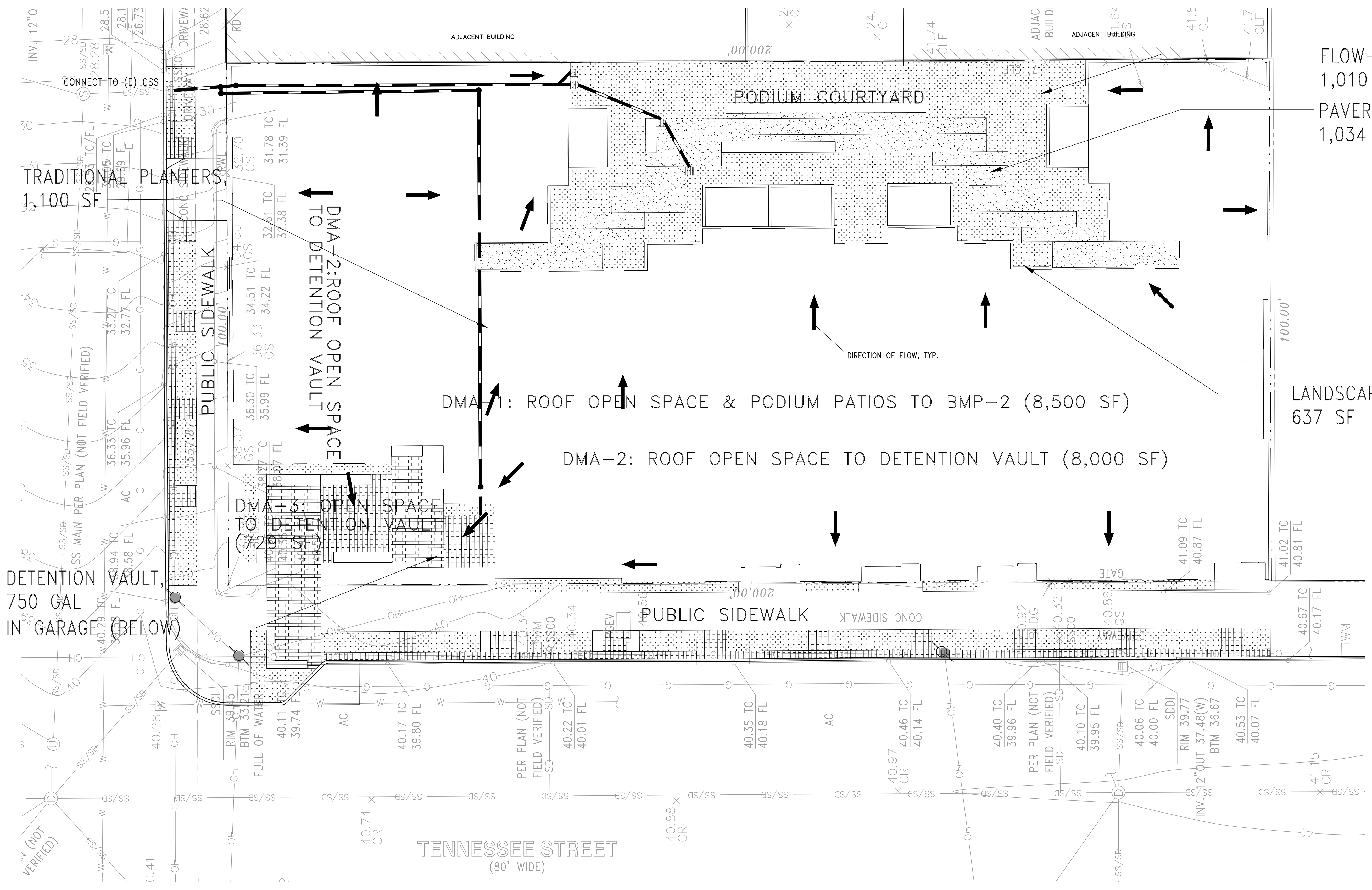
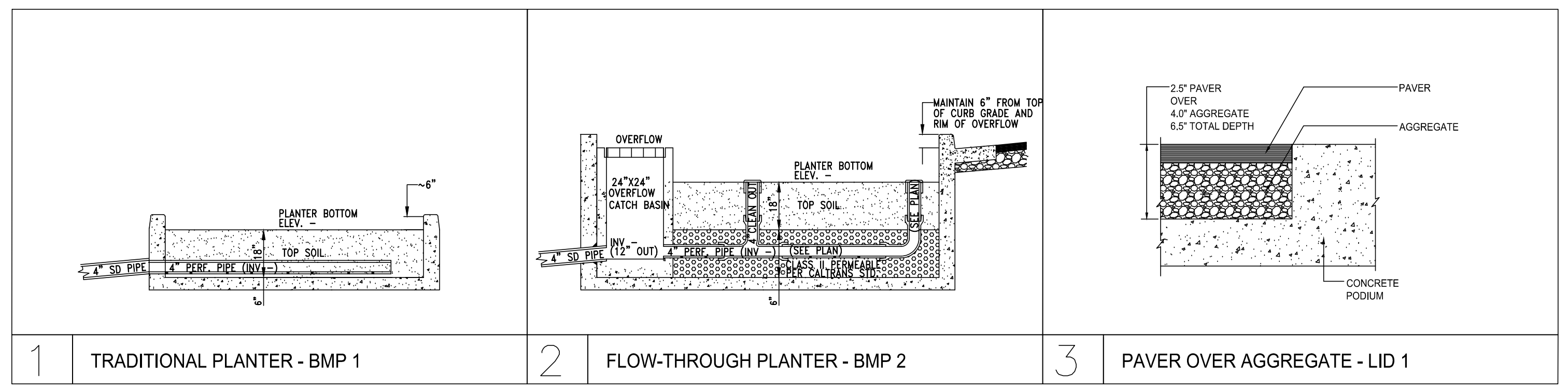


PRELIMINARY
SWMP

Scale: AS SHOWN
Drawn By: VPA

SUMMARY TABLE

SYMBOL	SURFACE AREAS	BMP/LID	AREA	SERVING DMA	DMA LINE SYMBOL	DMA AREA
[Pattern]	FLOW-THROUGH PLANTER	BMP-2	1,010 SF	DMA-1		8,500 SF
[Pattern]	PAVERS OVER AGGREGATE	LID-1	1,034 SF	DMA-4		1,034 SF
[Pattern]	TRADITIONAL PLANTER	BMP-1	1,100 SF	DMA-1&2		1,100 SF
[Pattern]	NON-LID LANDSCAPING	637 SF	NA	637 SF	DMA-4	637 SF
[Pattern]	NON-LID DETENTION VAULT 750 CF	NA	750 CF	DMA-2&3		8,729 SF
	TOTAL LID AREA		3,144 SF			20,000 SF



Affidavit for Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415

7-3-14

Date

I, Jonathan Emami, do hereby declare as follows:

a. The subject property is located at (address and block/lot):

815-825 Tennessee Street, San Francisco, CA

Address

4059/001A&B

Block / Lot

b. The proposed project at the above address is subject to the Inclusionary Affordable Housing Program, Planning Code Section 415 et seq.

The Planning Case Number and/or Building Permit Number is:

2013.0220X

Planning Case Number

Building Permit Number

This project requires the following approval:

Planning Commission approval (e.g. Conditional Use Authorization, Large Project Authorization)

This project is principally permitted.

The Current Planner assigned to my project within the Planning Department is:

Rich Sucre

Planner Name

Is this project within the Eastern Neighborhoods Plan Area?

Yes (if yes, please indicate Tier) A

No

This project is exempt from the Inclusionary Affordable Housing Program because:

This project uses California Debt Limit Allocation Committee (CDLAC) funding.

This project is 100% affordable.

c. This project will comply with the Inclusionary Affordable Housing Program by:

Payment of the Affordable Housing Fee prior to the first site or building permit issuance (Planning Code Section 415.5).

On-site or Off-site Affordable Housing Alternative (Planning Code Sections 415.6 and 416.7).

d. If the project will comply with the Inclusionary Affordable Housing Program through an **On-site or Off-site Affordable Housing Alternative**, please fill out the following regarding how the project is eligible for an alternative and the accompanying unit mix tables on page 4.

- Ownership.** All affordable housing units will be sold as ownership units and will remain as ownership units for the life of the project.
- Rental.** Exemption from Costa Hawkins Rental Housing Act.² The Project Sponsor has demonstrated to the Department that the affordable units are not subject to the Costa Hawkins Rental Housing Act, under the exception provided in Civil Code Sections 1954.50 through one of the following:
 - Direct financial contribution from a public entity.
 - Development or density bonus or other public form of assistance.
 - Development Agreement with the City. The Project Sponsor has entered into or has applied to enter into a Development Agreement with the City and County of San Francisco pursuant to Chapter 56 of the San Francisco Administrative Code and, as part of that Agreement, is receiving a direct financial contribution, development or density bonus, or other form of public assistance.

e. The Project Sponsor acknowledges that failure to sell the affordable units as ownership units or to eliminate the on-site or off-site affordable ownership-only units at any time will require the Project Sponsor to:

- (1) Inform the Planning Department and the Mayor's Office of Housing and, if applicable, fill out a new affidavit;
- (2) Record a new Notice of Special Restrictions; and
- (3) Pay the Affordable Housing Fee plus applicable interest (using the fee schedule in place at the time that the units are converted from ownership to rental units) and any applicable penalties by law.


f. The Project Sponsor must pay the Affordable Housing Fee in full sum to the Development Fee Collection Unit at the Department of Building Inspection for use by the Mayor's Office of Housing prior to the issuance of the first construction document, with an option for the Project Sponsor to defer a portion of the payment to prior to issuance of the first certificate of occupancy upon agreeing to pay a deferral surcharge that would be deposited into the Citywide Affordable Housing Fund in accordance with Section 107A.13.3 of the San Francisco Building Code.

g. I am a duly authorized officer or owner of the subject property.

I declare under penalty of perjury under the laws of the State of California that the foregoing is true and correct. Executed on this day in:

815-825 Tennessee Street, San Francisco
Location

7-3-14
Date


Signature

Jonathan Emami, Vice President
Name (Print), Title

cc: Mayor's Office of Housing
Planning Department Case Docket
Historic File, if applicable
Assessor's Office, if applicable

(408) 984-5600 x22
Contact Phone Number

² California Civil Code Section 1954.50 and following.

Unit Mix Tables

NUMBER OF ALL UNITS IN PRINCIPAL PROJECT:					
Total Number of Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units
69			26	38	5

If you selected an On-site or Off-Site Alternative, please fill out the applicable section below:

- On-site Affordable Housing Alternative (Charter Section 16.110 (g) and Planning Code Section 415.6): calculated at 12% of the unit total. **UMU Tier A is subject to 14.4% on-site.**

NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units
10			4	5	1

- Off-site Affordable Housing Alternative (Planning Code Section 415.7): calculated at 20% of the unit total.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

Area of Dwellings in Principal Project (in sq. feet)	Off-Site Project Address	
Area of Dwellings in Off-Site Project (in sq. feet)		
Off-Site Block/Lot(s)	Motion No. (if applicable)	Number of Market-Rate Units in the Off-site Project

- Combination of payment of a fee, on-site affordable units, or off-site affordable units with the following distribution:
Indicate what percent of each option would be implemented (from 0% to 99%) and the number of on-site and/or off-site below market rate units for rent and/or for sale.

1. Fee _____ % of affordable housing requirement.
2. On-Site _____ % of affordable housing requirement.

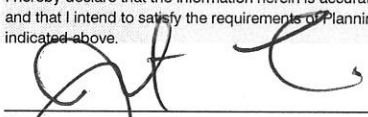
NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

3. Off-Site _____ % of affordable housing requirement.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

Area of Dwellings in Principal Project (in sq. feet)	Off-Site Project Address	
Area of Dwellings in Off-Site Project (in sq. feet)		
Off-Site Block/Lot(s)	Motion No. (if applicable)	Number of Market-Rate Units in the Off-site Project

Affidavit for **Compliance with the Inclusionary Affordable Housing Program**

CONTACT INFORMATION AND DECLARATION OF SPONSOR OF PRINCIPAL PROJECT	CONTACT INFORMATION AND DECLARATION OF SPONSOR OF OFF-SITE PROJECT (IF DIFFERENT)
Company Name ROEM Development	Company Name
Print Name of Contact Person Jonathan Emami, Vice President	Print Name of Contact Person
Address 1650 Lafayette Street	Address
City, State, Zip Santa Clara, CA 95050	City, State, Zip
Phone, Fax (408) 984-5600 X22	Phone, Fax
Email jemami@roemcorp.com	Email
I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.	I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.
 _____ Signature	_____ Signature
Jonathan Emami, Vice President Name (Print), Title	_____ Name (Print), Title



**SAN FRANCISCO
PLANNING
DEPARTMENT**

AFFIDAVIT FOR First Source Hiring Program Administrative Code Chapter 83

**Planning Department
1650 Mission Street
Suite 400
San Francisco, CA
94103-9425**

**T: 415.558.6378
F: 415.558.6409**

For all projects subject to Administrative Code Chapter 83, this completed form must be filed with the Planning Department prior to any Planning Commission hearing or, if principally permitted, Planning Department approval of the site permit.

PROJECT ADDRESS		BLOCK/LOT(S)
815-825 Tennessee Street, San Francisco, CA 94107		4059/001A&001B
BUILDING PERMIT APPLICATION NO.	CASE NO (IF APPLICABLE)	MOTION NO.
	2013.0200x	

Please check the boxes below that are applicable to this project. Select all that apply.

- 1A. The project is wholly residential.
- 1B. The project is wholly commercial. (For the purposes of Administrative Code Chapter 83, any project that is not residential is considered to be a commercial activity.)
- 1C. The project is a mixed use.
- 2A. The project will create ten (10) or more new residential units.
- 2B. The project will create 25,000 square feet or more of new or additional gross floor area.
- 3A. The project will create less than ten (10) new residential units.
- 3B. The project will create less than 25,000 square feet of new or additional gross floor area.

If you checked either 2A or 2B, your project is subject to the First Source Hiring Program. Please contact the First Source Hiring Program Manager with the San Francisco Human Services Agency's Workforce Development Division to develop a contract to satisfy this requirement.

If you checked 3A and 3B, your project is not subject to the First Source Hiring Program.

For questions, please contact the First Source Hiring Manager at (415) 401-4960. For frequently asked questions, you may access First Source information at www.onestopsf.org

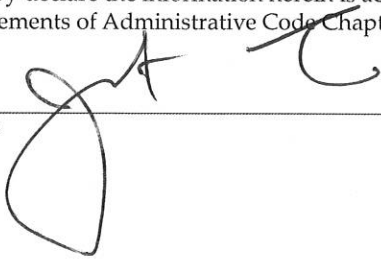
Affidavit for First Source Hiring Program

Contact Information and Declaration of Sponsor of Principal Project

NAME: Jonathan Emami	
ADDRESS: 1650 Lafayette Street, Santa Clara, CA 95050	TELEPHONE: (408) 984-5600 x22
	FAX: (408) 984-3111
	EMAIL: jemami@roemcorp.com

I hereby declare the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Administrative Code Chapter 83.

Signature



Date

7/2/14

Sucre, Richard (CPC)

From: Mark Leuthold <markrex@mac.com>
Sent: Wednesday, January 08, 2014 1:08 PM
To: Sucre, Richard
Cc: 701Minnesota@googlegroups.com
Subject: RE: project address 815-825 Tennessee Street

Dear Mr. Sucre,

I have just received a notification from the SF Planning Department regarding the proposed project for a six story 100 unit building at 815-825 Tennessee Street, SF, CA 94107, across the street from my condo. The case or file number is 2013.0220E on the Environmental Review Notification.

Presently there is a one story brick building on this lot. The northern end of the building has a second story. Even tho this 1926 industrial building is not grand, it fits the typical style of the funky Dogpatch neighborhood. To replace this small building with a 6 story residential monstrosity on this block of Tennessee Street, which has very low buildings, is hardly keeping the "Historic Dogpatch" neighborhood historic! Besides, this building will create a very dark shadow and ruin any morning sun I receive.

Please do not allow such radically out of scale to the rest of the neighborhood buildings to destroy our lovely Dogpatch, which is seeing such an unprecedented growth.

I would like my opposition to this overgrown project be known, and recommend the new building incorporate the existing brick building fascade with its present height.

Hoping to hear from you.

Best regards,

Mark Leuthold
701 Minnesota # 153
San Francisco, CA 94107
Tel. (415) 225-2924



SAN FRANCISCO PLANNING DEPARTMENT

Certificate of Determination EXEMPTION FROM ENVIRONMENTAL REVIEW

Case No.: 2013.0220E
 Project Address: 815-825 Tennessee Street
 Zoning: UMU (Urban Mixed Use) District
 58-X Height and Bulk District
 Block/Lot: 4059/001A, 001B
 Lot Size: 2 lots totaling 20,000 square feet
 Plan Area: Eastern Neighborhoods Area Plan
 Project Sponsor: Mark Pilarczyk, ROEM Development Corporation - (408) 984-5600
 Staff Contact: Brett Becker – (415) 554-1650, Brett.Becker@sfgov.org

1650 Mission St.
 Suite 400
 San Francisco,
 CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
 Information:
415.558.6377

PROJECT DESCRIPTION:

The proposed project includes partial demolition of an existing two-story 32,000-square-foot industrial building built in 1926 and construction of a five-story, 58-foot-tall, 74,359-square-foot residential apartment building with basement garage. A portion of the original industrial building that contains the principal brick façade will remain to serve as a visual reminder of the building's original use and the industrial history of the neighborhood. The new residential building would accommodate approximately 69 dwelling units. The 19,579 square-foot basement level garage would contain 48 off-street parking spaces and 70 bicycle spaces.

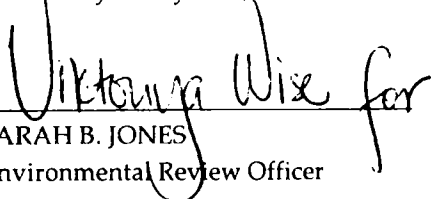
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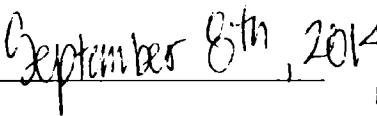
EXEMPT STATUS:

Exempt per Section 15183 of the California Environmental Quality Act (CEQA) Guidelines and California Public Resources Code Section 21083.3

DETERMINATION:

I do hereby certify that the above determination has been made pursuant to State and Local requirements.


 SARAH B. JONES
 Environmental Review Officer


 Date

cc: Mark Pilarczyk, Project Sponsor; Supervisor Malia Cohen, District 10; Richard Sucre, Current Planning Division; Virna Byrd, M.D.F.; Exemption/Exclusion File

PROJECT DESCRIPTION (continued):

The proposed project would provide approximately 10,552 square feet of common useable open space via a courtyard and roof top terrace. The project site is located within the Central Waterfront Plan Area on the block bounded by 19th, Tennessee, 20th and 3rd Streets.

PROJECT APPROVAL

The project is subject to a Large Project Authorization per Planning Code Section 329 for new construction over 25,000 gsf and requires discretionary review before the Planning Commission. The Planning Commission Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.

COMMUNITY PLAN EXEMPTION OVERVIEW:

California Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183 provide an exemption from environmental review for projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an Environmental Impact Report (EIR) was certified, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that: a) are peculiar to the project or parcel on which the project would be located; b) were not analyzed as significant effects in a prior EIR on the zoning action, general plan or community plan with which the project is consistent; c) are potentially significant off-site and cumulative impacts that were not discussed in the underlying EIR; or d) are previously identified in the EIR, but which, as a result of substantial new information that was not known at the time that the EIR was certified, are determined to have a more severe adverse impact than that discussed in the underlying EIR. Section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for the project solely on the basis of that impact.

This determination evaluates the potential project-specific environmental effects of the 815-825 Tennessee Street project described above, and incorporates by reference information contained in the Programmatic EIR for the Eastern Neighborhoods Rezoning and Area Plans (PEIR)¹. Project-specific studies were prepared for the proposed project to determine if the project would result in any significant environmental impacts that were not identified in the Eastern Neighborhoods PEIR.

After several years of analysis, community outreach, and public review, the Eastern Neighborhoods PEIR was adopted in December 2008. The Eastern Neighborhoods PEIR was adopted in part to support housing development in some areas previously zoned to allow industrial uses, while preserving an adequate supply of space for existing and future production, distribution, and repair (PDR) employment and businesses. The Eastern Neighborhoods PEIR also included changes to existing height and bulk districts in some areas, including the project site at 815-825 Tennessee Street.

The Planning Commission held public hearings to consider the various aspects of the proposed Eastern Neighborhoods Rezoning and Area Plans and related Planning Code and Zoning Map amendments. On

¹ Planning Department Case No. 2004.0160E and State Clearinghouse No. 2005032048

August 7, 2008, the Planning Commission certified the Eastern Neighborhoods PEIR by Motion 17659 and adopted the Preferred Project for final recommendation to the Board of Supervisors.^{2,3}

In December 2008, after further public hearings, the Board of Supervisors approved and the Mayor signed the Eastern Neighborhoods Rezoning and Planning Code amendments. New zoning districts include districts that would permit PDR uses in combination with commercial uses; districts mixing residential and commercial uses and residential and PDR uses; and new residential-only districts. The districts replaced existing industrial, commercial, residential single-use, and mixed-use districts.

The Eastern Neighborhoods PEIR is a comprehensive programmatic document that presents an analysis of the environmental effects of implementation of the Eastern Neighborhoods Rezoning and Area Plans, as well as the potential impacts under several proposed alternative scenarios. The Eastern Neighborhoods Draft EIR evaluated three rezoning alternatives, two community-proposed alternatives which focused largely on the Mission District, and a "No Project" alternative. The alternative selected, or the Preferred Project, represents a combination of Options B and C. The Planning Commission adopted the Preferred Project after fully considering the environmental effects of the Preferred Project and the various scenarios discussed in the PEIR.

A major issue of discussion in the Eastern Neighborhoods rezoning process was the degree to which existing industrially-zoned land would be rezoned to primarily residential and mixed-use districts, thus reducing the availability of land traditionally used for PDR employment and businesses. Among other topics, the Eastern Neighborhoods PEIR assesses the significance of the cumulative land use effects of the rezoning by analyzing its effects on the City's ability to meet its future PDR space needs as well as its ability to meet its housing needs as expressed in the City's General Plan.

As a result of the Eastern Neighborhoods rezoning process, the project site has been rezoned to UMU (Urban Mixed Use) District. The UMU District is intended to promote a vibrant mix of uses while maintaining the characteristics of this formerly industrially-zoned area. It is also intended to serve as a buffer between residential districts and PDR districts in the Eastern Neighborhoods. The proposed project and its relation to PDR land supply and cumulative land use effects is discussed further in the Community Plan Exemption (CPE) Checklist, under Land Use. The 815-825 Tennessee Street site, which is located in the Central Waterfront District of the Eastern Neighborhoods, was designated as a site with building up to 58 feet in height.

Individual projects that could occur in the future under the Eastern Neighborhoods Rezoning and Area Plans will undergo project-level environmental evaluation to determine if they would result in further impacts specific to the development proposal, the site, and the time of development and to assess whether additional environmental review would be required. This determination concludes that the proposed project at 815-825 Tennessee Street is consistent with and was encompassed within the analysis in the Eastern Neighborhoods PEIR. This determination also finds that the Eastern Neighborhoods PEIR adequately anticipated and described the impacts of the proposed 815-825 Tennessee Street project, and identified the mitigation measures applicable to the 815-825 Tennessee Street project. The proposed project is also consistent with the zoning controls and the provisions of the Planning Code applicable to

² San Francisco Planning Department. Eastern Neighborhoods Rezoning and Area Plans Final Environmental Impact Report (PEIR), Planning Department Case No. 2004.0160E, certified August 7, 2008. Available online at: <http://www.sf-planning.org/index.aspx?page=1893>, accessed August 17, 2012.

³ San Francisco Planning Department. San Francisco Planning Commission Motion 17659, August 7, 2008. Available online at: <http://www.sf-planning.org/Modules/ShowDocument.aspx?documentid=1268>, accessed August 17, 2012.

the project site.^{4,5} Therefore, no further CEQA evaluation for the 815-825 Tennessee Street project is required. In sum, the Eastern Neighborhoods PEIR and this Certificate of Exemption for the proposed project comprise the full and complete CEQA evaluation necessary for the proposed project.

PROJECT SETTING:

The project site is zoned Urban Mixed Use (UMU) and currently contains a two-story 32,000-square-foot industrial building historically used for manufacturing and food processing. The site is bounded to the north by 19th Street. Across 19th Street are a printing ink manufacturing building and a multi-story residential building. The site is bounded to the east by an industrial welding building and an automotive repair facility. The site is bounded to the south by a motorcycle repair and sales facility. The site is bounded to the west by Tennessee Street. Across Tennessee Street is a multi-story residential building.

POTENTIAL ENVIRONMENTAL EFFECTS:

The Eastern Neighborhoods PEIR included analyses of environmental issues including: land use; plans and policies; visual quality and urban design; population, housing, business activity, and employment (growth inducement); transportation; noise; air quality; parks, recreation and open space; shadow; archeological resources; historic architectural resources; hazards; and other issues not addressed in the previously issued initial study for the Eastern Neighborhoods Rezoning and Area Plans. The proposed 815-825 Tennessee Street project is in conformance with the height, use and density for the site described in the Eastern Neighborhoods PEIR and would represent a small part of the growth that was forecast for the Eastern Neighborhoods plan areas. Thus, the plan analyzed in the Eastern Neighborhoods PEIR considered the incremental impacts of the proposed 815-825 Tennessee Street project. As a result, the proposed project would not result in any new or substantially more severe impacts than were identified in the Eastern Neighborhoods PEIR.

Significant and unavoidable impacts were identified for the following topics: land use, historic architectural resources, transportation and circulation, and shadow. The proposed project would remove an existing PDR use due to the partial demolition of the existing industrial building and would therefore contribute to the significant and unavoidable land use impact relative to loss of PDR uses. The project would result in demolition of a historic building constructed before 1964, and would result in construction over 50 feet in height. Therefore, the project would contribute to the significant and unavoidable historic resource impact. Traffic and transit ridership generated by the project would not considerably contribute to the traffic and transit impacts identified in the Eastern Neighborhoods PEIR. As the project would exceed 40 feet in height but would not cast new shadow on nearby parks, the project would not be expected to have a shadow impact on surrounding parks.

The Eastern Neighborhoods PEIR identified feasible mitigation measures to address significant impacts related to noise, air quality, archeological resources, historical resources, hazardous materials, and transportation. **Table 1** below lists the mitigation measures identified in the Eastern Neighborhoods PEIR and states whether each measure would apply to the proposed project.

⁴ Adam Varat, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Citywide Planning and Policy Analysis, 815-825 Tennessee Street, January 24, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2013.0220E.

⁵ Jeff Joslin, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Current Planning Analysis, 815-825 Tennessee Street, July 18, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2013.0220E.

Table 1 – Eastern Neighborhoods PEIR Mitigation Measures

Mitigation Measure	Applicability
F. Noise	
F-1: Construction Noise (Pile Driving)	Not Applicable: project would not include pile driving.
F-2: Construction Noise	Applicable: temporary construction noise would occur from the use of heavy equipment.
F-3: Interior Noise Levels	Applicable: project is subject to California Noise Insulation Standards in Title 24. The requirements of this mitigation measure have been completed during the environmental review process through the preparation of a noise study.
F-4: Siting of Noise-Sensitive Uses	Applicable: project would add noise sensitive uses in areas where noise levels exceed 60 dBA (Ldn). The requirements of this mitigation measure have been completed during the environmental review process through the preparation of a noise study.
F-5: Siting of Noise-Generating Uses	Not Applicable: project is not proposing a noise-generating use.
F-6: Open Space in Noisy Environments	Applicable: project includes open space in a noisy environment
G. Air Quality	
G-1: Construction Air Quality	Not Applicable: project would comply with the San Francisco Dust Control Ordinance.
G-2: Air Quality for Sensitive Land Uses	Not Applicable: project is not in the Air Pollutant Exposure Zone.
G-3: Siting of Uses that Emit DPM	Not Applicable: proposed residential land uses are not uses that would emit substantial levels of DPM.
G-4: Siting of Uses that Emit other TACs	Not Applicable: proposed residential land uses are not uses that would emit substantial levels of other TACs.
J. Archeological Resources	
J-1: Properties with Previous Studies	Not Applicable: project site does not contain any previous archeological studies.
J-2: Properties with no Previous Studies	Applicable: project site is located in an area

Mitigation Measure	Applicability
	with no previous archeological studies. As a result, in compliance with Mitigation Measure J-2, the project contractor would be required to be on alert for archeological resources throughout the construction period.
J-3: Mission Dolores Archeological District	Not Applicable: project site is not located within the Mission Dolores Archeological District.
K. Historical Resources	
K-1: Interim Procedures for Permit Review in the Eastern Neighborhoods Plan Area	Not Applicable: plan-level mitigation completed by Planning Department
K-2: Amendments to Article 10 of the Planning Code Pertaining to Vertical Additions in the South End Historic District (East SoMa)	Not Applicable: plan-level mitigation completed by Planning Commission
K-3: Amendments to Article 10 of the Planning Code Pertaining to Alterations and Infill Development in the Dogpatch Historic District (Central Waterfront)	Not Applicable: plan-level mitigation completed by Planning Commission
L. Hazardous Materials	
L-1: Hazardous Building Materials	Applicable: project involves the partial demolition of an industrial building.
E. Transportation	
E-1: Traffic Signal Installation	Not Applicable: plan level mitigation by SFMTA
E-2: Intelligent Traffic Management	Not Applicable: plan level mitigation by SFMTA
E-3: Enhanced Funding	Not Applicable: plan level mitigation by SFMTA & SFTA
E-4: Intelligent Traffic Management	Not Applicable: plan level mitigation by SFMTA & Planning Department
E-5: Enhanced Transit Funding	Not Applicable: plan level mitigation by SFMTA
E-6: Transit Corridor Improvements	Not Applicable: plan level mitigation by SFMTA
E-7: Transit Accessibility	Not Applicable: plan level mitigation by SFMTA
E-8: Muni Storage and Maintenance	Not Applicable: plan level mitigation by

Mitigation Measure	Applicability
	SFMTA
E-9: Rider Improvements	Not Applicable: plan level mitigation by SFMTA
E-10: Transit Enhancement	Not Applicable: plan level mitigation by SFMTA
E-11: Transportation Demand Management	Not Applicable: plan level mitigation by SFMTA

Please see the attached Mitigation Monitoring and Reporting Program (MMRP) for the complete text of the applicable mitigation measures. With implementation of these mitigation measures the proposed project would not result in significant impacts beyond those analyzed in the Eastern Neighborhoods PEIR.

PUBLIC NOTICE AND COMMENT

A “Notification of Project Receiving Environmental Review” was mailed on January 6, 2014 to adjacent occupants and owners of properties within 300 feet of the project site. Overall, concerns and issues raised by the public in response to the notice were taken into consideration and incorporated in the environmental review as appropriate for CEQA analysis. Two comments were received regarding physical environmental effects. These comments were related to parking and the proposed building’s height and bulk, in that the proposed building would be taller than the existing building and could affect natural light on nearby private property and buildings. The proposed project would not result in significant adverse environmental impacts associated with the issues identified by the public beyond those identified in the Eastern Neighborhoods PEIR.

CONCLUSION

As summarized above and further discussed in the CPE Checklist⁶:

1. The proposed project is consistent with the development density established for the project site in the Eastern Neighborhoods Rezoning and Area Plans;
2. The proposed project would not result in effects on the environment that are peculiar to the project or the project site that were not identified as significant effects in the Eastern Neighborhoods PEIR;
3. The proposed project would not result in potentially significant off-site or cumulative impacts that were not identified in the Eastern Neighborhoods PEIR;
4. The proposed project would not result in significant effects, which, as a result of substantial new information that was not known at the time the Eastern Neighborhoods PEIR was certified, would be more severe than were already analyzed and disclosed in the PEIR; and

⁶ The CPE Checklist is available for review at the Planning Department, 1650 Mission Street, Suite 400, San Francisco, in Case File No. 2013.0220E.

5. The project sponsor will undertake feasible mitigation measures specified in the Eastern Neighborhoods PEIR to mitigate project-related significant impacts.

Therefore, the proposed project is exempt from further environmental review pursuant to Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183.

Attachment A: MITIGATION MONITORING AND REPORTING PROGRAM (Includes Text for Adopted Mitigation Measures)				
MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
MITIGATION MEASURES FROM THE EASTERN NEIGHBORHOODS AREA PLAN EIR				
<p>PMM-1 – Construction Noise (Mitigation Measure F-2 of the Eastern Neighborhoods PEIR). Where environmental review of a development project undertaken subsequent to the adoption of the proposed zoning controls determines that construction noise controls are necessary due to the nature of planned construction practices and the sensitivity of proximate uses, the Planning Director shall require that the sponsors of the subsequent development project develop a set of site-specific noise attenuation measures under the supervision of a qualified acoustical consultant. Prior to commencing construction, a plan for such measures shall be submitted to the Department of Building Inspection to ensure that maximum feasible noise attenuation will be achieved. These attenuation measures shall include as many of the following control strategies as feasible:</p> <ul style="list-style-type: none"> • Erect temporary plywood noise barriers around a construction site, particularly where a site adjoins noise-sensitive uses. • Utilize noise control blankets on a building structure as the building is erected to reduce noise emission from the site. • Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings housing sensitive uses. • Monitor the effectiveness of noise attenuation measures by taking noise measurements. • Post signs on-site pertaining to permitted construction days and hours and complaint procedures and who to notify in the event of a problem, with telephone numbers listed. 	Project sponsor	Prior to and during construction.	Project sponsor, contractor(s), shall provide Department of Building Inspection and the Planning Department with monthly reports during construction period.	Considered complete upon receipt of final monitoring report at completion of construction.

Attachment A: MITIGATION MONITORING AND REPORTING PROGRAM (Includes Text for Adopted Mitigation Measures)				
MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>PMM-2 – Open Space in Noisy Environments (Mitigation Measure F-6 of the Eastern Neighborhoods PEIR). To minimize effects on development in noisy areas, for new development including noise-sensitive uses, the Planning Department shall, through its building permit review process, in conjunction with noise analysis required pursuant to Eastern Neighborhoods Mitigation Measure F-4, require that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design.</p>	Project sponsor	Prior to construction	Department of Building Inspection and Planning Department.	Considered complete upon approval of final construction drawing set.
<p>PMM-3 – Hazardous Building Materials (Mitigation Measure L-1 of the Eastern Neighborhoods PEIR). The project sponsor shall ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.</p>	Project sponsor/ contractor	Prior to demolition of structures.	Project Sponsor/contractor shall submit a monitoring report to the Department of Public Health and Planning.	Considered complete upon receipt of final monitoring report.

Attachment A: MITIGATION MONITORING AND REPORTING PROGRAM (Includes Text for Adopted Mitigation Measures)				
MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
IMPROVEMENT MEASURES FROM COMMUNITY PLAN EXEMPTION FOR 815-825 TENNESSEE STREET				
<p>PIM-1 – Monitoring and Abatement of Queues. As an improvement measure to reduce the potential for queuing of vehicles accessing the project site, it shall be the responsibility of the project sponsor/property owner to ensure that recurring vehicle queues do not occur on 19th or Tennessee Streets adjacent to the site. A vehicle queue is defined as one or more vehicles (destined to the underground parking garage) blocking any portion of the 19th Street sidewalk or travel lane on 19th Street, or along Tennessee Street for a consecutive period of three minutes or longer on a daily and/or weekly basis. Because the proposed project would include a new off-street parking facility with more than 20 parking spaces (excluding loading and car-share spaces), the project is subject to conditions of approval set forth by the San Francisco Planning Department to address the monitoring and abatement of queues.</p>	<p>Project sponsor/ property owner</p>	<p>Ongoing during building operations.</p>	<p>Planning Department</p>	<p>Ongoing during building operations.</p>
<p>It shall be the responsibility of the owner/operator of any off-street parking facility with more than 20 parking spaces (excluding loading and car-share spaces) to ensure that recurring vehicle queues do not occur on the public right-of-way. A vehicle queue is defined as one or more vehicles (destined to the parking facility) blocking any portion of any public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis. If a recurring queue occurs, the owner/operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable).</p>				

Attachment A: MITIGATION MONITORING AND REPORTING PROGRAM (Includes Text for Adopted Mitigation Measures)				
MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as additional bicycle parking, customer shuttles, delivery services; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.</p> <p>If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Department shall notify the property owner in writing. Upon request, the owner/operator shall hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant shall prepare a monitoring report to be submitted to the Department for review. If the Department determines that a recurring queue does exist, the facility owner/operator shall have 90 days from the date of the written determination to abate the queue.</p>				
<p>PIM-2 – Installation of Visual/Audible Devices at Underground Garage Driveway. As an improvement measure to reduce potential conflicts between vehicles entering and exiting the underground garage and pedestrians traveling along the south side sidewalk of 19th Street, the Project Sponsor shall install visual and/or audible notifications (alarms) to alert pedestrians of vehicles traveling in and out of the underground parking garage.</p>	Project Sponsor	Design measures to be incorporated into project design; prior to issuance of a building permit.	Planning Department	Considered complete upon installation.

Attachment A: MITIGATION MONITORING AND REPORTING PROGRAM (Includes Text for Adopted Mitigation Measures)				
MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>PIM-3 – Installation of Crosswalks along 19th Street and Tennessee Street. As an improvement measure to reduce potential conflicts between pedestrians and other users of 19th and Tennessee Streets (e.g., autos, bus transit vehicles, freight/delivery vehicles, bicyclists, etc.), the project sponsor shall provide crosswalks to enhance pedestrian circulation and safe passage of pedestrians walking to and from the project site and reduce and/or eliminate any vehicle-pedestrian conflicts. The project sponsor shall provide two high-visibility crosswalks (e.g., continental, transverse, and/or ladder marking pattern) at the intersection of 19th Street and Tennessee Street, specifically along the east leg of the intersection (along 19th Street) and the south leg of the intersection (along Tennessee Street). Installation of crosswalks will provide enhanced pedestrian circulation, connectivity between the project site and adjacent uses, and enhance driver awareness of pedestrian activity at these locations.</p> <p>The crosswalks shall be constructed in accordance with the <i>California Manual on Uniform Traffic Control Devices</i> (MUTCD). These new crosswalks may require approvals by the San Francisco Planning Department, Department of Public Works, and SFMTA’s Livable Streets Subdivision, as appropriate.</p>	Project Sponsor	Design measures to be incorporated into project design; prior to issuance of a building permit.	Planning Department; SFMTA; Public Works	Considered complete upon installation.
<p>PIM-4 – Coordination of Move-in/Move-Out Operations and Large Deliveries. To reduce the potential for parking of delivery vehicles within the travel lane adjacent to the curb lane on 19th Street (in the event that the on-street loading is occupied), residential move-in and move-out activities and larger deliveries shall be scheduled and coordinated through building management. Appropriate move-in/move-out procedures shall be enforced to avoid any blockages of 19th Street over an extended period of time and reduce any potential conflicts between movers and pedestrians walking along 19th Street. Curb parking on 19th Street shall be reserved through SFMTA or by directly contacting the local 311 service.</p>	Project sponsor/ property owner	Ongoing during building operations.	Planning Department	Ongoing during building operations.

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MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>PIM-5 – Construction Management Plan. In addition to items required in the Construction Management Plan, the project sponsor shall include the following:</p> <ul style="list-style-type: none"> • <i>Carpool and Transit Access for Construction Workers</i> – As an improvement measure to minimize parking demand and vehicle trips associated with construction workers, the construction contractor shall include methods to encourage carpooling and transit use to the project site by construction workers in the Construction Management Plan contracts. • <i>Project Construction Updates</i> – As an improvement measure to minimize construction impacts on nearby businesses, the project sponsor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction inquiries or concerns. 	Project sponsor/ project contractor(s)	Prior to and during construction.	Project sponsor, contractor(s)	Considered complete upon completion of construction.



SAN FRANCISCO PLANNING DEPARTMENT

Community Plan Exemption Checklist

Case No.: 2013.0220E
Project Address: 815-825 Tennessee Street
Zoning: UMU (Urban Mixed Use) District
58-X Height and Bulk District
Block/Lot: 4059/001A, 001B
Lot Size: 2 lots totaling 20,000 square feet
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PROJECT DESCRIPTION

The proposed project includes demolition of an existing two-story 32,000-square-foot industrial building built in 1926 and construction of a five-story, 58-foot-tall, 74,359-square-foot residential apartment building with basement garage. A portion of the original industrial building that contains the principal brick façade will remain to serve as a visual reminder of the building's original use and the industrial history of the neighborhood. The residential building would accommodate approximately 69 dwelling units. The 19,579 square-foot basement level garage would contain 48 off-street parking spaces and 70 bicycle spaces. The proposed project would provide approximately 10,552 square feet of common useable open space via a courtyard and roof top terrace. The project site is located within the Central Waterfront Plan Area on the block bounded by 19th, Tennessee, 20th and 3rd Streets. See Figures 1 and 2 below for the project location and proposed design.

The proposed 815-825 Tennessee Street project would require the following approvals:

Actions by the Planning Commission

- Large Project Authorization per Planning Code Section 329 for new construction over 25,000 gsf.

Actions by the Department of Building Inspection

- Building Permit for demolition of the existing warehouse.
- Building Permit for construction of new residential building.

The Planning Commission Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.

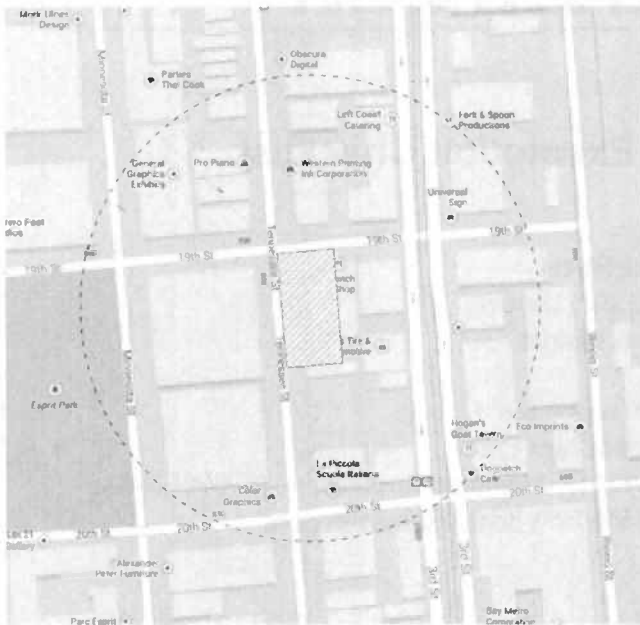


Figure 1: Project Location



Figure 2: Proposed Project Design

EVALUATION OF ENVIRONMENTAL EFFECTS

This Community Plan Exemption (CPE) Checklist evaluates whether the environmental impacts of the proposed project are addressed in the Programmatic Environmental Impact Report for the Eastern Neighborhoods Rezoning and Area Plans (Eastern Neighborhoods PEIR).¹ The CPE Checklist indicates whether the proposed project would result in significant impacts that: (1) are peculiar to the project or project site; (2) were not identified as significant project-level, cumulative, or off-site effects in the PEIR;

¹ San Francisco Planning Department, Eastern Neighborhoods Rezoning and Area Plans Final Environmental Impact Report (PEIR), Planning Department Case No. 2004.0160E, State Clearinghouse No. 2005032048, certified August 7, 2008. Available online at: <http://www.sf-planning.org/index.aspx?page=1893>, accessed August 17, 2012.

or (3) are previously identified significant effects, which as a result of substantial new information that was not known at the time that the Eastern Neighborhoods PEIR was certified, are determined to have a more severe adverse impact than discussed in the PEIR. Such impacts, if any, will be evaluated in a project-specific Mitigated Negative Declaration or Environmental Impact Report. If no such impacts are identified, the proposed project is exempt from further environmental review in accordance with Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183.

Mitigation measures identified in the PEIR are discussed under each topic area, and measures that are applicable to the proposed project are provided under the Mitigation Measures Section at the end of this checklist.

The Eastern Neighborhoods PEIR identified significant impacts related to land use, transportation, cultural resources, shadow, noise, air quality, and hazardous materials. Additionally, the PEIR identified significant cumulative impacts related to land use, transportation, and cultural resources. Mitigation measures were identified for the above impacts and reduced all impacts to less-than-significant except for those related to land use (cumulative impacts on PDR use), transportation (program-level and cumulative traffic impacts at nine intersections; program-level and cumulative transit impacts on seven Muni lines), cultural resources (cumulative impacts from demolition of historical resources), and shadow (program-level impacts on parks).

The proposed project would include construction of a five-story, 58-foot-tall, 74,359-square-foot residential apartment building with basement garage. As discussed below in this checklist, the proposed project would not result in new, significant environmental effects, or effects of greater severity than were already analyzed and disclosed in the Eastern Neighborhoods PEIR.

AESTHETICS AND PARKING IMPACTS FOR TRANSIT PRIORITY INFILL DEVELOPMENT

Public Resources Code Section 21099(d), effective January 1, 2014, provides that, "aesthetics and parking impacts of a residential, mixed-use residential, or employment center project on an infill site located within a transit priority area shall not be considered significant impacts on the environment." Accordingly, aesthetics and parking are no longer to be considered in determining if a project has the potential to result in significant environmental effects for projects that meet all of the following three criteria:

- a) The project is in a transit priority area;
- b) The project is on an infill site; and
- c) The project is residential, mixed-use residential, or an employment center.

The proposed project meets each of the above three criteria and thus, this checklist does not consider aesthetics or parking in determining the significance of project impacts under CEQA.² Project elevations are included in the project description, and an assessment of parking demand is included in the Transportation section for informational purposes.

² San Francisco Planning Department. Transit-Oriented Infill Project Eligibility Checklist for 815-825 Tennessee Street, July 9, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2013.0220E.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
1. LAND USE AND LAND USE PLANNING— Would the project:				
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial impact upon the existing character of the vicinity?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that adoption of the Area Plans would result in an unavoidable significant impact on land use due to the cumulative loss of PDR. Development of the proposed project would result in the net loss of approximately 32,000 square feet of PDR building space that is proximate to other PDR businesses and would therefore contribute to the significant cumulative land use impact related to loss of PDR uses that was identified in the Eastern Neighborhoods PEIR.

As a result of the Eastern Neighborhoods rezoning process, the project site has been rezoned to UMU (Urban Mixed Use) District. The UMU District is intended to promote a vibrant mix of uses while maintaining the characteristics of this formerly industrially-zoned area. It is also intended to serve as a buffer between residential districts and PDR districts in the Eastern Neighborhoods. The 815-825 Tennessee site, which is located in the Central Waterfront District of the Eastern Neighborhoods, was designated as a site with building up to 58 feet in height. The proposed project would be consistent with the zoning controls and the provisions of the Planning Code applicable to the project site.^{3,4} As a result, the proposed project would not conflict with any land use plans, policies, or regulations adopted for the purpose of avoiding or mitigating an environmental effect.

For these reasons, implementation of the proposed project would not result in significant impacts that were not identified in the Eastern Neighborhoods PEIR related to land use and land use planning, and no mitigation measures are necessary.

³ Adam Varat, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Citywide Planning and Policy Analysis, 815-825 Tennessee Street, January 24, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2013.0220E.

⁴ Jeff Joslin, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Current Planning Analysis, 815-825 Tennessee Street, July 18, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2013.0220E.

<u>Topics:</u>	<u>Significant Impact Peculiar to Project or Project Site</u>	<u>Significant Impact not Identified in PEIR</u>	<u>Significant Impact due to Substantial New Information</u>	<u>No Significant Impact not Previously Identified in PEIR</u>
2. POPULATION AND HOUSING— Would the project:				
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Displace substantial numbers of existing housing units or create demand for additional housing, necessitating the construction of replacement housing?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

One of the objectives of the Eastern Neighborhoods Area Plans is to identify appropriate locations for housing in the City’s industrially zoned land to meet the citywide demand for additional housing. The PEIR concluded that an increase in population in the Plan Areas is expected to occur as a secondary effect of the proposed rezoning and that any population increase would not, in itself, result in adverse physical effects, but would serve to advance key City policy objectives, such as providing housing in appropriate locations next to Downtown and other employment generators and furthering the City’s Transit First policies. It was anticipated that the rezoning would result in an increase in both housing development and population in all of the Area Plan neighborhoods. The Eastern Neighborhoods PEIR determined that the anticipated increase in population and density would not result in significant adverse physical effects on the environment. No mitigation measures were identified in the PEIR.

The project would result in 69 new residential units and would increase the number of residents living within the Central Waterfront District of the Eastern Neighborhoods. These direct effects of the proposed project on population and housing are within the scope of the population growth anticipated under the Eastern Neighborhoods Area Plan and evaluated in the Eastern Neighborhoods PEIR.

For the above reasons, the proposed project would not result in significant impacts on population and housing that were not identified in the Eastern Neighborhoods PEIR.

<u>Topics:</u>	<u>Significant Impact Peculiar to Project or Project Site</u>	<u>Significant Impact not Identified in PEIR</u>	<u>Significant Impact due to Substantial New Information</u>	<u>No Significant Impact not Previously Identified in PEIR</u>
3. CULTURAL AND PALEONTOLOGICAL RESOURCES—Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5, including those resources listed in Article 10 or Article 11 of the San Francisco Planning Code?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Topics:	Significant Impact Peculiar to Project or Project Site	Significant Impact not Identified in PEIR	Significant Impact due to Substantial New Information	No Significant Impact not Previously Identified in PEIR
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Historic Architectural Resources

Pursuant to CEQA Guidelines Sections 15064.5(a)(1) and 15064.5(a)(2), historical resources are buildings or structures that are listed, or are eligible for listing, in the California Register of Historical Resources or are identified in a local register of historical resources, such as Articles 10 and 11 of the San Francisco Planning Code. The Eastern Neighborhoods PEIR determined that future development facilitated through the changes in use districts and height limits under the Eastern Neighborhoods Area Plan could have substantial adverse changes on the significance of both individual historical resources and on historical districts within the Plan Area. The PEIR determined that approximately 32 percent of the known or potential historical resources in the Plan Area could potentially be affected under the preferred alternative. The Eastern Neighborhoods PEIR found this impact to be significant and unavoidable. This impact was addressed in a Statement of Overriding Considerations with findings and adopted as part of the Eastern Neighborhoods Rezoning and Area Plans approval on January 19, 2009.

The project at 815-825 Tennessee Street proposes to demolish most of the existing two-story industrial building constructed of unreinforced masonry in 1926 and construct a new 5-story residential building. A small portion of the original 1926 building, the northwest corner that contains the original main façade, would be retained. The industrial building was identified as a potential historic resource in Figure 30: Historic Resources in the Eastern Neighborhoods of the Eastern Neighborhoods PEIR. As noted in the 2008 Update to the Central Waterfront Historic Resource Survey, 815-825 Tennessee Street were assigned a California Historic Resource Status Code of "5B," which defines the properties as "locally significant both individually (listed, eligible or appears eligible) and as a contributor to a district that is locally listed, designated, determined eligible or appears eligible through survey evaluation." 815-825 Tennessee Street is a contributing resource to the Central Waterfront/Third Street Industrial Historic District, which is considered to be a historic resource for the purposes of CEQA.

The Planning Department reviewed the Historic Resource Evaluation (dated June 10, 2014) and the Supplemental to the Historic Resource Evaluation (dated June 27, 2014) prepared for the project and concurs with the findings and conclusions of these reports.⁵

According to the reports, the partial demolition of 815-825 Tennessee Street would not materially impair the eligibility of the Central Waterfront/Third Street Industrial Historic District for the California Register of Historical Resources. The surrounding district will still possess thirteen of the original sixteen contributing resources identified in the DPR 523D form (dated March 2008). 815-825 Tennessee Street is located at the edge of the district boundaries, and is one of several company buildings/former factories. Therefore, the demolition of 815-825 Tennessee Street would not cause a substantial adverse impact upon

⁵ Rich Sucre, San Francisco Planning Department. *Preservation Team Review Form: 815-825 Tennessee Street (includes review of Final Historic Resource Evaluation Report for Proposed Infill Construction at 815-825 Tennessee Street, San Francisco, prepared by Tetra Tech, Inc. [June 10, 2014] and Supplemental to Final HRE for Proposed Infill Construction [June 27, 2014]).* July 16, 2014. This report is available for review as part of Case No. 2013.0220E.

the surrounding eligible historic district. Further, the proposed new construction appears to be in general conformity with the Secretary of the Interior's Standards for Rehabilitation and appears to address district characteristics, including the district's predominant mass, height, and materiality. The project appears to be compatible, yet differentiated from the district contributors. Therefore, the proposed new construction would not cause a substantial adverse impact upon the significance of the Central Waterfront/Third Street Industrial Historic District. Finally, the demolition and new construction on the project site would not cause a substantial adverse impact upon any qualified historic resource in the vicinity of the project site, including the Dogpatch Landmark District. The project site is located outside of the boundaries of the Dogpatch Landmark District, and would not physically impact any resource located within the district boundaries. No historic resource mitigation measures would apply to the proposed project.

For these reasons, the proposed project would not result in significant impacts on historic architectural resources that were not identified in the Eastern Neighborhoods PEIR.

Archeological Resources

The Eastern Neighborhoods PEIR determined that implementation of the Area Plan could result in significant impacts on archeological resources and identified three mitigation measures that would reduce these potential impacts to a less than significant level. Eastern Neighborhoods PEIR Mitigation Measure J-1 applies to properties for which a final archeological research design and treatment plan is on file at the Northwest Information Center and the Planning Department. Mitigation Measure J-2 applies to properties for which no archeological assessment report has been prepared or for which the archeological documentation is incomplete or inadequate to serve as an evaluation of potential effects on archeological resources under CEQA. Mitigation Measure J-3, which applies to properties in the Mission Dolores Archeological District, requires that a specific archeological testing program be conducted by a qualified archeological consultant with expertise in California prehistoric and urban historical archeology.

The proposed project at 815-825 Tennessee Street would involve soil disturbance and approximately 12-14 feet of below-grade excavation in an area where no previous archeological studies have been prepared. Therefore the project would be subject to Mitigation Measure J-2. The Department conducted a Preliminary Archeological Review (PAR) of the project and determined that it would not have the potential to adversely affect archeological resources on the site.⁶ The PAR fulfills Mitigation Measure J-2, and no additional review or project mitigation is required. Therefore, the proposed project would not result in any significant impacts on archeological resources that were not identified in the Eastern Neighborhoods PEIR.

For these reasons, the proposed project would not result in significant impacts on cultural and paleontological resources that were not identified in the Eastern Neighborhoods PEIR.

⁶ Randall Dean, San Francisco Planning Department. *Environmental Planning Preliminary Archeological Review: 815-825 Tennessee Street*. January 16, 2014. This report is available for review as part of Case No. 2013.0220E.

<u>Topics:</u>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
4. TRANSPORTATION AND CIRCULATION— Would the project:				
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with an applicable congestion management program, including but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels, obstructions to flight, or a change in location, that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR anticipated that growth resulting from the zoning changes would not result in significant impacts related to pedestrians, bicyclists, loading, emergency access, or construction. As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on pedestrians, bicyclists, loading, emergency access, or construction beyond those analyzed in the Eastern Neighborhoods PEIR.

However, the Eastern Neighborhoods PEIR anticipated that growth resulting from the zoning changes could result in significant impacts on traffic and transit ridership, and identified 11 transportation mitigation measures. Even with mitigation, however, it was anticipated that the significant adverse cumulative traffic impacts and the cumulative impacts on transit lines could not be fully mitigated. Thus, these impacts were found to be significant and unavoidable.

The project site is not located within an airport land use plan area, or in the vicinity of a private airstrip. Therefore, the Community Plan Exemption Checklist topic 4c is not applicable.

Trip Generation

The proposed project would construct a five-story, 58-foot-tall, 74,359-square-foot residential apartment building with basement garage. The residential building would accommodate approximately 69 dwelling units. The 19,579 square-foot basement level garage would contain 48 off-street parking spaces and 70 bicycle spaces.

A Transportation Impact Study was prepared for the proposed project.⁷ The following discussion relies on the information provided in the Transportation Impact Study. Trip generation of the proposed project was calculated using information in the *2002 Transportation Impacts Analysis Guidelines for Environmental Review* (SF Guidelines) developed by the San Francisco Planning Department. The proposed project would generate an estimated 108 p.m. peak-hour person-trips (inbound and outbound) on a weekday daily basis, consisting of 58 person trips by auto, 33 transit trips, 4 walk trips and 11 trips by other modes (accounting for vehicle occupancy data for this Census Tract).

Traffic

The proposed project's vehicle trips would travel through the intersections surrounding the project block. Intersection operating conditions are characterized by the concept of Level of Service (LOS), which ranges from A to F and provides a description of an intersection's performance based on traffic volumes, intersection capacity, and vehicle delays. LOS A represents free flow conditions, with little or no delay, while LOS F represents congested conditions, with extremely long delays; LOS D (moderately high delays) is considered the lowest acceptable level in San Francisco. The intersections near the project site (within approximately 1,500 feet) include Third Street/Mariposa Street, Third Street/19th Street, Third Street/20th Street, Third Street/22nd Street, Tennessee Street/19th Street, Tennessee Street/20th Street, Mariposa Street/I-280 Northbound Off-Ramp, and Mariposa Street/I-280 Southbound On-Ramp. Table 1 provides existing LOS data gathered for these intersections, per the 2290 Third Street Final Transportation Impact Study.⁸

Table 1

<u>Intersection</u>	<u>Existing LOS (2013)</u>
Third/Mariposa	C
Third/19 th	A
Third/20 th	C
Third/22 nd	A
Tennessee/19 th	A
Tennessee/20 th	A
Mariposa/I-280N Ramp	D
Mariposa/I-280S Ramp	F

Sources: LOS is for the year 2013 (2290 Third Street Final Transportation Impact Study).

The proposed project would generate an estimated 53 new p.m. peak-hour vehicle trips that could travel through surrounding intersections. This amount of new p.m. peak-hour vehicle trips would not substantially increase traffic volumes at these or other nearby intersections, would not substantially increase average delay that would cause intersections that currently operate at acceptable LOS to deteriorate to unacceptable LOS, or would not substantially increase average delay at intersections that currently operate at unacceptable LOS.

The Eastern Neighborhoods PEIR determined that each of the rezoning options would have significant and unavoidable cumulative (2025) impacts relating to weekday p.m. peak-hour traffic conditions, with the Preferred Project having significant impacts at several intersections. Specific mitigation measure were

⁷ Transportation Impact Study for 815-825 Tennessee Street, CHS Consulting Group, May 9, 2014. This document is on file and available for public review at the Planning Department, 1650 Mission Street, Suite 400.

⁸ LOS is for the year 2013 (2290 Third Street Final Transportation Impact Study). Traffic documents are available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of their respective case file numbers.

not proposed for these intersections, but general mitigation measures were proposed for the entire Plan Area, which include intelligent traffic management, enhanced transportation funding, and parking management to discourage driving.

The proposed project would not contribute considerably to these conditions as its contribution of an estimated 53 new p.m. peak-hour vehicle trips would not be a substantial proportion of the overall traffic volume or of the new vehicle trips generated by Eastern Neighborhoods Area Plan projects. At the unsignalized intersection of Mariposa Street and I-280 southbound on-ramp, during the p.m. peak-hour, the east bound Mariposa Street approach would operate at LOS F under Existing plus Project conditions with or without the proposed project. The proposed project would add two vehicles to the critical eastbound through movement. This project-related contribution would represent less than one percent of the total p.m. peak-hour volume at this poorly operating intersection and would therefore not be considered substantial. The proposed project would also not contribute considerably to 2025 cumulative conditions and thus, the proposed project would not have any significant cumulative traffic impacts.

Although the proposed project would not result in any potential adverse effects to existing traffic patterns, Project Improvement Measure 1: Monitoring and Abatement of Queues would further reduce the proposed project's less-than-significant impacts related to vehicular access to the project site by requiring monitoring of the project access driveway on 19th Street, and if a recurring queue occurs, the Project Sponsor/Property Owner shall employ abatement methods as needed to abate the queue.

For the above reasons, the proposed project would not result in significant impacts on traffic that were not identified in the Eastern Neighborhoods PEIR.

Transit

The project site is located within a quarter mile of several local transit lines including Muni lines 22 Fillmore, 48 Quintara-24th, and T Third. The proposed project would be expected to generate 179 daily transit trips, including 33 during the p.m. peak-hour. Given the wide availability of nearby transit, the addition of 33 p.m. peak-hour transit trips would be accommodated by existing capacity. As such, the proposed project would not result in unacceptable levels of transit service or cause a substantial increase in delays or operating costs such that significant adverse impacts in transit service could result.

Each of the rezoning options in the Eastern Neighborhoods PEIR identified significant and unavoidable cumulative impacts relating to increases in transit ridership on Muni lines, with the Preferred Project having significant impacts on seven lines. Of those lines, the project site is located within a quarter-mile of Muni line 22 Fillmore. Mitigation measures E-5, E-6, E-7, E-8, and E-9 would address these impacts for Muni lines in the Eastern Neighborhoods by pursuing enhanced transit funding; conducting transit corridor and service improvements; increasing transit accessibility, expanding storage/maintenance capabilities; and providing service information. Even with mitigation, however, cumulative impacts on the above lines were found to be significant and unavoidable and a Statement of Overriding Considerations related to the significant and unavoidable cumulative transit impacts was adopted as part of the PEIR Certification and project approval.

The proposed project would not contribute considerably to these conditions as its contribution of 33 p.m. peak-hour transit trips would not be a substantial proportion of the overall additional transit volume generated by Eastern Neighborhood projects. The proposed project would also not contribute considerably to 2025 cumulative transit conditions and thus would not result in any significant cumulative transit impacts.

For the above reasons, the proposed project would not result in significant impacts that were not identified in the Eastern Neighborhoods PEIR related to transit and would not contribute considerably to cumulative transit impacts that were identified in the Eastern Neighborhoods PEIR.

Parking

Public Resources Code Section 21099(d), effective January 1, 2014, provides that, “aesthetics and parking impacts of a residential, mixed-use residential, or employment center project on an infill site located within a transit priority area shall not be considered significant impacts on the environment.” Accordingly, aesthetics and parking are no longer to be considered in determining if a project has the potential to result in significant environmental effects for projects that meet all of the following three criteria:

- a) The project is in a transit priority area;
- b) The project is on an infill site; and
- c) The project is residential, mixed-use residential, or an employment center.

The proposed project meets each of the above three criteria and thus, this determination does not consider the adequacy of parking in determining the significance of project impacts under CEQA.⁹ The Planning Department acknowledges that parking conditions may be of interest to the public and the decision makers. Therefore, the following parking demand analysis is provided for informational purposes only.

The parking demand for the new residential and retail uses associated with the proposed project was determined based on the methodology presented in the Transportation Guidelines. On an average weekday, the demand for parking would be for 93 spaces. The proposed project would provide 48 off-street spaces. Thus, as proposed, the project would have an unmet parking demand of an estimated 45 spaces. At this location, the unmet parking demand could be accommodated within existing on-street and off-street parking spaces within a reasonable distance of the project vicinity. Additionally, the project site is well served by public transit and bicycle facilities. Therefore, any unmet parking demand associated with the project would not materially affect the overall parking conditions in the project vicinity such that hazardous conditions or significant delays would be created.

Further, the project site is located in a UMU zoning district where under Section 151.1 of the Planning Code, the proposed project would not be required to provide any off-street parking spaces. It should be noted that the Planning Commission has the discretion to adjust the number of on-site parking spaces included in the proposed project, typically at the time that the project entitlements are sought. The Planning Commission may not support the parking ratio proposed. In some cases, particularly when the proposed project is in a transit rich area, the Planning Commission may not support the provision of any off-street parking spaces. This is, in part, owing to the fact that the parking spaces are not ‘bundled’ with the residential units. In other words, residents would have the option to rent or purchase a parking space, but one would not be automatically provided with the residential unit.

If the project were ultimately approved with no off-street parking spaces, the proposed project would have an unmet demand of 93 spaces. As mentioned above, the unmet parking demand could be accommodated within existing on-street and off-street parking spaces nearby and through alternative

⁹ San Francisco Planning Department, Transit-Oriented Infill Project Eligibility Checklist for 815-825 Tennessee Street, July 9, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2013.0220E.

modes such as public transit and bicycle facilities. Given that the unmet demand could be met by existing facilities and given that the proposed project site is well-served by transit and bicycle facilities, a reduction in the number of off-street parking spaces associated with the proposed project, even if no off-street spaces are provided, would not result in significant delays or hazardous conditions.

Parking conditions are not static, as parking supply and demand varies from day to day, from day to night, from month to month, etc. Hence, the availability of parking spaces (or lack thereof) is not a permanent physical condition, but changes over time as people change their modes and patterns of travel. While parking conditions change over time, a substantial shortfall in parking caused by a project that creates hazardous conditions or significant delays to traffic, transit, bicycles or pedestrians could adversely affect the physical environment. Whether a shortfall in parking creates such conditions will depend on the magnitude of the shortfall and the ability of drivers to change travel patterns or switch to other travel modes. If a substantial shortfall in parking caused by a project creates hazardous conditions or significant delays in travel, such a condition could also result in secondary physical environmental impacts (e.g., air quality or noise impacts caused by congestion), depending on the project and its setting.

The absence of a ready supply of parking spaces, combined with available alternatives to auto travel (e.g., transit service, taxis, bicycles or travel by foot) and a relatively dense pattern of urban development, induces many drivers to seek and find alternative parking facilities, shift to other modes of travel, or change their overall travel habits. Any such resulting shifts to transit service or other modes (walking and biking), would be in keeping with the City's "Transit First" policy and numerous San Francisco General Plan Policies, including those in the Transportation Element. The City's Transit First Policy, established in the City's Charter Article 8A, Section 8A.115, provides that "parking policies for areas well served by public transit shall be designed to encourage travel by public transportation and alternative transportation."

The transportation analysis accounts for potential secondary effects, such as cars circling and looking for a parking space in areas of limited parking supply, by assuming that all drivers would attempt to find parking at or near the project site and then seek parking farther away if convenient parking is unavailable. The secondary effects of drivers searching for parking is typically offset by a reduction in vehicle trips due to others who are aware of constrained parking conditions in a given area, and thus choose to reach their destination by other modes (i.e. walking, biking, transit, taxi). If this occurs, any secondary environmental impacts that may result from a shortfall in parking in the vicinity of the proposed project would be minor, and the traffic assignments used in the transportation analysis, as well as in the associated air quality, noise and pedestrian safety analyses, would reasonably address potential secondary effects.

Pedestrian

The proposed project would not include sidewalk narrowing, roadway widening, or removal of a center median; conditions that can adversely affect pedestrians. The proposed project would generate 37 pedestrian trips during a typical weekday p.m. peak-hour. The proposed project would have one entrance for residents located in the lobby area along the building frontage on Tennessee Street, near the proposed bulbout and intersection at 19th street. ADA-accessible curb ramps would be located at the intersection of Tennessee and 19th Streets, near the building entrance.

In general, sidewalks in the project vicinity are between 12 and 15 feet wide, and currently have moderate to low levels of pedestrian activity. The 37 new pedestrian trips could be accommodated on the existing sidewalks and crosswalks adjacent to the project site. As such, the proposed project would not cause a

hazard to pedestrians or otherwise interfere with pedestrian accessibility to the project site and adjoining areas. Pedestrian activity may increase as a result of the proposed project, but not to a degree that would result in substantial overcrowding on public sidewalks.

While pedestrian impacts would be less than significant, improvement measures could be implemented to further reduce these less-than-significant impacts. Project Improvement Measure 2: Installation of Visual/Audible Devices at Underground Garage Driveway, would require the Project Sponsor to install visual and/or audible notifications (alarms) to alert pedestrians of vehicles traveling in and out of the underground parking garage and to further reduce potential vehicle-pedestrian conflicts. Project Improvement Measure 3: Installation of Crosswalks along 19th Street and Tennessee Street, would enhance pedestrian circulation and safe passage of pedestrians walking to and from the project site and reduce and/or eliminate any vehicle-pedestrian conflicts.

For the above reasons, the proposed project would not result in significant impacts on pedestrian safety that were not identified in the Eastern Neighborhoods PEIR.

Bicycle

There are three designated bicycle routes in the vicinity of the proposed project: Bicycle Route 5, 7, and 23. Although the proposed project would result in an increase in the number of vehicles in the project vicinity, this increase would not substantially affect bicycle travel in the area.

The proposed project would not introduce any design features that would eliminate or impede access to existing bicycle routes in proximity to the project site. In addition, the frequency of vehicles entering and exiting the project site would not be substantial enough to cause a hazard to bicyclists. For the above reasons, the proposed project would not result in significant impacts on bicycle safety that were not identified in the Eastern Neighborhoods PEIR.

Loading

Planning Code Section 152.1 requires no off-street loading for residential development less than 100,000 sf or retail use less than 10,000 sf in gross floor area. The proposed project includes 74,359 sf of residential use. Therefore, no off-street loading spaces would be required. Therefore, the proposed project would meet the loading requirements of the Planning Code.

The proposed project would generate a demand for one freight/delivery loading space during both the average and peak-hour of loading activities. The project is proposing one on-street 20-foot "yellow zone" along the project's frontage on 19th Street, designated as on-street commercial loading and to accommodate delivery vehicles. The loading demand would be accommodated with the proposed on-street vehicle loading space.

While loading impacts would be less than significant, Project Improvement Measure 4: Coordination of Move-in/Move-Out Operations and Large Deliveries, could be implemented to further reduce these less-than-significant impacts by enforcing appropriate loading procedures to avoid any blockages along 19th Street during loading activities and reducing any potential conflicts between movers and pedestrians walking along 19th Street.

For the above reasons, the proposed project would not result in significant impacts on transportation and circulation related to loading that were not identified in the Eastern Neighborhoods PEIR.

Emergency Access

The proposed project would not close off any existing streets or entrances to public uses. Therefore, the proposed project would not result in any significant impacts on emergency access that were not identified in the Eastern Neighborhoods PEIR.

Construction

The proposed project’s construction activities would last approximately 18 months and would include below-ground surface construction and building construction. Although construction activities would result in additional vehicle trips to and from the project site from workers and material and equipment deliveries, these activities would be limited in duration.

While construction related impacts would be less than significant, Project Improvement Measure 5: Construction Management Plan would further reduce construction-related less-than-significant impacts by requiring the development of a construction management plan to minimize conflicts with all modes of travel.

Therefore, the proposed project’s construction would not result in significant impacts on transportation that were not identified in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
5. NOISE—Would the project:				
a) Result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan area, or, where such a plan has not been adopted, in an area within two miles of a public airport or public use airport, would the project expose people residing or working in the area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project located in the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Be substantially affected by existing noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR identified potential conflicts related to residences and other noise-sensitive uses in proximity to noisy uses such as PDR, retail, entertainment,

cultural/institutional/educational uses, and office uses. In addition, the Eastern Neighborhoods PEIR noted that implementation of the Area Plan would incrementally increase traffic-generated noise on some streets in the Plan Area and result in construction noise impacts from pile driving and other construction activities. The Eastern Neighborhoods PEIR therefore identified six noise mitigation measures that would reduce noise impacts to less-than-significant levels.

Eastern Neighborhoods PEIR Mitigation Measures F-1 and F-2 relate to construction noise. Mitigation Measure F-1 addresses individual projects that include pile-driving, and Mitigation Measure F-2 addresses individual projects that include particularly noisy construction procedures (including pile-driving). Mitigation Measure F-1 does not apply because the proposed project would not involve pile-driving. However, the project could involve noisy construction activities. Therefore, Eastern Neighborhoods Mitigation Measure F-2 applies to the project and has been identified as Project Mitigation Measure 1. Compliance with this mitigation measure would result in less-than-significant construction noise impacts.

In addition, all construction activities for the proposed project (approximately 18 months) would be subject to and would comply with the San Francisco Noise Ordinance (Article 29 of the San Francisco Police Code) (Noise Ordinance). Construction noise is regulated by the Noise Ordinance. The Noise Ordinance requires that construction work be conducted in the following manner: (1) noise levels of construction equipment, other than impact tools, must not exceed 80 dBA¹⁰ (Ldn¹¹) at a distance of 100 feet from the source (the equipment generating the noise); (2) impact tools must have intake and exhaust mufflers that are approved by the Director of the Department of Public Works (DPW) or the Director of the Department of Building Inspection (DBI) to best accomplish maximum noise reduction; and (3) if the noise from the construction work would exceed the ambient noise levels at the site property line by 5 dBA, the work must not be conducted between 8:00 p.m. and 7:00 a.m. unless the Director of DPW authorizes a special permit for conducting the work during that period.

DBI is responsible for enforcing the Noise Ordinance for private construction projects during normal business hours (8:00 a.m. to 5:00 p.m.). The Police Department is responsible for enforcing the Noise Ordinance during all other hours. Nonetheless, during the most active construction period for the proposed project of approximately 10 months, occupants of the nearby properties could be disturbed by construction noise. Times may occur when noise could interfere with indoor activities in nearby residences and other businesses near the project site and may be considered an annoyance by occupants of nearby properties. The increase in noise in the project area during project construction would not be considered a significant impact of the proposed project, because the construction noise would be temporary (approximately 18 months), intermittent, and restricted in occurrence and level, as the contractor would be subject to and would comply with the Noise Ordinance.

Eastern Neighborhoods PEIR Mitigation Measures F-3, F-4, F-5, and F-6 include additional measures for individual projects that include new noise-sensitive uses. Mitigation Measure F-3 requires that new development that includes noise-sensitive uses located along streets with noise levels above 60 dBA (Ldn), where such development is not already subject to California Noise Insulation Standards in Title 24,

¹⁰ The dBA, or A-weighted decibel, refers to a scale of noise measurement that approximates the range of sensitivity of the human ear to sounds of different frequencies. On this scale, the normal range of human hearing extends from about 0 dBA to about 140 dBA. A 10-dBA increase in the level of a continuous noise represents a perceived doubling of loudness.

¹¹ The Ldn is the Leq, or Energy Equivalent Level, of the A-weighted noise level over a 24-hour period with a 10 dB penalty applied to noise levels between 10:00 p.m. to 7:00 a.m. The Leq is the level of a steady noise which would have the same energy as the fluctuating noise level integrated over the time period of interest.

the project sponsor shall conduct a detailed analysis of noise reduction requirements. Mitigation Measure F-4 requires the preparation of an analysis that includes, at minimum, a site survey to identify potential noise-generating uses within 900 feet of and that have a direct line of site to the project site, and at least one 24-hour noise measurement (with maximum noise levels taken every 15 minutes) to demonstrate that acceptable interior noise levels consistent with Title 24 can be attained. As the project is located in an area where traffic-related noise exceeds 60 dBA (Ldn) and involves construction of a residential building (a noise-sensitive use), Mitigation Measures F-3 and F-4 apply to the project. Accordingly, the project sponsor has conducted an environmental noise study demonstrating that the proposed project can feasibly attain acceptable interior noise levels consistent with Title 24.¹² Therefore, the proposed project has complied with Eastern Neighborhoods PEIR Mitigation Measures F-3 and F-4 and additional analysis is not required.

Eastern Neighborhoods PEIR Mitigation Measure F-5 addresses impacts related to individual projects that include new noise-generating uses that would be expected to generate noise levels in excess of ambient noise in the proposed project site vicinity. The project does not include such noise-generating uses, thus Mitigation Measure F-5 is not applicable to the project.

Mitigation Measure F-6 requires that open space required under the Planning Code for individual projects located in noisy areas be protected, to the maximum feasible extent, from existing ambient noise levels. As previously discussed, the project is located in an area where traffic-related noise levels exceed 60 dBA (Ldn). The project includes approximately 10,552 square feet of open space, divided between two private courtyards and a common terrace on the roof. Therefore, Eastern Neighborhoods Mitigation Measure F-6 applies the project, and has been identified as Project Mitigation Measure 2. Compliance with this mitigation measure would result in less-than-significant noise impacts on noise sensitive receptors.

The project site is not located within an airport land use plan area, within two miles of a public airport, or in the vicinity of a private airstrip. Therefore, topic 12e and f from the CEQA Guidelines, Appendix G is not applicable.

For the above reasons, the proposed project would not result in significant noise impacts that were not identified in the Eastern Neighborhoods PEIR.

¹² Nigel Breitz Acoustics, Acoustical Memo, Environmental Noise 815 Tennessee Street, February 3, 2014. This report is available for review as part of Case No. 2013.0220E.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
6. AIR QUALITY—Would the project:				
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal, state, or regional ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR identified potentially significant air quality impacts resulting from construction activities and impacts to sensitive land uses¹³ as a result of exposure to elevated levels of diesel particulate matter (DPM) and other toxic air contaminants (TACs). The Eastern Neighborhoods PEIR identified four mitigation measures that would reduce these air quality impacts to less-than-significant levels. All other air quality impacts were found to be less than significant.

Construction Dust Control

Eastern Neighborhoods PEIR Mitigation Measure G-1 Construction Air Quality requires individual projects involving construction activities to include dust control measures and to maintain and operate construction equipment so as to minimize exhaust emissions of particulates and other pollutants. The San Francisco Board of Supervisors subsequently approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Construction Dust Control Ordinance (Ordinance 176-08, effective July 30, 2008). The intent of the Construction Dust Control Ordinance is to reduce the quantity of fugitive dust generated during site preparation, demolition, and construction work in order to protect the health of the general public and of on-site workers, minimize public nuisance complaints, and to avoid orders to stop work by DBI. Project-related construction activities would result in construction dust, primarily from ground-disturbing activities. In compliance with the Construction Dust Control Ordinance, the project sponsor and contractor responsible for construction activities at the project site would be required to control construction dust on the site through a combination of watering disturbed areas, covering stockpiled materials, street and sidewalk sweeping and other measures.

The regulations and procedures set forth by the San Francisco Dust Control Ordinance would ensure that construction dust impacts would not be significant. These requirements supersede the dust control provisions of PEIR Mitigation Measure G-1. Therefore, the portion of PEIR Mitigation Measure G-1 Construction Air Quality that addresses dust control is not applicable to the proposed project.

¹³ The Bay Area Air Quality Management District (BAAQMD) considers sensitive receptors as: children, adults or seniors occupying or residing in: 1) residential dwellings, including apartments, houses, condominiums, 2) schools, colleges, and universities, 3) daycares, 4) hospitals, and 5) senior care facilities. BAAQMD, Recommended Methods for Screening and Modeling Local Risks and Hazards, May 2011, page 12.

Health Risk

Eastern Neighborhoods PEIR Mitigation Measure G-1 addresses air quality impacts during construction, Mitigation Measure G-2 addresses the siting of sensitive land uses near sources of TACs and PEIR Mitigation Measures G-3 and G-4 address proposed uses that would emit DPM and other TACs.

Subsequent to certification of the PEIR, San Francisco (in partnership with the Bay Area Air Quality Management District (BAAQMD)) inventoried and assessed air pollution and exposures from mobile, stationary, and area sources within San Francisco and identified portions of the City that result in additional health risks for affected populations ("Air Pollutant Exposure Zone"). The Air Pollutant Exposure Zone was identified based on two health based criteria:

- (1) Areas where the excess cancer risk from all sources is greater than 100; or
- (2) Areas where $PM_{2.5}$ concentrations from all sources (including ambient concentrations) are greater than $10\mu g/m^3$.

The project site is not located within an identified Air Pollutant Exposure Zone. Therefore, the ambient health risk to sensitive receptors from air pollutants is not considered substantial and the remainder of Mitigation Measure G-1 that requires the minimization of construction exhaust emissions is not applicable to the proposed project.

The proposed project would include development of residential uses and is considered a sensitive land use for purposes of air quality evaluation. As discussed above, the ambient health risk to sensitive receptors from air pollutants is not considered substantial and Eastern Neighborhoods PEIR Mitigation Measure G-2 Air Quality for Sensitive Land Uses is not applicable to the proposed project. Furthermore, the proposed residential land uses are not uses that would emit substantial levels of DPM or other TACs and Eastern Neighborhoods Mitigation Measures G-3 and G-4 are similarly not applicable.

Criteria Air Pollutants

While the Eastern Neighborhoods PEIR determined that at a program-level the Eastern Neighborhoods Rezoning and Area Plans would not result in significant regional air quality impacts, the PEIR states that "Individual development projects undertaken in the future pursuant to the new zoning and area plans would be subject to a significance determination based on the BAAQMD's quantitative thresholds for individual projects."¹⁴ The BAAQMD's *CEQA Air Quality Guidelines* (Air Quality Guidelines) provide screening criteria¹⁵ for determining whether a project's criteria air pollutant emissions would violate an air quality standard, contribute to an existing or projected air quality violation, or result in a cumulatively considerable net increase in criteria air pollutants. Pursuant to the Air Quality Guidelines, projects that meet the screening criteria do not have a significant impact related to criteria air pollutants. For projects that do not meet the screening criteria, a detailed air quality assessment is required to further evaluate whether project-related criteria air pollutant emissions would exceed BAAQMD significance thresholds. Criteria air pollutant emissions during construction and operation of the proposed project would meet the Air Quality Guidelines screening criteria. Therefore, the project would not have a significant impact related to criteria air pollutants, and a detailed air quality assessment is not required.

¹⁴ San Francisco Planning Department, Eastern Neighborhood's Rezoning and Area Plans Final Environmental Impact Report. See page 346. Available online at: <http://www.sf-planning.org/Modules/ShowDocument.aspx?documentid=4003>. Accessed June 4, 2014.

¹⁵ Bay Area Air Quality Management District, CEQA Air Quality Guidelines, updated May 2011. See pp. 3-2 to 3-3.

For the above reasons, none of the Eastern Neighborhoods PEIR air quality mitigation measures are applicable to the proposed project and the project would not result in significant air quality impacts that were not identified in the PEIR.

<u>Topics:</u>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
7. GREENHOUSE GAS EMISSIONS—Would the project:				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR assessed the GHG emissions that could result from rezoning of the Central Waterfront Area Plan under the three rezoning options. The Eastern Neighborhoods Rezoning Options A, B, and C are anticipated to result in GHG emissions on the order of 4.2, 4.3 and 4.5 metric tons of CO₂E¹⁶ per service population,¹⁷ respectively. The Eastern Neighborhoods PEIR concluded that the resulting GHG emissions from the three options analyzed in the Eastern Neighborhoods Area Plans would be less than significant. No mitigation measures were identified in the PEIR.

Regulations outlined in San Francisco’s Strategies to Address Greenhouse Gas Emissions have proven effective as San Francisco’s GHG emissions have measurably reduced when compared to 1990 emissions levels, demonstrating that the City has met and exceeded EO S-3-05, AB 32, and the Bay Area 2010 Clean Air Plan GHG reduction goals for the year 2020. The proposed project was determined to be consistent with San Francisco’s GHG Reduction Strategy. Other existing regulations, such as those implemented through AB 32, will continue to reduce a proposed project’s contribution to climate change. Therefore, the proposed project’s GHG emissions would not conflict with state, regional, and local GHG reduction plans and regulations, and thus the proposed project’s contribution to GHG emissions would not be cumulatively considerable or generate GHG emissions, either directly or indirectly, that would have a significant impact on the environment.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on greenhouse gas emissions beyond those analyzed in the Eastern Neighborhoods PEIR.

¹⁶ CO₂E, defined as equivalent Carbon Dioxide, is a quantity that describes other greenhouse gases in terms of the amount of Carbon Dioxide that would have an equal global warming potential.

¹⁷ Memorandum from Jessica Range to Environmental Planning staff, Greenhouse Gas Analyses for Community Plan Exemptions in Eastern Neighborhoods, April 20, 2010. This memorandum provides an overview of the GHG analysis conducted for the Eastern Neighborhoods PEIR and provides an analysis of the emissions using a service population (equivalent of total number of residents and employees) metric.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
8. WIND AND SHADOW—Would the project:				
a) Alter wind in a manner that substantially affects public areas?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create new shadow in a manner that substantially affects outdoor recreation facilities or other public areas?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Wind

Based upon experience of the Planning Department in reviewing wind analyses and expert opinion on other projects, it is generally (but not always) the case that projects under 80 feet in height do not have the potential to generate significant wind impacts. Although the proposed 58-foot-tall building would be taller than the immediately adjacent buildings, it would be similar in height to existing buildings in the surrounding area.

For the above reasons, the proposed project is not anticipated to cause significant impacts related to wind that were not identified in the Eastern Neighborhoods PEIR.

Shadow

Planning Code Section 295 generally prohibits new structures above 40 feet in height that would cast additional shadows on open space that is under the jurisdiction of the San Francisco Recreation and Park Commission between one hour after sunrise and one hour before sunset, at any time of the year, unless that shadow would not result in a significant adverse effect on the use of the open space. Under the Eastern Neighborhoods Area Plan, sites surrounding parks could be redeveloped with taller buildings without triggering Section 295 of the Planning Code because certain parks are not subject to Section 295 of the Planning Code (i.e., under jurisdiction by departments other than the Recreation and Parks Department or privately owned). The Eastern Neighborhoods PEIR could not conclude if the rezoning and community plans would result in less-than-significant shadow impacts because the feasibility of complete mitigation for potential new shadow impacts of unknown proposed proposals could not be determined at that time. Therefore, the PEIR determined shadow impacts to be significant and unavoidable. No mitigation measures were identified in the PEIR.

The proposed project would construct a 58-foot-tall building; therefore, the Planning Department prepared a preliminary shadow fan analysis to determine whether the project would have the potential to cast new shadow on nearby parks. The preliminary shadow fan analysis determined that there would be the potential for the project to cast new shadow on Esprit Park. The project sponsor prepared a detailed shadow analysis in order to assess new shadow impacts caused by implementation of the project.¹⁸ The analysis was conducted based on a “solar year” to provide a sample of representative sun angles throughout the entire calendar year. The solar year is from June 21st through December 20th. The sun angles during the “other” side of the calendar year, or December 21st through June 20th, mirror the solar year sun angles. Since the angles are mirrored, an analysis of the “other” time period was not conducted and, instead, a multiplier was used to put the sample results into calendar year units. For the purposes of the Section 295 analysis, shadow impacts were calculated based on square foot hours recorded. To ensure

¹⁸ 815-825 Tennessee Street Shadow Analysis, ROEM Corporation and BDE Architecture, June 25, 2014. This document is on file and available for public review at the Planning Department, 1650 Mission Street, Suite 400.

a complete and accurate description of the proposed project’s potential shadow impacts, this analysis identified the days when the shadow cast by the proposed project: (1) would be at its largest size by area, and (2) would result in the overall greatest shadow impact in terms of size and duration (i.e., the maximum net new shadow as measured in shadow foot hours). The “worst day” is the day with the maximum net new shadow. The maximum net new shadow or “worst shadow day” would occur on June 28th/June 14th. On June 28th / June 14th, the proposed project would cast a new shadow on Esprit Park from sunrise at 6:48am till approximately 7am (a few minutes). However, since the existing two-story residential building located at 701 Minnesota Street is directly to the east of the subject property between the proposed project and the park, the new shadow would be blocked by this existing structure and no new shadow would be cast on Esprit Park. Therefore, there would no new shadow impact from the proposed 815-825 Tennessee Street project.

The proposed project would also shade portions of nearby streets and sidewalks and private property at times within the project vicinity. Shadows upon streets and sidewalks would not exceed levels commonly expected in urban areas and would be considered a less-than-significant effect under CEQA. Although occupants of nearby property may regard the increase in shadow as undesirable, the limited increase in shading of private properties as a result of the proposed project would not be considered a significant impact under CEQA.

For the above reasons, the proposed project would not result in significant impacts related to shadow that were not identified in the Eastern Neighborhoods PEIR.

<u>Topics:</u>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
9. RECREATION—Would the project:				
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facilities would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Include recreational facilities or require the construction or expansion of recreational facilities that might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Physically degrade existing recreational resources?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR concluded that implementation of the Eastern Neighborhoods Rezoning and Area Plans would not result in substantial or accelerated deterioration of existing recreational resources or require the construction or expansion of recreational facilities that may have an adverse effect on the environment. No mitigation measures related to recreational resources were identified in the Eastern Neighborhoods PEIR.

As the proposed project would not degrade recreational facilities and is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on recreation beyond those analyzed in the Eastern Neighborhoods PEIR.

<u>Topics:</u>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
10. UTILITIES AND SERVICE SYSTEMS—Would the project:				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Have sufficient water supply available to serve the project from existing entitlements and resources, or require new or expanded water supply resources or entitlements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater treatment provider that would serve the project that it has inadequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the anticipated increase in population would not result in a significant impact to the provision of water, wastewater collection and treatment, and solid waste collection and disposal. No mitigation measures were identified in the PEIR.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on utilities and service systems beyond those analyzed in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
11. PUBLIC SERVICES—Would the project:				
a) Result in substantial adverse physical impacts associated with the provision of, or the need for, new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any public services such as fire protection, police protection, schools, parks, or other services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the anticipated increase in population would not result in a significant impact to public services , including fire protection, police protection, and public schools. No mitigation measures were identified in the PEIR.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on public services beyond those analyzed in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
12. BIOLOGICAL RESOURCES—Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

As discussed in the Eastern Neighborhoods PEIR, the Eastern Neighborhoods Plan Area is in a developed urban environment that does not provide native natural habitat for any rare or endangered plant or animal species. There are no riparian corridors, estuaries, marshes, or wetlands in the Plan Area that could be affected by the development anticipated under the Area Plan. In addition, development envisioned under the Eastern Neighborhoods Area Plan would not substantially interfere with the movement of any resident or migratory wildlife species. For these reasons, the PEIR concluded that implementation of the Area Plan would not result in significant impacts on biological resources, and no mitigation measures were identified.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on biological resources beyond those analyzed in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
13. GEOLOGY AND SOILS—Would the project:				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? (Refer to Division of Mines and Geology Special Publication 42.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code, creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Change substantially the topography or any unique geologic or physical features of the site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR concluded that implementation of the Plan would indirectly increase the population that would be subject to an earthquake, including seismically induced ground-shaking, liquefaction, and landslides. The PEIR also noted that new development is generally safer than comparable older development due to improvements in building codes and construction techniques. Compliance with applicable codes and recommendations made in project-specific geotechnical analyses would not eliminate earthquake risks, but would reduce them to an acceptable level, given the seismically active characteristics of the Bay Area. Thus, the PEIR concluded that implementation of the Plan would not result in significant impacts with regard to geology, and no mitigation measures were identified in the Eastern Neighborhoods PEIR.

A geotechnical investigation was prepared for the proposed project.¹⁹ The geotechnical investigation concluded that the site would be suitable for support of the proposed project if constructed in accordance with the report recommendations.

The project is required to conform to the San Francisco Building Code, which ensures the safety of all new construction in the City. DBI will review the project-specific geotechnical report during its review of the building permit for the project. In addition, DBI may require additional site specific soils report(s) through the building permit application process, as needed. The DBI requirement for a geotechnical report and review of the building permit application pursuant to DBI's implementation of the Building Code would ensure that the proposed project would have no significant impacts related to soils, seismic or other geological hazards.

In light of the above, the proposed project would not result in a significant effect related to seismic and geologic hazards. Therefore, the proposed project would not result in significant impacts related to geology and soils that were not identified in the Eastern Neighborhoods PEIR, and no mitigation measures are necessary.

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14. HYDROLOGY AND WATER QUALITY—Would the project:				
a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

¹⁹ Geotechnical Investigation for 815-825 Tennessee Street, Treadwell & Rollo, April 30, 2013. This document is on file and available for public review at the Planning Department, 1650 Mission Street, Suite 400.

<u>Topics:</u>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner that would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner that would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other authoritative flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures that would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Expose people or structures to a significant risk of loss, injury or death involving inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the anticipated increase in population would not result in a significant impact on hydrology and water quality, including the combined sewer system and the potential for combined sewer outflows. No mitigation measures were identified in the PEIR.

The existing project site is completely covered by an existing building. The proposed project would construct a new building on the entirety of the project site and would not change the impervious surface coverage. As a result, the proposed project would not increase stormwater runoff.

Therefore, the proposed project would not result in any significant impacts related to hydrology and water quality that were not identified in the Eastern Neighborhoods PEIR.

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15. HAZARDS AND HAZARDOUS MATERIALS— Would the project:				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR noted that implementation of any of the proposed project’s rezoning options would encourage construction of new development within the project area. The PEIR found that there is a high potential to encounter hazardous materials during construction activities in many parts of the project area because of the presence of 1906 earthquake fill, previous and current land uses associated with the use of hazardous materials, and known or suspected hazardous materials cleanup cases. However, the PEIR found that existing regulations for facility closure, underground storage tank (UST) closure, and investigation and cleanup of soil and groundwater would ensure implementation of measures to protect workers and the community from exposure to hazardous materials during construction.

Hazardous Building Materials

The Eastern Neighborhoods PEIR determined that future development in the Plan Area may involve demolition or renovation of existing structures containing hazardous building materials. Some building materials commonly used in older buildings could present a public health risk if disturbed during an accident or during demolition or renovation of an existing building. Hazardous building materials addressed in the PEIR include asbestos, electrical equipment such as transformers and fluorescent light ballasts that contain PCBs or di (2 ethylhexyl) phthalate (DEHP), fluorescent lights containing mercury

vapors, and lead-based paints. Asbestos and lead based paint may also present a health risk to existing building occupants if they are in a deteriorated condition. If removed during demolition of a building, these materials would also require special disposal procedures. The Eastern Neighborhoods PEIR identified a significant impact associated with hazardous building materials including PCBs, DEHP, and mercury and determined that that Mitigation Measure L-1: Hazardous Building Materials, would reduce effects to a less-than-significant level. Because the proposed development includes demolition of an existing building, Mitigation Measure L-1 would apply to the proposed project, and has been identified as Project Mitigation Measure 3.

Soil and Groundwater Contamination

The proposed project would involve excavation of approximately 11,000 cubic yards in order to construct the building foundation and basement-level garage on a site that is in a Maher area. Therefore, the project is subject to Article 22A of the Health Code, also known as the Maher Ordinance, which is administered and overseen by the Department of Public Health (DPH). The Maher Ordinance requires the project sponsor to retain the services of a qualified professional to prepare a Phase I Environmental Site Assessment (ESA) that meets the requirements of Health Code Section 22.A.6.

The Phase I ESA would determine the potential for site contamination and level of exposure risk associated with the project. Based on that information, the project sponsor may be required to conduct soil and/or groundwater sampling and analysis. Where such analysis reveals the presence of hazardous substances in excess of state or federal standards, the project sponsor is required to submit a site mitigation plan (SMP) to the DPH or other appropriate state or federal agency(ies), and to remediate any site contamination in accordance with an approved SMP prior to the issuance of any building permit.

In compliance with the Maher Ordinance, the project sponsor will be required to submit a Maher Application to DPH. PES Environmental, Inc. undertook a Phase I ESA of the project site on behalf of the project sponsor.²⁰ The Phase I ESA reviews and summarizes previous environmental documents prepared for other sites in proximity to the project site, lists current and past operations, reviews environmental agency databases and records, report site reconnaissance observations, and discusses potential contamination issues. The Phase I ESA did not find any physical or documentary evidence of any use, storage, or disposal of any chemicals, hazardous materials, reportable substances or hazardous waste at the site. No Recognized Environmental Concerns are associated with the property and none were identified in the nearby areas.

The proposed project would be required to remediate potential soil and/or groundwater contamination described above in accordance with Article 22A of the Health Code. Therefore, the proposed project would not result in any significant impacts related to hazardous materials that were not identified in the Eastern Neighborhoods PEIR.

Therefore, the proposed project would not result in significant impacts related to hazards or hazardous materials that were not identified in the Eastern Neighborhoods PEIR.

²⁰ Phase 1 Environmental Site Assessment at 815-825 Tennessee Street, PES Environmental, Inc., May 9, 2013.

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16. MINERAL AND ENERGY RESOURCES— Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Encourage activities which result in the use of large amounts of fuel, water, or energy, or use these in a wasteful manner?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the Area Plan would facilitate the construction of both new residential units and commercial buildings. Development of these uses would not result in use of large amounts of fuel, water, or energy in a wasteful manner or in the context of energy use throughout the City and region. The energy demand for individual buildings would be typical for such projects and would meet, or exceed, current state and local codes and standards concerning energy consumption, including Title 24 of the California Code of Regulations enforced by DBI. The Plan Area does not include any natural resources routinely extracted and the rezoning does not result in any natural resource extraction programs. Therefore, the Eastern Neighborhoods PEIR concluded that implementation of the Area Plan would not result in a significant impact on mineral and energy resources. No mitigation measures were identified in the PEIR.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on mineral and energy resources beyond those analyzed in the Eastern Neighborhoods PEIR.

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17. AGRICULTURE AND FOREST RESOURCES:—Would the project:				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code Section 12220(g)) or timberland (as defined by Public Resources Code Section 4526)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<u>Topics:</u>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that no agricultural resources exist in the Area Plan; therefore the rezoning and community plans would have no effect on agricultural resources. No mitigation measures were identified in the PEIR. The Eastern Neighborhoods PEIR did not analyze the effects on forest resources.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on agriculture and forest resources beyond those analyzed in the Eastern Neighborhoods PEIR.

MITIGATION AND IMPROVEMENT MEASURES

Project Mitigation Measure 1 – Construction Noise (Mitigation Measure F-2 of the Eastern Neighborhoods PEIR)

Where environmental review of a development project undertaken subsequent to the adoption of the proposed zoning controls determines that construction noise controls are necessary due to the nature of planned construction practices and the sensitivity of proximate uses, the Planning Director shall require that the sponsors of the subsequent development project develop a set of site-specific noise attenuation measures under the supervision of a qualified acoustical consultant. Prior to commencing construction, a plan for such measures shall be submitted to the Department of Building Inspection to ensure that maximum feasible noise attenuation will be achieved. These attenuation measures shall include as many of the following control strategies as feasible:

- Erect temporary plywood noise barriers around a construction site, particularly where a site adjoins noise-sensitive uses.
- Utilize noise control blankets on a building structure as the building is erected to reduce noise emission from the site.
- Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings housing sensitive uses.
- Monitor the effectiveness of noise attenuation measures by taking noise measurements.
- Post signs on-site pertaining to permitted construction days and hours and complaint procedures and who to notify in the event of a problem, with telephone numbers listed.

Project Mitigation Measure 2 – Open Space in Noisy Environments (Mitigation Measure F-6 of the Eastern Neighborhoods PEIR)

To minimize effects on development in noisy areas, for new development including noise-sensitive uses, the Planning Department shall, through its building permit review process, in conjunction with noise analysis required pursuant to Eastern Neighborhoods Mitigation Measure F-4, require that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from

existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design.

Project Mitigation Measure 3 – Hazardous Building Materials (Mitigation Measure L-1 of the Eastern Neighborhoods PEIR)

The City shall condition future development approvals to require that the subsequent project sponsors ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.

Project Improvement Measure 1 – Monitoring and Abatement of Queues

As an improvement measure to reduce the potential for queuing of vehicles accessing the project site, it shall be the responsibility of the project sponsor/property owner to ensure that recurring vehicle queues do not occur on 19th or Tennessee Streets adjacent to the site. A vehicle queue is defined as one or more vehicles (destined to the underground parking garage) blocking any portion of the 19th Street sidewalk or travel lane on 19th Street, or along Tennessee Street for a consecutive period of three minutes or longer on a daily and/or weekly basis.

Because the proposed project would include a new off-street parking facility with more than 20 parking spaces (excluding loading and car-share spaces), the project is subject to conditions of approval set forth by the San Francisco Planning Department to address the monitoring and abatement of queues.

It shall be the responsibility of the owner/operator of any off-street parking facility with more than 20 parking spaces (excluding loading and car-share spaces) to ensure that recurring vehicle queues do not occur on the public right-of-way. A vehicle queue is defined as one or more vehicles (destined to the parking facility) blocking any portion of any public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis.

If a recurring queue occurs, the owner/operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable).

Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as additional bicycle parking, customer shuttles, delivery services; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.

If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Department shall notify the property owner in writing. Upon request, the owner/operator shall hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant shall prepare a monitoring report to be submitted to the Department for review. If the Department determines that a recurring queue does exist, the facility owner/operator shall have 90 days from the date of the written determination to abate the queue.

Project Improvement Measure 2 – Installation of Visual/Audible Devices at Underground Garage Driveway

As an improvement measure to reduce potential conflicts between vehicles entering and exiting the underground garage and pedestrians traveling along the south side sidewalk of 19th Street, the Project Sponsor shall install visual and/or audible notifications (alarms) to alert pedestrians of vehicles traveling in and out of the underground parking garage.

Project Improvement Measure 3 – Installation of Crosswalks along 19th Street and Tennessee Street

As an improvement measure to reduce potential conflicts between pedestrians and other users of 19th and Tennessee Streets (e.g., autos, bus transit vehicles, freight/delivery vehicles, bicyclists, etc.), the project sponsor shall provide crosswalks to enhance pedestrian circulation and safe passage of pedestrians walking to and from the project site and reduce and/or eliminate any vehicle-pedestrian conflicts. The project sponsor shall provide two high-visibility crosswalks (e.g., continental, transverse, and/or ladder marking pattern) at the intersection of 19th Street and Tennessee Street, specifically along the east leg of the intersection (along 19th Street) and the south leg of the intersection (along Tennessee Street). Installation of crosswalks will provide enhanced pedestrian circulation, connectivity between the project site and adjacent uses, and enhance driver awareness of pedestrian activity at these locations.

The crosswalks shall be constructed in accordance with the *California Manual on Uniform Traffic Control Devices* (MUTCD). These new crosswalks may require approvals by the San Francisco Planning Department, Department of Public Works, and SFMTA's Livable Streets Subdivision, as appropriate.

Project Improvement Measure 4 – Coordination of Move-in/Move-Out Operations and Large Deliveries

To reduce the potential for parking of delivery vehicles within the travel lane adjacent to the curb lane on 19th Street (in the event that the on-street loading is occupied), residential move-in and move-out activities and larger deliveries shall be scheduled and coordinated through building management. Appropriate move-in/move-out procedures shall be enforced to avoid any blockages of 19th Street over an extended period of time and reduce any potential conflicts between movers and pedestrians walking along 19th Street. Curb parking on 19th Street shall be reserved through SFMTA or by directly contacting the local 311 service.

Project Improvement Measure 5 – Construction Management Plan

In addition to items required in the Construction Management Plan, the project sponsor shall include the following:

- *Carpool and Transit Access for Construction Workers* – As an improvement measure to minimize parking demand and vehicle trips associated with construction workers, the construction contractor shall include methods to encourage carpooling and transit use to the project site by construction workers in the Construction Management Plan contracts.

- *Project Construction Updates* – As an improvement measure to minimize construction impacts on nearby businesses, the project sponsor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction inquiries or concerns.