



SAN FRANCISCO PLANNING DEPARTMENT

Executive Summary Large Project Authorization

HEARING DATE: JULY 24, 2014

Date: July 17, 2014
Case No.: **2012.1333X**
Project Address: **923 Folsom Street**
Zoning: MUR (Mixed Use Residential) Zoning District
45/85-X Height and Bulk District
Block/Lot: 3753/106, 141 and 142
Project Sponsor: Christopher Davenport
Trumark Urban
4185 Blackhawk Circle, Suite 200
Danville, CA 94506
Staff Contact: Doug Vu – (415) 575-9120
Doug.Vu@sfgov.org
Recommendation: **Approval with Conditions**

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PROJECT DESCRIPTION

The proposed project includes demolition of the existing structure and new construction of a mixed-use project consisting of a nine-story 85-foot tall building fronting Folsom Street, and a four-story 45-foot tall building fronting Shipley Street. The buildings will be connected via a ground floor corridor along the western boundary of the project, have a total area of 138,980 gross square feet, and will include a total of up to 114 dwelling units, approximately 1,560 square feet of commercial space, and share a basement-level garage that will contain up to 80 off-street parking spaces (including stackers) and 114 Class 1 and eight Class 2 bicycle parking spaces.

The Folsom Street façade will include a ground floor that contains a storefront entrance to the commercial space, a residential lobby, and eight stories of residential use above. The Shipley Street façade includes four stories of residential use, including four townhouse units at the ground floor that directly access the street. The project includes a dwelling unit mix consisting of eight studios, 59 one-bedroom units, and 47 two-bedroom units. Open space for the dwelling units is provided through private balconies, a 5,670 square feet mid-block courtyard, and an 8,770 square feet roof deck atop the building fronting Shipley Street. A second pedestrian access to the residential uses is provided via a Shipley Street entrance, and vehicular access to the basement-level garage will be provided via a ramp off Shipley Street.

SITE DESCRIPTION AND PRESENT USE

The project site is located on a block bound by Folsom Street to the north, Fifth Street to the east, Shipley Street to the south, and Falmouth Street to the west. The project site is irregular in shape, is comprised of three contiguous lots that total 24,438 sq. ft., has 125' of frontage along both Folsom Street, and 175' of frontage along Shipley Street. The subject lots are currently improved with an approximately 7,500 square-foot, two-story office building originally constructed in 1967 and a 20,625 sq. ft. automobile

parking lot that also accommodates large charter buses. The entire frontage of the site along Folsom and Shipley Streets are surrounded by a chain-link fence, and there are four street trees within the pedestrian right-of-way along Folsom Street.

SURROUNDING PROPERTIES AND NEIGHBORHOOD

The project site is located in the East SoMa neighborhood, which is characterized by a mixture of light industrial, residential, and commercial uses. The Transbay Terminal is located approximately five blocks east of the project site, and Market Street is located three blocks to the north. The Intercontinental Hotel and Westfield San Francisco Shopping Center at 5th Street are located one and two blocks to the north, respectively, and the recently completed Mosso (previously Avant Housing) 282-unit mixed-use residential building is located across the street on the north side of Folsom Street. The project site is bordered to the east by a four-story hotel with ground-floor commercial uses along the Folsom Street frontage, and a four-story multi-family residential building along the Shipley Street frontage. To the west, the site is bordered by the City owned Fire Station 1.

Numerous public transit routes are located near the proposed project. Within a several block radius, the routes include the MUNI and BART Powell Street station, the MUNI F line, the 8A-8X-8BX, 12, 14-14L-14X, 27, 30, 45, and several other MUNI bus lines north of Market Street. Golden Gate transit and SamTrans also operate bus routes in this radius.

The Mixed Use-Residential (MUR) District serves as a buffer between the higher density, predominantly commercial area of Yerba Buena Center to the east and the lower scale, mixed use service/industrial and housing area west of Sixth Street. The MUR serves as a major housing opportunity area within the eastern portion of the South of Market. The district controls are intended to facilitate the development of high-density, mid-rise housing, including family-sized housing and residential hotels. The district is also designed to encourage the expansion of retail, business service, and commercial and cultural arts activities. Continuous ground floor commercial frontage with pedestrian-oriented retail activities along major thoroughfares is encouraged. Hotels, nighttime entertainment, movie theaters, adult entertainment and heavy industrial uses are not permitted.

ENVIRONMENTAL REVIEW

Pursuant to the Guidelines of the State Secretary of Resources for the implementation of the California Environmental Quality Act (CEQA), on June 20, 2014, the Planning Department of the City and County of San Francisco determined that the proposed application was exempt from further environmental review under Section 15183 of the CEQA Guidelines and California Public Resources Code Section 21083.3. The Project is consistent with the adopted zoning controls in the Eastern Neighborhoods Area Plan and was encompassed within the analysis contained in the Eastern Neighborhoods Area Plan Final EIR. Since the Final EIR was finalized, there have been no substantial changes to the Eastern Neighborhoods Area Plan and no substantial changes in circumstances that would require major revisions to the Final EIR due to the involvement of new significant environmental effects or an increase in the severity of previously identified significant impacts, and there is no new information of substantial importance that would change the conclusions set forth in the Final EIR.

HEARING NOTIFICATION

TYPE	REQUIRED PERIOD	REQUIRED NOTICE DATE	ACTUAL NOTICE DATE	ACTUAL PERIOD
Classified News Ad	20 days	July 4, 2014	July 2, 2014	22 days
Posted Notice	20 days	July 4, 2014	July 2, 2014	22 days
Mailed Notice	20 days	July 4, 2014	July 3, 2014	21 days

The proposal requires a Section 312 Neighborhood notification, which was conducted in conjunction with the required hearing notification for the Large Project Authorization.

PUBLIC COMMENT

The Department has received 25 letters and 110 signatures in support of the Project, and no correspondence in opposition.

ISSUES AND OTHER CONSIDERATIONS

- As part of the Large Project Authorization (LPA), the Commission may grant modifications from certain Planning Code requirements for projects that exhibit outstanding overall design and are complementary to the design and values of the surrounding area. The proposed project requests modifications from the rear yard (Planning Code Section 134), off-street parking (Planning Code Section 151.1), off-street loading (Planning Code Section 152.1), and special height exemptions (Planning Code Section 263) requirements. Department staff is generally in agreement with the proposed modifications given the overall project and its design.
- The Project will result in significant improvements to the surrounding pedestrian environment including numerous street trees, sidewalk upgrades, site furniture, and ground level dwelling units along Shipley Street that will help activate a portion of Folsom and Shipley Streets.
- The Project Sponsor has engaged the community beginning in 2013 that has resulted in a significant amount of public outreach to gather feedback and develop partnerships with organizations such as United Playaz, City Crossroads Ministries, Yerba Buena Community Benefit District, and South of Market Community Action Network.
- The Project will provide fourteen Below Market Rate (BMR) units on-site, and these BMR units will remain as ownership units for the life of the project.
- The Project would be subject to the Eastern Neighborhood Impacts Fees for the construction of new mixed-use development. These fees are estimated as follows:

FEE TYPE	PLANNING CODE SECTION / FEE	AMOUNT
Eastern Neighborhoods Impact Fee (1,559 gsf – Tier 1; Non-Residential)	423.3(a) / \$0	\$0

Eastern Neighborhoods Impact Fee (5,941 gsf – Tier 1; Non-Residential to Residential)	423.3(a) / \$2.00 per gsf	\$11,882
Eastern Neighborhoods Impact Fee (112,479 gsf – Tier 1; New Residential)	423.2(a) / \$9.25 per gsf	\$1,040,430.70
Alternative Means of Satisfying Open Space Requirements in EN Mixed Use Districts	426 / \$87.84 per sf	\$702.72
	TOTAL	\$1,053,015.40

These fees are subject to change between Planning Commission approval and approval of the associated Building Permit Application, as based upon the annual updates managed by the Development Impact Fee Unit of the Department of Building Inspection.

REQUIRED COMMISSION ACTION

In order for the project to proceed, the Commission must grant a Large Project Authorization pursuant to Planning Code Section 329 to allow the construction of a mixed-use project consisting of a nine-story 85-foot tall building fronting Folsom Street, and a four-story 45-foot tall building fronting Shipley Street with a total of 114 dwelling units, 1,559 square feet of commercial space, and 80 off-street parking spaces in a below-grade basement, and to allow modifications to the requirements for rear yard (Planning Code Section 134), off-street parking (Planning Code Section 151.1), off-street loading (Planning Code Section 152.1), and special height exemptions (Planning Code Section 263).

BASIS FOR RECOMMENDATION

- The Project in an appropriate in-fill development that will add 114 new dwelling units to the City’s housing stock and 1,560 square feet of commercial space in an established urban area planned for mid to high density development near downtown employment and entertainment centers.
- The Project is situated in close proximity to numerous public transportation options that will allow many residents to walk, bicycle, and/or use public transit as their primary modes of transportation.
- The Project’s design is of high quality and will complement the rapidly changing nature of its location on the border of the Yerba Buena entertainment area and the more traditionally industrial SOMA neighborhood.
- The Project will result in significant improvements to the surrounding pedestrian environment, including street trees, landscaping, sidewalk upgrades, and site furniture.
- The Project will provide 47 two-bedroom units.
- The Project will provide fourteen BMR units on-site.
- The project proposes a parking ratio of approximately .68 spaces per dwelling unit, or 78 spaces, which is below the maximum conditionally permitted ratio of 0.75, or 86 spaces.
- The Project will provide approximately \$1.05 million in impact fees to the Eastern Neighborhoods Public Benefit Fund.

- The Project is consistent with the Planning Code, Priority Policies, and the General Plan.

RECOMMENDATION: Approval with Conditions

Attachments:

- Draft Large Project Authorization Motion
- Parcel Map
- Sanborn Map
- Aerial Photograph
- Zoning Map
- Environmental Determination
- Public Correspondence
- Project Sponsor Submittal:
 - Site Photographs
 - Project Renderings
 - Reduced Plans

Attachment Checklist

- | | |
|---|---|
| <input checked="" type="checkbox"/> Executive Summary | <input checked="" type="checkbox"/> Context Photos |
| <input checked="" type="checkbox"/> Draft Motion | <input checked="" type="checkbox"/> Site Photos |
| <input checked="" type="checkbox"/> Environmental Determination | <input checked="" type="checkbox"/> Project sponsor submittal |
| <input checked="" type="checkbox"/> Zoning District Map | Drawings: <u>Existing Conditions</u> |
| <input checked="" type="checkbox"/> Parcel Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Sanborn Map | Drawings: <u>Proposed Project</u> |
| <input checked="" type="checkbox"/> Aerial Photo | <input checked="" type="checkbox"/> Check for legibility |

Exhibits above marked with an "X" are included in this packet

_____ DV
Planner's Initials

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SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- Affordable Housing (Sec. 415)
- Jobs Housing Linkage Program (Sec. 413)
- Downtown Park Fee (Sec. 412)
- First Source Hiring (Admin. Code)
- Child Care Requirement (Sec. 414)
- Other (EN Impact Fee – Sec. 423)

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 Danville, CA 94506
Staff Contact: Doug Vu – (415) 575-9120
Doug.Vu@sfgov.org

ADOPTING FINDINGS RELATING TO A LARGE PROJECT AUTHORIZATION PURSUANT TO PLANNING CODE SECTION 329, TO ALLOW EXCEPTIONS TO (1) REAR YARD PURSUANT TO PLANNING CODE SECTION 134, (2) OFF-STREET PARKING PURSUANT TO PLANNING CODE SECTION 151.1, (3) OFF-STREET LOADING PURSUANT TO PLANNING CODE SECTION 152.1, AND (4) SPECIAL HEIGHT EXEMPTIONS PURSUANT TO PLANNING CODE SECTION 263, TO ALLOW CONSTRUCTION OF A NINE-STORY 85-FOOT TALL BUILDING FRONTING FOLSOM STREET, A FOUR-STORY 45-FOOT TALL BUILDING FRONTING SHIPLEY STREET WITH A COMBINED TOTAL INCLUDING UP TO 114 DWELLING UNITS, APPROXIMATELY 1,559 SQUARE FEET OF COMMERCIAL SPACE, AND UP TO 80 PARKING SPACES IN A BASEMENT LEVEL GARAGE LOCATED AT 923 FOLSOM STREET, LOTS 106, 141 AND 142 IN ASSESSOR'S BLOCK 3753, WITHIN THE MUR (MIXED USE RESIDENTIAL) DISTRICT, SOMA YOUTH AND FAMILY SPECIAL USE DISTRICT, AND AN 45/85-X HEIGHT AND BULK DISTRICT, AND ADOPTING FINDINGS UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT.

PREAMBLE

On May 23, 2013, Christopher Davenport of Trumark Urban (hereinafter "Project Sponsor") filed an application with the Planning Department (hereinafter "Department") for Large Project Authorization (hereinafter "Application") under Planning Code Section 329 to allow construction of a nine-story 85-foot tall building fronting Folsom Street, and a four-story 45-foot tall building fronting Shipley Street with up to 114 dwelling units, approximately 1,559 square feet of commercial space, 80 parking spaces, and

exceptions including rear yard, off-street parking, off-street loading, and special height exemptions within the MUR (Mixed Use Residential) District, SOMA Youth and Family Special Use District, and a 85/45-X Height and Bulk District.

The environmental effects of the Project were determined by the San Francisco Planning Department to have been fully reviewed under the Eastern Neighborhoods Area Plan Environmental Impact Report (hereinafter "EIR"). The EIR was prepared, circulated for public review and comment, and, at a public hearing on August 7, 2008, by Motion No. 17661, certified by the Commission as complying with the California Environmental Quality Act (Cal. Pub. Res. Code Section 21000 et seq., (hereinafter "CEQA"). The Commission has reviewed the Final EIR, which has been available for this Commissions review as well as public review.

The Eastern Neighborhoods EIR is a Program EIR. Pursuant to CEQA Guideline 15168(c)(2), if the lead agency finds that no new effects could occur or no new mitigation measures would be required of a proposed project, the agency may approve the project as being within the scope of the project covered by the program EIR, and no additional or new environmental review is required. In approving the Eastern Neighborhoods Plan, the Commission adopted CEQA Findings in its Motion No. 17661 and hereby incorporates such Findings by reference.

Additionally, State CEQA Guidelines Section 15183 provides a streamlined environmental review for projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified, except as might be necessary to examine whether there are project-specific effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that (a) are peculiar to the project or parcel on which the project would be located, (b) were not analyzed as significant effects in a prior EIR on the zoning action, general plan or community plan with which the project is consistent, (c) are potentially significant off-site and cumulative impacts which were not discussed in the underlying EIR, or(d) are previously identified in the EIR, but which are determined to have a more severe adverse impact than that discussed in the underlying EIR. Section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for that project solely on the basis of that impact.

On June 20, 2014, the Department determined that the proposed application did not require further environmental review under Section 15183 of the CEQA Guidelines and Public Resources Code Section 21083.3. The Project is consistent with the adopted zoning controls in the Eastern Neighborhoods Area Plan and was encompassed within the analysis contained in the Eastern Neighborhoods Final EIR. Since the Eastern Neighborhoods Final EIR was finalized, there have been no substantial changes to the Eastern Neighborhoods Area Plan and no substantial changes in circumstances that would require major revisions to the Final EIR due to the involvement of new significant environmental effects or an increase in the severity of previously identified significant impacts, and there is no new information of substantial importance that would change the conclusions set forth in the Final EIR. The file for this project, including the Eastern Neighborhoods Final EIR and the Community Plan Exemption certificate, is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, San Francisco, California.

Planning Department staff prepared a Mitigation Monitoring and Reporting Program (MMRP) setting forth mitigation measures that were identified in the Eastern Neighborhoods Plan EIR that are applicable to the project. These mitigation measures are set forth in their entirety in the MMRP attached to the draft Motion as Exhibit C.

The Planning Department, Jonas P. Ionin, is the custodian of records, located in the File for Case No. 2012.1333X at 1650 Mission Street, Fourth Floor, San Francisco, California.

On July 24, 2014, the Planning Commission (hereinafter "Commission") conducted a duly noticed public hearing at a regularly scheduled meeting on Large Project Authorization Application No. 2012.1333X.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Large Project Authorization requested in Application No. 2012.1333X, subject to the conditions contained in "EXHIBIT A" of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The project site is located on a block bound by Folsom Street to the north, Fifth Street to the east, Shipley Street to the south, and Falmouth Street to the west. The project site is irregular in shape, is comprised of three contiguous lots that total 24,438 sq. ft., has 125' of frontage along both Folsom Street, and 175' of frontage along Shipley Street. The subject lots are currently improved with an approximately 7,500 square-foot, two-story office building originally constructed in 1967 and a 20,625 sq. ft. automobile parking lot that also accommodates large charter buses. The entire frontage of the site along Folsom and Shipley Streets are surrounded by a chain-link fence, and there are four street trees within the pedestrian right-of-way along Folsom Street.
3. **Surrounding Properties and Neighborhood.** The project site is located in the East SoMa neighborhood, which is characterized by a mixture of light industrial, residential, and commercial uses. The Transbay Terminal is located approximately five blocks east of the project site, and Market Street is located three blocks to the north. The Intercontinental Hotel and Westfield San Francisco Shopping Center at 5th Street are located one and two blocks to the north, respectively, and the recently completed Mosso (previously Avant Housing) 282-unit mixed-use residential building is located across the street on the north side of Folsom Street. The project site is bordered to the east by a four-story hotel with ground-floor commercial uses along the Folsom Street frontage, and a four-story multi-family residential building along the Shipley Street frontage. To the west, the site is bordered by the City owned Fire Station 1.

Numerous public transit routes are located near the proposed project. Within a several block radius, the routes include the MUNI and BART Powell Street station, the MUNI F line, the 8A-8X-8BX, 12, 14-14L-14X, 27, 30, 45, and several other MUNI bus lines north of Market Street. Golden Gate transit and SamTrans also operate bus routes in this radius.

The Mixed Use - Residential District serves as a buffer between the higher density, predominantly commercial area of Yerba Buena Center to the east and the lower scale, mixed use service/industrial and housing area west of Sixth Street. The MUR serves as a major housing opportunity area within the eastern portion of the South of Market. The district controls are intended to facilitate the development of high-density, mid-rise housing, including family-sized housing and residential hotels. The district is also designed to encourage the expansion of retail, business service, and commercial and cultural arts activities. Continuous ground floor commercial frontage with pedestrian-oriented retail activities along major thoroughfares is encouraged. Hotels, nighttime entertainment, movie theaters, adult entertainment and heavy industrial uses are not permitted.

4. **Project Description.** The proposed project includes demolition of the existing structure and new construction of a mixed-use project consisting of a nine-story 85-foot tall building fronting Folsom Street, and a four-story 45-foot tall building fronting Shipley Street. The buildings will be connected via a ground floor corridor along the western boundary of the project, have a total area of 138,980 gross square feet, and will include a total of up to 114 dwelling units, approximately 1,560 square feet of commercial space, and share a basement-level garage that will contain up to 80 off-street parking spaces (including stackers) and 114 Class 1 and eight Class 2 bicycle parking spaces.

The Folsom Street façade will include a ground floor that contains a storefront entrance to the commercial space, a residential lobby, and eight stories of residential use above. The Shipley Street façade includes four stories of residential use, including four townhouse units at the ground floor that directly access the street. The project includes a dwelling unit mix consisting of eight studios, 59 one-bedroom units, and 47 two-bedroom units. Open space for the dwelling units is provided through private balconies, a 5,670 square foot mid-block courtyard, and an 8,770 square foot roof deck atop the building fronting Shipley Street. A second pedestrian access to the residential uses is provided via a Shipley Street entrance, and vehicular access to the basement-level garage will be provided via a ramp off Shipley Street.

5. **Public Comment.** The Department has received 25 letters and 110 signatures in support of the Project, and no correspondence in opposition.
6. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:
 - A. **Use.** Planning Code Section 841.09 identifies a minimum residential to non-residential ratio of at least 3 to 1, whereas dwelling units and retail sales and services are principally permitted pursuant to Planning Code Sections 841.20 and 841.45, respectively.

The Project includes 1,559 sq. ft. of non-residential use on the ground floor of the building that fronts Folsom Streets, and 118,420 sq. ft. of residential use on every floor of both buildings that front Folsom and Shipley Streets, resulting in a residential to non-residential ratio well above 3 to 1.

- B. **Rear Yard.** Planning Code Section 134 requires a minimum rear yard equal to 25 percent of the total lot depth beginning at the lowest story containing a dwelling unit.

The Project does not comply with the rear yard requirement and is seeking an exception as part of the Large Project Authorization (See discussion below).

- C. **Residential Open Space.** Planning Code Section 135 requires that usable open space be located on the same lot as the dwelling units it serves. At least 80 sq. ft. of usable open space per dwelling unit, or 54 sq. ft. per dwelling unit of publicly accessible open space, is required. Up to 50 percent of the publicly accessible open space may be provided off-site. The Project has a residential open space requirement of 9,120 sq. ft. of usable open space if private, or 6,156 sq. ft. of publicly accessible open space.

The Project includes an 8,773 sq. ft. roof deck atop the building that fronts Shipley Street, and also includes nine private decks totaling 351 sq. ft. that meet the dimensional requirements of the Planning Code for usable open space, for a total of 9,124 sq. ft., which exceeds the minimum usable open space requirement.

- D. **Commercial Open Space.** Planning Code Section 135.3 requires usable open space for uses other than dwelling units. For retail use, one square foot per 250 sq. ft. of occupied floor area of usable open space is required. In Eastern Neighborhoods Mixed Use Districts, this open space requirement may be satisfied through payment of a fee of \$87.84 for each sq. ft. of usable square footage not provided pursuant to this Code section.

The Project is required to provide at least eight sq. ft. of commercial open space, and will comply with this Code section by submitting a payment of \$702.72 prior to the issuance of the first site permit or building permit.

- E. **Streetscape and Pedestrian Improvements.** Planning Code Section 138.1 requires improvement of the public right-of-way associated with development projects that includes the installation of one street tree (minimum of 24-inch box) for every twenty feet of frontage of the property along each street or public alley, with any remaining fraction of ten feet or more of frontage requiring an additional tree. Planning Code Section 138.1 also requires streetscape and pedestrian elements in conformance with the Better Streets Plan when a project is on a lot that is greater than ½-acre in total area and the project includes new construction.

The Project will include nine trees along the Shipley Street frontage and six trees along the Folsom Street frontage, which complies with this Code section. Since the Project will be located on three current lots that total 0.56 acres), a streetscape plan was submitted and will be reviewed by the Planning Department for compliance with the Better Streets Plan.

- F. **Street Frontages.** Planning Code Section 145.1 requires the following for street frontages in Eastern Neighborhood Mixed Use Districts: (1) not more than 1/3 the width of the building facing the street may be devoted to ingress/egress to parking; (2) off-street parking at street grade must be set back at least 25 feet; (3) "active" use shall be provided within the first 25 feet of building depth at the ground floor; (4) ground floor non-residential uses in MUR zoning district shall have a floor-to-floor height of 14-feet; (5) frontages with active uses shall be fenestrated with transparent windows; and, (6) decorative railings or grillwork placed in front of or behind ground floor windows, shall be at least 75 percent open to perpendicular views.

The Project meets the requirements of Section 145.1 by (1) providing an approximately 20' wide garage opening along Shipley Street that is less than 1/3 the width of the approximately 175' wide building; (2) providing off-street parking that is entirely below grade; (3) incorporating an active commercial space and a residential lobby within the first 25' of the building depth at the ground floor along Folsom Street, and residential units along Shipley Street; (4) providing a floor-to-floor ground floor height of 18'-8" for the commercial frontage; and (5) providing transparent windows at the ground floor active use.

- G. **Bird-Safe Standards.** Planning Code Section 139 outlines bird-safe standards for new construction to reduce bird mortality from circumstances that are known to pose a high risk to birds and are considered to be "bird hazards." Feature-related hazards may create increased risk to birds and need to be mitigated. The project site is not located within an urban bird refuge.

The Project meets the requirements of Planning Code Section 139 and does not contain any feature-related hazards such as free-standing glass walls, wind barriers, or balconies that have unbroken glazed segments 24 square feet or larger in size.

- H. **Dwelling Unit Exposure.** Planning Code Section 140 requires dwelling units to have at least one window facing a street or alley, a Code-complying rear yard, open space or inner court.

All proposed dwelling units enjoy ample light and air by facing either Folsom or Shipley Streets, or the proposed mid-block court yard that meets the dimensional and square footage requirements for dwelling unit exposure.

- I. **Street Frontages.** Planning Code Section 145.1 requires the following for street frontages in Eastern Neighborhood Mixed Use Districts: (1) not more than 1/3 the width of the building facing the street may be devoted to ingress/egress to parking; (2) off-street parking at street grade must be set back at least 25 feet; (3) "active" use shall be provided within the first 25 feet of building depth at the ground floor; (4) ground floor non-residential uses in UMU zoning district shall have a floor-to-floor height of 17-feet; (5) frontages with active uses shall be fenestrated with transparent windows; and, (6) decorative railings or grillwork placed in front of or behind ground floor windows, shall be at least 75 percent open to perpendicular views.

The project complies with the requirements of Section 145.1 as follows: (1) providing one 12-foot wide garage opening, which totals less than 1/3 the width of the approximately 105-foot wide building; (2)

the off-street parking at street grade is set back at least 35 feet from Illinois Street; (3) incorporating active uses on all street frontages, including commercial, dwellings with stoops and flex units within the first 25 feet of the building depth at ground floor; (4) providing a floor-to-floor ground floor height of 18 feet for the commercial frontage; and, (5) providing transparent windows at the ground floor.

- J. **Shadow.** Planning Code Section 147 requires reduction of substantial shadow impacts on public plazas and other publicly accessible spaces other than those protected under Planning Code Section 295. Section 295 restricts new shadow, cast by structures exceeding a height of 40 feet, upon property under the jurisdiction of the Recreation and Park Commission.

The Shadow Analysis conducted for the Project indicates that the Project will not cast shadow upon any existing Public, Publicly Accessible or Publicly Financed or Subsidized Open Space.

- K. **Off-Street Parking.** Planning Section 151.1 allows for the provision of up to three parking spaces for each four dwelling units, subject to the requirements of Sections 151.1(g) below. No additional parking is permitted above these amounts.

The Project proposes a parking ratio of 0.68 and is seeking an exception as part of the Large Project Authorization (See discussion below).

- L. **Off-Street Loading.** Planning Code Section 152.1 requires one off-street freight loading space for a residential use in the Eastern Neighborhoods Mixed Use Districts when the gross floor area is more than 100,000 square feet and up to 200,000 square feet.

The Project proposes an on-street loading space in-lieu of an off-street space and is seeking an exception as part of the Large Project Authorization (See discussion below).

- M. **Bicycle Parking.** Planning Code Section 155.2 requires one Class One bicycle space for each dwelling unit and one Class Two space for every 20 dwelling units.

The Project includes 114 dwelling units and requires a total of 114 Class One bicycle parking spaces and six Class Two bicycle parking spaces. The Project meets and exceeds this requirement by providing 114 Class One bicycle parking spaces and eight Class Two bicycle parking spaces.

- N. **Car Share.** Planning Code Section 166 requires one space for projects proposing between 50 and 200 dwelling units.

One car share space is required for the proposed 114 dwelling units. The Project meets and exceeds the minimum requirement by providing two designated car share spaces.

- O. **Unbundled Parking.** Planning Code Section 167 requires that all off-street parking spaces accessory to residential uses in new structures of 10 dwelling units or more be leased or sold separately from the rental or purchase fees for dwelling units for the life of the dwelling units.

The off-street parking spaces provided for the dwelling units will be unbundled and sold and/or leased separately from the dwelling units. Therefore, the Project meets this requirement.

- P. **Dwelling Unit Mix.** Planning Code Section 207.6 requires at least 40 percent of the total number of proposed dwelling units to contain two or more bedrooms. Any fraction resulting from this calculation shall be rounded to the nearest whole number of dwelling units.

The Project will be required to provide a minimum of 46 two-bedroom units or larger, and will meet and exceed this Code section by providing 47 two-bedroom units (or 41 percent).

- Q. **Height Limit.** Planning Code Section 260 requires that the height of buildings not exceed the limits specified in the Zoning Map and defines rules for the measurement of height. The Project Site is within a split 85/45-foot Height District.

The portion of the Project Site that has frontage along Shipley Street has a height limit of 45' for one-half the depth of the two through Lots (141 and 142) and the entire depth of the third Subject Lot (106), whereas the remainder of Lots 141 and 142 that have frontage along Folsom Street have a height limit of 85'. The Project complies with this requirement as the heights of the buildings do not exceed their respective height limits.

- A. **Narrow Streets.** Planning Code Section 261.1 outlines height and massing requirements for projects that front onto a "narrow street," which is defined as a public right of way less than or equal to 40-ft in width. Shipley Street measures approximately 35' wide, and is considered an east-west "narrow street." All subject frontages onto a narrow street shall have upper stories set back at least 10-ft at the property line above a height equivalent to 1.25 times the width of the abutting "narrow street." Therefore, the Project is required to provide a 10-ft setback above a height of 43-ft 9-in. The project site is located on the north side of an east-west "narrow street."

The proposed four-story building that fronts onto Shipley Street has a maximum height of 43'-9" and complies with Planning Code 261.1.

- R. **Height Limits: Special Exceptions.** Planning Code Section 263.21 permits vertical architectural elements on large lots that are excepted from the applicable height limits which mitigate the effect of long, monotonous individual buildings and create visual focal points that build upon the architectural tradition and character of an area. The mostly topographically flat Eastern Neighborhoods, particularly the South or Market, Showplace, and Mission Districts have a strong tradition of larger and longer buildings incorporating vertical architectural elements above the predominant roofline. These vertical elements are proportioned to the building and are generally not visible on the skyline or distant view.

The Project proposes a 12' tall architectural element at the north corner of the building that exceeds the 85' building height limit and is seeking an exception as part of the Large Project Authorization (See discussion below).

- S. **SoMa Youth and Family Special Use District.** The Project site is located in the SoMa Youth and Family Special Use District, which requires Conditional Use Authorization for certain

land uses and increased affordable housing requirements for properties that front only on smaller streets within the District.

The Project does not propose and land uses restricted by this Special Use District, and Lot 106 that fronts exclusively on Shipley Street is not subject to the increased affordable housing requirements because the proposed building does not exceed 45' in height, pursuant to Planning Code Section 249.40A(c)(C).

- T. **Inclusionary Affordable Housing Program.** Planning Code Section 415 sets forth the requirements and procedures for the Inclusionary Affordable Housing Program. Under Planning Code Section 415.3, these requirements would apply to projects that consist of 10 or more units, where the first application (EE or BPA) was applied for on or after July 18, 2006. Pursuant to Planning Code Section 415.5 and 415.6, the Inclusionary Affordable Housing Program requirement for the On-site Affordable Housing Alternative is to provide 12% of the proposed dwelling units as affordable.

The Project Sponsor has demonstrated that it is eligible for the On-Site Affordable Housing Alternative under Planning Code Section 415.5 and 415.6, and has submitted a 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to satisfy the requirements of the Inclusionary Affordable Housing Program by providing the affordable housing on-site instead of through payment of the Affordable Housing Fee. In order for the Project Sponsor to be eligible for the On-Site Affordable Housing Alternative, the Project Sponsor must submit an 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to the Planning Department stating that any affordable units designated as on-site units shall be sold as ownership units and will remain as ownership units for the life of the project. The Project Sponsor submitted such Affidavit on April 21, 2014. The EE application was submitted on January 16, 2014. Pursuant to Planning Code Section 415.3 and 415.6, the on-site requirement is 12%. Fourteen units (one studio, eight one-bedroom, and five two-bedroom) of the 114 units provided will be affordable units. If the Project becomes ineligible to meet its Inclusionary Affordable Housing Program obligation through the On-site Affordable Housing Alternative, it must pay the Affordable Housing Fee with interest, if applicable.

- U. **Eastern Neighborhoods Infrastructure Impact Fees.** The project shall comply with the provisions of Planning Code Section 423, including payment of the Eastern Neighborhoods Impact Fee, or execution of an In-Kind Agreement with the Planning Department prior to issuance of the first site or building permit.

The Project includes the continuation of 1,559 sq. ft. of non-residential use, the conversion of approximately 5,941 sq. ft. of existing non-residential use to residential use, and the establishment of approximately 112,479 sq. ft. of new residential use. This new residential use is subject to the Eastern Neighborhoods Infrastructure Impact Fees, as outlined in Planning Code Section 423. These fees must be paid by the Project Sponsor prior to the issuance of the building permit application.

7. **Large Project Authorization in Eastern Neighborhoods Mixed Use District.** Planning Code Section 329(c) lists nine aspects of design review in which a project must comply; the Planning Commission finds that the project is compliant with these nine aspects as follows:

A. Overall building massing and scale;

The Project conforms to the applicable height and bulk requirements. The community in the vicinity of the Project is constantly evolving with development in the entire South of Market region and the Eastern Neighborhoods Area Plans, and contains a range of building masses. The Project, with residential and a commercial use, will be consistent with the evolving character of the area. The Project massing will improve the character of the neighborhood and general pedestrian accessibility by providing streetscape improvements along both Folsom and Shipley Streets. Additionally, the Project steps down in height as it approaches Shipley Street, providing light and air access to this narrow right-of-way.

The most substantial structure in the vicinity is the Intercontinental Hotel (888 Howard St.), which rises 32-stories (340 feet) above the northeast corner of the intersection of Fifth and Howard Streets, one block north of the project site. There are several residential loft or condominium buildings within the surrounding blocks, including a 65' residential building (436 Clementina Street) to the northwest of the site, an 85' residential building (829 Folsom Street) one half-block to the east on Folsom Street, and the recently completed 85' Mosso mixed-use residential buildings (900 Folsom Street and 260 5th Street) that contain more than 460 residential units. The Project will replace a commercial office building and a surface parking lot with a new residential building with ground floor commercial space along Folsom Street. A defining trend in the area has been redevelopment and adaptive reuse of underutilized parcels to meet the increased demand for housing and service-based businesses. The project falls within this trend and will be compatible with its surroundings. The proposed building fronting Folsom Street will be approximately 85', and the building fronting Shipley Street will be 45'. The buildings will fall within height restrictions set in the Planning Code and will be similar in scale to uses within a block of the project site, such as the aforementioned recently constructed projects. In addition, the Yerba Buena Lofts building (885 Folsom Street), one block east of the project site, is also 85' in height. Additionally, the 14-story Alexis Towers apartment building located one block north on Fifth Street is approximately 120' in height.

B. Architectural treatments, facade design and building materials;

The architecture of this Project responds to the site's location between the traditional warehouse aesthetic of West Soma, and the contemporary architecture of the Yerba Buena District to the east. The Project's facade along Folsom Street presents a fenestration pattern and scale that is unified and includes an architectural ribbon motif of composite resin panels that wraps up the building, breaking up the massing of the structure. This motif creates a distinctive facade design that adds interest to the high-performance, lightly tinted, non-reflective vision glass that comprises the balance of the residential floors. Juliet balconies are incorporated throughout this facade, providing residents access to the outdoors while recognizing the limited usefulness of these features along a major roadway. At the top of the Folsom Street building is an architectural element with subtly placed accent colors that balances the building, drawing the eye up to the top of the fritted glass and interior lighting that creates a signature visual element.

The building along Shipley Street is smaller in scale and incorporates a variety of materials including two colors of cladding, decorative perforated metal gates and balcony guardrails, horizontal plank tile and patina steel planters. The design also includes articulation and massing breaks consistent with the scale of the surrounding structures with a majority of the dwelling units having private open space looking out, or opening onto Shipley Street.

- C. The design of lower floors, including building setback areas, commercial space, townhouses, entries, utilities, and the design and siting of rear yards, parking and loading access;

The ground floor character of the Folsom Street building is active with a ground floor commercial space that has a 17' clear ceiling height and clear glass façade to allow natural light into the ground floor spaces and provide visual interest at the pedestrian level. To clearly mark the residential lobby entry, the façade is set back 3'-6" from the property line and delineated by distinctive color accent panels and a 3' deep canopy. It is also flanked on both sides by a public art wall or a bike repair area to create additional interest and depth at a pedestrian scale.

- D. The provision of required open space, both on- and off-site. In the case of off-site publicly accessible open space, the design, location, access, size, and equivalence in quality with that otherwise required on-site;

The Project provides a combined 9,124 sq. ft. of private and common usable open space through private balconies, stoops, and common open space. Common open space is provided by an 8,773 sq. ft. fifth floor roof deck overlooking Shipley Street and 351 sq. ft. of private open space in nine units fronting Shipley Street. Furthermore, ten ground floor dwelling units that face the interior mid-block courtyard will have private access to additional open space. Finally, the nominal requirement for eight square feet of publicly-accessible open space will be satisfied through a payment of an in-lieu fee by the Sponsor.

- E. Streetscape and other public improvements, including tree planting, street furniture, and lighting;

The Project will include the installation of fifteen street trees along both frontages, sidewalk upgrades and other improvements that are consistent with the Better Streets Plan.

- F. Circulation, including streets, alleys and mid-block pedestrian pathways;

The Project proposes only one 20' wide ingress/egress access at Shipley Street and is not anticipated to create circulation problems. No other ingress/egress is proposed anywhere to prevent possible conflicts and congestion, and an indoor corridor that is accessible to residents and connects the two buildings will be located along the western boundary of the Project.

- G. Bulk limits;

The Project site is located in an X Bulk District, which provides no bulk restrictions.

- H. Other changes necessary to bring a project into conformance with any relevant design guidelines, Area Plan or Element of the General Plan.

The Project generally meets the Objectives and Policies of the General Plan and is compliant with the East SoMa Area Plan.

8. **Exceptions.** Proposed Planning Code Section 329 allows exceptions for Large Projects in the Eastern Neighborhoods Mixed Use Districts.

- A. Planning Code Section 134 requires a minimum rear yard equal to 25 percent of the total lot depth beginning at the lowest story containing a dwelling unit. The subject property is an

irregular lot with two frontages. Planning Code Section 329(d) allows an exception for the rear yard requirement pursuant to requirements of Planning Code Section 134(f).

1. Residential uses are included in the new or expanding development and a comparable amount of readily accessible usable open space is provided elsewhere on the lot:

The Project is occupied by a residential uses except for a small amount of ground floor commercial space, and a comparable amount of readily accessible open space is proposed. Per the Planning Code, the required rear yard should equal 25 percent of the lot area, which is approximately 6,110 sq. ft. for this property. The proposed interior courtyard provides 5,670 sq. ft. (23.2 percent), and is a nominal deficit of 440 sq. ft. However, this deficit is mitigated through the provision of an additional 9,124 sq. ft of usable open space.

2. The proposed new or expanding structure will not significantly impede the access to light and air from adjacent properties:

The Project is located mid-block and the two buildings will occupy the northwest and southeast edges of the property. The 85' tall building will front Folsom Street, which has an approximately 80' wide right-of-way, and the 43'-6" tall building will front Shipley Street in compliance with the Planning Code. As a result, the Project will have no significant impact on light and air to the adjacent properties.

3. The proposed new or expanding structure will not adversely affect the interior block open space formed by the rear yards of adjacent properties:

The adjacent properties to the east have no rear yard, and the area within the rear yard for the adjacent property to the west is used as an automobile parking lot; therefore, there is no existing interior block open space. However, the Project will include a 35' wide accessible interior courtyard

- B. Planning Code Section 151.1 allows for the provision of up to three parking spaces for each four dwelling units, subject to the requirements of Sections 151.1(g) below. No additional parking is permitted above these amounts.

(1)(A) Parking for All Uses.

- (i) Vehicle movement on or around the project does not unduly impact pedestrian spaces or movement, transit service, bicycle movement, or the overall traffic movement in the district;
- (ii) Accommodating excess accessory parking does not degrade the overall urban design quality of the project proposal;
- (iii) All above-grade parking is architecturally screened and lined with active uses according to the standards of Section 145.1, and the project sponsor is not requesting any exceptions or variances requiring such treatments elsewhere in this Code; and
- (iv) Excess accessory parking does not diminish the quality and viability of existing or planned streetscape enhancements.

The Project will include 78 off-street parking spaces (not including the two car-share spaces), which represents 0.68 spaces per dwelling unit, and one 20' wide, two-way vehicular access ramp off Shipley Street that accesses a subterranean parking garage. This singular ingress/egress point to the off-street

parking will minimize any impact to pedestrian spaces or movement. All parking spaces will be located underground, will not be visible from the street, and will not diminish the quality and viability of the existing and planned streetscape. The proposed bicycle parking is at the ground level and will be accessible through the building lobby.

(B) Parking for Residential Uses.

- (i) For projects with 50 dwelling units or more, all residential accessory parking in excess of 0.5 spaces per unit shall be stored and accessed by mechanical stackers or lifts, valet, or other space-efficient means that reduces space used for parking and maneuvering, and maximizes other uses.

The maximum number of parking spaces permitted is 86, or a parking ratio of approximately 0.75 spaces per dwelling unit, not including any required car-share spaces. The project proposes 78 parking spaces, or a parking ratio of approximately 0.68 spaces per dwelling unit, whereby a minimum of 21 spaces must be accessed by mechanical stackers. The Project complies with this Code section by proposing 60 spaces that will be accessed by mechanical stackers.

- C. Planning Code Section 152.1 requires one off-street freight loading space for a residential use in the Eastern Neighborhoods Mixed Use Districts when the gross floor area is more than 100,000 square feet and up to 200,000 square feet.

The Project proposes to designate a loading zone on Shipley Street that would be used in conjunction with a scissors lift within an adjacent service vestibule to allow loading to be completed at the ground level and access to the basement level via an interior corridor without the need to use the automobile ramp. The permanent lift system would bridge the 3' elevation change between the Shipley Street grade and the interior corridor, and will eliminate safety hazards associated with drivers colliding with moving personnel and "runaway" dollies full of heavy items. The proposed loading zone will be used primarily by residents moving into/out of the building and service vehicles for building maintenance. Since the width of the Shipley street right-of-way is between 35' and 40', the establishment of an on-street loading zone would not negatively impact automobile or pedestrian circulation.

- D. Planning Code Section 263.21 permits vertical architectural elements on large lots that are excepted from the applicable height limits which mitigate the effect of long, monotonous individual buildings and create visual focal points that build upon the architectural tradition and character of an area. The mostly topographically flat Eastern Neighborhoods, particularly the South or Market, Showplace, and Mission Districts have a strong tradition of larger and longer buildings incorporating vertical architectural elements above the predominant roofline. These vertical elements are proportioned to the building and are generally not visible on the skyline or distant view.

The Project proposes an approximately 12' tall architectural element atop the north corner of the Folsom Street building that has a 25' x 25' footprint and is clad with subtly placed accent colors that balances the building, drawing the eye up to the top of the fritted glass and interior lighting that creates a signature visual element. This architectural element is equal in height to the adjacent screened areas for the rooftop mechanical equipment, but is distinguished by the cladding materials, fritted glazing, and interior lighting, and will create a visual focal point to mitigate any effects of a long and monotonous individual building.

9. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

HOUSING

Objectives and Policies

OBJECTIVE 1:

IDENTIFY AND MAKE AVAILABLE FOR DEVELOPMENT ADEQUATE SITES TO MEET THE CITY'S HOUSING NEEDS, ESPECIALLY PERMANENTLY AFFORDABLE HOUSING.

Policy 1.1

Plan for the full range of housing needs in the City and County of San Francisco, especially affordable housing.

The Project is a residential mixed-use development in an area that features civic, commercial and residential uses. The subject lots are an ideal infill site occupied by a single commercial office building and an at-grade parking lot. The project site was rezoned to MUR as part of a long range planning goal to create a cohesive, higher density residential and mixed-use neighborhood. The surrounding neighborhood features a wide variety of zoning, which is consistent with the desired mixed-use character. The Project will provide opportunities for affordable housing across the City by offering fourteen on-site BMR units that will remain affordable for the life of the Project.

OBJECTIVE 11:

SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORHOODS.

Policy 11.1

Promote the construction and rehabilitation of well-designed housing that emphasizes beauty, flexibility, and innovative design, and respects existing neighborhood character.

Policy 11.2

Ensure implementation of accepted design standards in project approvals.

Policy 11.3

Ensure growth is accommodated without substantially and adversely impacting existing residential neighborhood character.

Policy 11.4

Continue to utilize zoning districts which conform to a generalized residential land use and density plan and the General Plan.

Policy 11.6

Foster a sense of community through architectural design, using features that promote community interaction.

Policy 11.8

Consider a neighborhood's character when integrating new uses, and minimize disruption caused by expansion of institutions into residential areas.

The architecture of this Project responds to the site's location as a mixed-use area with civic, commercial and residential uses. The Project's facades provide an expression that relates to the surrounding neighborhood, while providing for a material palette and aesthetic that is contemporary in character and relatively simple in design. The exterior is designed with modern materials vision and spandrel glass, composite black opal cladding, accent tiles, and metal slab edge cover. The massing and scale are indicative of the urban fabric of the surrounding area.

RECREATION AND OPEN SPACE ELEMENT

Objectives and Policies

OBJECTIVE 4:

PROVIDE OPPORTUNITIES FOR RECREATION AND THE ENJOYMENT OF OPEN SPACE IN EVERY SAN FRANCISCO NEIGHBORHOOD.

Policy 4.5

Require private usable outdoor open space in new residential development.

Policy 4.6

Assure the provision of adequate public open space to serve new residential development.

The Project will create private and common outdoor open spaces in a new residential mixed-use development through private balconies, an interior courtyard, and a roof deck. It will not cast shadows over any open spaces under the jurisdiction of the Recreation and Park Department.

TRANSPORTATION ELEMENT

Objectives and Policies

OBJECTIVE 24:

IMPROVE THE AMBIENCE OF THE PEDESTRIAN ENVIRONMENT.

Policy 24.2

Maintain and expand the planting of street trees and the infrastructure to support them.

Policy 24.3

Install pedestrian-serving street furniture where appropriate.

Policy 24.4

Preserve pedestrian-oriented building frontages.

The Project will install street trees at approximately 20' intervals along the frontages of Folsom and Shipley Streets, and landscaping with pedestrian-serving site furniture will also be installed. The frontages are designed with active spaces oriented at the pedestrian level that have a 17' clear ceiling height along Folsom Street, and dwelling units with raised entry stoops that directly access the pedestrian realm along Shipley Street.

OBJECTIVE 28:

PROVIDE SECURE AND CONVENIENT PARKING FACILITIES FOR BICYCLES.

Policy 28.1:

Provide secure bicycle parking in new governmental, commercial, and residential developments.

Policy 28.3:

Provide parking facilities which are safe, secure, and convenient.

The Project includes 114 Class One bicycle parking spaces in secure, convenient locations on the basement-level garage and eight Class Two spaces in the Folsom Street public right-of-way.

OBJECTIVE 34:

RELATE THE AMOUNT OF PARKING IN RESIDENTIAL AREAS AND NEIGHBORHOOD COMMERCIAL DISTRICTS TO THE CAPACITY OF THE CITY'S STREET SYSTEM AND LAND USE PATTERNS.

Policy 34.1:

Regulate off-street parking in new housing so as to guarantee needed spaces without requiring excesses and to encourage low auto ownership in neighborhoods that are well served by transit and are convenient to neighborhood shopping.

Policy 34.3:

Permit minimal or reduced off-street parking supply for new buildings in residential and commercial areas adjacent to transit centers and along transit preferential streets.

Policy 34.5:

Minimize the construction of new curb cuts in areas where on-street parking is in short supply and locate them in a manner such that they retain or minimally diminish the number of existing on-street parking spaces.

The Project has a parking to dwelling unit ratio of 0.68 spaces per unit, below the maximum permitted ratio of 0.75. The parking spaces are accessed by one ingress/egress point measuring 20' wide from Shipley Street. The Project does not provide any off-street parking for the commercial use, which is not required by the Eastern Neighborhoods Area Plan. Overall, the parking is adequate for the project and complies with maximum prescribed by the Planning Code.

URBAN DESIGN ELEMENT

Objectives and Policies

OBJECTIVE 1:

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

Policy 1.7:

Recognize the natural boundaries of districts, and promote connections between districts.

The Project is located within the East SoMa neighborhood surrounded by the Western SoMa Special Use and the Service/Arts/Light Industrial Districts that is characterized with civic, commercial and residential uses. As such, the Project provides an appropriate ground floor, massing, and scale that respond to the form and scale of the existing neighborhood, while also providing a new contemporary architectural vocabulary and a better pedestrian experience, as compared to the existing site.

OBJECTIVE 4:

IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY.

Policy 4.5:

Design walkways and parking facilities to minimize danger to pedestrians.

Policy 4.13:

Improve pedestrian areas by providing human scale and interest.

Although the Project Site has two street frontages, it will only provide one vehicular access point for the entire project, limiting conflicts with pedestrians and bicyclists. Numerous street trees will be planted along each street, and the Project provides ample frontages for commercial and residential use and an active ground floor, which appropriately engages the street. Along the project site, the pedestrian experience will be greatly improved with upgraded sidewalks, landscaping and site furniture, whereas the existing site contains a commercial office building and an at-grade parking lot.

EAST SOMA AREA PLAN

Objectives and Policies

Land Use

OBJECTIVE 1.1:

ENCOURAGE PRODUCTION OF HOUSING AND OTHER MIXED-USE DEVELOPMENT IN EAST SOMA WHILE MAINTAINING ITS EXISTING SPECIAL MIXED-USE CHARACTER.

Policy 1.1.8

Permit small and moderate retail establishments in mixed use areas of East SoMa, but permit larger retail only as part of a mixed-use development.

OBJECTIVE 1.2

MAXIMIZE HOUSING POTENTIAL IN KEEPING WITH NEIGHBORHOOD CHARACTER

Policy 1.2.1

Encourage development of new housing throughout East SoMa.

Policy 1.2.2

Ensure that in-fill housing development is compatible with its surroundings.

Policy 1.2.3

For new construction, and as part of major expansion of existing buildings, encourage housing development over commercial.

Policy 1.2.4

In general, where residential development is permitted, control residential density through building height and bulk guidelines and bedroom mix requirements.

Housing

OBJECTIVE 2.3

ENSURE THAT NEW RESIDENTIAL DEVELOPMENTS SATISFY AN ARRAY OF HOUSING NEEDS WITH RESPECT TO TENURE, UNIT MIX AND COMMUNITY SERVICES.

Policy 2.3.2

Prioritize the development of affordable family housing, both rental and ownership, particularly along transit corridors and adjacent to community amenities.

Policy 2.3.3

Require that 40 percent of all units in new developments have two or more bedrooms and encourage that at least 10 percent of all units in new development have three or more bedrooms, except Senior Housing and SRO developments.

Policy 2.3.5

Explore a range of revenue- generating tools including impact fees, public funds and grants, assessment districts, and other private funding sources, to fund community and neighborhood improvements.

Policy 2.3.6

Establish an Eastern Neighborhoods Public Benefit Fund to mitigate the impacts of new development on transit, pedestrian, bicycle, and street improvements, park and recreational facilities, and community facilities such as libraries, child care and other neighborhood services in the area.

The Project will provide fourteen BMR units on-site that will remain affordable ownership units for the life of the project. The appropriate dwelling unit mix is proposed with approximately 41%, or 47 of the units being two-bedroom dwellings. The Project will also pay the appropriate development impact fees, including the Eastern Neighborhoods Impact Fees.

Built Form

OBJECTIVE 3.1

PROMOTE AN URBAN FORM THAT REINFORCES THE EAST SOMA'S DISTINCTIVE PLACE IN THE CITY'S LARGER FORM AND STRENGTHENS ITS PHYSICAL FABRIC AND CHARACTER.

Policy 3.1.1

Adopt heights that are appropriate for SoMa's location in the city, the prevailing street and block pattern, and the anticipated land uses, while preserving the character of its neighborhood enclaves.

Policy 3.1.8

New development should respect existing patterns of rear yard open space. Where an existing pattern of rear yard open space does not exist, new development on mixed-use-zoned parcels should have greater flexibility as to where open space can be located.

Policy 3.1.11

Establish and require height limits along alleyways to create the intimate feeling of an urban room.

OBJECTIVE 3.2

PROMOTE AN URBAN FORM AND ARCHITECTURAL CHARACTER THAT SUPPORTS WALKING AND SUSTAINS A DIVERSE, ACTIVE AND SAFE PUBLIC REALM.

Policy 3.2.1

Require high quality design of street-facing building exteriors.

Policy 3.2.4

Strengthen the relationship between a building and its fronting sidewalk.

The Project is located within the prescribed height and bulk guidelines and is largely residential, but does include one ground floor commercial space that provides the mix of uses encouraged by the Area Plan for this location. Further, the Project includes the appropriate massing setbacks along Shipley Street, which is identified as a narrow street. The Project introduces a contemporary architectural vocabulary, which responds to the prevailing scale and neighborhood fabric and compliments the broader context of large buildings along Howard, Mission and Market Streets. The Project provides strong, repeating vertical articulation to achieve the visual presence necessary to sustain pedestrian interest and activity. Massing is differentiated with different building heights, notches, recesses, and projections. The Project provides strong, repeating vertical articulation to achieve the visual presence necessary to sustain pedestrian interest and activity. Massing is differentiated with different building heights, notches, recesses, and projections. The proposed fenestration represents the commercial and residential uses behind them which minimizes visual clutter, harmonizes with the prevailing conditions, and provides architectural interest. Proposed windows are recessed and are generally oriented vertically with metal mullions and trim. The Project provides an exterior that features a variety of materials, colors, and textures, including cement plaster, metal panels, vision glass, and tile trim.

10. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project does comply with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

There are no existing neighborhood-serving retail uses on the site, and the Project will provide approximately 1,559 square feet of ground floor commercial space. The Project will also add new residents, visitors, and employees to the neighborhood, which would strengthen nearby retail uses and provide new opportunity for retail employment/ownership.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

No housing exists on the project site. The project will provide up to 114 new dwelling units, which will significantly increase the neighborhood housing stock. The design of the Project is compatible and relates to the scale and form of the surrounding neighborhood by providing architectural gestures to the surrounding residential complexes and residential hotels. For these reasons, the proposed project would protect and preserve the cultural and economic diversity of the neighborhood.

- C. That the City's supply of affordable housing be preserved and enhanced.

The Project will not displace any affordable housing because there is currently no housing on the site. The Project will comply with the City's Inclusionary Housing Program by providing fourteen BMR units, therefore increasing the stock of affordable housing units in the City.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The project site is well-served by public transportation and is located within half a block from various bus routes. In addition, the Project is within walking distance to the Powell Street MUNI and BART Stations. Future residents would be afforded close proximity to bus or rail transit. The Project also provides sufficient off-street parking at a ratio of 0.68 per dwelling unit, and sufficient bicycle parking for residents and their guests. The majority of future residents are expected to use alternative methods of transportation other than private automobiles, and the number of vehicle trips generated by this project would not impede MUNI transit service or overburden streets.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The Project is consistent with the East SoMa Area Plan, which encourages new mixed-use development. The Project will enhance opportunities for resident employment and ownership by providing new housing and a retail space, which will increase the diversity of the City's housing supply, which is a top priority in the City, and also provide a new neighborhood-serving use.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The project will be designed and constructed to conform to the structural and seismic safety requirements of the Building Code. This proposal will not impact the property's ability to withstand an earthquake.

- G. That landmarks and historic buildings be preserved.

There are no landmarks or historic buildings on the project site or within the immediate vicinity.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The Project will not affect the City's parks or open space or their access to sunlight and vistas. A shadow study was completed and concluded that the Project will not cast shadows on any property under the jurisdiction of, or designated for acquisition by, the Recreation and Park Commission.

11. **First Source Hiring.** The Project is subject to the requirements of the First Source Hiring Program as they apply to permits for residential development (Section 83.4(m) of the Administrative Code), and the Project Sponsor shall comply with the requirements of this Program as to all construction work and on-going employment required for the Project. Prior to the issuance of any building permit to construct or a First Addendum to the Site Permit, the Project Sponsor shall have a First Source Hiring Construction and Employment Program approved by the First Source Hiring Administrator, and evidenced in writing. In the event that both the Director of Planning and the First Source Hiring Administrator agree, the approval of the Employment Program may be delayed as needed.

The Project Sponsor submitted a First Source Hiring Affidavit and prior to issuance of a building permit, will execute a First Source Hiring Memorandum of Understanding and a First Source Hiring Agreement with the City's First Source Hiring Administration.

12. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
13. The Commission hereby finds that approval of the Large Project authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Large Project Authorization Application No. 2012.1333X** subject to the following conditions attached hereto as "EXHIBIT A" in general conformance with plans on file, dated July 16, 2014, and stamped "EXHIBIT B", which is incorporated herein by reference as though fully set forth.

The Planning Commission hereby adopts the MMRP attached hereto as Exhibit C and incorporated herein as part of this Motion by this reference thereto. All required mitigation measures identified in the Eastern Neighborhoods Plan EIR and contained in the MMRP are included as conditions of approval.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Large Project Authorization to the Board of Appeals within fifteen (15) days after the date of this Motion No. XXXXX. The effective date of this Motion shall be the date of this Motion if not appealed (After the 15-day period has expired) OR the date of the decision of the Board of Appeals if appealed to the Board of Appeals. For further information, please contact the Board of Appeals at (415) 575-6880, 1650 Mission Street, Room 304, San Francisco, CA 94103.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission's adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator's Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission **ADOPTED** the foregoing Motion on July 24, 2014.

Jonas P. Ionin
Acting Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED: July 24, 2014

EXHIBIT A

AUTHORIZATION

This authorization is for a Large Project Authorization to allow for the construction of a nine-story residential building fronting Folsom Street and a four-story building fronting Shipley Street with a total of up to 114 dwelling units, approximately 1,559 square feet of commercial space, and up to 80 off-street parking spaces in a below-grade garage, and modification to the requirements for rear yard, off-street parking, off-street loading, and special height exceptions, located at 923 Folsom Street, Lots 106, 141 and 142 in Assessor's Block 3753 pursuant to Planning Code Section 329 within the MUR (Mixed Use - Residential) Zoning District, and a split 85/45-X Height and Bulk District; in general conformance with plans, dated July 16, 2014, and stamped "EXHIBIT B" included in the docket for Case No. 2012.1333X and subject to conditions of approval reviewed and approved by the Commission on July 24, 2014, under Motion No. XXXXX. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on July 24, 2014, under Motion No. XXXXX.

PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. XXXXX shall be reproduced on the Index Sheet of construction plans submitted with the Site or Building permit application for the Project. The Index Sheet of the construction plans shall reference to the Large Project Authorization and any subsequent amendments or modifications.

SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new Large Project Authorization.

Conditions of Approval, Compliance, Monitoring, and Reporting

PERFORMANCE

Validity. The authorization and right vested by virtue of this action is valid for three (3) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this three-year period.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Expiration and Renewal. Should a Building or Site Permit be sought after the three (3) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Diligent pursuit. Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than three (3) years have passed since this Authorization was approved.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Extension. All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Conformity with Current Law. No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

DESIGN

Final Materials. The Project Sponsor shall continue to work with the Planning Department on the building design and the design and development of the streetscape and pedestrian elements in conformance with the Better Streets Plan. Final materials, glazing, color, texture, landscaping, and

detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6613, www.sf-planning.org

Garbage, composting and recycling storage. Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the building permit plans. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided at the ground level of the buildings.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6613, www.sf-planning.org

Rooftop Mechanical Equipment. Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

Transformer Vault. The location of individual project PG&E Transformer Vault installations has significant impacts to San Francisco streetscapes when improperly located. However, they may not have any impact if they are installed in preferred locations. Therefore, the Planning Department recommends the following preference schedule in locating new transformer vaults, in order of most to least desirable:

- A. On-site, in a basement area accessed via a garage or other access point without use of separate doors on a ground floor façade facing a public right-of-way;
- B. On-site, in a driveway, underground;
- C. On-site, above ground, screened from view, other than a ground floor façade facing a public right-of-way;
- D. Public right-of-way, underground, under sidewalks with a minimum width of 12 feet, avoiding impacts on streetscape elements, such as street trees; and based on Better Streets Plan guidelines;
- E. Public right-of-way, underground; and based on Better Streets Plan guidelines;
- F. Public right-of-way, above ground, screened from view; and based on Better Streets Plan guidelines;
- G. On-site, in a ground floor façade (the least desirable location).

Unless otherwise specified by the Planning Department, Department of Public Work's Bureau of Street Use and Mapping (DPW BSM) should use this preference schedule for all new transformer vault installation requests.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-5810, <http://sfdpw.org>

Noise, Ambient. Interior occupiable spaces shall be insulated from ambient noise levels. Specifically, in areas identified by the Environmental Protection Element, Map1, "Background Noise Levels," of the

General Plan that exceed the thresholds of Article 29 in the Police Code, new developments shall install and maintain glazing rated to a level that insulate interior occupiable areas from Background Noise and comply with Title 24.

For information about compliance, contact the Environmental Health Section, Department of Public Health at (415) 252-3800, www.sfdph.org

Noise. Plans submitted with the building permit application for the approved project shall incorporate acoustical insulation and other sound proofing measures to control noise.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

Street Trees. Pursuant to Planning Code Section 138.1 (formerly 143), the Project Sponsor shall submit a site plan to the Planning Department prior to Planning approval of the building permit application indicating that street trees, at a ratio of one street tree of an approved species for every 20 feet of street frontage along public or private streets bounding the Project, with any remaining fraction of 10 feet or more of frontage requiring an extra tree, shall be provided. The street trees shall be evenly spaced along the street frontage except where proposed driveways or other street obstructions do not permit. The exact location, size and species of tree shall be as approved by the Department of Public Works (DPW). In any case in which DPW cannot grant approval for installation of a tree in the public right-of-way, on the basis of inadequate sidewalk width, interference with utilities or other reasons regarding the public welfare, and where installation of such tree on the lot itself is also impractical, the requirements of this Section 428 may be modified or waived by the Zoning Administrator to the extent necessary.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

AFFORDABLE HOUSING

Number of Required Units. Pursuant to Planning Code Section 415.6, the Project is required to provide 12% of the proposed dwelling units as affordable to qualifying households. The Project contains 114 units; therefore, fourteen affordable units are required. The Project Sponsor will fulfill this requirement by providing the fourteen affordable units on-site. If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.

Unit Mix. The Project contains eight studios, 59 one-bedroom, and 47 two-bedroom units; therefore, the required affordable unit mix is one studio, eight one-bedroom, and five two-bedroom, for a total of fourteen affordable units. If the market-rate unit mix changes, the affordable unit mix will be modified accordingly with written approval from Planning Department staff in consultation with MOH.

Unit Location. The BMR units shall be designated on a reduced set of plans recorded as a Notice of Special Restrictions on the property prior to the issuance of first construction permit.

Phasing. If any building permit is issued for partial phasing of the Project, the Project Sponsor shall have designated not less than sixteen percent (16%) of the each phase's total number of dwelling units as on-site BMR units. Alternatively, if the Project Sponsor has entered into an agreement with the City to provide rental housing for 30 years under Section 419.5(b) of the Planning Code, the Project shall have designated not less than thirteen percent (13%) of the each phase's total number of dwelling units as on-site BMR units.

Duration. Under Planning Code Section 419.8, all units constructed pursuant to Section 419.6, must remain affordable to qualifying households for the life of the project.

Other Conditions. The Project is subject to the requirements of the Inclusionary Affordable Housing Program under Section 415 et seq. of the Planning Code and City and County of San Francisco Inclusionary Affordable Housing Program Monitoring and Procedures Manual ("Procedures Manual"). The Procedures Manual, as amended from time to time, is incorporated herein by reference, as published and adopted by the Planning Commission, and as required by Planning Code Section 415. Terms used in these conditions of approval and not otherwise defined shall have the meanings set forth in the Procedures Manual. A copy of the Procedures Manual can be obtained at the MOHCD at 1 South Van Ness Avenue or on the Planning Department or MOHCD websites, including on the internet at: <http://sf-planning.org/Modules/ShowDocument.aspx?documentid=4451>. As provided in the Inclusionary Affordable Housing Program, the applicable Procedures Manual is the manual in effect at the time the subject units are made available for sale.

- a. The affordable unit(s) shall be designated on the building plans prior to the issuance of the first construction permit by the Department of Building Inspection ("DBI"). The affordable unit(s) shall (1) reflect the unit size mix in number of bedrooms of the market rate units, (2) be constructed, completed, ready for occupancy and marketed no later than the market rate units, and (3) be evenly distributed throughout the building; and (4) be of comparable overall quality, construction and exterior appearance as the market rate units in the principal project. The interior features in affordable units should be generally the same as those of the market units in the principal project, but need not be the same make, model or type of such item as long they are of good and new quality and are consistent with then-current standards for new housing. Other specific standards for on-site units are outlined in the Procedures Manual.
- b. If the units in the building are offered for sale, the affordable unit(s) shall be sold to first time home buyer households, as defined in the Procedures Manual, whose gross annual income, adjusted for household size, does not exceed an average of ninety (90) percent of Area Median Income under the income table called "Maximum Income by Household Size derived from the Unadjusted Area Median Income for HUD Metro Fair Market Rent Area that contains San Francisco." The initial sales price of such units shall be calculated according to the Procedures Manual. Limitations on (i) reselling; (ii) renting; (iii) recouping capital improvements; (iv) refinancing; and (v) procedures for inheritance apply and are set forth in the Inclusionary Affordable Housing Program and the Procedures Manual.
- c. The Project Sponsor is responsible for following the marketing, reporting, and monitoring requirements and procedures as set forth in the Procedures Manual. MOHCD shall be

- responsible for overseeing and monitoring the marketing of affordable units. The Project Sponsor must contact MOHCD at least six months prior to the beginning of marketing for any unit in the building.
- d. Required parking spaces shall be made available to initial buyers or renters of affordable units according to the Procedures Manual.
 - e. Prior to the issuance of the first construction permit by DBI for the Project, the Project Sponsor shall record a Notice of Special Restriction on the property that contains these conditions of approval and a reduced set of plans that identify the affordable units satisfying the requirements of this approval. The Project Sponsor shall promptly provide a copy of the recorded Notice of Special Restriction to the Department and to MOHCD or its successor.
 - f. The Project Sponsor has demonstrated that it is eligible for the On-site Affordable Housing Alternative under Planning Code Section 415.6 instead of payment of the Affordable Housing Fee, and has submitted the *Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415* to the Planning Department stating that any affordable units designated as on-site units shall be sold as ownership units and will remain as ownership units for the life of the Project.
 - g. If the Project Sponsor fails to comply with the Inclusionary Affordable Housing Program requirement, the Director of DBI shall deny any and all site or building permits or certificates of occupancy for the development project until the Planning Department notifies the Director of compliance. A Project Sponsor's failure to comply with the requirements of Planning Code Section 419 et seq. shall constitute cause for the City to record a lien against the development project and to pursue any and all available remedies at law.
 - h. If the Project becomes ineligible at any time for the On-site Affordable Housing Alternative, the Project Sponsor or its successor shall pay the Affordable Housing Fee prior to issuance of the first construction permit or may seek a fee deferral as permitted under Ordinances 0107-10 and 0108-10. If the Project becomes ineligible after issuance of its first construction permit, the Project Sponsor shall notify the Department and MOH and pay interest on the Affordable Housing Fee at a rate equal to the Development Fee Deferral Surcharge Rate in Section 107A.13.3.2 of the San Francisco Building Code and penalties, if applicable.

PARKING AND TRAFFIC

Unbundled Parking. All off-street parking spaces shall be made available to Project residents only as a separate "add-on" option for purchase or rent and shall not be bundled with any Project dwelling unit for the life of the dwelling units. The required parking spaces may be made available to residents within a quarter mile of the project. All affordable dwelling units pursuant to Planning Code Section 415 shall have equal access to use of the parking as the market rate units, with parking spaces priced commensurate with the affordability of the dwelling unit. Each unit within the Project shall have the first right of refusal to rent or purchase a parking space until the number of residential parking spaces are no longer available. No conditions may be placed on the purchase or rental of dwelling units, nor may

homeowner's rules be established, which prevent or preclude the separation of parking spaces from dwelling units.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Car Share. Pursuant to Planning Code Section 166, at least one car share space shall be made available, at no cost, to a certified car share organization for the purposes of providing car share services for its service subscribers.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Bicycle Parking. Pursuant to Planning Code Sections 155.1, 155.4, and 155.5, the Project shall provide no fewer than 122 bicycle parking spaces (114 Class 1 spaces and 8 Class 2 spaces).

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Parking Maximum. Pursuant to Planning Code Section 151.1, the Project shall provide no more than 86 off-street parking spaces.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Managing Traffic During Construction. The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation impacts during construction of the Project.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

PROVISIONS

First Source Hiring. The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415-581-2335, www.onestopSF.org

Eastern Neighborhoods Infrastructure Impact Fee. Pursuant to Planning Code Section 423 (formerly 327), the Project Sponsor shall comply with the Eastern Neighborhoods Public Benefit Fund provisions through payment of an Impact Fee pursuant to Article 4.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

MONITORING

Enforcement. Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Revocation due to Violation of Conditions. Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

OPERATION

Sidewalk Maintenance. The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org/>

Garbage, Recycling, and Composting Receptacles. Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-.5810, <http://sfdpw.org>

Community Liaison. Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

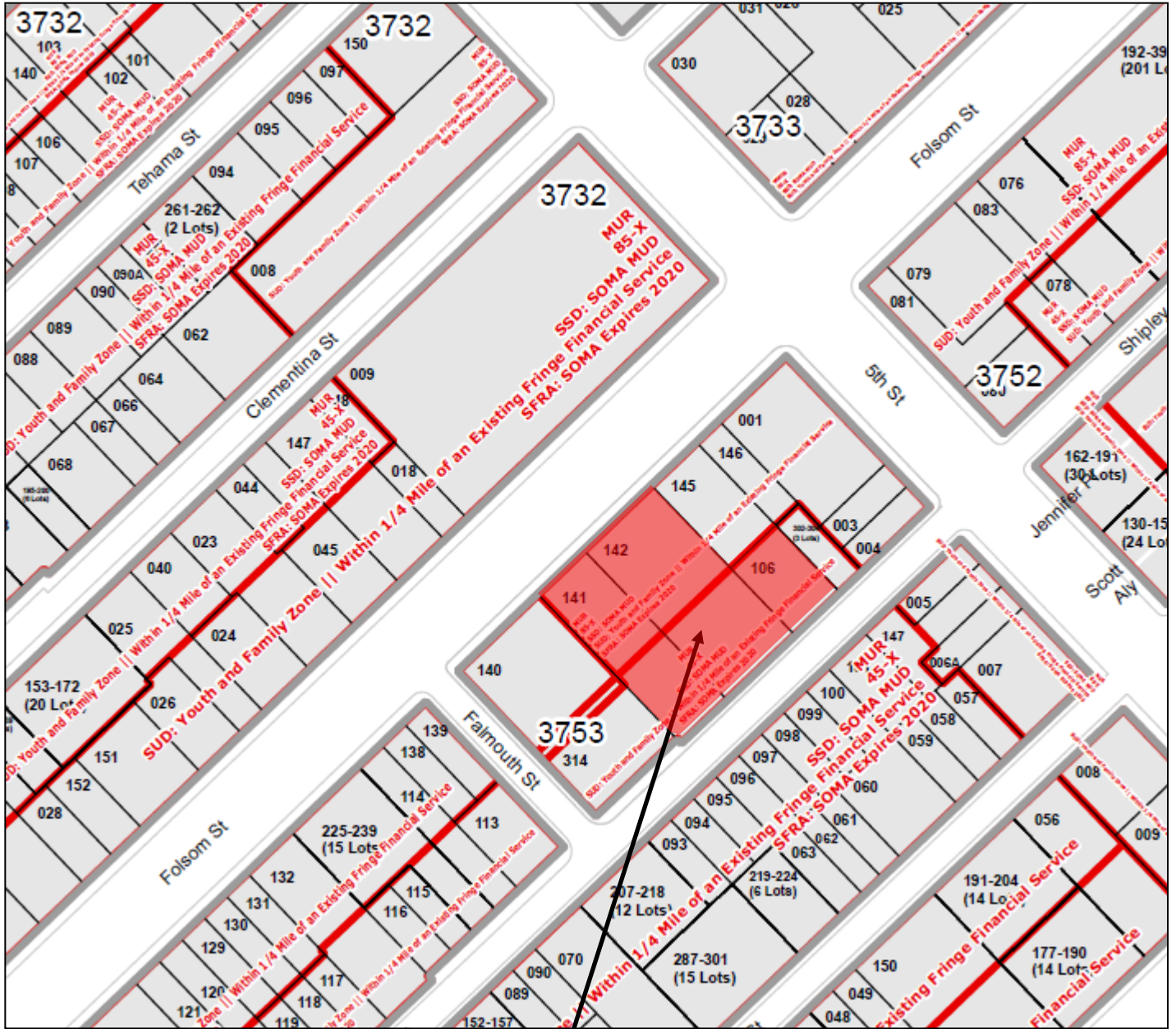
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Mitigation Measures. Mitigation measures described in the MMRP for the Eastern Neighborhoods Plan EIR (Case No. 2012.1333E) attached as Exhibit C are necessary to avoid potential significant effects of the proposed project and have been agreed to by the project sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

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Block Book Map



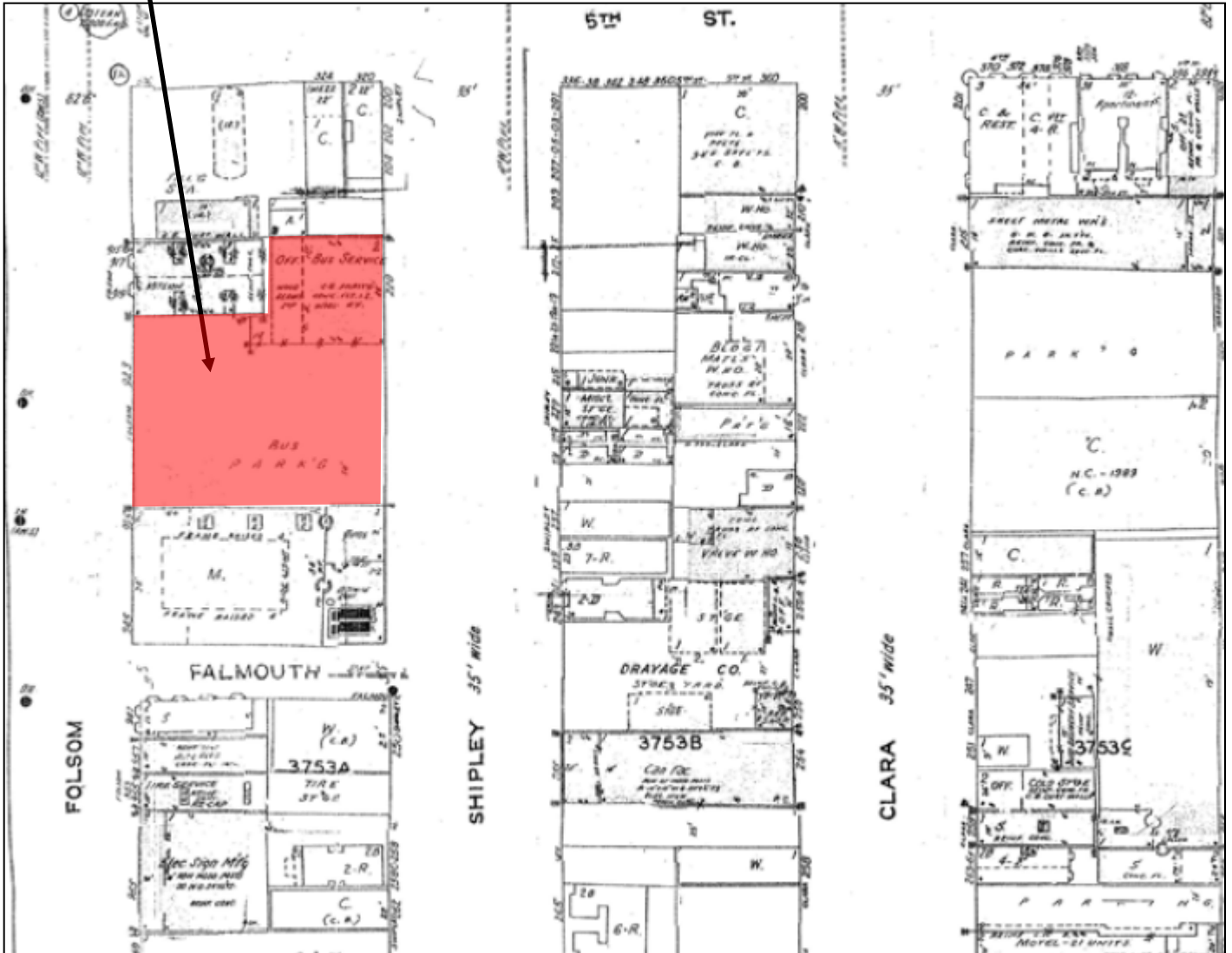
SUBJECT PROPERTIES



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Sanborn Map*

SUBJECT PROPERTIES

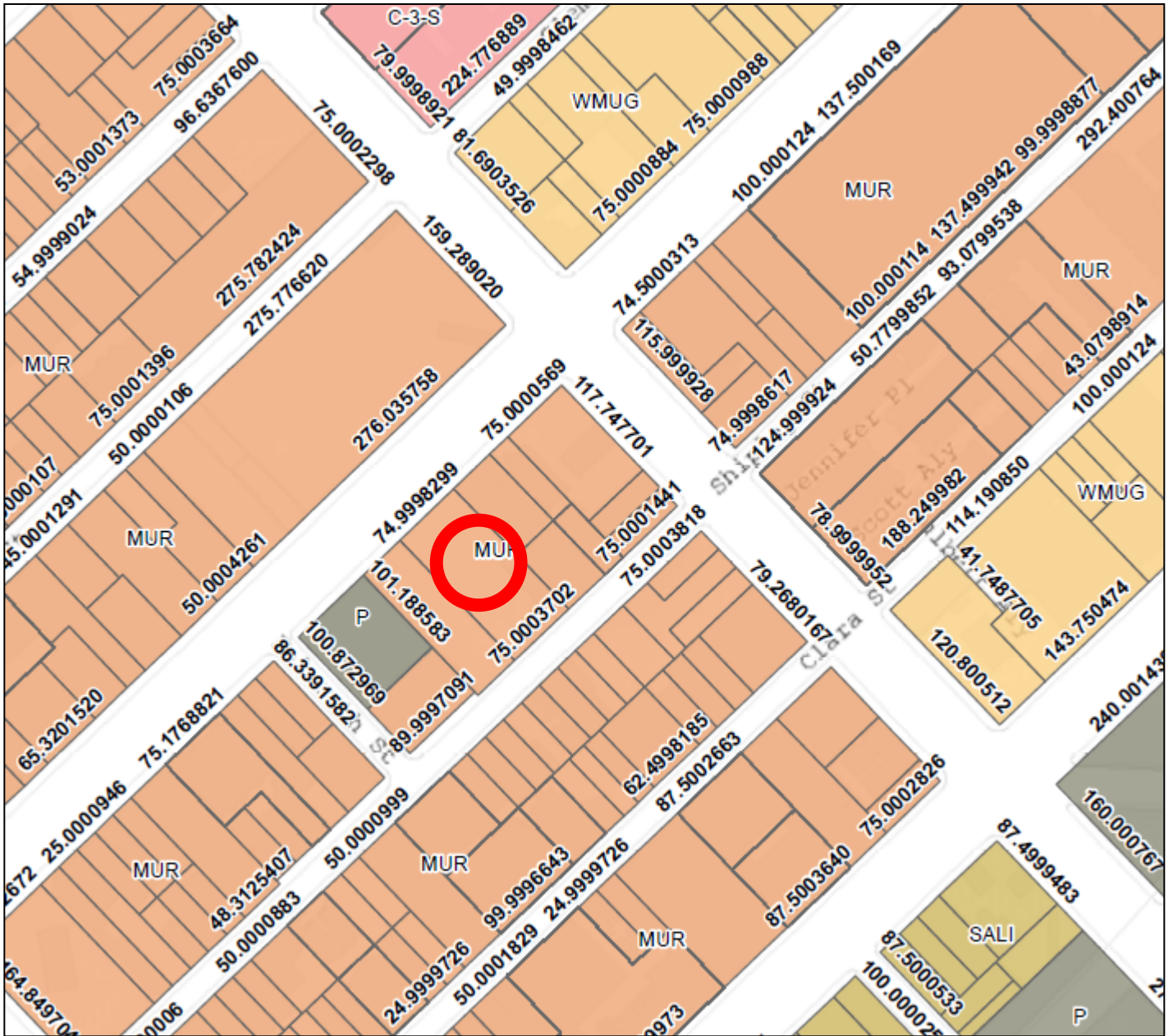


*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Zoning Map



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Aerial Photo

facing south



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Aerial Photo

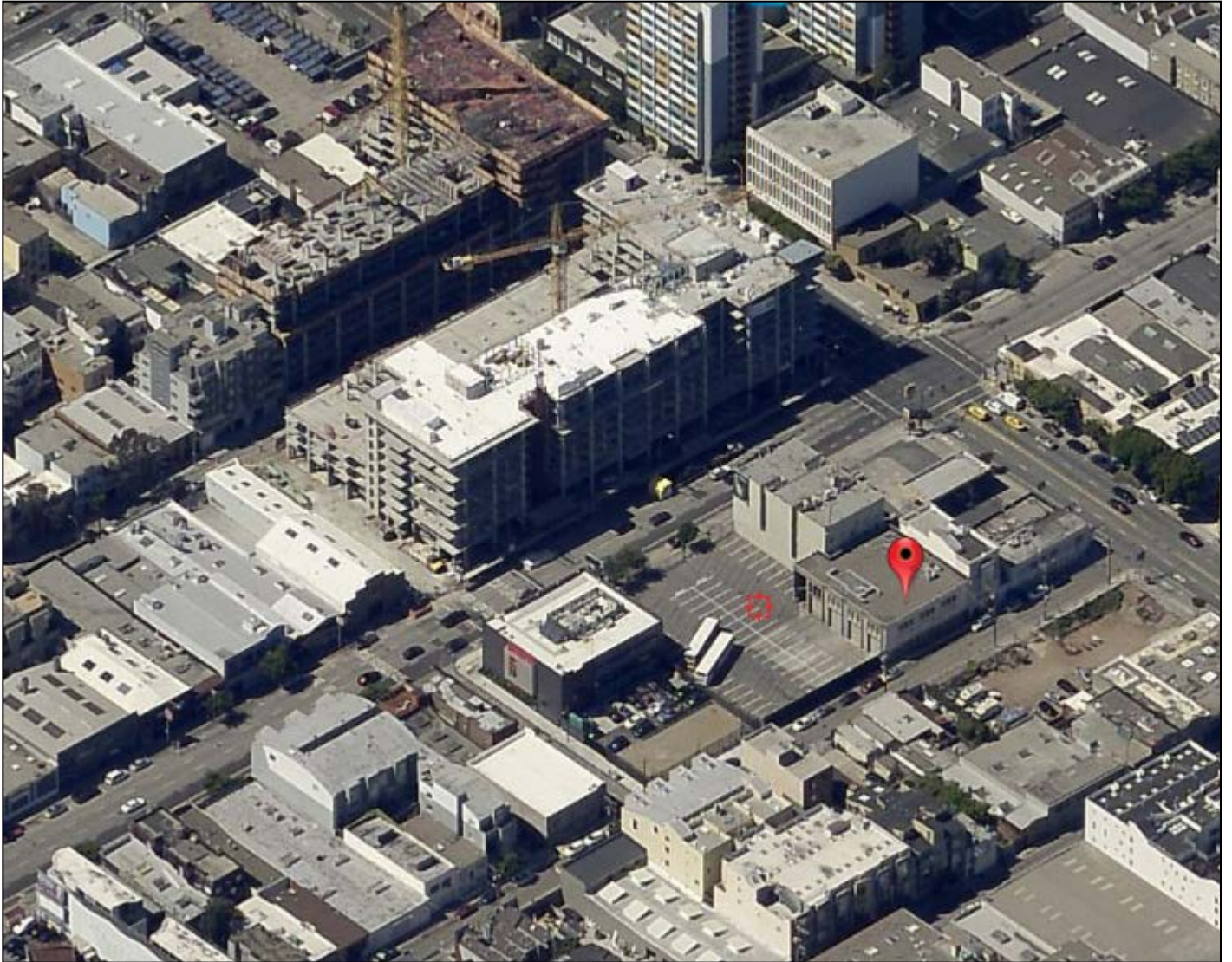
facing west



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Aerial Photo

facing north



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Aerial Photo

view facing east



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Site Photo

Folsom Street Frontage



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street

Site Photo

Shipley Street Frontage



Large Project Authorization
Case No. 2012.1333X
923 Folsom Street



SAN FRANCISCO PLANNING DEPARTMENT

Certificate of Determination EXEMPTION FROM ENVIRONMENTAL REVIEW

Case No.: 2012.1333E
 Project Address: 923 Folsom Street
 Zoning: MUR (Mixed Use Residential) Use District
 45-X / 85-X Height and Bulk District
 Block/Lot: 3753/106, 141, 142
 Lot Size: 24,375 square feet
 Plan Area: East SoMa Subarea of the Eastern Neighborhoods Area Plan
 Project Sponsor: Christopher Davenport, Trumark Homes, (925) 309-2503
 Staff Contact: Tania Sheyner, Tania.Sheyner@sfgov.org, (415) 575-9127

1650 Mission St.
 Suite 400
 San Francisco,
 CA 94103-2479

Reception:
 415.558.6378

Fax:
 415.558.6409

Planning
 Information:
 415.558.6377

PROJECT DESCRIPTION:

The project site is located on a block bound by Folsom Street to the north, Fifth Street to the east, Shipley Street to the south and Falmouth Street to the west, in San Francisco's South of Market neighborhood. The project site encompasses three contiguous parcels and has frontages along both Folsom and Shipley Streets. It spans 24,375 square feet of total space and currently contains a 3,750-square-foot, two-story, commercial office building (constructed in 1967) and surface parking areas.

The proposed project includes the demolition of the existing structure and construction of a mixed-use project consisting of two buildings – a nine-story building (approximately 85 feet in height) fronting Folsom Street and a four-story building (approximately 44 feet in height) fronting Shipley Street.

(Continued on next page.)

EXEMPT STATUS:

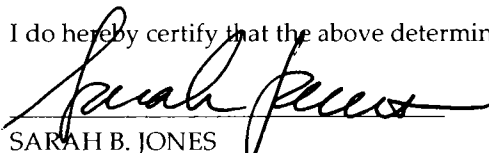
Exempt per Section 15183 of the California Environmental Quality Act (CEQA) Guidelines and California Public Resources Code Section 21083.3

REMARKS:

(See next page.)

DETERMINATION:

I do hereby certify that the above determination has been made pursuant to State and Local requirements.


 SARAH B. JONES

Environmental Review Officer

June 20, 2014
 Date

cc: Christopher Davenport, Project Sponsor; Supervisor Jane Kim, District 6; Doug Vu, Current Planning Division; Virna Byrd, M.D.F.; Exemption/Exclusion File

PROJECT DESCRIPTION (continued):

The project would include 115 dwelling units, consisting of 8 junior one-bedroom units, 61 one-bedroom units, and 46 two-bedroom units. In addition, it would provide approximately 1,900 square feet of commercial space on the ground level of the Folsom Street structure. The two buildings would share a basement-level garage, which would contain 87 stacked residential off-street parking spaces as well as 104 secured bicycle spaces. The project would also provide open space in the form of a mid-block courtyard and a roof deck atop the Shipley Street structure. Pedestrian access to the residential uses would be provided via Shipley and Folsom Street entrances, while access to commercial uses would be provided via a storefront entrance on Folsom Street. Vehicular access into the basement-level garage would be provided via a ramp off of Shipley Street.

Project Approval

The approval of a Large Project Authorization by the Planning Commission (per Planning Code Section 329) is the Approval Action for the proposed project. The Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.

Other approvals that would be required to implement the proposed project include the following: approval of a site permit (*Planning Department, Department of Building Inspection*); approval of grading and building permits (*Planning Department and Department of Building Inspection*); approval of a stormwater control plan (*San Francisco Public Utilities Commission*); approval of project compliance with the Stormwater Control Guidelines (*Department of Public Works*); approval of a three-lot merger (*Department of Public Works*); and approval of the installation of a new north-south crosswalk along Shipley Street at the intersection at Fifth Street (*San Francisco Municipal Transportation Agency*).

REMARKS:

CEQA Guidelines Section 15183 provides an exemption from environmental review for projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an Environmental Impact Report (EIR) was certified, except as might be necessary to examine whether there are project-specific effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that: a) are peculiar to the project or parcel on which the project would be located; b) were not analyzed as significant effects in a prior EIR on the zoning action, general plan or community plan with which the project is consistent; c) are potentially significant off-site and cumulative impacts which were not discussed in the underlying EIR; and d) are previously identified in the EIR, but which are determined to have a more severe adverse impact than that discussed in the underlying EIR. Section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for the project solely on the basis of that impact.

This determination evaluates the potential project-specific environmental effects peculiar to the 923 Folsom Street project described above, and incorporates by reference information contained within the Eastern Neighborhoods Rezoning and Area Plans Final EIR (FEIR) (Planning Department Case No. 2004.0160E and State Clearinghouse No. 2005032048), which is the underlying EIR for the proposed project. Project-specific studies summarized in this determination were prepared for the proposed project to determine if there would be any additional potentially significant impacts attributable to (i.e., "peculiar" to) the proposed project.

This determination assesses the proposed project's potential to cause environmental impacts and concludes that the proposed project would not result in new, significant environmental effects, or effects of greater severity than were already analyzed and disclosed in the FEIR. This determination does not identify new or additional information that would alter the conclusions of the FEIR. In addition, this determination identifies mitigation measures contained in the FEIR that would be applicable to the proposed project. Relevant information pertaining to prior environmental review conducted for the FEIR as well as an evaluation of potential environmental effects are provided in the Community Plan Exemption (CPE) Checklist for the proposed project.¹

BACKGROUND:

After several years of analysis, community outreach, and public review, the Eastern Neighborhoods FEIR was adopted in December 2008. The Eastern Neighborhoods FEIR was adopted in part to support housing development in some areas previously zoned to allow industrial uses, while preserving an adequate supply of space for existing and future production, distribution, and repair (PDR) employment and businesses. The Eastern Neighborhoods FEIR also included changes to existing height and bulk districts in some areas, including the project site at 923 Folsom Street.

During the Eastern Neighborhoods adoption phase, the Planning Commission held public hearings to consider the various aspects of the proposed area plans, and Planning Code and Zoning Map amendments. On August 7, 2008, the Planning Commission certified the Eastern Neighborhoods FEIR by Motion 17659 and adopted the Preferred Project for final recommendation to the Board of Supervisors.^{2,3}

In December 2008, after further public hearings, the Board of Supervisors approved and the Mayor signed the Eastern Neighborhoods Rezoning and Planning Code amendments. New zoning districts include districts that would permit PDR uses in combination with commercial uses; districts mixing residential and commercial uses and residential and PDR uses; and new residential-only districts. The districts replaced existing industrial, commercial, residential single-use, and mixed-use districts.

The Eastern Neighborhoods FEIR is a comprehensive programmatic document that presents an analysis of the environmental effects of implementation of the Eastern Neighborhoods Rezoning and Area Plans, as well as the potential impacts under several proposed alternative scenarios. The Eastern Neighborhoods Draft EIR evaluated three rezoning alternatives, two community-proposed alternatives which focused largely on the Mission District, and a "No Project" alternative. The alternative selected, or the Preferred Project, represents a combination of Options B and C. The Planning Commission adopted the Preferred Project after fully considering the environmental effects of the Preferred Project and the various scenarios discussed in the FEIR.

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- 1 The CPE Checklist is available for review at the Planning Department, 1650 Mission Street, Suite 400, San Francisco, in Case File No. 2012.1333E.
 - 2 San Francisco Planning Department. *Eastern Neighborhoods Rezoning and Area Plans Final Environmental Impact Report (FEIR)*, Planning Department Case No. 2004.0160E, certified August 7, 2008. Available online at: <http://www.sf-planning.org/index.aspx?page=1893>, accessed August 17, 2012.
 - 3 San Francisco Planning Department. San Francisco Planning Commission Motion 17659, August 7, 2008. Available online at: <http://www.sf-planning.org/Modules/ShowDocument.aspx?documentid=1268>, accessed August 17, 2012.

A major issue of discussion in the Eastern Neighborhoods rezoning process was the degree to which existing industrially-zoned land would be rezoned to primarily residential and mixed-use districts, thus reducing the availability of land traditionally used for PDR employment and businesses. Among other topics, the Eastern Neighborhoods FEIR assesses the significance of the cumulative land use effects of the rezoning by analyzing its effects on the City's ability to meet its future PDR space needs as well as its ability to meet its housing needs as expressed in the City's General Plan.

As a result of the Eastern Neighborhoods rezoning process, the project site has been rezoned to MUR (Mixed Use-Residential) District. The MUR use district is intended to provide housing opportunities within the eastern portion of the South of Market (East SoMa). The district controls are intended to facilitate the development of high-density, mid-rise housing, including family-sized housing and residential hotels. The district is designed to encourage the expansion of retail, business service and commercial (office) uses and cultural arts activities and also serves as a buffer between the higher-density, predominantly commercial area of Yerba Buena Center to the east and the lower-scale, mixed use service/industrial and housing area west of Sixth Street. The proposed project and its relation to PDR land supply and cumulative land use effects is discussed further in CPE Checklist, under Land Use. The 923 Folsom Street site, which is located in the East SoMa Plan Area of the Eastern Neighborhoods, was designated as a site with building up to 85 feet in height along Folsom Street and 45 feet in height along Shipley Street.

Individual projects that could occur in the future under the Eastern Neighborhoods Rezoning and Area Plans will undergo project-level environmental evaluation to determine if they would result in further impacts specific to the development proposal, the site, and the time of development and to assess whether additional environmental review would be required. This determination concludes that the proposed project at 923 Folsom Street is consistent with and was encompassed within the analysis in the Eastern Neighborhoods FEIR. This determination also finds that the Eastern Neighborhoods FEIR adequately anticipated and described the impacts of the proposed 923 Folsom Street project, and identified the mitigation measures applicable to the 923 Folsom Street project. The proposed project is also consistent with the zoning controls and the provisions of the Planning Code applicable to the project site.^{4,5} Therefore, no further CEQA evaluation for the 923 Folsom Street project is required. In sum, the Eastern Neighborhoods FEIR and this Certificate of Exemption for the proposed project comprise the full and complete CEQA evaluation necessary for the proposed project.

PROJECT SETTING:

The project site is located on a block bound by Folsom Street to the north, Fifth Street to the east, Shipley Street to the south, and Falmouth Street to the west, in San Francisco's South of Market neighborhood. The project site, which is irregular in shape, encompasses three contiguous parcels and has frontages along both Folsom and Shipley Streets. To the east, the project site is bordered by a four-story hotel (with ground-floor commercial uses) along the Folsom Street frontage and a four-story multi-family residential

4 Adam Varat, San Francisco Planning Department, *Community Plan Exemption Eligibility Determination, Citywide Planning and Policy Analysis, 923 Folsom Street*, June 20, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

5 Jeff Joslin, San Francisco Planning Department, *Community Plan Exemption Eligibility Determination, Current Planning, 923 Folsom Street*, November 22, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

building along the Shipley Street frontage. To the west, the project site is bordered by a City-owned fire station (Fire Station 1).

The project vicinity is characterized by a mix of building types, sizes, and architectural styles. Along Folsom Street, the dominant feature on the project block is the eight-story mixed-use building at 900 Folsom Street (on the corner of Folsom and Fifth Streets), which is currently under construction. Other buildings along Folsom Street include one- to four-story light-industrial and commercial uses as well as a gas station. Along Shipley Street are one- to four-story primarily residential buildings and an empty lot on the corner of Shipley and Fifth Street that is being used as a surface parking lot.

POTENTIAL ENVIRONMENTAL EFFECTS:

The Eastern Neighborhoods FEIR included analyses of environmental issues including: land use; plans and policies; visual quality and urban design; population, housing, business activity, and employment (growth inducement); transportation; noise; air quality; parks, recreation and open space; shadow; archeological resources; historic architectural resources; hazards; and other issues not addressed in the previously issued initial study for the Eastern Neighborhoods project. The proposed 923 Folsom Street project is in conformance with the height, use and density for the site described in the Eastern Neighborhoods FEIR and would represent a small part of the growth that was forecast for the Eastern Neighborhoods. Thus, the project analyzed in the Eastern Neighborhoods FEIR considered the incremental impacts of the proposed 923 Folsom Street project. As a result, the proposed project would not result in any new or substantially more severe impacts than were identified in the Eastern Neighborhoods FEIR.

Significant and unavoidable impacts were identified in the Eastern Neighborhoods FEIR for the following topics: land use, historic architectural resources, transportation and circulation, and shadow. The proposed project would not contribute to the significant unavoidable impacts identified in the FEIR. Specifically, the proposed project would not contribute to the significant and unavoidable land use impact because the project is not located in the Western SoMa subarea and is not being rezoned from PDR. In regards to significant and unavoidable transportation impacts related to transit, as discussed in the CPE Checklist, the proposed project would not contribute significantly to impacts on Muni Service. The proposed project would also not contribute to significant and unavoidable historical resources impacts since the proposed project would not include the demolition of a historical resource (based on the Department's South of Market Area Historic Resource Survey, the project site does not contain any historical structure, sites, or architectural features). Lastly, the proposed project would not contribute to significant and unavoidable shadow impacts since the proposed project would not result in new shadows on any nearby parks.

The Eastern Neighborhoods FEIR identified feasible mitigation measures to address significant impacts related to: Noise (F-1, F-2, F-3, F-4, F-5, and F-6), Air Quality (G-1, G-2, G-3, and G-4), Archeological Resources (J-1, J-2, and J-3), Historical Resources (K-1, K-2, and K-3), Hazardous Materials (L-1), and Transportation (E-1, E-2, E-3, E-4, E-5, E-6, E-7, E-8, E-9, E-10, and E-11).

As analyzed and discussed in the CPE Checklist, the following mitigation measures identified in the FEIR do not apply to the proposed project. Mitigation Measures F-1 and F-2 do not apply to the proposed project, because it would not include pile-driving or other particularly noisy construction methods. Mitigation Measure F-3 does not apply to the proposed project, because it only applies to projects that are

not already subject to California Noise Insulation Standards in Title 24 and, as a multi-unit residential development, the project is subject to requirements of Title 24. Mitigation Measure F-5 does not apply to the proposed project because the project would construct residential and commercial uses, which are not considered noise-generating uses that would be expected to generate noise levels in excess of ambient noise in the proposed project site vicinity. Mitigation Measures G-3 and G-4 are not applicable to the proposed project because the project would not include any uses that would emit diesel particulate matter or other toxic air contaminants. Mitigation Measure J-1 is not applicable to the proposed project because the project site is not located within Archeological Mitigation Zone A, which is an area for which a final archeological research design and treatment plan is on file at the Northwest Information Center and the Planning Department. Mitigation Measure J-3 is also not applicable to the proposed project because the project site is not located within the Mission Dolores Archeological District. Mitigation Measures K-1 through K-3 are not applicable to the proposed project because the project does not involve demolition of a historical resource. Lastly, Mitigation Measures E-1 through E-11 are not applicable to the proposed project because they all call for improvements and programs that are associated with the implementation of the *Eastern Neighborhoods Rezoning and Area Plans* rather than a specific development project.

As discussed in the CPE Checklist, Eastern Neighborhoods Plan FEIR Mitigation Measures F-4, F-6, G-1, G-2, J-2, L-1 were determined to apply to the proposed project for the following reasons. The project would be subject to Mitigation Measures F-4 and F-6 because the project site is located along streets with noise levels above 60 dBA (Ldn) and because the project would site noise-sensitive uses and open space in a noisy environment. The project would also be subject to the portion of Mitigation Measure G-1 that has not been superseded by the Construction Dust Control Ordinance, which is the portion that addresses maintenance and operation of construction equipment. This measure requires individual projects that include construction activities to include dust control measures and maintain and operate construction equipment so as to minimize exhaust emissions of particulates and other pollutants. The proposed project would also be subject to Mitigation Measure G-2, because the project site is located within an identified air pollutant exposure zone (i.e., an area that experiences higher levels of air pollution) and the proposed project would, therefore, have the potential to expose sensitive receptors to substantial concentrations of air pollutants. Mitigation Measure J-2 is applicable to the project because it applies to properties for which no archeological assessment report has been prepared or for which the archeological documentation is incomplete or inadequate to serve as an evaluation of potential effects on archeological resources under CEQA, which includes the project property. Lastly, Mitigation Measure L-1 is applicable to the proposed project because it would involve demolition of an existing structure and would need to address the removal of hazardous building materials. Please see the attached Mitigation Monitoring and Reporting Program (MMRP) for the complete text of the applicable mitigation measures.

With implementation of these mitigation measures the proposed project would not result in significant impacts beyond those analyzed in the FEIR.⁶

Public Notice and Comment

A "Notification of Project Receiving Environmental Review" was mailed on July 1, 2013 to adjacent occupants and owners of properties within 300 feet of the project site. One comment was received stating concern with proposing access to the garage off of Shipley Street. The commenter stated that, given

⁶ Please refer the CPE Checklist for a complete discussion.

Shipley Street's narrow width and its location in the "fire zone," it could be overtaxed with new vehicles entering and exiting the proposed garage. This concern is addressed in the transportation section in the CPE Checklist. Overall, concerns and issues raised by the public in response to the notice were taken into consideration and incorporated in the environmental review as appropriate for CEQA analysis. The proposed project would not result in significant adverse environmental impacts associated with the issues identified by the public.

Conclusion

The Eastern Neighborhoods Plan FEIR incorporated and adequately addressed all potential impacts of the proposed 923 Folsom Street project. As described above, the proposed 923 Folsom Street project would not have any project-specific significant adverse effects that are peculiar to the project or its site that were not examined in the Eastern Neighborhoods Plan FEIR, nor has any new or additional information come to light that would alter the conclusions of the Eastern Neighborhoods Plan FEIR. Thus, the proposed project would not have any new significant effects on the environment not previously identified in the Eastern Neighborhoods Plan FEIR, nor would any environmental impacts be substantially greater than described in the Eastern Neighborhoods Plan FEIR. Therefore, the proposed project is exempt from further environmental review pursuant to Section 21083.3 of CEQA and Section 15183 of the CEQA Guidelines.



SAN FRANCISCO PLANNING DEPARTMENT

COMMUNITY PLAN EXEMPTION CHECKLIST

Case No.: 2012.1333E
Project Title: 923 Folsom Street
Zoning: MUR (Mixed Use Residential) Use District
45-X / 85-X Height and Bulk District
Block/Lot: 3753/106, 141, 142
Lot Size: 24,375 square feet
Plan Area: East SoMa Subarea of the Eastern Neighborhoods Area Plan
Project Sponsor: Christopher Davenport, Trumark Homes
(925) 309-2503
Staff Contact: Tania Sheyner – (415) 575-9127
Tania.Sheyner@sfgov.org

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

PROJECT DESCRIPTION:

Project Location

The project site is located on a block bound by Folsom Street to the north, Fifth Street to the east, Shipley Street to the south, and Falmouth Street to the west, in San Francisco's South of Market neighborhood.¹ The project site (Assessor's Block 3753, Lots 106, 141, and 142), which is irregular in shape, encompasses three contiguous parcels and has frontages along both Folsom and Shipley Streets. It spans 24,375 square feet of total space and currently contains an approximately 11,000-square-foot, two-story, commercial office building (constructed in 1967) in the southeastern portion of the site (along Shipley Street) and surface parking areas throughout the remainder of the site. The surface parking lot spans approximately 18,875 square feet of the site, while the existing building takes up approximately 5,500 square feet of the site. The site was previously occupied by a bus parking lot for a passenger bus operation and the building housed company management and bus maintenance facilities. Currently, the site operates as a parking lot for commuters who work nearby.

A chain-link fence borders the site along both Folsom and Shipley Street frontages, and a curb cut along Folsom Street provides vehicular access to the site. Pedestrian access to the existing commercial building is provided through the parking lot. No trees or landscaping currently exist on the project site. Five street trees are planted along the Folsom Street sidewalk with none along the Shipley Street sidewalk. The project site is flat.

To the east, the project site is bordered by a four-story hotel (with ground-floor commercial uses) along the Folsom Street frontage and a four-story multi-family residential building along the Shipley Street frontage. To the west, the project site is bordered by a City-owned fire station (Fire Station 1).

The project site is within the Mixed-Use Residential (MUR) Use District and within the 85-X Height and Bulk District along Folsom Street and 45-X Height and Bulk District along Shipley Street. Other land uses

¹ In the South of Market area, streets that run in the northwest/southeast direction are generally considered north-south streets, whereas streets that run in the southwest/northeast direction are generally considered east-west streets. This convention is used throughout this document.

on the project block include residential, commercial, and light industrial (automotive) uses as well as a gas station.

Project Characteristics

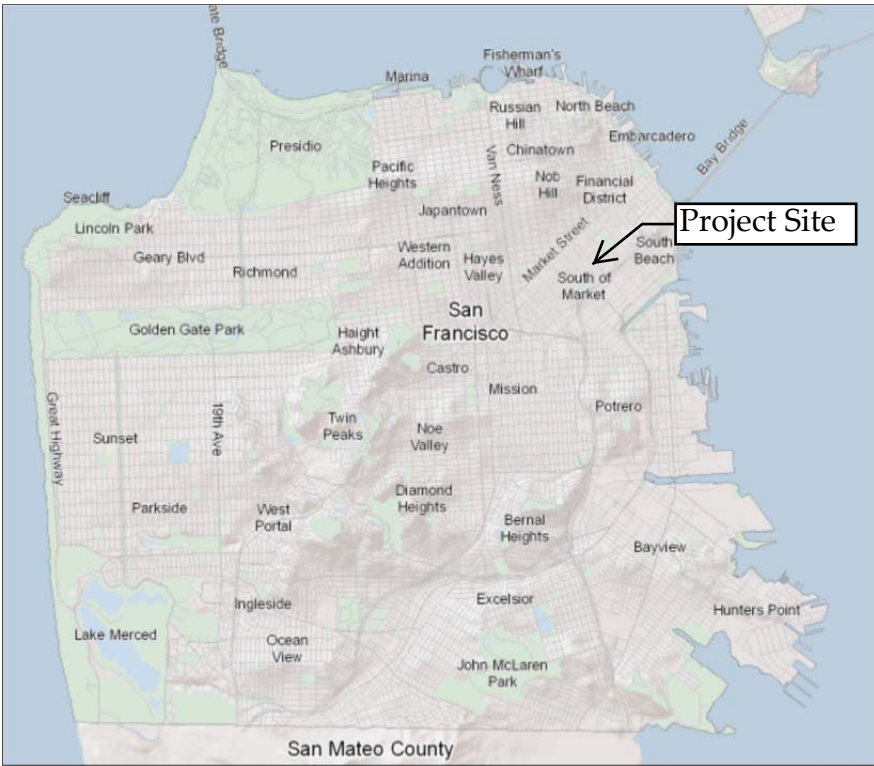
The proposed project would demolish the existing structure and surface parking lot and construct a mixed-use residential project, encompassing a total of approximately 139,000 sf, which would include 115 dwelling units, 1,900 square feet of commercial space, 87 residential off-street parking spaces, and a mid-block courtyard. The proposed unit mix would include 8 junior one-bedroom units ranging from 425 to 500 square feet, 61 one-bedroom units ranging from 539 to 741 square feet, and 46 two-bedroom units ranging from 822 to 976 square feet. The residential uses would be contained within two connected buildings – a nine-story building (approximately 85 feet in height) fronting Folsom Street and a four-story building (approximately 44 feet in height) fronting Shipley Street. The Folsom Street structure would also contain commercial uses on the ground level, currently envisioned as a single-tenant space. The proposed buildings would occupy the entire parcel and would be connected by a corridor along the site's western frontage. With the exception of this connection, which would span four stories, the two buildings would appear distinct and would be separated by a mid-block interior courtyard at the ground level.

The ground floor of the Folsom Street structure would contain a double-height lobby and a mail area adjacent to the commercial space, with elevators and residential units across a hallway. Additional residential units would be located on the second story. On levels three through nine, the Folsom Street structure would consist of a double-loaded corridor arrangement, with units located along both sides of a linear hallway. All levels of the Shipley Street structure would likewise consist of a double-loaded corridor arrangement. No elevators would serve the Shipley Street structure.

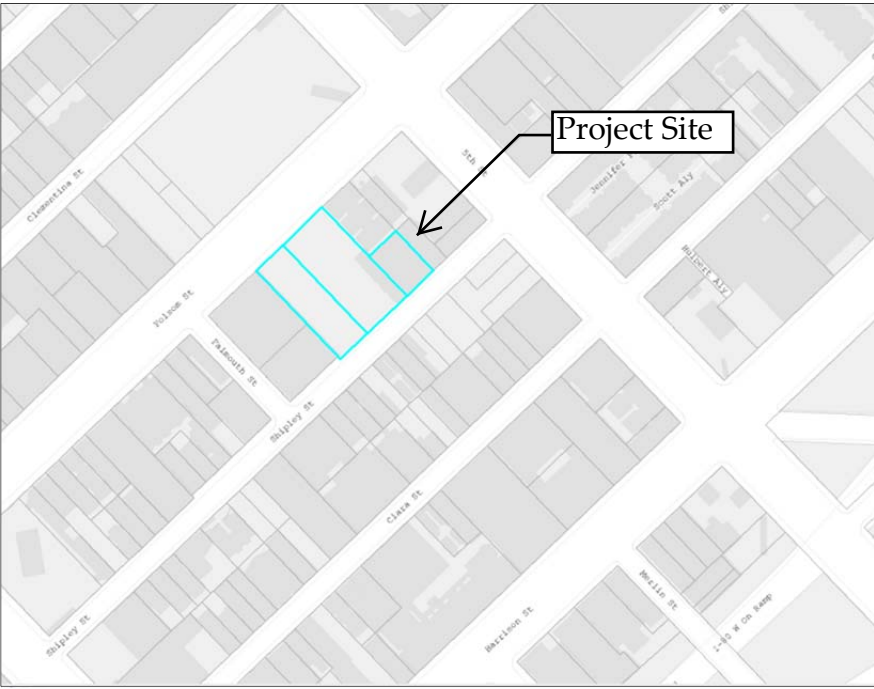
The below-grade parking level would extend approximately 20 feet below grade and would contain a total of 87 parking spaces, consisting of 81 parking spaces in mechanical stackers, 3 handicap-accessible spaces, 1 standard space, and 2 car-share spaces. The parking facility would be for residential use only. The basement level would also contain 104 Class 1 bicycle spaces, as well as utilities, mechanical rooms, trash and residential storage space. The garage would be accessible via a new 10-foot-wide curb cut on the west side of the Shipley Street frontage. The existing curb along Folsom Street would be abandoned and the curb would be leveled out to the existing sidewalk elevation.

Pedestrian access to the residential uses would be provided via Shipley and Folsom Street entrances, while access to commercial uses would be provided via a storefront entrance on Folsom Street. Vehicular access into the proposed garage would be provided via a ramp off of Shipley Street.

As noted above, the project sponsor proposes to provide open space in the form of a ground-level mid-block courtyard, an open space at the fifth floor roof deck atop the Shipley Street structure, and residential balconies. The ground-level courtyard, which would be approximately 5,700-sf in size, would be accessible to the buildings' residents via residential lobbies on the ground level. It would be developed with terraces for units facing the courtyard, a common gathering space for residents of the building, and raised planters landscaped with ground cover, shrubs and trees. The rooftop open space atop the Shipley Street structure, which would be approximately 8,800 sf in size, would also be accessible to all residents and would contain seating and gathering areas, an outdoor grill, dining spaces and raised planters with landscaping.



Citywide Location Map

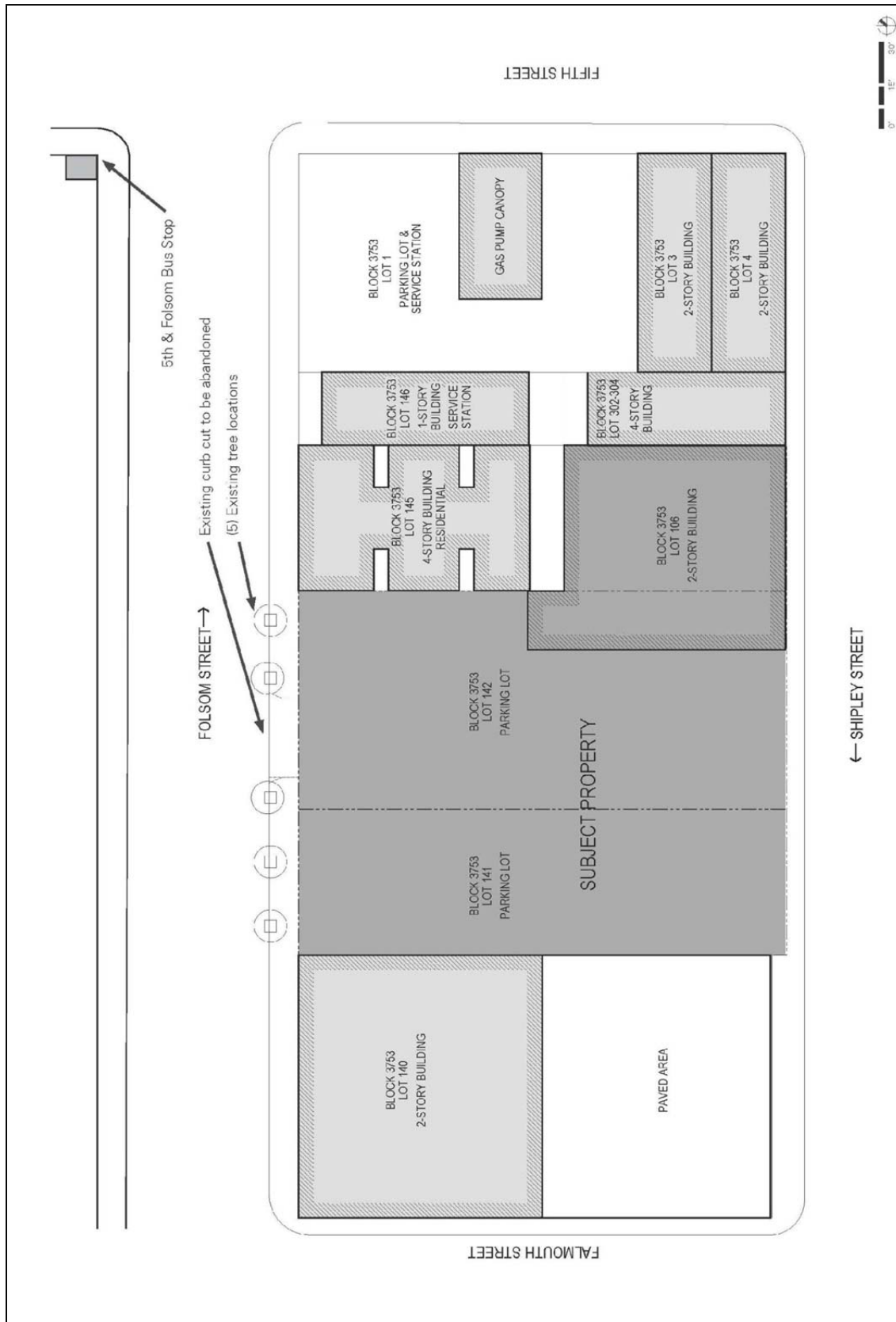


Project Vicinity Map

Source: San Francisco Planning Department, 2014

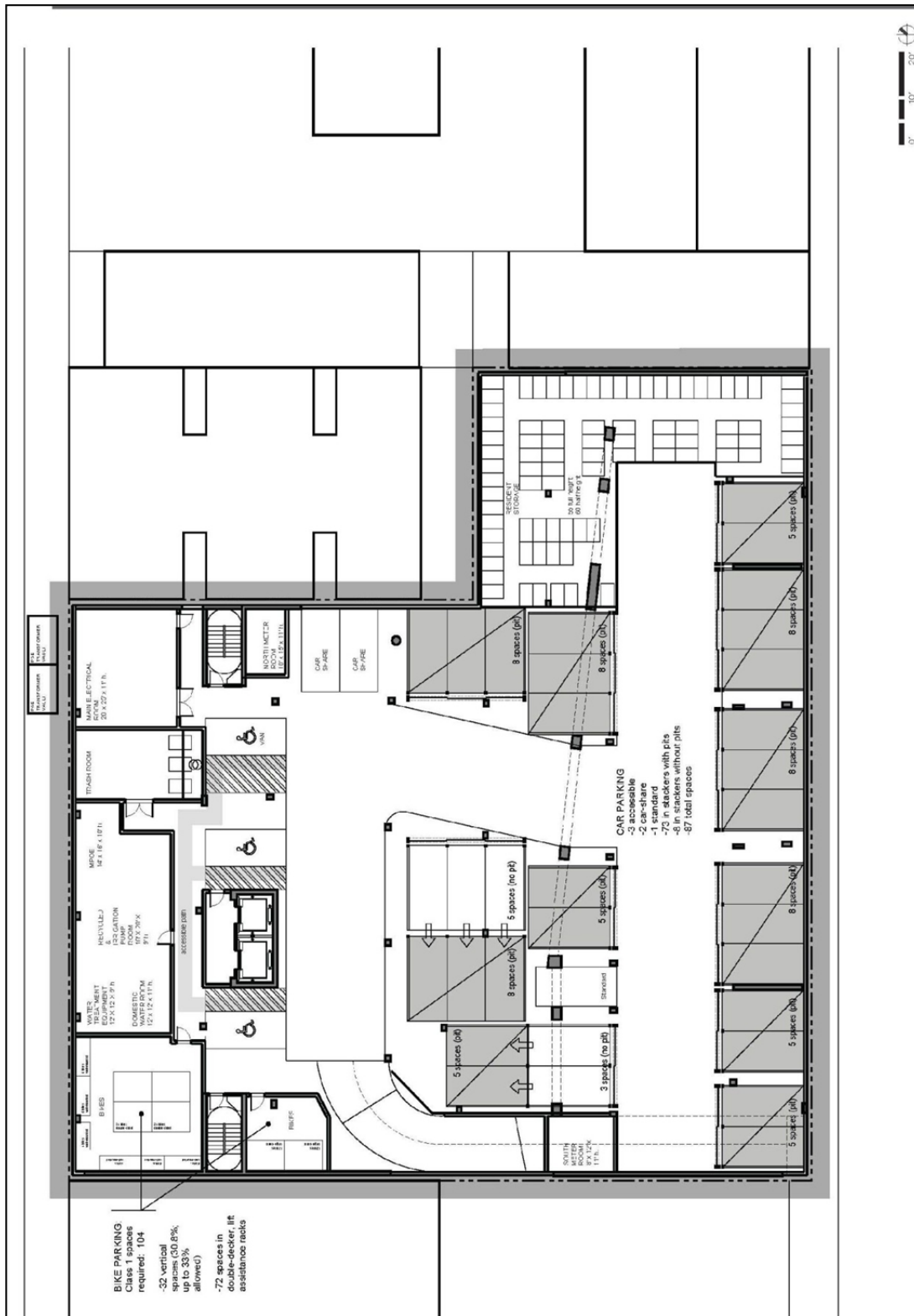
NOT TO SCALE

Figure 1: Project Site Location



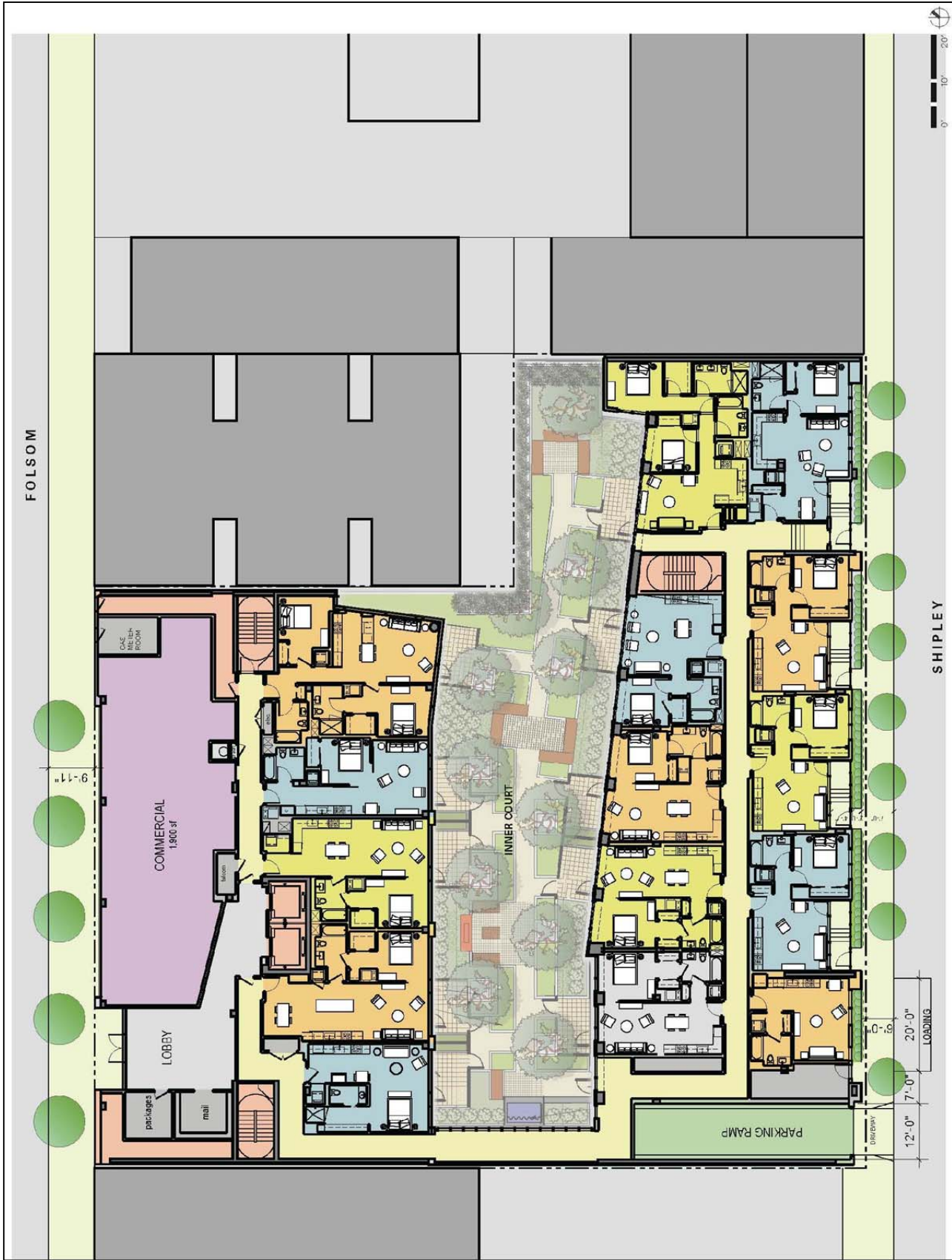
Source: Solomon Cordwell Buenz, 2013
 NOT TO SCALE

Figure 2: Proposed Site Plan



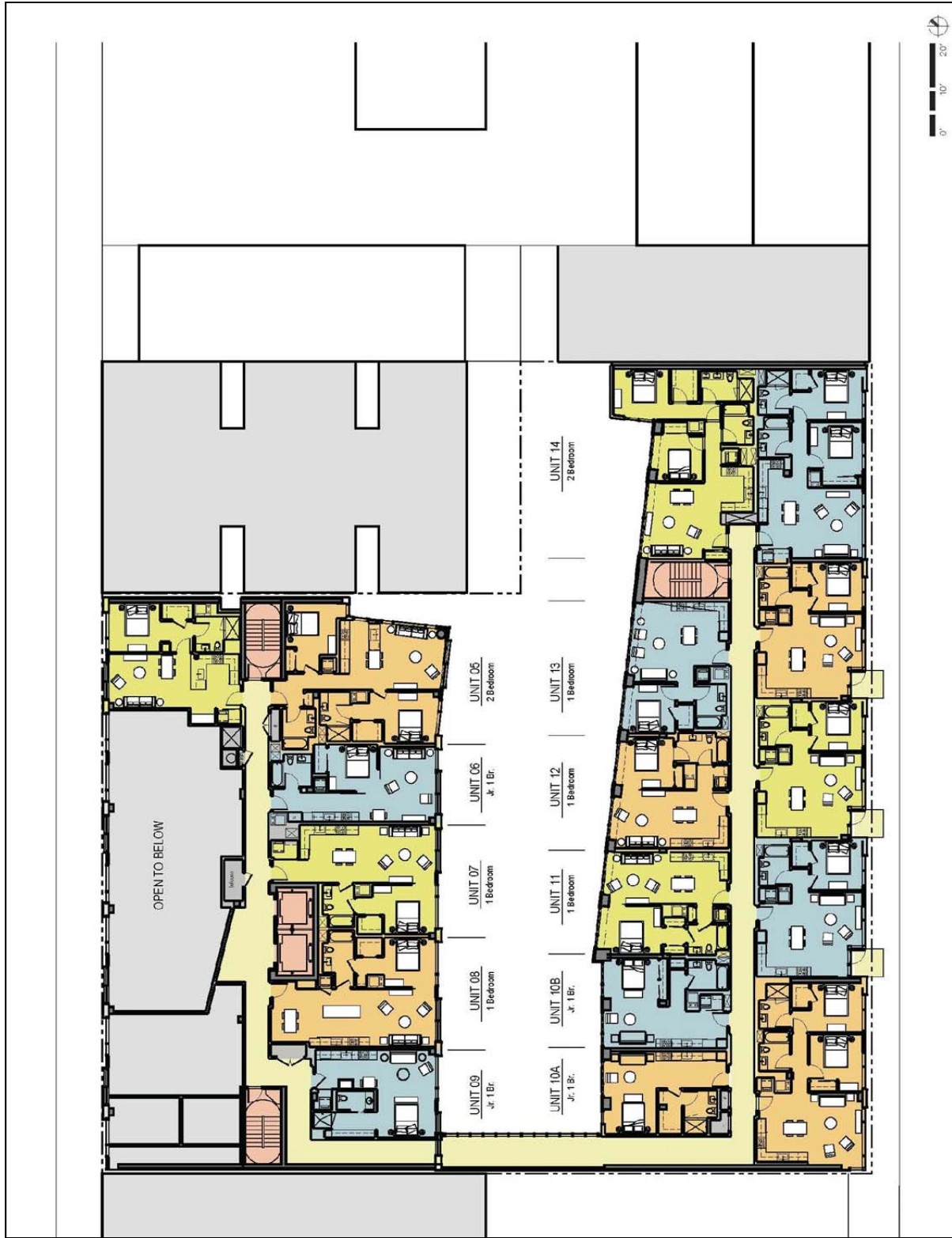
Source: Solomon Cordwili Bueinz, 2013
 NOT TO SCALE

Figure 3: Proposed Garage Plan



Source: Solomon Cordwell Buenz, 2013
 NOT TO SCALE

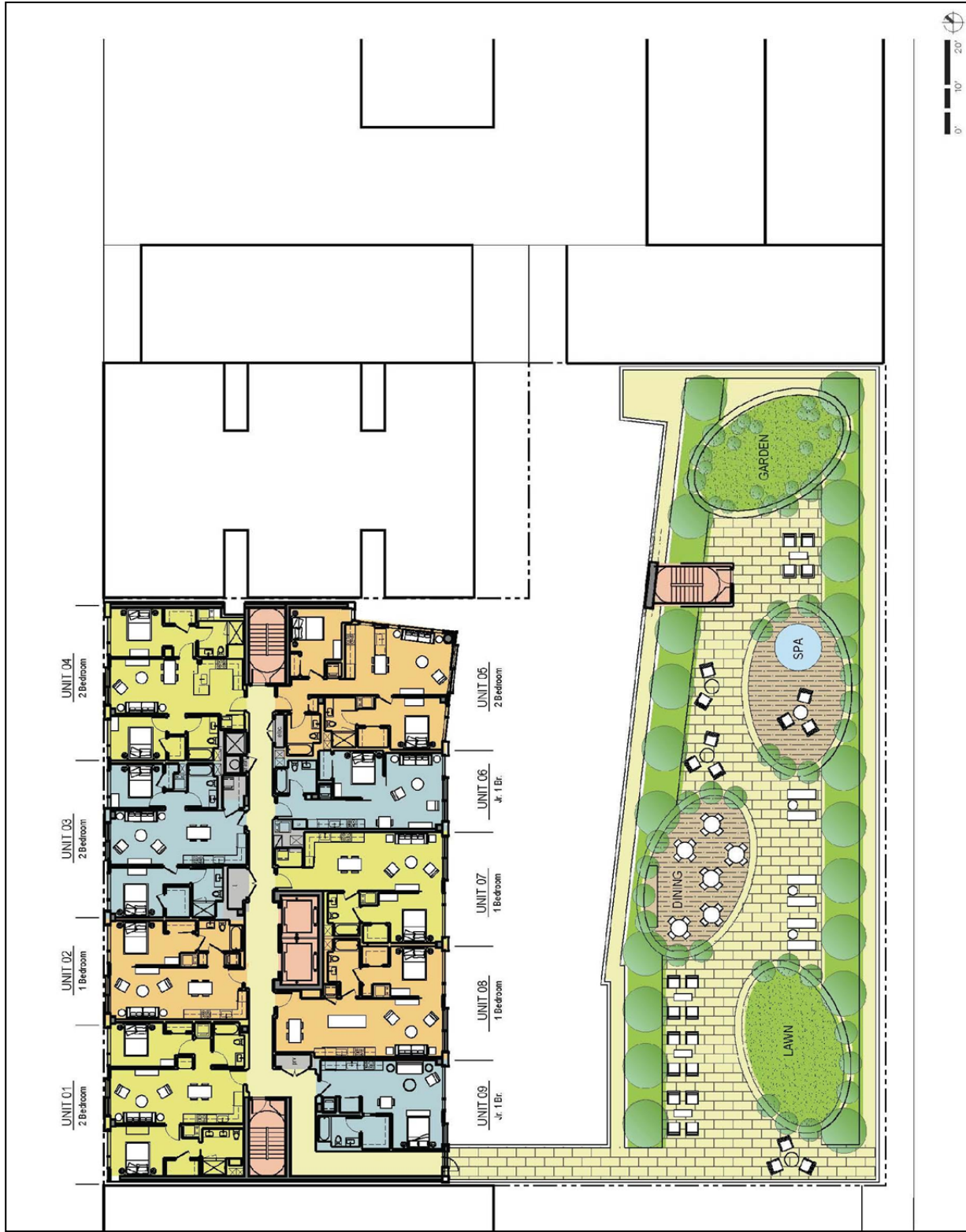
Figure 4: Proposed First Floor Plan



Source: Solomon Cordwell Buenz, 2013

NOT TO SCALE

Figure 5: Proposed Second and Third Floor Plans



Source: Solomon Cordwell Buenz, 2013

NOT TO SCALE

Figure 6: Proposed Fourth Floor Plan



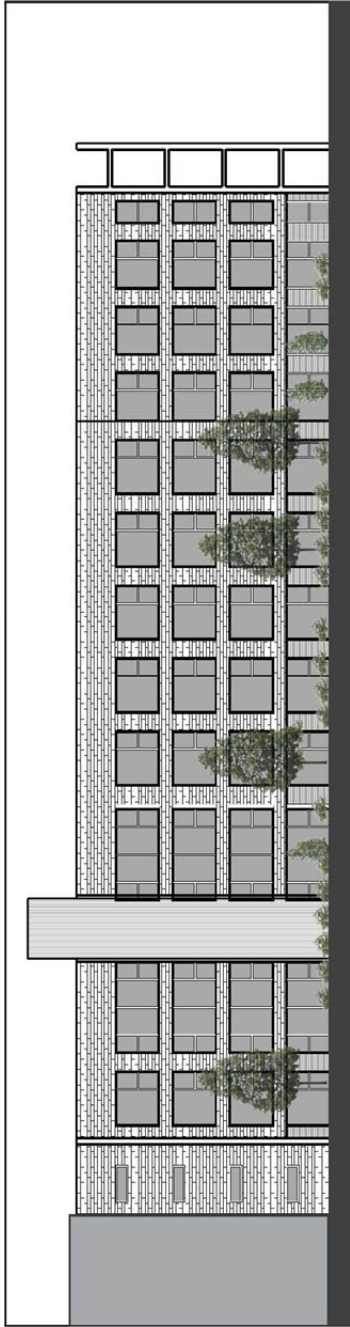
NORTH-WEST ELEVATION (FOLSOM)

Source: Solomon Cordwell Buenz, 2013
 NOT TO SCALE

Figure 7: Elevation



SHIPLEY (SOUTH-EAST) ELEVATION



NORTH-WEST ELEVATION (COURTYARD SIDE)



Source: Solomon Cordwell Buenz, 2013

NOT TO SCALE

Figure 8: Elevations

Folsom Street has an existing 10-foot wide sidewalk for the public right-of-way. The project sponsor proposes five new street trees spaced across the length of the property along Folsom Street, located to avoid the existing amber alert sign and proposed transformer vaults. A three-foot wide curbside zone is proposed that would include permeable concrete pavers and planting areas for street trees. Six bicycle racks would be located in permeable paver area of this curbside zone. The remaining 6.5-foot wide path of travel would be paved with concrete.

Shipley Street is a narrow alley with an existing 7-foot wide sidewalk. The project sponsor proposes to add nine new street trees along this frontage, spaced across the length of the property and aligned with elements of the building façade. In addition, the sponsor proposes a 2.5-foot wide curbside zone that would contain a variation of planting areas (with trees and shrubs) and paved areas with permeable pavers. The remaining four-foot width of the curb would be paved with concrete.

Adjacent to the building along the edge of the entry stoop stairs along Shipley Street, the project sponsor proposes planters with shrubs and vines to provide privacy for the first floor units and additional vegetation along the public right-of-way.

In addition, to enhance pedestrian connectivity and safety near the project site, the proposed project would include installation of a new north-south crosswalk along Shipley Street at the intersection at Fifth Street, subject to review and approval by SFMTA. This intersection currently includes ADA-accessible ramps along the north and south sides of Shipley Street; however, there is no marked (striped) crosswalk present. SFMTA recently recommended that the project sponsor consider constructing a raised crosswalk at the corner of Fifth and Shipley Streets, along the west side. A raised crosswalk would consist of poured concrete and pavement markings and pedestrians would not need to step down into the roadbed to cross Shipley Street. The crosswalk would be designed per standards set forth in the San Francisco *Better Streets Plan* as approved by the SFMTA Sustainable Streets Division and Department of Public Works.

Project Construction

Construction phases would consist of demolition, below-grade construction, superstructure construction, exterior wall construction and glazing, and building interior and finishes. Project construction is anticipated to begin in summer 2014 and is expected to last approximately 20 months.

Demolition of the existing building on the project site would be completed in approximately six weeks. Following demolition, the site would be excavated to allow for one level below grade over the entire footprint of the site. Approximately 14,000 cubic yards of soil is slated for excavation with approximately 11,000 cubic yards of soil slated for removal. The remainder of the excavated soil will be mixed and reused as part of project construction. Excavation work is estimated to last two months.

Due to the presence of fill material and "new bay mud" on the site, a combination of pile foundations and grade beams is proposed (contingent on the final geotechnical report). As an alternate, a mat foundation is being considered. Drilled (not driven) piles would also be installed as required by the buildings seismic resisting systems. The remainder of the foundation would be shallow grade beams. Foundation work is estimated to last two months.

The building superstructure would be constructed over four months and would consist of conventional concrete columns and slabs and post-tensioned shear walls. Construction equipment to be used during this phase would include a tower crane, concrete pump trucks, and concrete/rebar/framing delivery trucks. Installation of the building exterior skin will start towards the fourth month of superstructure and be completed in about three months. The anticipated date of occupancy is the first quarter of 2016.

The proposed 923 Folsom Street project would require the following approvals:

Approval Action by the Planning Commission

- The approval of a Large Project Authorization by the Planning Commission (per Planning Code Section 329) is the Approval Action for the proposed project. The Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.

Actions by City Departments

- Approval of site permit (*Planning Department, Department of Building Inspection*)
- Approval of grading and building permits (*Planning Department, Department of Building Inspection*)
- Approval of a stormwater control plan (*San Francisco Public Utilities Commission*)
- Approval of project compliance with the Stormwater Control Guidelines (*Department of Public Works*)
- Approval of a three-lot merger (*Department of Public Works*)
- Approval of the installation of a new north-south crosswalk along Shipley Street at the intersection at Fifth Street (*San Francisco Municipal Transportation Agency*)

EVALUATION OF ENVIRONMENTAL EFFECTS:

This Community Plan Exemption (CPE) Checklist examines the potential environmental impacts that would result from implementation of the proposed project and indicates whether such impacts are addressed in the applicable programmatic FEIR (PEIR)² for the *Eastern Neighborhoods Rezoning and Area Plans EIR* (FEIR) (Planning Department Case No. 2004.0160E and State Clearinghouse No. 2005032048).³ Items checked "Project-Specific Significant Impact Not Identified in PEIR" identify topics for which the proposed project would result in a significant impact that is peculiar to the project, i.e., the impact is not identified as significant in the PEIR. Any impacts not identified in the PEIR are addressed in the CPE Checklist below.

Items checked "Significant Unavoidable Impact Identified in PEIR" identify topics for which a significant impact is identified in the PEIR. In such cases, the analysis considers whether the proposed project would result in impacts that would contribute to the impact identified in the PEIR. Mitigation measures identified in the PEIR are discussed under each topic area, and mitigation measures that are applicable to the proposed project are identified under each topic area and on pp. 64-73.

For any topic that was found to result in less-than-significant (LTS) impacts in the PEIR and for the proposed project, or would have no impacts, the topic is marked "No Significant Impact (Project or PEIR)" and is discussed in the CPE Checklist below.

2 In this CPE Checklist, the acronyms FEIR and PEIR both refer to the Eastern Neighborhoods Plan FEIR and are used interchangeably.

3 San Francisco Planning Department, *Eastern Neighborhoods Rezoning and Area Plans Final Environmental Impact Report (FEIR)*, Planning Department Case No. 2004.0160E, certified August 7, 2008. Available online at: <http://www.sf-planning.org/index.aspx?page=1893>, accessed August 17, 2012.

Public Resources Code Section 21099(d), effective January 1, 2014, provides that, “aesthetics and parking impacts of a residential, mixed-use residential, or employment center project on an infill site located within a transit priority area shall not be considered significant impacts on the environment.” Accordingly, aesthetics and parking are no longer to be considered in determining if a project has the potential to result in significant environmental effects for projects that meet all of the following three criteria:

- a) The project is in a transit priority area;
- b) The project is on an infill site; and
- c) The project is residential, mixed-use residential, or an employment center.

The proposed project meets each of the above three criteria and thus, this checklist does not consider aesthetics in determining the significance of project impacts under CEQA.⁴ The Planning Department acknowledges that parking conditions may be of interest to the public and the decision makers. Therefore, this determination presents a parking demand analysis for informational purposes, under Transportation and Circulation.

<u>Topics:</u>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
1. LAND USE AND LAND USE PLANNING—Would the project:						
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial impact upon the existing character of the vicinity?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

The division of an established community typically involves the construction of a physical barrier to neighborhood access, such as a new freeway, or the removal of a means of access, such as a bridge or a roadway. The proposed project would replace an existing two-story building and a surface parking lot with two new buildings – a nine-story building (approximately 85 feet in height) fronting Folsom Street and a four-story building (approximately 44 feet in height) fronting Shipley Street. The project would be limited to the project site and would not construct a physical barrier to neighborhood access or remove an

⁴ San Francisco Planning Department. *Transit-Oriented Infill Project Eligibility Checklist for 923 Folsom Street, April 14, 2014*. This document is available for review as part of Case File No. 2012.1333E at the San Francisco Planning Department, 1650 Mission Street, Suite 400, San Francisco, California 94103.

existing means of access. Moreover, the project would not alter the established street grid or permanently close any streets or sidewalks. Although portions of the sidewalk adjacent to the project site could be closed for periods of time during project construction, these closures would be temporary in nature. As a result, the proposed project would not physically divide an established community.

The project site is within the East SoMa Area Plan of the San Francisco General Plan and is within a Mixed Use - Residential (MUR) use district. As discussed in Section 841 of the Planning Code, the MUR use district intends to provide housing opportunities within the eastern portion of the South of Market. The district controls are intended to facilitate the development of high-density, mid-rise housing, including family-sized housing and residential hotels. The district is designed to encourage the expansion of retail, business service and commercial (office) uses and cultural arts activities and also serves as a buffer between the higher-density, predominantly commercial area of Yerba Buena Center to the east and the lower-scale, mixed use service/industrial and housing area west of Sixth Street. Restrictions on the size of non-residential uses prohibit the development of large-scale retail and office uses. Allowed uses within the MUR use district include residential and office uses as well as PDR uses such as light manufacturing, home and business services, arts activities, warehouses, and wholesaling. Additional permitted uses include retail, educational facilities, and recreational facilities.

The existing uses on the project site are office and parking. By developing residential and limited commercial uses, the proposed change in use would be consistent with those uses permitted within the MUR use district. Moreover, residential and commercial uses in the MUR use district are consistent both with the policies of the Area Plan and specific zoning adopted pursuant to the Area Plan for this particular location. Thus, the proposed project would be consistent with uses permitted within the MUR use district.

The proposed height and bulk of the building would be substantially larger than what is currently on the site and also bigger than many other structures in the project area. However, the proposed project would be similar in size and bulk to the project directly across Folsom Street and, in general, would not result in significant, adverse impacts to the overall character of the neighborhood. Moreover, it would not physically divide or disrupt an established community.

The Citywide and Current Planning Divisions of the Planning Department have additionally determined that the proposed project is consistent with the Eastern Neighborhoods Plan and satisfies the requirements of the General Plan and the Planning Code.^{5, 6} Specifically, the proposed building is consistent with the established height and bulk controls of the site, and the proposed uses are consistent with the MUR zoning controls of the site, all of which were analyzed in the Eastern Neighborhoods FEIR.

The Eastern Neighborhoods Area Plan rezoned much of the city's industrially zoned land. The goals of the Area Plan were to reflect local values, increase housing, maintain some industrial land supply, and improve the quality of all existing areas with future development. A major issue discussed in the Area Plan process was the degree to which existing industrially zoned land would be rezoned to primarily residential and mixed-use districts, thus reducing the availability of land traditionally used for PDR (Production, Distribution, and Repair) employment and businesses.

5 Adam Varat, San Francisco Planning Department, *Community Plan Exemption Eligibility Determination, Citywide Planning and Policy Analysis*, 923 Folsom Street, June 20, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

6 Jeff Joslin, San Francisco Planning Department, *Community Plan Exemption Eligibility Determination, Current Planning*, 923 Folsom Street, November 22, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

The Eastern Neighborhoods FEIR identified a significant and unavoidable impact on land use due to the cumulative loss of PDR use in the Plan Area. Whether an individual project contributes to this impact is based upon two factors: (a) whether the site allowed PDR uses under its former zoning (prior to the rezoning that occurred under the Eastern Neighborhoods Rezoning and Area Plans process) and (b) whether the site would be able to support PDR uses under its current zoning. The project site was formerly zoned as Residential Service District (RSD), which generally allowed PDR uses. The project site was rezoned to the MUR use district, which also allows most PDR uses. As discussed above, the project site is currently occupied by a commercial office building and a surface parking lot. However, by developing retail and residential uses on the property, the proposed project would preclude PDR uses from being developed on the project site. Thus, the project would contribute to the significant and unavoidable impact on land use identified in the Eastern Neighborhoods FEIR.

The Eastern Neighborhoods FEIR included one mitigation measure, Mitigation Measure A-1, for land use controls in Western SoMa that could incorporate, at a minimum, no net loss of land currently designated for PDR uses, restrict non-PDR uses on industrial (or other PDR-designated) land, and incorporate restrictions on potentially incompatible land uses proximate to PDR zones. The measure was judged to be infeasible, because the outcome of the community-based Western SoMa planning process could not be known at the time, and the measure was seen to conflict with other City policy goals, including the provision of affordable housing.

For the reasons stated above, implementation of the proposed project would not result in significant impacts that were not identified in the Eastern Neighborhoods FEIR related to land use and land use planning, and no mitigation measures are necessary.

Topics:	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
2. POPULATION AND HOUSING— Would the project:						
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Displace substantial numbers of existing housing units or create demand for additional housing, necessitating the construction of replacement housing?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

One of the objectives of the Eastern Neighborhoods Area Plan is to identify appropriate locations for housing in the City’s industrially zoned land to meet the citywide demand for additional housing. The Eastern Neighborhoods FEIR concluded that an increase in population in the Plan Area is expected to

occur as a secondary effect of the proposed rezoning and that any population increase would not, in itself, result in adverse physical effects, but would serve to advance key City policy objectives, such as providing housing in appropriate locations next to Downtown and other employment generators and furthering the City's Transit First policies. It was anticipated that the rezoning would result in an increase in both housing development and population in all of the Area Plan neighborhoods. The Eastern Neighborhoods FEIR determined that the anticipated increase in population and density would not result in significant adverse physical effects on the environment. No mitigation measures were identified in the FEIR.

The addition of 115 residential units would increase the residential population on the site by approximately 232 persons.⁷ The proposed commercial use would employ a total of approximately 6 staff at the proposed building once it is completed. Therefore, the proposed project's residential and commercial uses are expected to add a population of approximately 238 persons to the site (include a daytime population of 6 persons). These direct effects of the proposed project on population and housing are within the scope of the population growth anticipated under the Eastern Neighborhoods Rezoning and Area Plans, and evaluated in the Eastern Neighborhoods FEIR.

For the above reasons, the proposed project would not result in significant impacts on population and housing that were not identified in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
3. CULTURAL AND PALEONTOLOGICAL RESOURCES—Would the project:						
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5, including those resources listed in Article 10 or Article 11 of the San Francisco <i>Planning Code</i> ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

⁷ The project site is located in Census Tract 178.02, which is generally bounded by Howard Street to the north, Harrison Street to the south, 5th Street to the east and 11th Street to the west. The population calculation is based on Census data (2008-2012 American Community Survey), which estimates 2.02 persons per household in Census Tract 178.02.

Historic Architectural Resources

Pursuant to CEQA Guidelines Sections 15064.5(a)(1) and 15064.5(a)(2), historical resources are buildings or structures that are listed, or are eligible for listing, in the California Register of Historical Resources or are identified in a local register of historical resources, such as Articles 10 and 11 of the San Francisco Planning Code. The Eastern Neighborhoods FEIR determined that future development facilitated through the changes in use districts and height limits under the Eastern Neighborhoods Area Plan could have substantial adverse changes on the significance of both individual historical resources and on historical districts within the Plan Area. The FEIR determined that approximately 32 percent of the known or potential historical resources in the Plan Area could potentially be affected under the preferred alternative. The Eastern Neighborhoods FEIR found this impact to be significant and unavoidable. This impact was addressed in a Statement of Overriding Considerations with findings and adopted as part of the Eastern Neighborhoods Rezoning and Area Plans approval on January 19, 2009.

The FEIR identifies three mitigation measures that could reduce the severity of impacts of development enabled under the Eastern Neighborhoods Plan in some cases: Mitigation Measure K-1 established interim building permit review policies to protect historical resources within the Plan Area, pending completion of an historical resources survey of the Plan Area and implementation of revised Preservation Policies for protection of historical resources within the Plan Area; Mitigation Measure K-2 identified amendments to Article 10 of the Planning Code pertaining to vertical additions in the South End Historic District that would reduce potential impacts to contributing structures in this historic district; and Mitigation Measure K-3 identified amendments to Article 10 of the Planning Code pertaining to alteration and infill development in the Dogpatch Historic District that would reduce potential impacts on contributing structures in this historic district. However, because the demolition or substantial alteration of a historical resource typically cannot be fully mitigated, the FEIR concluded that the Eastern Neighborhoods Plan would have a significant and unavoidable impact on historical resources.

The existing project site contains an 11,000-square-foot, two-story, commercial office building (constructed in 1967) in the southeastern portion of the site (along Shipley Street) and surface parking areas throughout the remainder of the site. The existing building on the property was included the Planning Department's South of Market Area Historic Resource Survey and was given a California Historical Resource Status Code of 6Z, indicating that it was found ineligible for California Register, National Register, or Local designation through survey evaluation. Therefore, the project site does not contain any historical structures, sites, or architectural features. For these reasons, the proposed project would not contribute to the significant and unavoidable historical resource impacts identified in the Eastern Neighborhoods FEIR and no mitigation measures would be required.

Archeological Resources

The Eastern Neighborhoods FEIR determined that implementation of the Area Plan could result in significant impacts on archeological impacts and identified three mitigation measures that would reduce these potential impacts to a less than significant level. Eastern Neighborhoods FEIR Mitigation Measure J-1 applies to properties for which a final archeological research design and treatment plan is on file at the Northwest Information Center and the Planning Department. Mitigation Measure J-2 applies to properties for which no archeological assessment report has been prepared or for which the archeological documentation is incomplete or inadequate to serve as an evaluation of potential effects on archeological resources under CEQA. Mitigation Measure J-3, which applies to properties in the Mission Dolores Archeological District, requires that a specific archeological testing program be conducted by a qualified archeological consultant with expertise in California prehistoric and urban historical archeology.

No final archeological research design and treatment plan is on file for the project site at the Northwest Information Center and the Planning Department; therefore, Mitigation Measure J-1 does not apply to the proposed project. The project site is also not located within the Mission Dolores Archeological District; therefore Mitigation Measure J-3 also does not apply to the proposed project. No archeological assessment report has been previously prepared for the project site; thus Eastern Neighborhoods FEIR Mitigation Measure J-2 is applicable to the proposed project.

Eastern Neighborhoods FEIR Mitigation Measure J-2 requires a project-specific archeological sensitivity assessment. A preliminary archeological review (PAR)⁸ was conducted by a Planning Department's archeological technical specialist to serve as the project-specific archeological sensitivity assessment. Implementation of the archeological recommendations of this PAR (which includes implementation of an Archeological Testing Mitigation Requirement) will result in the reduction of any potential effects of the proposed project to archeological resources to a less-than-significant level. The results of the PAR are summarized below.

According to the PAR, the project site was located within and along the northern periphery of a former old high tidal marsh (Sullivan Marsh) that gradually expanded beyond Mission Bay westward past Mission and 7th Streets within the last 4,000 years. The area northeast of this large wetland became a place of dense prehistoric settlement and use during a period of relative sand dune stability beginning 2,000 years BP until 1,000 years BP. This concentration of prehistoric sites consists mostly of buried shell midden sites usually containing some human remains, artifactual and faunal material, and various features. The prehistoric sites in the South of Market area are frequently notable for their good state of preservation buried beneath later sand dune deposits. Several of these sites are situated along the former shoreline and marshes of Mission Bay or near sources of fresh water. Prehistoric shell mound deposits sometimes occur within areas that historically were submerged, such as within tidal wetlands or within submerged waters. Due to its location and the cluster of prehistoric sites in this area, the project site has sensitivity for prehistoric archaeological resources beneath the fill.

By the mid- to late-1850's the project site had been filled in and was under cultivation with an associated structure located on the corner of 5th and Folsom (outside of the project site). By 1869, development is shown along both Folsom and Shipley Streets, although the type of development is not currently known. By the late 1880s, based on the Sanborn Map, the project area has been primarily built out with stores, a bakery, and residential buildings (tenements, flats, and dwellings) along with various out buildings along rear property lines. The 1899 Sanborn map shows a similar collection of buildings within the project area. All of these buildings were destroyed in the 1906 Earthquake and Fire. Limited development is shown on the 1913 Sanborn map. Besides the insertion and removal of underground storage tanks, which based on the geotechnical report prepared for the project site only took up a small portion of the site, the project site has seen limited subsurface disturbance during the 20th century.

There are no previous archeological field investigations of the project site. The project site was covered in the Archaeological Resources Inventory for the South of Market Redevelopment Project (1995). Archeological testing by WSA at the nearby SFMOMA Fire Station Relocation and Housing Project (2012) located at 935 Folsom did not encounter any archeological resources but did identify the site's stratigraphy, noting that the project is underlain by earthquake fill (5-to-8.5 foot thick layer), which is underlain by a layer of loose, moist, brown to very dark brown, undifferentiated dune sand (varying in

⁸ *Environmental Planning Preliminary Archeology Review: Checklist for 923 Folsom Street from Allison Vanderslice (EP archeologist), November 26, 2013. This document is on file and available for public review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of Case File 2012.1333E.*

thickness from 3-to-11 feet), beginning at 5-to-6 feet bgs (average is 5.8 feet bgs) and extending to 8-to-16 feet bgs (average is 10.8 feet bgs), with a marsh deposit (of 5-to-17 feet thick) identified beneath the dune sand in 11 of 20 bores. The marsh deposit is underlain by bay mud (17-to-26 feet bgs), which is underlain by Colma Formation sands.

The State Historic Preservation Office has recently determined that seven prehistoric shell midden sites form an archeological district (Prehistoric Native American Shellmiddens on Mission Bay, San Francisco) eligible for listing to the National Register of Historic Places (NRHP).⁹ These seven sites are a series of well-developed middens that formed on sand dunes. The project site lies just west of the sites currently identified in the prehistoric shell midden archeological district and is also within several blocks of two additional recorded prehistoric sites. While South of Market prehistoric shell middens are small to moderate in size, they indicate substantial occupation as both core and satellite villages. These sites are remarkable for the unusual state of integrity since they have often been buried under late period sand dune activity. Human remains have been encountered in several of these shell midden sites.

The PAR concluded that excavation for the proposed below-grade parking level could affect archeological deposits, specifically late 19th Century deposits, such as possible CRHR-eligible privy pits or wells, which may exist within the project site. The assessment also noted that there is a probability that the various deep foundation techniques recommended by the geotechnical reports prepared for the project could adversely impact archeological deposits that, in the case of prehistoric features, would most likely be CRHR-eligible. However, implementation of the Planning Department's standard Archeological Testing Mitigation Requirement, below, would reduce potential effects of the proposed project to archeological resources to a less-than-significant level.

Project Mitigation Measure M-CP-1 – Archeological Testing (Implements Mitigation Measure J-2 of the Eastern Neighborhoods FEIR)

Based on a reasonable presumption that archeological resources may be present within the project site, the following measures shall be undertaken to avoid any potentially significant adverse effect from the proposed project on buried or submerged historical resources. The project sponsor shall retain the services of an archaeological consultant from the rotational Department Qualified Archaeological Consultants List (QACL) maintained by the Planning Department archaeologist. The project sponsor shall contact the Department archeologist to obtain the names and contact information for the next three archeological consultants on the QACL. The archeological consultant shall undertake an archeological testing program as specified herein. In addition, the consultant shall be available to conduct an archeological monitoring and/or data recovery program if required pursuant to this requirement. The archeological consultant's work shall be conducted in accordance with this requirement at the direction of the Environmental Review Officer (ERO). All plans and reports prepared by the consultant as specified herein shall be submitted first and directly to the ERO for review and comment, and shall be considered draft reports subject to revision until final approval by the ERO. Archeological monitoring and/or data recovery programs required by this requirement could suspend construction of the project for up to a maximum of four weeks. At the direction of

⁹ Anthropological Studies Center (ASC), *Site Specific Archaeological Research Design, Evaluation, and Data Recovery and Treatment Plan for Prehistoric Midden Deposits at Fourth and Howard Streets, San Francisco*. Prepared for the San Francisco Municipal Transportation Agency, September 15, 2010.

the ERO, the suspension of construction can be extended beyond four weeks only if such a suspension is the only feasible means to reduce to a less than significant level potential effects on a significant archeological resource as defined in CEQA Guidelines Sect. 15064.5 (a)(c).

Consultation with Descendant Communities: On discovery of an archeological site¹⁰ associated with descendant Native Americans or the Overseas Chinese an appropriate representative¹¹ of the descendant group and the ERO shall be contacted. The representative of the descendant group shall be given the opportunity to monitor archeological field investigations of the site and to consult with ERO regarding appropriate archeological treatment of the site, of recovered data from the site, and, if applicable, any interpretative treatment of the associated archeological site. A copy of the Final Archaeological Resources Report shall be provided to the representative of the descendant group.

Archeological Testing Program. The archeological consultant shall prepare and submit to the ERO for review and approval an archeological testing plan (ATP). The archeological testing program shall be conducted in accordance with the approved ATP. The ATP shall identify the property types of the expected archeological resource(s) that potentially could be adversely affected by the proposed project, the testing method to be used, and the locations recommended for testing. The purpose of the archeological testing program will be to determine to the extent possible the presence or absence of archeological resources and to identify and to evaluate whether any archeological resource encountered on the site constitutes an historical resource under CEQA.

At the completion of the archeological testing program, the archeological consultant shall submit a written report of the findings to the ERO. If based on the archeological testing program the archeological consultant finds that significant archeological resources may be present, the ERO in consultation with the archeological consultant shall determine if additional measures are warranted. Additional measures that may be undertaken include additional archeological testing, archeological monitoring, and/or an archeological data recovery program. If the ERO determines that a significant archeological resource is present and that the resource could be adversely affected by the proposed project, at the discretion of the project sponsor either:

- A) The proposed project shall be re-designed so as to avoid any adverse effect on the significant archeological resource; or
- B) A data recovery program shall be implemented, unless the ERO determines that the archeological resource is of greater interpretive than research significance and that interpretive use of the resource is feasible.

10 By the term "archeological site" is intended here to minimally included any archeological deposit, feature, burial, or evidence of burial.

11 An "appropriate representative" of the descendant group is here defined to mean, in the case of Native Americans, any individual listed in the current Native American Contact List for the City and County of San Francisco maintained by the California Native American Heritage Commission and in the case of the Overseas Chinese, the Chinese Historical Society of America.

Archeological Monitoring Program. If the ERO in consultation with the archeological consultant determines that an archeological monitoring program shall be implemented the archeological monitoring program shall minimally include the following provisions:

- The archeological consultant, project sponsor, and ERO shall meet and consult on the scope of the AMP reasonably prior to any project-related soils disturbing activities commencing. The ERO in consultation with the archeological consultant shall determine what project activities shall be archeologically monitored. In most cases, any soils- disturbing activities, such as demolition, foundation removal, excavation, grading, utilities installation, foundation work, driving of piles (foundation, shoring, etc.), site remediation, etc., shall require archeological monitoring because of the risk these activities pose to potential archaeological resources and to their depositional context;
- The archeological consultant shall advise all project contractors to be on the alert for evidence of the presence of the expected resource(s), of how to identify the evidence of the expected resource(s), and of the appropriate protocol in the event of apparent discovery of an archeological resource;
- The archeological monitor(s) shall be present on the project site according to a schedule agreed upon by the archeological consultant and the ERO until the ERO has, in consultation with project archeological consultant, determined that project construction activities could have no effects on significant archeological deposits;
- The archeological monitor shall record and be authorized to collect soil samples and artifactual/ecofactual material as warranted for analysis;
- If an intact archeological deposit is encountered, all soils-disturbing activities in the vicinity of the deposit shall cease. The archeological monitor shall be empowered to temporarily redirect demolition/excavation/pile driving/construction activities and equipment until the deposit is evaluated. If in the case of pile driving activity (foundation, shoring, etc.), the archeological monitor has cause to believe that the pile driving activity may affect an archeological resource, the pile driving activity shall be terminated until an appropriate evaluation of the resource has been made in consultation with the ERO. The archeological consultant shall immediately notify the ERO of the encountered archeological deposit. The archeological consultant shall make a reasonable effort to assess the identity, integrity, and significance of the encountered archeological deposit, and present the findings of this assessment to the ERO.

Whether or not significant archeological resources are encountered, the archeological consultant shall submit a written report of the findings of the monitoring program to the ERO.

Archeological Data Recovery Program. The archeological data recovery program shall be conducted in accord with an archeological data recovery plan (ADRP). The archeological consultant, project

sponsor, and ERO shall meet and consult on the scope of the ADRP prior to preparation of a draft ADRP. The archeological consultant shall submit a draft ADRP to the ERO. The ADRP shall identify how the proposed data recovery program will preserve the significant information the archeological resource is expected to contain. That is, the ADRP will identify what scientific/historical research questions are applicable to the expected resource, what data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. Data recovery, in general, should be limited to the portions of the historical property that could be adversely affected by the proposed project. Destructive data recovery methods shall not be applied to portions of the archeological resources if nondestructive methods are practical.

The scope of the ADRP shall include the following elements:

- *Field Methods and Procedures.* Descriptions of proposed field strategies, procedures, and operations.
- *Cataloguing and Laboratory Analysis.* Description of selected cataloguing system and artifact analysis procedures.
- *Discard and Deaccession Policy.* Description of and rationale for field and post-field discard and deaccession policies.
- *Interpretive Program.* Consideration of an on-site/off-site public interpretive program during the course of the archeological data recovery program.
- *Security Measures.* Recommended security measures to protect the archeological resource from vandalism, looting, and non-intentionally damaging activities.
- *Final Report.* Description of proposed report format and distribution of results.
- *Curation.* Description of the procedures and recommendations for the curation of any recovered data having potential research value, identification of appropriate curation facilities, and a summary of the accession policies of the curation facilities.

Human Remains and Associated or Unassociated Funerary Objects. The treatment of human remains and of associated or unassociated funerary objects discovered during any soils disturbing activity shall comply with applicable State and Federal laws. This shall include immediate notification of the Coroner of the City and County of San Francisco and in the event of the Coroner's determination that the human remains are Native American remains, notification of the California State Native American Heritage Commission (NAHC) who shall appoint a Most Likely Descendant (MLD) (Pub. Res. Code Sec. 5097.98). The archeological consultant, project sponsor, and MLD shall make all reasonable efforts to develop an agreement for the treatment of, with appropriate dignity, human remains and associated or unassociated funerary objects (CEQA Guidelines. Sec. 15064.5(d)). The agreement should take into consideration the appropriate excavation, removal, recordation, analysis, custodianship, curation, and final disposition of the human remains and associated or unassociated funerary objects.

Final Archeological Resources Report. The archeological consultant shall submit a Draft Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of

any discovered archeological resource and describes the archeological and historical research methods employed in the archeological testing/monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.

Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning division of the Planning Department shall receive one bound, one unbound and one unlocked, searchable PDF copy on CD of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest in or the high interpretive value of the resource, the ERO may require a different final report content, format, and distribution than that presented above.

For these reasons, the proposed project would not result in significant impacts on archeological resources that were not identified in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
4. TRANSPORTATION AND CIRCULATION—Would the project:						
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Conflict with an applicable congestion management program, including but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels, obstructions to flight, or a change in location, that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Topics:	Project-Specific Significant Impact Not Identified in PEIR	Significant Unavoidable Impact Identified in PEIR	Mitigation Identified in PEIR	PEIR Mitigation Applies to Project	PEIR Mitigation Does Not Apply to Project	No Significant Impact (Project or PEIR)
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

The Eastern Neighborhoods FEIR anticipated that growth resulting from the zoning changes could result in significant impacts on traffic and transit ridership, and identified 11 transportation mitigation measures. Even with mitigation, however, it was anticipated that the significant adverse cumulative traffic impacts at certain local intersections and the cumulative impacts on certain transit lines could not be fully mitigated. Thus, these impacts were found to be significant and unavoidable.

The project site is not located within an airport land use plan area, or in the vicinity of a private airstrip. Therefore, topic 16c from the CEQA Guidelines, Appendix G is not applicable.

Trip Generation

The project would include 115 dwelling units, consisting of 8 junior one-bedroom units, 61 one-bedroom units, and 46 two-bedroom units. In addition, it would provide approximately 1,900 square feet of commercial space on the ground level of the Folsom Street structure. The two buildings would share a basement-level garage, which would contain 87 stacked residential off-street parking spaces as well as 104 secured bicycle spaces.

Using the guidance in the *2002 Transportation Impacts Analysis Guidelines for Environmental Review* (SF Guidelines) developed by the San Francisco Planning Department, a project-specific transportation study for the 923 Folsom Street project was prepared, and is summarized below.¹² The proposed project would generate an estimated 2,118 person trips (inbound and outbound) on a weekday daily basis, consisting of 805 person trips by auto, 466 transit trips, 614 walk trips and 233 trips by other modes. During the p.m. peak hour, the proposed project would generate an estimated 92 vehicle trips (accounting for vehicle occupancy data for this Census Tract).

Traffic

The proposed project's vehicle trips would travel through the intersections surrounding the project block. Intersection operating conditions are characterized by the concept of Level of Service (LOS), which ranges from A to F and provides a description of an intersection's performance based on traffic volumes, intersection capacity, and vehicle delays. LOS A represents free flow conditions, with little or no delay, while LOS F represents congested conditions, with extremely long delays; LOS D (moderately high delays) is considered the lowest acceptable level in San Francisco.

The proposed project would generate an estimated 82 new a.m. peak hour vehicle trips and an estimated 92 new p.m. peak hour vehicle trips that could travel through surrounding intersections. The

12 CHS Consulting Group, *Transportation Impact Study, 923 Folsom Street Mixed-Use Residential Project*, April 11, 2014. This study is on file and available for public review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333!

intersections near the project site (within approximately 1,500 feet) include Fifth Street/Howard Street, Fifth Street/ Folsom Street, Fifth Street/Harrison Street/Freeway (I-80) off-ramp, Sixth Street/Howard Street, Sixth Street/Folsom Street, and Sixth Street/Harrison Street. All study intersections would operate at the same service levels as under existing conditions during both a.m. and p.m. peak hours, and the majority of study intersections would continue to operate satisfactorily (LOS D or better). At the signalized intersection of Fifth Street/Harrison Street/I-80 off-ramp, during the p.m. peak hour, the intersection would operate at LOS E with or without the proposed project. The proposed project would add seven vehicles to the critical southbound through movement. This project-related contribution would represent 1.5 percent to the total p.m. peak hour volume at this critical movement under existing conditions. The proposed project's contributions to this poorly operating intersection would therefore not be considered substantial and the proposed project would result in a less-than-significant traffic impact at the intersection of Fifth Street/Harrison Street/I-80 off-ramp.

The 92 new p.m. peak hour vehicle trips would not substantially increase traffic volumes at this or other nearby intersection, would not substantially increase average delay that would cause intersections that currently operate at acceptable LOS to deteriorate to unacceptable LOS, and would not substantially increase average delay at intersections that currently operate at unacceptable LOS.

Each of the rezoning options in the Eastern Neighborhoods FEIR identified significant and unavoidable cumulative (2025) impacts relating to weekday p.m. peak hour traffic conditions, with the Preferred Project having significant impacts at several intersections. Of these, the closest intersections to the project site are Sixth and Brannan Streets and Seventh and Harrison Streets. However, the project would have no effect on these intersections due to their distance from the site (over 1,500 feet). Therefore, the project would not have a considerable contribution to significant cumulative impacts identified in the FEIR.

In terms of cumulative traffic impacts on intersections not studied in the FEIR, the intersection of Fifth Street/Howard Street would operate at acceptable LOS conditions (LOS D) during the a.m. peak hour under 2040 cumulative conditions, but would operate poorly (LOS F) during the p.m. peak hour. The intersection of Fifth Street/Harrison Street/Freeway (I-80) off-ramp would operate acceptably at LOS D during the p.m. peak hour, but would operate poorly (LOS E) during the a.m. peak hour. The remaining study intersections would operate at unacceptable LOS conditions (LOS E or F) in the p.m. peak hour under cumulative conditions. The proposed project contributions at the study intersections projected to operate poorly (at LOS E or F) under 2040 cumulative conditions were further evaluated to determine if the project-generated vehicle trips would represent a substantial (generally greater than five percent contribution to a poorly operating critical movement) contribution to these LOS E or F intersections.

AM Peak Hour

At the signalized intersection of Fifth and Folsom Street, during the a.m. peak hour, the intersection would operate at LOS F under 2040 cumulative conditions. The proposed project would add 11 vehicles to the critical eastbound left-turning movement and three vehicles to the critical eastbound through movement. This project-related contribution would represent 3.9 and less than 1 percent, respectively, of the total a.m. peak hour volume for these critical movements under cumulative conditions. The proposed project's contributions to this intersection under 2040 cumulative conditions would therefore, not be considered substantial and the proposed project would not result in a considerable contribution to cumulative traffic impact at the intersection of Fifth Street/Folsom Street during the a.m. peak hour.

At the signalized intersection of Fifth Street/Howard Street/Freeway I-80 off-ramp, during the a.m. peak hour, the intersection would operate at LOS E under 2040 cumulative conditions. The proposed project would add four vehicles to the critical northbound through movement and 10 vehicles to the critical

southbound through movement. This project-related contribution would represent 0.9 and 1.6 percent, respectively, of the total a.m. peak hour volume for these critical movements under cumulative conditions. The proposed project's contributions to this intersection under 2040 cumulative conditions would therefore, not be considered substantial and the proposed project would not result in a considerable contribution to cumulative traffic impact at the intersection of Fifth Street/Howard Street/Freeway I-80 off-ramp.

PM Peak Hour

At the signalized intersection of Fifth and Howard Street, during the p.m. peak hour, the intersection would operate at LOS E under 2040 cumulative conditions. The proposed project would add seven vehicles to the northbound through critical movement and 10 vehicles to the southbound through critical movement, which would represent 1.1 and 1.5 percent respectively, of the total p.m. peak hour volume for these critical movements under cumulative conditions. The proposed project's contributions to this intersection under 2040 cumulative conditions would therefore, not be considered substantial and the proposed project would not result in a considerable contribution to cumulative traffic impact at the intersection of Fifth Street and Howard Street/Freeway I-80 off-ramp.

At the signalized intersection of Fifth and Folsom Street, during the p.m. peak hour, the intersection would operate at LOS F under 2040 cumulative conditions. The proposed project would add seven vehicles to the eastbound left-turn critical movement and two vehicles to the eastbound through critical movement, which would represent 3.1 and 0.2 respectively, of the total p.m. peak hour volume for these critical movements under cumulative conditions. In addition, the proposed project would add 10 vehicles to the southbound through critical movement, which would represent 2.1 percent of the total p.m. peak hour volume for this critical movement under cumulative conditions. The proposed project's contributions to this intersection under 2040 cumulative conditions would therefore, not be considered substantial and the proposed project would not result in a considerable contribution to cumulative traffic impact at the intersection of Fifth and Folsom Street during the p.m. peak hour.

At the signalized intersection of Sixth and Howard Street, during the p.m. peak hour, the intersection would operate at LOS F under 2040 cumulative conditions. However, at this study intersection, the proposed project would not add any vehicle trips to the westbound left-turn and westbound through critical movements, but would add seven vehicles, or a 0.7 percent contribution to the northbound through critical movement volumes. The proposed project's contributions to this intersection under 2040 cumulative conditions would therefore, not be considered substantial and the proposed project would not result in a considerable contribution to cumulative traffic impact at the intersection of Sixth and Howard Street.

At the signalized intersection of Sixth and Folsom Street, during the p.m. peak hour, the intersection would operate at LOS F under 2040 cumulative conditions. The proposed project would add 10 vehicles to the southbound left-turn/through critical movement, which would represent 0.8 percent contribution to the total p.m. peak hour volume for this critical movement under cumulative conditions. The proposed project's contributions to this intersection under 2040 cumulative conditions would therefore, not be considered substantial and the proposed project would not result in a considerable contribution to cumulative traffic impact at the intersection of Sixth and Folsom Street during the p.m. peak hour.

At the signalized intersection of Sixth and Harrison Street, during the p.m. peak hour, the intersection would operate at LOS F under 2040 cumulative conditions. The proposed project would add 12 vehicles to the southbound through/right-turning critical movement, which would represent 0.04 percent contribution to the total p.m. peak hour volume for this critical movement under cumulative conditions.

The proposed project's contributions to this intersection under 2040 cumulative conditions would therefore, not be considered substantial and the proposed project would not result in a considerable contribution to cumulative traffic impact at the intersection of Sixth and Harrison Street during the p.m. peak hour.

It is noted that the proposed roadway network changes throughout the study area include substantial modifications to existing roadway capacities, including the reduction to the number of general traffic lanes along multiple streets in order to accommodate new transit, pedestrian, and bicycle facility improvements. The lessening in roadway capacities and reduction of travel lanes at study intersections, in combination with the anticipated land use would result in the worsening of traffic operating conditions and vehicle delay. However, the proposed project would not result in a considerable contribution to cumulative traffic conditions and project cumulative traffic impacts would be considered less than significant.

For the above reasons, the proposed project would not result in significant impacts on traffic that were not identified in the Eastern Neighborhoods FEIR.

As noted in the Project Description, vehicle access to the underground parking garage would be provided along the north side of Shipley Street. Shipley Street is approximately 23 feet wide and includes parking along the north side of the alleyway; therefore there is one travel lane along its extents. A 12-foot-wide parking ramp would be used for vehicle ingress and egress movements in and out of the underground parking garage. Vehicles attempting to enter the garage would have to wait for the gates to open as well as yield to any vehicles exiting the garage prior to entry. There would be no refuge area or dedicated on- or off-street space for vehicles to dwell (stop) prior to entering the underground garage or waiting for an exiting vehicle; therefore vehicles would be stopped along Shipley Street before entering the garage. Because Shipley Street is a relatively narrow alleyway with one travel lane and due to the lack of a refuge area for vehicles attempting to enter the garage, the proposed project could result in adverse queuing effects along Shipley Street, and such conditions could be exacerbated during peak commute time periods (i.e., when Shipley Street could experience heightened levels of vehicle traffic and a higher amount of vehicles traveling in and out of the underground parking garage). In addition, the queuing of vehicles along Shipley Street could impede access to other neighboring residences as well as result in new hazardous conditions to all users of the alleyway, including bicyclists and pedestrians attempting to access other nearby buildings or travel along Shipley Street. Although the proposed project would not result in any potential adverse effects to existing traffic patterns, implementation of Improvement Measure I-TR-1, Monitoring and Abatement of Queues, below, would further reduce the proposed project's less-than-significant impacts related to vehicular access to the project site.

Project Improvement Measure I-TR-1: Monitoring and Abatement of Queues

As an improvement measure to reduce the potential for queuing of vehicles accessing the project site, it shall be the responsibility of the property owner to ensure that recurring vehicle queues do not occur within the public right-of-way. A vehicle queue is defined as one or more vehicles (destined to the underground parking garage) blocking any portion of the Shipley Street sidewalk or travel lane for a consecutive period of three minutes or longer on a daily or weekly basis.

If a recurring queue occurs, the owner/operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will

vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable). Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as additional bicycle parking, customer shuttles, delivery services; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.

If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Planning Department shall notify the Project Sponsor in writing. Upon request, the owner/operator shall hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant shall prepare a monitoring report to be submitted to the Planning Department for review. If the Planning Department determines that a recurring queue does exist, the facility owner/operator shall have 90 days from the date of the written determination to abate the queue.

Transit

The project site is located within a quarter mile of several local transit lines including Muni lines 8X/8AX/8BX, 12, 14/14L, 27, 30, 45, and 47. In addition, the project site is within ½ mile to ¾ mile of regional transit providers and lines: BART, Caltrain, AC Transit, SamTrans, and Golden Gate Transit. The proposed project would be expected to generate 466 daily transit trips, including 72 during the p.m. peak hour. Given the wide availability of nearby transit, the addition of 72 p.m. peak hour transit trips would be accommodated by existing capacity. As such, the proposed project would not result in unacceptable levels of transit service or cause a substantial increase in delays or operating costs such that significant adverse impacts in transit service could result.

Each of the rezoning options in the Eastern Neighborhoods FEIR identified significant and unavoidable cumulative impacts relating to increases in transit ridership on Muni lines, with the Preferred Project having significant impacts on seven lines. Of those lines, the project site is located within a quarter-mile of Muni line 27. Mitigation measures proposed to address these impacts related to pursuing enhanced transit funding; conducting transit corridor and service improvements; and increasing transit accessibility, service information and storage/maintenance capabilities for Muni lines in the Eastern Neighborhoods. Even with mitigation, however, cumulative impacts on the above lines were found to be significant and unavoidable and a Statement of Overriding Considerations related to the significant and unavoidable cumulative transit impacts was adopted as part of the FEIR Certification and project approval.

The proposed project would not contribute considerably to these conditions as its minor contribution of 72 p.m. peak hour transit trips would not be a substantial proportion of the overall additional transit volume generated by Eastern Neighborhood projects. The proposed project would also not contribute considerably to 2025 cumulative transit conditions and thus would not result in any significant cumulative transit impacts.

For the above reasons, the proposed project would not result in significant impacts that were not identified in the Eastern Neighborhoods FEIR related to transit and would not contribute considerably to cumulative transit impacts that were identified in the Eastern Neighborhoods FEIR.

Pedestrian

The proposed project would generate 166 pedestrian trips during a typical weekday p.m. peak hour. The proposed buildings would have two entrances for residents: one located at the lobby in the nine-story building along Folsom Street, which would be used by residents and one located along Shipley Street, for residents to access the four-story building. There would also be a separate entrance for customers/patrons of the retail use along the south side of Folsom Street. Standard curb ramps are present at the intersections of Fifth and Shipley Streets, and Sixth and Shipley Streets.

In general, sidewalks in the project vicinity are between six and 15 feet wide, and currently have moderate to low levels of pedestrian activity. However, the sidewalks along Shipley Street between Fifth and Sixth Streets are relatively narrow (about seven feet wide) and are generally in poor condition (e.g., cracked and uneven surfaces). The 166 new p.m. peak hour pedestrian trips could be accommodated on the existing sidewalks and crosswalks adjacent to the project site and would not substantially overcrowd the current pedestrian conditions along Folsom, Fifth, or Sixth Streets.

The proposed project would also include installation of a new north-south, raised crosswalk along Shipley Street at the intersection at Fifth Street if SFMTA approves the proposal. The proposed crosswalk would enhance pedestrian connectivity and safety, and would better alert drivers of pedestrian activity at this intersection. The crosswalk would connect to the existing ADA-accessible ramps located along the north and south sides of Shipley Street. The design of the crosswalk would be in accordance with and subject to approval by the SFMTA Sustainable Streets Division and Department of Public Works.

Further, the proposed project is subject to the *Better Streets Plan*. The proposed streetscape improvements would comply with the *Better Streets Plan* requirements and would improve the pedestrian realm adjacent to the project site (and nearby at the crosswalk at Shipley and Fifth streets) and promote pedestrian safety and comfort, and would allow for adequate public space and maneuverability for safe pedestrian passage along the sidewalk areas.

A peak hour demand of less than one loading vehicle is expected for the residential portion of the proposed project. Access to the loading zone (a 20-foot curb along the north side of Shipley Street) would be from Shipley Street. There would be a seven-foot-long “no parking” zone between the underground garage driveway and the 20-foot-long loading zone to allow for maneuvering of service vehicles within the loading area and providing this gap between the driveway ramp and loading would increase visibility of these larger vehicles for pedestrians along Shipley Street. In addition, there would be visual notifications at the garage driveway to alert pedestrians of vehicles entering and exiting the driveway which would reduce potential conflicts between pedestrians and vehicles turning in and out of the underground garage.

The proposed project would not result in an increase in the amount of overcrowding on public sidewalks, interfere with pedestrian circulation and circulation to nearby areas and buildings, or create potentially hazardous conditions for pedestrians, and would create additional corridors for pedestrian circulation. As such, pedestrian impacts resulting from the project would be less than significant. Further, the

proposed project would provide adequate sidewalk capacity along Folsom and Shipley Streets and would enhance the pedestrian environment through installation of Better Streets Plan improvements (e.g., adequate pedestrian throughways along sidewalks, street trees and planters, adequate curb widths, and adequate distance between the sidewalk and moving vehicles). While pedestrian impacts would be less than significant, the following improvement measure could be implemented to further reduce these less-than-significant impacts.

Project Improvement Measure I-TR-2: Installation of Visual/Audible Devices at Underground Garage Driveway

As an improvement measure to reduce potential conflicts between vehicles entering and exiting the underground garage and pedestrians traveling along the north side sidewalk of Shipley Street, the Project Sponsor shall install visual and/or audible notifications (alarms) to alert pedestrians of vehicles traveling in and out of the underground parking garage.

The proposed project would not include sidewalk narrowing, roadway widening, or removal of a center median; conditions that can adversely affect pedestrians. As such, the proposed project would not cause a hazard to pedestrians or otherwise interfere with pedestrian accessibility to the project site and adjoining areas. Pedestrian activity may increase as a result of the proposed project, but not to a degree that would result in substantial overcrowding on public sidewalks. For the above reasons, the proposed project would not result in significant impacts on pedestrian safety that were not identified in the Eastern Neighborhoods FEIR.

Bicycle

The proposed project would include a total of 112 bicycle parking spaces, comprised of 104 Class 1 bicycle parking spaces located within the basement-level parking garage (accessed via Shipley Street) and eight Class 2 bicycle parking spaces along the south side of Folsom Street, adjacent to the project site. The Class 2 bicycle parking spaces would be provided for patrons and/or employees associated with the proposed retail use at the project site. The bicycle routes on Howard, Folsom, and Fifth Streets are located adjacent to, and near the project site. These routes provide direct connectivity to several bicycle routes throughout the SoMa area and provide links to other regions of the city, including downtown San Francisco and the Financial District as well as major transit hubs (Caltrain, Ferry Building, Transbay Terminal, etc.). Although the proposed project would result in an increase in the number of vehicles in the project vicinity, this increase would not substantially affect bicycle travel in the area.

Based on the existing bicycle network within the project vicinity, it is reasonable to assume that the anticipated increase in bicyclists associated with the proposed project would be accommodated by existing bicycle network facilities within the project vicinity.

It is noted that although the proposed project would result in an increase in the number of vehicles in the vicinity of the project site, this anticipated increase would not be substantial enough to create potentially hazardous conditions for bicyclists. Further, the proposed project would remove a driveway on Folsom Street, which would remove one possible conflict point between vehicles and bicycles along a designated bicycle route. The project driveway and loading area would be located on Shipley Street, which is not a designated bicycle route, therefore potential conflicts between bicyclists and vehicles and/or trucks would be reduced. The proposed project would not otherwise substantially interfere with bicycle accessibility to the site and adjoining areas. For the above reasons, the proposed project would not result in significant impacts on bicycle safety that were not identified in the Eastern Neighborhoods FEIR.

Loading

Based on the SF Guidelines, the proposed project would generate a demand for one loading space during both the average and peak hour of loading activities. The San Francisco *Planning Code* (Sections 152.1, 153 and 154) requires one off-street loading space for the proposed project. The proposed project would not provide any off-street loading facilities. The project is proposing an on-street 20 foot “yellow zone” (subject to SFMTA approval) along the project’s frontage on Shipley Street, designated as on-street commercial loading to accommodate delivery vehicles. This designated space would accommodate up to one loading vehicle and would be located east of the underground garage driveway along the north side of Shipley Street. There would also be an additional seven foot “no parking” space to serve as a buffer between the underground garage driveway and loading space to accommodate maneuvering of service vehicles and to reduce any potential blockages at the garage driveway from service vehicles.

Since the proposed project would not comply with the off-street loading space requirements set forth in the San Francisco Planning Code, the project sponsor would request a variance from Sections 152.1, 153 and 154 of the Planning Code in order to provide the loading on street. Alternatively, for projects in the Eastern Neighborhoods Mixed Use Districts that are subject to Section 329, Large Project Authorization, the Planning Commission may waive these requirements per the procedures of Section 329 if it finds that the design of the project would be improved and that such loading could be sufficiently accommodated on adjacent streets and alleys. For the above reasons, the proposed project would not result in significant impacts on transportation and circulation related to loading that were not identified in the Eastern Neighborhoods FEIR.

No mitigation measures would be required because the loading demand or activities generated by the proposed project could be accommodated within the proposed weekday daytime on-street commercial loading space along the project’s frontage on Shipley Street. In addition, the proposed project’s loading would not create potentially hazardous traffic conditions or significant delays affecting traffic, transit, bicycles or pedestrians. The following improvement measures can be implemented to further reduce the less-than-significant loading-related impacts:

Project Improvement Measure I-TR-3: Coordination of Move-in/Move-Out Operations and Large Deliveries

To reduce the potential for parking of delivery vehicles within the travel lane adjacent to the curb lane on Shipley Streets (in the event that the on-street loading is occupied), residential move-in and move-out activities and larger deliveries shall be scheduled and coordinated through building management. Appropriate move-in/move-out procedures shall be enforced to avoid any blockages of Shipley Street over an extended period of time and reduce any potential conflicts between movers and pedestrians walking along Shipley Street. Curb parking on Shipley Street shall be reserved through SFMTA.

Emergency Access

The street network serving the project area currently accommodates the movements of emergency vehicles that travel to the project site. In the event of an emergency, vehicles would be able to access the project site similar to existing conditions, from Folsom and Shipley Streets, immediately adjacent to the site and with access to both buildings on the property. Furthermore, although the proposed project would generate additional traffic to the area, such an increase in vehicles would not impede or hinder the movement of emergency vehicles in the project area, for example from the neighboring fire station (Fire

Department Fire Station No.1). Based on these findings, the proposed project's impact to emergency vehicle access would be less than significant.

Construction

The proposed project's construction activities would last approximately 20 months and would include below-ground surface construction and building construction.

The proposed project would require between 15 and 20 workers during the early phases of construction. Throughout the entire construction of the project, the project would require an average of 70 workers per day; however, between 15 and 20 workers would be required during the early phases of construction and up to 140 workers would be required during peak construction periods. The proposed project would require about 24 days of hauling of spoils and other related materials. As such, during this period of construction, the project would generate approximately five haul trucks per hour during a 10-hour hauling period per day. Based on these findings, the proposed project would generate approximately 140 two-way trips (70 round trips) per day, on average; and up to 280 one-way trips during peak construction periods. The proposed project would also generate up to five haul truck trips (ten round trips) per hour over a 24-day period.

Construction staging areas would be located on site and in the underground parking garage once constructed. Parking for construction worker vehicles would be provided within public parking lots and garages in the vicinity of the project site. No permanent or temporary roadway or travel lane closures would be required during construction, with the exception of Folsom Street, where a portion of the south side, right-most travel lane may be temporarily closed during daytime construction hours.

Although construction activities would result in additional vehicle trips to and from the project site from workers and material and equipment deliveries, these activities would be limited in duration. Therefore, the proposed project's construction would not result in significant impacts on transportation that were not identified in the Eastern Neighborhoods FEIR.

While construction related impacts would be less than significant, the TIS recommends the following improvement measures to further reduce these impacts:

Project Improvement Measure I-TR-4: Construction Truck Deliveries During Off-Peak Periods

Any construction traffic occurring between 7:00 a.m. and 9:00 a.m. or between 3:30 p.m. and 6:00 p.m. would coincide with peak hour traffic and could temporarily impede traffic and transit flow, although it would not be considered a significant impact. Limiting truck movements to the hours between 9:00 a.m. and 3:30 p.m. (or other times, if approved by SFMTA) would further minimize disruption of the general traffic flow on adjacent streets during the a.m. and p.m. peak periods.

As required, the Project Sponsor and construction contractor(s) shall meet with the Sustainable Streets Division of the SFMTA, the Fire Department, Muni, and the Planning Department to determine feasible measures to reduce traffic congestion, including potential transit disruption, and pedestrian circulation impacts during construction of the project. To minimize cumulative traffic impacts due to project construction, the Project Sponsor shall coordinate with construction contractors for any concurrent nearby projects (e.g., along Fifth Street, between Howard and Folsom Streets) that are planned for construction or which later become known.

Project Improvement Measure I-TR-5: Construction Management Plan

In addition to items required in the Construction Management Plan, the project sponsor shall include the following:

- Carpool and Transit Access for Construction Workers – As an improvement measure to minimize parking demand and vehicle trips associated with construction workers, the construction contractor shall include methods to encourage carpooling and transit use to the project site by construction workers in the Construction Management Plan/contracts.
- Project Construction Updates – As an improvement measure to minimize construction impacts on nearby businesses, the project sponsor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction inquiries or concerns.

Parking

Public Resources Code Section 21099(d), effective January 1, 2014, provides that, “aesthetics and parking impacts of a residential, mixed-use residential, or employment center project on an infill site located within a transit priority area shall not be considered significant impacts on the environment.” Accordingly, aesthetics and parking are no longer to be considered in determining if a project has the potential to result in significant environmental effects for projects that meet all of the following three criteria:

- a) The project is in a transit priority area;
- b) The project is on an infill site; and
- c) The project is residential, mixed-use residential, or an employment center.

The proposed project meets each of the above three criteria and thus, this determination does not consider the adequacy of parking in determining the significance of project impacts under CEQA.¹³ The Planning Department acknowledges that parking conditions may be of interest to the public and the decision makers. Therefore, this determination presents a parking demand analysis for informational purposes.

Parking conditions are not static, as parking supply and demand varies from day to day, from day to night, from month to month, etc. Hence, the availability of parking spaces (or lack thereof) is not a permanent physical condition, but changes over time as people change their modes and patterns of travel. While parking conditions change over time, a substantial shortfall in parking caused by a project that creates hazardous conditions or significant delays to traffic, transit, bicycles or pedestrians could adversely affect the physical environment. Whether a shortfall in parking creates such conditions will depend on the magnitude of the shortfall and the ability of drivers to change travel patterns or switch to other travel modes. If a substantial shortfall in parking caused by a project creates hazardous conditions or significant delays in travel, such a condition could also result in secondary physical environmental impacts (e.g., air quality or noise impacts caused by congestion), depending on the project and its setting.

13 San Francisco Planning Department, *Transit-Oriented Infill Project Eligibility Checklist for 923 Folsom Street*, April 14, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

The absence of a ready supply of parking spaces, combined with available alternatives to auto travel (e.g., transit service, taxis, bicycles or travel by foot) and a relatively dense pattern of urban development, induces many drivers to seek and find alternative parking facilities, shift to other modes of travel, or change their overall travel habits. Any such resulting shifts to transit service or other modes (walking and biking), would be in keeping with the City's "Transit First" policy and numerous San Francisco General Plan Policies, including those in the Transportation Element. The City's Transit First Policy, established in the City's Charter Article 8A, Section 8A.115, provides that "parking policies for areas well served by public transit shall be designed to encourage travel by public transportation and alternative transportation."

The transportation analysis accounts for potential secondary effects, such as cars circling and looking for a parking space in areas of limited parking supply, by assuming that all drivers would attempt to find parking at or near the project site and then seek parking farther away if convenient parking is unavailable. The secondary effects of drivers searching for parking is typically offset by a reduction in vehicle trips due to others who are aware of constrained parking conditions in a given area, and thus choose to reach their destination by other modes (i.e. walking, biking, transit, taxi). If this occurs, any secondary environmental impacts that may result from a shortfall in parking in the vicinity of the proposed project would be minor, and the traffic assignments used in the transportation analysis, as well as in the associated air quality, noise and pedestrian safety analyses, would reasonably address potential secondary effects.

The parking demand for the new residential and retail uses associated with the proposed project was determined based on the methodology presented in the Transportation Guidelines. The proposed project would have a peak parking demand of about 146 long-term spaces and about 15 short-term spaces, for a total demand of about 161 spaces. The project would also result in the removal on an existing surface parking lot with space for about 56 vehicles, and could result in the removal of approximately six on-street parking spaces along the north side of Shipley Street to accommodate the proposed 12-foot driveway ramp, 20-foot "loading zone" and seven foot "no parking" zone between the garage ramp and loading zone, if approved by SFMTA. The proposed project would provide a subterranean parking garage with 87 off-street parking spaces, of which three would be ADA-compliant accessible spaces (one of which would be dedicated for van-sized accessible parking). The proposed project would also provide one regular parking space, two car share parking spaces, and 81 stacked parking spaces (i.e., parking spaces that would be on mechanical/hydraulic lifts). Thus, as proposed, the project would have an unmet parking demand of an estimated of 59 spaces. Furthermore, because the proposed project would not provide any on-site parking for short-term users associated with the retail use at the site, there would be an additional unmet demand of 15 spaces. As a result, the proposed project would have an unmet parking demand of approximately 74 spaces. At this location, the unmet parking demand could be accommodated within existing on-street and off-street parking spaces within a reasonable distance of the project vicinity. Additionally, the project site is well served by public transit and bicycle facilities. Therefore, any unmet parking demand associated with the project would not materially affect the overall parking conditions in the project vicinity such that hazardous conditions or significant delays would be created.

The proposed project is located within the Eastern Neighborhoods Mixed Use-Residential District and per Planning Code Section 151.1, developments within this district are to provide up to one parking space for every four dwelling units and allows, with additional findings, up to a maximum of 0.75 parking spaces per unit. Retail uses are allowed to provide up to one parking space for every 1,500 square feet of occupied floor area. As a result, the proposed project would be allowed to provide a maximum of 92

parking spaces (73 spaces for units less than 1,000 square feet, 18 spaces for units over 1,000 square feet, and one space for retail use). Furthermore, the proposed project would be required to provide up to one car share parking space.

It should be noted that the Planning Commission has the discretion to adjust the number of on-site parking spaces included in the proposed project, typically at the time that the project entitlements are sought. The Planning Commission may not support the parking ratio proposed. In some cases, particularly when the proposed project is in a transit rich area, the Planning Commission may not support the provision of any off-street parking spaces. This is, in part, owing to the fact that the parking spaces are not 'bundled' with the residential units. In other words, residents would have the option to rent or purchase a parking space, but one would not be automatically provided with the residential unit.

If the project were ultimately approved with no off-street parking spaces, the proposed project would have an unmet demand of 161 spaces. As mentioned above, the unmet parking demand could be accommodated within existing on-street and off-street parking spaces nearby and through alternative modes such as public transit and bicycle facilities. Given that the unmet demand could be met by existing facilities and given that the proposed project site is well-served by transit and bicycle facilities, a reduction in the number of off-street parking spaces associated with the proposed project, even if no off-street spaces are provided, would not result in significant delays or hazardous conditions.

In summary, the proposed project would not result in a substantial parking shortfall that would create hazardous conditions or significant delays affecting traffic, transit, bicycles or pedestrians. The following improvement measure, although not required, would further reduce traffic and parking demand of the project and encourage the use of alternative modes of transportation.

Project Improvement Measure I-TR-6: Implement Travel Demand Management (TDM) Measures

Prior to issuance of a temporary permit of building occupancy, the Project Sponsor must execute an agreement with the Planning Department for the provision of TDM services. Recommended components of the TDM program include the following:

- Provide information in the move-in packets and lobby (or electronic) bulletin boards for transit service (Muni and BART lines, schedules and fares), particularly for local trips (such as to the nearest grocery store, hardware store, shopping center, restaurants, and other nearby neighborhood commercial areas), information on where transit passes could be purchased in person and on-line, and information on the Clipper Card and 511 Regional Rideshare Program;
- Include one or more Muni FastPass (loaded onto a Clipper card) as part of the monthly rent, or homeowner association fee;
- Provide TDM training for property managers and coordinators; and have at least one contact person, preferably in the building for tenants with alternative mode travel questions.
- Promote and coordinate ridesharing activities (i.e. establish a "ride board") for all building residents and employees, particularly to popular local events;
- Facilitate access to carshare spaces provided in the parking garage through on-site signage and information on the carshare company, rates, and how to enroll in the carshare program;

- Ensure that the points of access to bicycle parking through elevators on the ground floor and the garage ramp include signage indicating the location of these facilities.
- Ensure that bicycle safety strategies are developed along the sides of the property, avoiding conflicts with private cars, transit vehicles and loading vehicles, posting signs where necessary to increase awareness of the presence of bicycle traffic;
- Facilitate access to the Folsom Street, Howard Street, and Fifth Street bicycle routes via on-site signage;
- Actively encourage alternative mode choice by actively monitoring above efforts effectiveness, and fostering local deliveries from nearby businesses where appropriate; and
- Participate with other project sponsors in a network of transportation brokerage services.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
5. NOISE—Would the project:						
a) Result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Result in exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) Result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) For a project located within an airport land use plan area, or, where such a plan has not been adopted, in an area within two miles of a public airport or public use airport, would the project expose people residing or working in the area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project located in the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Be substantially affected by existing noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

The Eastern Neighborhoods FEIR identified potential conflicts related to residences and other noise-sensitive uses in proximity to noisy uses such as PDR, retail, entertainment, cultural/institutional/educational uses, and office uses. In addition, the Eastern Neighborhoods FEIR noted that implementation of the Area Plan would incrementally increase traffic-generated noise on some streets in the Area Plan and result in construction noise impacts from pile driving and other construction activities. The Eastern Neighborhoods FEIR therefore identified six noise mitigation measures that would reduce noise impacts to less-than-significant levels.

Eastern Neighborhoods FEIR Mitigation Measures F-1 and F-2 relate to construction noise. Measure F-1 addresses individual projects that include pile-driving, and Mitigation Measure F-2 addresses individual projects that include particularly noisy construction procedures (including pile-driving). The project site is within close proximity to residential uses along both Folsom and Shipley Streets. In addition, a project that proposes approximately 270 residential units is currently under construction across Folsom Street from the project site and may be partially occupied by the time project construction commences. The proposed project would include excavation to allow for one level below grade over the entire footprint of the site. Moreover, according to the geotechnical investigation prepared for the proposed project, due to the presence of fill material and "new bay mud" on the site, the proposed buildings can be accommodated with several deep foundation alternatives, including driven pipe piles, augercast piles, Fundex or Tubex piles.¹⁴ Driven piles would likely result in high noise and vibration. The other three types of foundation recommended by the geotechnical report would result in comparatively lower noise and vibration effects. The project sponsor has indicated that pile driving would not be used for the proposed project and that the project is likely to include drilled piles instead.¹⁵ The proposed project would not include pile-driving or other particularly noise construction methods; therefore, Mitigation Measures F-1 and F-2 are not applicable.

In addition, all construction activities for the proposed project (approximately 20 months) would be subject to and would comply with the San Francisco Noise Ordinance (Article 29 of the San Francisco Police Code) (Noise Ordinance) as outlined below. Construction noise is regulated by the Noise Ordinance. The Noise Ordinance requires that construction work be conducted in the following manner: (1) noise levels of construction equipment, other than impact tools, must not exceed 80 dBA at a distance of 100 feet from the source (the equipment generating the noise); (2) impact tools must have intake and exhaust mufflers that are approved by the Director of the Department of Public Works (DPW) to best accomplish maximum noise reduction; and (3) if the noise from the construction work would exceed the ambient noise levels at the site property line by 5 dBA, the work must not be conducted between 8:00 p.m. and 7:00 a.m. unless the Director of DPW authorizes a special permit for conducting the work during that period.

DBI is responsible for enforcing the Noise Ordinance for private construction projects during normal business hours (8:00 a.m. to 5:00 p.m.). The Police Department is responsible for enforcing the Noise Ordinance during all other hours. Nonetheless, during the construction period for the proposed project of approximately 20 months, occupants of the nearby properties could be disturbed by construction noise. Times may occur when noise could interfere with indoor activities in nearby residences and other

14 Cornerstone Earth Group, *Preliminary Geotechnical Investigation, Folsom Street Residential Development (923 Folsom Street)*, November 12, 2012. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

15 Chris Davenport, Trumark Urban, Project Sponsor. *Email to Tania Sheyner, San Francisco Planning Department, 923 Folsom Street*, June 19, 2013. This email is available for review as part of Case File No. 2012.1333E at the San Francisco Planning Department, 1650 Mission Street, Suite 400, San Francisco, California 94103.

businesses near the project site and may be considered an annoyance by occupants of nearby properties. The increase in noise in the project area during project construction would not be considered a significant impact of the proposed project, because the construction noise would be temporary (approximately 20 months), intermittent, and restricted in occurrence and level, as the contractor would be subject to and would comply with the Noise Ordinance.

Eastern Neighborhoods FEIR Mitigation Measures F-3, F-4, and F-6 include additional measures for individual projects that include new noise-sensitive uses. Mitigation Measure F-3 requires that new development that includes noise-sensitive uses located along streets with noise levels above 60 dBA (Ldn), where such development is not already subject to California Noise Insulation Standards in Title 24, the project sponsor shall conduct a detailed analysis of noise reduction requirements.¹⁶ Mitigation Measure F-4 requires the preparation of an analysis that includes, at minimum, a site survey to identify potential noise-generating uses within 900 feet of and that have a direct line of site to the project site, and at least one 24-hour noise measurement (with maximum noise levels taken every 15 minutes) to demonstrate that acceptable interior noise levels consistent with Title 24 can be attained. The proposed project, as a multi-unit residential development, is subject to requirements of Title 24; therefore, Mitigation Measure F-3 would not be applicable to the proposed project. However, Mitigation Measure F-4, below, is applicable to the proposed project. Accordingly, the project sponsor has conducted an environmental noise study demonstrating that the proposed project can feasibly attain acceptable interior noise levels consistent with Title 24.¹⁷

Project Mitigation Measure M-NO-1: Siting of Noise-Sensitive Uses (Implements Mitigation Measure F-4 of the Eastern Neighborhoods FEIR)

To reduce potential conflicts between existing noise-generating uses and new sensitive receptors, for new development including noise-sensitive uses, the Planning Department shall require the preparation of an analysis that includes, at a minimum, a site survey to identify potential noise-generating uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis shall be prepared by persons qualified in acoustical analysis and/or engineering and shall demonstrate with reasonable certainty that Title 24 standards, where applicable, can be met, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels in the vicinity. Should such concerns be present, the Department may require the completion of a detailed noise assessment by person(s) qualified in acoustical analysis and/or engineering prior to the first project approval action, in order to demonstrate that acceptable interior noise levels consistent with those in the Title 24 standards can be attained.

The noise study included a 24-hour noise measurement and site survey of noise-generating uses within two blocks of the project site. According to the noise study, the noise environment at the site is dominated by vehicular traffic along Folsom Street, including buses, and the project site is also likely affected by intermittent noise associated with the adjacent fire station (Fire Station 1). Fifth Street vehicle traffic and distant noise from Interstate 80 also contribute to the overall noise environment. The study noted that

16 Title 24 of the California Code of Regulations establishes uniform noise insulation standards for multi-unit residential projects, including hotels, motels, and live-work developments.

17 Charles M. Salter Associates, Inc., *Environmental Noise Study*, 923 Folsom Street, San Francisco, California, CSA Project Number 13-0316, July 9, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

various commercial and light industrial uses located within 900 feet of the project site are not expected to be above the measured ambient noise sources and are unlikely to significantly contribute to the existing noise environment in the project site. The 24-hour noise measurements recorded two day-night average sound levels (DNL) near the project site: along Folsom Street, the recorded noise level was 80 dB while along Shipley Street, the recorded noise level was 68 dB. The noise study concluded that, to meet the Building Code indoor DNL 45 dB requirement, windows and exterior doors would need to be sound rated with Sound Transmission Class (STC) ratings up to STC 42.

With respect to noise associated with operations at the adjacent Fire Station 1, while these could pose an occasional and intermittent nuisance to the proposed residences and commercial uses on the project site, noise from emergency vehicle use and operations, including sirens and emergency equipment testing, is exempt from the City's Noise Ordinance.¹⁸ Therefore, noise associated with the adjacent Fire Station 1 along Folsom Street would not expose persons to or generate noise levels in excess of applicable standards established by the City or other agencies. Nonetheless, noise insulation measures mandated by Title 24, which the project sponsor would be required to implement, would ensure that residential and commercial uses are shielded to the greatest extent possible from outside noise, including that generated by fire station operations.

Mitigation Measure F-6 requires that open space required under the Planning Code for individual projects located in noisy areas be protected, to the maximum feasible extent, from existing ambient noise levels. The project site is located along streets with noise levels above 60 dBA and the proposed project would both result in a new noise sensitive use (residential) and requires new open space under the Planning Code; therefore this mitigation measure is applicable.

Project Mitigation Measure M-NO-2: Open Space in Noisy Environments (Implements Mitigation Measure F-6 of the Eastern Neighborhoods FEIR)

To minimize effects on development in noisy areas, for new development including noise sensitive uses, the Planning Department shall, through its building permit review process, in conjunction with noise analysis required pursuant to Mitigation Measure F-4, require that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design.

The noise study analyzed the proposed open space on the roof deck of the Shipley Street building and concluded that it would be adequately shielded by the surrounding buildings in the area as well as the proposed building elements of the 923 Folsom Street project. The project sponsor confirmed that the project would include some shielding of the proposed rooftop open space on top of the Shipley Street building.

18 Intermittent noise associated with Fire Station 1, including sirens and emergency vehicle testing, was studied in the San Francisco Museum of Modern Art Expansion/Fire Station Relocation and Housing Project EIR (Case Nos. 2009.0291E and 2010.0275E), which was certified on November 10, 2011. This EIR is available on the Planning Department website at <http://www.sf-planning.org/index.aspx?page=1828>, accessed on June 5, 2014.

Eastern Neighborhoods FEIR Mitigation Measure F-5 addresses impacts related to individual projects that include new noise-generating uses that would be expected to generate noise levels in excess of ambient noise in the proposed project site vicinity. Ambient noise levels in San Francisco are largely influenced by traffic-related noise. The proposed project would be located along two streets, Folsom Street and Shipley Street, identified as having noise levels above 65 L_{dn}.¹⁹ An approximate doubling in traffic volumes in the area would be necessary to produce an increase in ambient noise levels barely perceptible to most people (3 decibel increase). The proposed project would not double traffic volumes because the proposed project would generate approximately 565 daily vehicle trips, with approximately 92 trips during the p.m. peak-hour. In addition, operation of the proposed project would not include any other constant or short-term noise sources (e.g., diesel generator) that would be perceptible in the project vicinity. Therefore, the proposed project would not result in a substantial permanent increase in ambient noise levels in the project vicinity, and thus Mitigation Measure F-5 does not apply.

The project site is not located within an airport land use plan area, within two miles of a public airport, or in the vicinity of a private airstrip. Therefore, topic 12e and f from the CEQA Guidelines, Appendix G is not applicable.

For the above reasons, the proposed project would not result in significant impacts that were not identified in the Eastern Neighborhoods FEIR related to noise.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
6. AIR QUALITY: Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations.— Would the project:						
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal, state, or regional ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

¹⁹ The Noise Model layer is in L_{dn} (level day night) and is based on San Francisco traffic as determined by the San Francisco Metropolitan Transportation Agency's SFCHAMP model. Traffic noise emissions were modeled using the FHWA Stamina model.

The Eastern Neighborhoods FEIR identified potentially significant air quality impacts related to construction activities that may cause wind-blown dust and pollutant emissions; roadway-related air quality impacts on sensitive land uses; and the siting of uses that emit diesel particulate matter (DPM) and toxic air contaminants (TACs) as part of everyday operations. These significant impacts would conflict with the applicable air quality plan at the time, the Bay Area 2005 Ozone Strategy. The Eastern Neighborhoods FEIR identified four mitigation measures that would reduce air quality impacts to less-than-significant levels.

Eastern Neighborhoods FEIR Mitigation Measure G-1 requires individual projects that include construction activities to include dust control measures and maintain and operate construction equipment so as to minimize exhaust emissions of particulates and other pollutants. This mitigation measure was identified in the Initial Study. Subsequent to publication of the Initial Study, the San Francisco Board of Supervisors approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Construction Dust Control Ordinance (Ordinance 176-08, effective July 30, 2008). The intent of the Construction Dust Control Ordinance is to reduce the quantity of dust generated during site preparation, demolition, and construction work in order to protect the health of the general public and of on-site workers, minimize public nuisance complaints, and to avoid orders to stop work by DBI. Construction activities from the proposed project would result in dust, primarily from ground-disturbing activities. The proposed project would be subject to and would be required to comply with the Construction Dust Control Ordinance.

Also subsequent to publication of the Initial Study, the Bay Area Air Quality Management District (BAAQMD), the regional agency with jurisdiction over the nine-county San Francisco Bay Area Air Basin (SFBAAB), provided updated 2011 BAAQMD CEQA Air Quality Guidelines (Air Quality Guidelines),²⁰ which provided new methodologies for analyzing air quality impacts, including construction activities. The Air Quality Guidelines provide screening criteria for determining whether a project's criteria air pollutant emissions may violate an air quality standard, contribute to an existing or projected air quality violation, or result in a cumulatively considerable net increase in criteria air pollutants. If a project meets the screening criteria, then the lead agency or applicant would not need to perform a detailed air quality assessment of their proposed project's air pollutant emissions and construction or operation of the proposed project would result in a less-than-significant air quality impact.

For determining potential health risk impacts, San Francisco has partnered with the BAAQMD to inventory and assess air pollution and exposures from mobile, stationary, and area sources within San Francisco and identify portions of the City that result in additional health risks for affected populations ("Air Pollutant Exposure Zone"). The Air Pollutant Exposure Zone was identified based on two health based criteria:

- (1) Excess cancer risk from all sources > 100; and
- (2) PM_{2.5} concentrations from all sources including ambient >10µg/m³.

Sensitive receptors²¹ within the Air Pollutant Exposure Zone are more at risk for adverse health effects from exposure to substantial air pollutant concentrations than sensitive receptors located outside the Air

²⁰ Bay Area Air Quality Management District, CEQA Air Quality Guidelines, updated May 2011.

²¹ The BAAQMD considers sensitive receptors as: children, adults or seniors occupying or residing in: 1) Residential dwellings, including apartments, houses, condominiums, 2) schools, colleges, and universities, 3) daycares, 4) hospitals, and 5) senior care facilities. Bay Area Air Quality Management District (BAAQMD), Recommended Methods for Screening and Modeling Local Risks and Hazards, May 2011, page 12.

Pollutant Exposure Zone. These locations (i.e., within the Air Pollutant Exposure Zone) require additional consideration when projects or activities have the potential to emit TACs, including DPM emissions from temporary and variable construction activities.

Construction activities from the proposed project may result in dust, primarily from ground-disturbing activities outside the existing structures (e.g., modifications to curb cuts and driveways). The proposed project would be subject to and would comply with the Construction Dust Control Ordinance, therefore the portions of Mitigation Measure G-1 that deal with dust control are not applicable to the proposed project. Construction activities from the proposed project would also result in the emission of criteria air pollutants and DPM from equipment exhaust, construction-related vehicular activity, and construction worker automobile trips. Construction would last approximately 20 months. Diesel-generating equipment would be required for approximately 12 months.

The project site is located within an identified air pollutant exposure zone, therefore, the proposed project's temporary and variable construction activities would result in short-term emissions of DPM and other TACs that would add emissions to areas already adversely affected by poor air quality. The proposed project's construction activities would be temporary and variable in nature and the proposed project would be subject to California regulations limiting idling times to five minutes, which would further reduce sensitive receptors exposure to temporary and variable DPM emissions.²² Moreover, the proposed project meets the construction screening criteria provided in the BAQMD studies for construction-related criteria air pollutants. Nevertheless, the remainder of Eastern Neighborhoods FEIR Mitigation Measure G-1 that addresses maintenance and operation of construction equipment is applicable to the proposed project. Compliance with this Construction Emissions Minimization measure (which is discussed below), would result in less-than-significant impacts from construction vehicles and equipment. In accordance with the Eastern Neighborhoods FEIR requirements, the project sponsor has agreed to implement this mitigation measure, as updated.

Project Mitigation Measure M-AQ-1: Construction Emissions Minimization (Implements a portion of Mitigation Measure G-1 of the Eastern Neighborhoods FEIR)

- A. *Construction Emissions Minimization Plan.* Prior to issuance of a construction permit, the project sponsor shall submit a Construction Emissions Minimization Plan (Plan) to the Environmental Review Officer (ERO) for review and approval by an Environmental Planning Air Quality Specialist. The Plan shall detail project compliance with the following requirements:
1. All off-road equipment greater than 25 hp and operating for more than 20 total hours over the entire duration of construction activities shall meet the following requirements:
 - a) Where access to alternative sources of power are available, portable diesel engines shall be prohibited;
 - b) All off-road equipment shall have:
 - i. Engines that meet or exceed either USEPA or ARB Tier 2 off-road emission standards, *and*
 - ii. Engines that are retrofitted with an ARB Level 3 Verified Diesel Emissions Control Strategy (VDECS).²³
 - c) Exceptions:

²² California Code of Regulations, Title 13, Division 3, § 2485.

²³ Equipment with engines meeting Tier 4 Interim or Tier 4 Final emission standards automatically meet this requirement, therefore a VDECS would not be required.

- i. Exceptions to A(1)(a) *may* be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that an alternative source of power is limited or infeasible at the project site and that the requirements of this exception provision apply. Under this circumstance, the sponsor shall submit documentation of compliance with A(1)(b) for onsite power generation.
- ii. Exceptions to A(1)(b)(ii) *may* be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that a particular piece of off-road equipment with an ARB Level 3 VDECS is: (1) technically not feasible, (2) would not produce desired emissions reductions due to expected operating modes, (3) installing the control device would create a safety hazard or impaired visibility for the operator, or (4) there is a compelling emergency need to use off-road equipment that are not retrofitted with an ARB Level 3 VDECS and the sponsor has submitted documentation to the ERO that the requirements of this exception provision apply. If granted an exception to A(1)(b)(ii), the project sponsor must comply with the requirements of A(1)(c)(iii).
- iii. If an exception is granted pursuant to A(1)(c)(ii), the project sponsor shall provide the next cleanest piece of off-road equipment as provided by the step down schedules in Table A1 below.

**TABLE A1
OFF-ROAD EQUIPMENT COMPLIANCE STEP DOWN SCHEDULE***

Compliance Alternative	Engine Emission Standard	Emissions Control
1	Tier 2	ARB Level 2 VDECS
2	Tier 2	ARB Level 1 VDECS
3	Tier 2	Alternative Fuel*

*How to use the table. If the requirements of (A)(1)(b) cannot be met, then the project sponsor would need to meet Compliance Alternative 1. Should the project sponsor not be able to supply off-road equipment meeting Compliance Alternative 1, then Compliance Alternative 2 would need to be met. Should the project sponsor not be able to supply off-road equipment meeting Compliance Alternative 2, then Compliance Alternative 3 would need to be met.

**Alternative fuels are not a VDECS

- 2. The project sponsor shall require the idling time for off-road and on-road equipment be limited to no more than *two* minutes, except as provided in exceptions to the applicable state regulations regarding idling for off-road and on-road equipment. Legible and visible signs shall be posted in multiple languages (English, Spanish, Chinese) in designated queuing areas and at the construction site to remind operators of the two minute idling limit.
- 3. The project sponsor shall require that construction operators properly maintain and tune equipment in accordance with manufacturer specifications.
- 4. The Plan shall include estimates of the construction timeline by phase with a description of each piece of off-road equipment required for every construction phase. Off-road equipment descriptions and information may include, but is not limited to: equipment type, equipment

manufacturer, equipment identification number, engine model year, engine certification (Tier rating), horsepower, engine serial number, and expected fuel usage and hours of operation. For VDECS installed: technology type, serial number, make, model, manufacturer, ARB verification number level, and installation date and hour meter reading on installation date. For off-road equipment using alternative fuels, reporting shall indicate the type of alternative fuel being used.

5. The Plan shall be kept on-site and available for review by any persons requesting it and a legible sign shall be posted at the perimeter of the construction site indicating to the public the basic requirements of the Plan and a way to request a copy of the Plan. The project sponsor shall provide copies of Plan to members of the public as requested.

B. *Reporting.* Quarterly reports shall be submitted to the ERO indicating the construction phase and off-road equipment information used during each phase including the information required in A(4). In addition, for off-road equipment using alternative fuels, reporting shall include the actual amount of alternative fuel used.

1. Within six months of the completion of construction activities, the project sponsor shall submit to the ERO a final report summarizing construction activities. The final report shall indicate the start and end dates and duration of each construction phase. For each phase, the report shall include detailed information required in A(4). In addition, for off-road equipment using alternative fuels, reporting shall include the actual amount of alternative fuel used.

C. *Certification Statement and On-site Requirements.* Prior to the commencement of construction activities, the project sponsor must certify (1) compliance with the Plan, and (2) all applicable requirements of the Plan have been incorporated into contract specifications.

Mitigation Measure G-2 requires new residential development near high-volume roadways and/or warehousing and distribution centers to include an analysis of DPM and/or TACs, and, if warranted, to incorporate upgraded ventilation systems to minimize exposure of future residents to DPM and other pollutant emissions, as well as odors. As noted above, the proposed project is located within an identified Air Pollutant Exposure Zone (i.e., an area that experiences higher levels of air pollution). The proposed project therefore would have the potential to expose sensitive receptors to substantial concentrations of air pollutants. Therefore, the Air Filtration Measure, required as part of building design, would be applicable to the proposed project and would require the project sponsor install a filtered air supply system capable of removing 80 percent of outdoor particulates indoors. Compliance with this mitigation measure, which is discussed further below, would satisfy the requirements of Eastern Neighborhoods FEIR Mitigation Measure G-2.

Project Mitigation Measure M-AQ-2 – Air Filtration (Implements Mitigation Measure G-2 of the Eastern Neighborhoods FEIR)

Air Filtration and Ventilation Requirements for Sensitive Land Uses. Prior to receipt of any building permit, the project sponsor shall submit a ventilation plan for the proposed building(s). The ventilation plan shall show that the building ventilation system removes at least 80 percent of the outdoor PM_{2.5} concentrations from habitable areas and be designed by an engineer certified by ASHRAE, who shall provide a written report documenting that the system meets the 80 percent performance standard identified in this measure and offers the best available technology to minimize outdoor to indoor transmission of air pollution.

Maintenance Plan. Prior to receipt of any building permit, the project sponsor shall present a plan that ensures ongoing maintenance for the ventilation and filtration systems.

Disclosure to buyers and renters. The project sponsor shall also ensure the disclosure to buyers (and renters) that the building is located in an area with existing sources of air pollution and as such, the building includes an air filtration and ventilation system designed to remove 80 percent of outdoor particulate matter and shall inform occupants of the proper use of the installed air filtration system.

Mitigation Measure G-3 minimizes potential exposure of sensitive receptors to DPM by requiring uses that would be served by at least 100 trucks per day or 40 refrigerated trucks per day be located no less than 1,000 feet from residential units and other sensitive receptors. The proposed project would not include any commercial uses and would not be served by 100 trucks per day or 40 refrigerator trucks per day. Therefore, Mitigation Measure G-3 is not applicable to the proposed project.

Mitigation Measure G-4 involves the siting of commercial, industrial, or other uses that emit TACs as part of everyday operations. The proposed project would involve demolition of an existing two-story commercial office building and a surface parking lot and construction of a project consisting of two residential buildings (85 feet and 44 feet in height), containing a total of 115 dwelling units above a ground-floor retail/commercial space and a below-grade parking garage containing 87 spaces. The project would not generate more than 10,000 vehicle trips per day, 1,000 truck trips per day, or include a new stationary source, items that would emit TACs as part of everyday operations. Therefore, Mitigation Measure G-4 is not applicable to the proposed project.

The proposed project would result in an increase in operational-related criteria air pollutants including from the generation of daily vehicle trips and energy demand. The proposed project meets the screening criteria provided in the BAAQMD CEQA Air Quality Guidelines (May 2011) for operational-related criteria air pollutants.

For the above reasons, the proposed project would not result in significant impacts on air quality that were not identified in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
7. GREENHOUSE GAS EMISSIONS—Would the project:						
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR assessed the GHG emissions that could result from rezoning of the East SoMa Area Plan under the three rezoning options. The Eastern Neighborhoods Rezoning Options A, B, and C are anticipated to result in GHG emissions on the order of 4.2, 4.3 and 4.5 metric tons of CO₂E per service population,²⁴ respectively. The Eastern Neighborhoods FEIR concluded that the resulting GHG emissions from the three options analyzed in the Eastern Neighborhoods Area Plans would be less than significant. No mitigation measures were identified in the FEIR.

Regulations outlined in San Francisco’s Strategies to Address Greenhouse Gas Emissions, have proven effective as San Francisco’s GHG emissions have measurably reduced when compared to 1990 emissions levels, demonstrating that the City has met and exceeded EO S-3-05, AB 32, and the Bay Area 2010 Clean Air Plan GHG reduction goals for the year 2020. The proposed project was determined to be consistent with San Francisco’s GHG Reduction Strategy.²⁵ Other existing regulations, such as those implemented through AB 32, will continue to reduce a proposed project’s contribution to climate change. Therefore, the proposed project’s GHG emissions would not conflict with state, regional, and local GHG reduction plans and regulations, and thus the proposed project’s contribution to GHG emissions would not be cumulatively considerable or generate GHG emissions, either directly or indirectly, that would have a significant impact on the environment.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on greenhouse gas emissions beyond those analyzed in the Eastern Neighborhoods FEIR.

Topics:	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
8. WIND AND SHADOW—Would the project:						
a) Alter wind in a manner that substantially affects public areas?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create new shadow in a manner that substantially affects outdoor recreation facilities or other public areas?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Wind

No significant impacts related to wind were anticipated to result from the implementation of the Eastern Neighborhoods Rezoning and Area Plans. Specific projects within Eastern Neighborhoods require analysis of wind impacts where deemed necessary. Thus, wind impacts were determined not to be significant in the Eastern Neighborhoods Initial Study and were not analyzed in the Eastern Neighborhoods FEIR. No mitigation measures relative to wind impacts were identified in the Eastern Neighborhoods FEIR.

24 Memorandum from Jessica Range, MEA to MEA staff, *Greenhouse Gas Analyses for Community Plan Exemptions in Eastern Neighborhoods*, April 20, 2010. This memorandum provides an overview of the GHG analysis conducted for the Eastern Neighborhoods Rezoning EIR and provides an analysis of the emissions using a service population (equivalent of total number of residents and employees) metric.

25 San Francisco Planning Department, *Greenhouse Gas Analysis: Compliance Checklist*, June 16, 2013. This document is on file and available for public review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

Based on the height and the location of the proposed project and given that it proposes to construct a new building with a mechanical penthouse up to 101 feet tall, a project-specific pedestrian wind assessment (“Wind Assessment”) was prepared by a qualified wind consultant for the proposed project.²⁶ The objective of the Wind Assessment was to provide a qualitative evaluation of the potential wind impacts of the proposed development, which provides a screening-level estimation of the potential wind impact. A summary of the analysis is presented below.

The project site currently contains a parking lot surrounded by low-rise buildings of similar heights and a large parking lot to the northwest. Prevailing winds from the west and northwest blowing over the surrounding buildings are directed down to pedestrian level near the site. Due to the low heights of the existing and surrounding buildings, it is unlikely that current wind conditions exceed the wind hazard criterion of 26 mph and may meet the 11 mph comfort criterion at the surrounding pedestrian areas.

The proposed nine-story structure on Folsom Street would be taller than most of the immediate surroundings development, while the proposed four-story building on Shipley Street would be similar in height to the surrounding buildings. Since the mid-rise building faces the predominant wind direction, west-northwesterly winds may be downwashed to the street level on Folsom Street, but the resultant wind conditions are expected to meet the hazard criterion. The proposed trees along the sidewalk, although not required as a wind control measure, would be an effective means to reduce the impact of downwashing flows. The project would also not be expected to result in any wind hazard criterion exceedances along Shipley Street, or in the general surroundings of Folsom or Shipley Streets.

Entrances to the proposed buildings would be located on Folsom and Shipley Streets. While the Shipley street entrance would be sheltered from the predominant winds, the Folsom Street entrance could be affected by downwashing flows. However, the impact is not likely to be significant because the entrance is recessed from the main facade. If desired, additional shelter may be provided to the main entrance on Folsom Street by having the entrance recessed further from the building façade. Alternatively, if feasible, the Wind Assessment recommends placing a canopy above this entrance, extending 6 feet or more out from the door.

The location of the taller building on the northwest of the site would largely shelter the inner courtyard from the prevailing northwesterly winds. It is likely that the westerly winds would interact with the mid-rise building, wrap around the building corner on the west side and be redirected to the inner courtyard. However, since the inner courtyard is lined by mid-rise (or taller) buildings, these redirected flows would not cause a significant impact. In addition, trees and other tall landscaping features in the inner courtyard would diffuse flows in the space. The roof deck on top of the Shipley Street building would likewise be sheltered from the predominant winds by the proposed Folsom Street building and would include various features that would moderate wind exposure, including the screen along the west edge of the passageway between the two buildings, the proposed parapet, and landscaping.

The Wind Assessment concluded that the existing wind conditions on the adjacent streets do not exceed the 26-mile-per-hour wind hazard criterion for a single full hour, or approximately 0.0114 percent of the time, as outlined in the San Francisco Planning Code Section 148. The Wind Assessment also found that the proposed building would not cause winds that would reach or exceed the 26-mile-per-hour wind hazard criterion at all pedestrian areas on and around the proposed development and that wind speeds at building entrances and public sidewalks would be suitable for the intended pedestrian usage.

²⁶ RWDI, *Pedestrian Wind Assessment, 923 Folsom Street, San Francisco, California*, November 4, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

As a result, the proposed project would not have any significant wind impacts, either individually or cumulatively.

Shadow

Planning Code Section 295 generally prohibits new structures above 40 feet in height that would cast additional shadows on open space that is under the jurisdiction of the San Francisco Recreation and Park Commission between one hour after sunrise and one hour before sunset, at any time of the year, unless that shadow would not result in a significant adverse effect on the use of the open space. Under the Eastern Neighborhoods Area Plan, sites surrounding parks could be redeveloped with taller buildings without triggering Section 295 of the Planning Code because certain parks are not subject to Section 295 of the Planning Code (i.e., under jurisdiction by departments other than the Recreation and Parks Department or privately owned). The Eastern Neighborhoods FEIR could not conclude if the rezoning and community plans would result in less-than-significant shadow impacts because the feasibility of complete mitigation for potential new shadow impacts of unknown proposed proposals could not be determined at that time. Therefore, the FEIR determined shadow impacts to be significant and unavoidable. No mitigation measures were identified in the FEIR.

The proposed project would construct two buildings on the project site – a nine-story building (approximately 85 feet in height) fronting Folsom Street and a four-story building (approximately 44 feet in height) fronting Shipley Street. The Folsom Street building would have an approximately 15-foot rooftop mechanical penthouse, bringing the total height of the structure, for shadow analysis purposes, to approximately 100 feet. Therefore, a shadow analysis was conducted pursuant to Planning Code Section 295.²⁷ The shadow analysis found that the proposed project would not cast any net new shadow on any property under the jurisdiction of the Recreation and Parks Commission, including the SoMa Recreation Center, which is approximately 800 feet to the southwest of the project site.

The proposed project would also shade portions of nearby streets and sidewalks and private property at times. Shadows upon streets and sidewalks would not exceed levels commonly expected in urban areas and would be considered a less-than-significant effect under CEQA. Although occupants of nearby property may regard the increase in shadow as undesirable, the limited increase in shading of private properties as a result of the proposed project would not be considered a significant impact under CEQA.

For the above reasons, the proposed project would not contribute to significant impacts related to shadow that were identified in the Eastern Neighborhoods FEIR.

²⁷ San Francisco Planning Department, *Shadow Analysis, 923 Folsom Street*, January 18, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

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9. RECREATION—Would the project:						
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facilities would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Include recreational facilities or require the construction or expansion of recreational facilities that might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Physically degrade existing recreational resources?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR concluded that implementation of the Eastern Neighborhoods Rezoning and Area Plans would not result in substantial or accelerated deterioration of existing recreational resources or require the construction or expansion of recreational facilities that may have an adverse effect on the environment. No mitigation measures related to recreational resources were identified in the Eastern Neighborhoods FEIR.

As the proposed project does not degrade recreational facilities and is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on recreation beyond those analyzed in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
10. UTILITIES AND SERVICE SYSTEMS—Would the project:						
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
d) Have sufficient water supply available to serve the project from existing entitlements and resources, or require new or expanded water supply resources or entitlements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater treatment provider that would serve the project that it has inadequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR determined that the anticipated increase in population would not result in a significant impact to the provision of water, wastewater collection and treatment, and solid waste collection and disposal. No mitigation measures were identified in the FEIR.

The project would be subject to the City's Stormwater Management Ordinance, which requires the project to maintain or reduce the existing volume and rate of stormwater runoff discharged from the site. To achieve this, the project would implement and install appropriate stormwater management systems that retain runoff on site, promote stormwater reuse, and limit site discharges entering the combined sewer collection system. This, in turn, would limit the incremental demand on both the collection system and wastewater facilities resulting from stormwater discharges, and minimize the potential need for expanding or constructing new facilities. Thus, the project would not require or result in the construction of new stormwater drainage facilities or expansion of existing facilities, the construction of which would cause significant environmental effects.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on utilities and service systems beyond those analyzed in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
11. PUBLIC SERVICES—Would the project:						
a) Result in substantial adverse physical impacts associated with the provision of, or the need for, new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any public services such as fire protection, police protection, schools, parks, or other services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR determined that the anticipated increase in population would not result in a significant impact to public services, including fire protection, police protection, and public schools. No mitigation measures were identified in the FEIR.

The proposed project would result in 115 new residential units and approximately 1,900 sf of commercial space. This population growth would generate an increase in demand for public services, but this additional demand would not exceed the planned service levels and capacity discussed in the Eastern Neighborhoods FEIR. In addition, no new facilities would need to be constructed in order to maintain acceptable service ratios, response times, or other performance objectives for any public services.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
12. BIOLOGICAL RESOURCES—Would the project:						
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

As discussed in the Eastern Neighborhoods FEIR, the Eastern Neighborhoods Plan Area is in a developed urban environment that does not provide native natural habitat for any rare or endangered plant or animal species. There are no riparian corridors, estuaries, marshes, or wetlands in the Plan Area that could be affected by the development anticipated under the Area Plan. In addition, development envisioned under the Eastern Neighborhoods Area Plan would not substantially interfere with the movement of any resident or migratory wildlife species. For these reasons, the FEIR concluded that implementation of the Area Plan would not result in significant impacts on biological resources, and no mitigation measures were identified.

The project site contains a commercial structure and surface parking area, and is located in a developed urban area. There are no candidate, sensitive, or special-status species, riparian habitat, or wetlands on the project site, so implementation of the proposed project would not adversely affect a candidate, sensitive, or special-status species, a riparian habitat, or wetlands.

San Francisco is located within the Pacific Flyway, a major north-south route of travel for migratory birds along the western portion of the Americas, extending from Alaska to Patagonia, Argentina. Every year, migratory birds travel some or all of this distance in the spring and autumn, following food sources, heading to and from breeding grounds, or traveling to and from overwintering sites. High-rise buildings are potential obstacles that can injure or kill birds in the event of a collision, and bird strikes are a leading cause of worldwide declines in bird populations.

Planning Code Section 139, Standards for Bird-Safe Buildings, establishes building design standards to reduce avian mortality rates associated with bird strikes. This ordinance focuses on location-specific

hazards and building feature-related hazards. Location-specific hazards apply to buildings in, or within 300 feet of and having a direct line of sight to, an Urban Bird Refuge, which is defined as an open space “two acres and larger dominated by vegetation, including vegetated landscaping, forest, meadows, grassland, or wetlands, or open water.” The project site is not in or within 300 feet of an Urban Bird Refuge, so the standards related to location-specific hazards are not applicable to the proposed project. Feature-related hazards, which can occur on buildings anywhere in San Francisco, are defined as freestanding glass walls, wind barriers, skywalks, balconies, and greenhouses on rooftops that have unbroken glazed segments of 24 square feet or larger. The proposed project would comply with the feature-related standards of Planning Code Section 139 by using bird-safe glazing treatment on 100 percent of any feature-related hazards. As a result, the proposed project would not interfere substantially with the movement of any native resident or migratory wildlife species or with established native resident or migratory wildlife corridors.

There are no existing trees or other vegetation on the project site that would need to be removed as part of the proposed project. The five existing trees adjacent to the project site along the Folsom Street frontage would also remain with project implementation.²⁸ Moreover, in compliance with the provisions of the San Francisco Green Landscape Ordinance, ten new street trees would be planted within the right-of-way along the project site frontage on Shipley Street. As a result, the proposed project would not conflict with any local policies or ordinances that protect biological resources.

The project site is not within an area covered by an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, state, or regional habitat conservation plan. As a result, the proposed project would not conflict with the provisions of any such plan.

For these reasons, implementation of the proposed project would not result in impacts on biological resources beyond those analyzed in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
13. GEOLOGY AND SOILS—Would the project: a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:						

²⁸ Vern Lohman (Authorized Agent for the Project Sponsor), *Tree Planting and Protection Checklist*, 923 Folsom Street, April 8, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

Topics:	Project-Specific Significant Impact Not Identified in PEIR	Significant Unavoidable Impact Identified in PEIR	Mitigation Identified in PEIR	PEIR Mitigation Applies to Project	PEIR Mitigation Does Not Apply to Project	No Significant Impact (Project or PEIR)
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? (Refer to Division of Mines and Geology Special Publication 42.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code, creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Change substantially the topography or any unique geologic or physical features of the site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR concluded that implementation of the Plan would indirectly increase the population that would be subject to an earthquake, including seismically induced ground-shaking, liquefaction, and landslides. The FEIR also noted that new development is generally safer than comparable older development due to improvements in building codes and construction techniques. Compliance with applicable codes and recommendations made in project-specific geotechnical analyses would not eliminate earthquake risks, but would reduce them to an acceptable level, given the seismically active characteristics of the Bay Area. Thus, the FEIR concluded that implementation of the Plan would not result in significant impacts with regard to geology, and no mitigation measures were identified in the Eastern Neighborhoods FEIR.

A Preliminary Geotechnical Investigation was prepared for the proposed project.²⁹ The following discussion relies on the information provided in the geotechnical investigation.

The project site is developed with a commercial building and a parking area. The site is underlain by approximately 6 to 10.5 feet of undocumented fill consisting of loose to dense silty and clayey sand. The fill layer is underlain by loose to medium dense, fine- to medium-grained, poorly-graded and silty sands to depth of approximately 12 to 15 feet below ground surface (bgs), which is immediately underlain by soft, compressible silty clay known as Bay Mud. Beneath Bay Mud are older alluvial soils and dense sands. Ground water levels beneath the site were observed at approximately 8-10 feet bgs ground surface. The Preliminary Geotechnical Investigation noted that the project site has a potential for liquefaction during a significant seismic event, as well as static and seismic settlement.

Soil-disturbing activities would be required for the foundation system and excavation for the proposed subterranean garage. According to the geotechnical report prepared for this project, the foundation work for the project would require soils disturbance to a maximum depth of approximately 14 feet. The project sponsor has indicated that a combination of pile foundations and grade beams is proposed (contingent on the final geotechnical report). As an alternate, a mat foundation is being considered. Drilled (not driven) piles would also be installed, as required by the buildings seismic resisting systems. The remainder of the foundation would be shallow grade beams. The completed project would not alter the overall topography of the site.

The Preliminary Geotechnical Investigation stated that the proposed project is feasible, but recommended that a design-level geotechnical investigation be performed to confirm the preliminary recommendations and to develop detailed recommendations for design and construction. Some of the concerns that should be addressed in the design-level geotechnical investigation include the potential for settlement and liquefaction, presence of undocumented fills, shallow ground water and soft, unstable basement subgrade excavation.

The final building plans would be reviewed by DBI. In reviewing building plans, DBI refers to a variety of information sources to determine existing hazards. Sources reviewed include maps of Special Geologic Study Areas and known landslide areas in San Francisco as well as the building inspectors' working knowledge of areas of special geologic concern. DBI will review the geotechnical report and building plans for the proposed project to determine the adequacy of the proposed engineering and design features and to ensure compliance with all applicable San Francisco Building Code provisions regarding structural safety. The above-referenced geotechnical investigation report would be available for use by DBI during its review of building permits for the site. In addition, DBI could require that additional site specific soils report(s) be prepared in conjunction with permit applications, as needed. The DBI requirement for a geotechnical report and review of the building permit application pursuant to DBI's implementation of the Building Code would ensure that the proposed project would have no significant impacts related to soils or geology.

For these reasons, the proposed project would not result in significant impacts related to geology and soils that were not identified in the Eastern Neighborhoods FEIR, and no mitigation measures are necessary.

29 Cornerstone Earth Group, *Preliminary Geotechnical Investigation, 923 Folsom Street, San Francisco, California*, November 12, 2012. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
14. HYDROLOGY AND WATER QUALITY—Would the project:						
a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner that would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner that would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other authoritative flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures that would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Expose people or structures to a significant risk of loss, injury or death involving inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR determined that the anticipated increase in population would not result in a significant impact on hydrology and water quality, including the combined sewer system and the potential for combined sewer outflows. No mitigation measures were identified in the FEIR.

The existing approximately 24,375-square-foot project site is completely covered by impervious surfaces. The proposed project would neither increase nor decrease this amount and the site would remain entirely covered by impervious surfaces. The proposed project would therefore neither increase nor decrease the amount of stormwater runoff and drainage.

The proposed project would be constructed in compliance with all applicable federal, state and local regulations governing water quality and discharges to surface and ground water bodies. The proposed project would not alter drainage patterns in a manner that would result in substantial erosion, siltation, or flooding. Runoff from the project site would drain into the City's combined stormwater/sewer system, ensuring that such runoff is properly treated at the Southeast Water Pollution Control Plant before being discharged into San Francisco Bay. In accordance with the City's Stormwater Management Ordinance (Ordinance No. 83-10), the proposed project would be subject to Low Impact Design (LID) approaches and stormwater management systems to comply with the Stormwater Design Guidelines. In addition, the project sponsor would be required to prepare a Stormwater Pollution Prevention Plan (SWPPP) that would be reviewed, approved, and enforced by the San Francisco Public Utilities Commission. The SWPPP would specify best management practices and erosion and sedimentation control measures to prevent sedimentation from entering the City's combined stormwater/sewer system. As a result, the proposed project would not violate any water quality standards or waste discharge requirements or otherwise substantially degrade water quality.

Groundwater is relatively shallow throughout the project site, approximately 8-10 feet bgs. The proposed project would involve excavation to this depth and is therefore may encounter groundwater. Any groundwater that is encountered during construction would be subject to requirements of the City's Sewer Use Ordinance (Ordinance Number 19-92, amended 116-97), as supplemented by Department of Public Works Order No. 158170, requiring a permit from the Wastewater Enterprise Collection System Division of the San Francisco Public Utilities Commission. A permit may be issued only if an effective pretreatment system is maintained and operated. Each permit for such discharge shall contain specified water quality standards and may require the project sponsor to install and maintain meters to measure the volume of the discharge to the combined sewer system. Effects from lowering the water table due to dewatering, if any, would be temporary and would not be expected to substantially deplete groundwater resources. As a result, the proposed project would not deplete groundwater supplies or substantially interfere with groundwater recharge.

The project site is not in a designated flood zone, so the proposed project would not place housing within a 100-year flood hazard area, would not impede or redirect flood flows in a 100-year flood hazard area, and would not expose people or structures to a significant risk of loss, injury, or death involving flooding, including flooding as a result of the failure of a levee or dam. As shown on Map 5, Tsunami Hazard Zones, San Francisco, 2012, in the Community Safety Element of the *General Plan*, the project site is not

within a tsunami hazard zone.³⁰ As a result, the proposed project would not expose people or structures to a significant risk of loss, injury, or death involving inundation by seiche or tsunami.

For these reasons, the proposed project would not result in significant impacts on hydrology and water quality that were not identified in the Eastern Neighborhoods FEIR, and no mitigation measures are necessary.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
15. HAZARDS AND HAZARDOUS MATERIALS—Would the project:						
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

30 San Francisco Planning Department, San Francisco General Plan, Community Safety Element, p. 15. Available online at http://www.sf-planning.org/ftp/General_Plan/Community_Safety_Element_2012.pdf

The Eastern Neighborhoods FEIR noted that implementation of any of the proposed project's rezoning options would encourage construction of new development within the project area. The FEIR found that there is a high potential to encounter hazardous materials during construction activities in many parts of the project area because of the presence of 1906 earthquake fill, previous and current land uses associated with the use of hazardous materials, and known or suspected hazardous materials cleanup cases. However, the FEIR found that existing regulations for facility closure, Under Storage Tank (UST) closure, and investigation and cleanup of soil and groundwater would ensure implementation of measures to protect workers and the community from exposure to hazardous materials during construction.

Hazardous Building Materials

The Eastern Neighborhoods FEIR determined that future development in the Plan Area may involve demolition or renovation of existing structures containing hazardous building materials. Some building materials commonly used in older buildings could present a public health risk if disturbed during an accident or during demolition or renovation of an existing building. Hazardous building materials addressed in the FIER include asbestos, electrical equipment such as transformers and fluorescent light ballasts that contain PCBs or di (2 ethylhexyl) phthalate (DEHP), fluorescent lights containing mercury vapors, and lead-based paints. Asbestos and lead based paint may also present a health risk to existing building occupants if they are in a deteriorated condition. If removed during demolition of a building, these materials would also require special disposal procedures. The Eastern Neighborhoods FEIR identified a significant impact associated with hazardous building materials including PCBs, DEHP, and mercury and determined that that Mitigation Measure L-1: Hazardous Building Materials, as outlined below, would reduce effects to a less-than-significant level. Because the proposed development includes demolition of an existing building, Mitigation Measure L-1 would apply to the proposed project.

Project Mitigation Measure M-HZ-1 – Hazardous Building Materials (Implements Mitigation Measure L-1 of the Eastern Neighborhoods FEIR)

The City shall condition future development approvals to require that the subsequent project sponsors ensure that any equipment containing PCBs or DEHP, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.

Soil and Groundwater Contamination

The project site was formerly used for a variety of light industrial uses, including garage, warehouse, and transportation-service businesses, which may have used, generated, stored, or disposed of hazardous materials. The proposed project would demolish an existing two-story commercial office building and a surface parking lot and construct a project consisting of two residential buildings containing a total of 115 dwelling units above a ground-floor retail/commercial space and a below-grade parking garage. The project would introduce a new sensitive use to the site and would extensive excavation and soil disturbance. Therefore, the project is subject to Article 22A of the Health Code, also known as the Maher Ordinance, which is administered and overseen by the Department of Public Health (DPH). The Maher Ordinance requires the project sponsor to retain the services of a qualified professional to prepare a Phase I Environmental Site Assessment (ESA) that meets the requirements of Health Code Section 22.A.6.

The Phase I ESA would determine the potential for site contamination and level of exposure risk associated with the project. Based on that information, the project sponsor may be required to conduct

soil and/or groundwater sampling and analysis. Where such analysis reveals the presence of hazardous substances in excess of state or federal standards, the project sponsor is required to submit a site mitigation plan (SMP) to the DPH or other appropriate state or federal agency(ies), and to remediate any site contamination in accordance with an approved SMP prior to the issuance of any building permit.

Although the Maher Ordinance became effective after the initiation of this environmental review (in August 2013), the project sponsor has effectively complied with all of the requirements associated with the Maher Program. Specifically, the project sponsor has submitted the Phase I, Phase II and Geotechnical report documents to the DPH in mid-2013 and entered into the DPH's Voluntary Remedial Action Program (VRAP), which was the precursor to the Maher Program prior to its adoption. The main findings of the Phase I and Phase II reports are summarized below, along with DPH staff response.

Cornerstone Earth Group prepared the Phase I ESA for the proposed project.³¹ The Phase I ESA documented a number of potential concerns associated with the proposed project: (1) the history of light industrial uses; (2) the use of hazardous materials on the site in association with automotive uses and the encounter with such contaminants during the project construction phase; (3) the former presence of four underground storage tanks (USTs) beneath the site (two containing diesel, one containing waste oil and one containing lubricating oil, all of which were removed in 1996) and the subsequent identification of petroleum impacted soil and groundwater in the vicinity of the former USTs (DPH issued a case closure letter for the site in May 2010 and stated that "no further action related to the petroleum releases at the site is required"; (4) the listing of several nearby properties on the Envirostor regulatory agency database as being under DTSC oversight due to elevated levels of lead, petroleum hydrocarbons and/or polycyclic aromatic hydrocarbons (PAHs) in the fill material beneath those structures; (5) impacts of petroleum hydrocarbons on the ground water beneath the site; and (6) potential presence of asbestos containing building materials (ACBMS) and/or lead-based paint within the existing structure on the site. In response to these concerns, Cornerstone Earth Group prepared a Preliminary Soil and Ground Water Quality Evaluation for the site, which documented soil and groundwater sampling.³²

The Preliminary Soil and Ground Water Quality Evaluation determined that the soil and groundwater at the project site exceed applicable regulatory standards for petroleum hydrocarbons, PAHs, and/or lead in soil samples taken at the site that would require remediation and/or further testing in accordance with Maher Ordinance requirements.

DPH staff reviewed the Phase I ESA, the Preliminary Soil and Ground Water Quality Evaluation, and the Geotechnical Report³³ for the proposed project and provided a memorandum to the project sponsor outlining additional requirements for complying with the Maher Ordinance.³⁴ Among them were requirements for the project sponsor to (1) submit a work plan for a supplemental subsurface investigation; (2) submit the Site Assessment and Mitigation (SAM) to DPH at least six weeks prior to

³¹ Cornerstone Earth Group, *Phase I Environmental Site Assessment, 923 and 931 Folsom Street and 218 Shipley Street, San Francisco, California*, October 30, 2012. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

³² Cornerstone Earth Group, *Preliminary Soil and Ground Water Quality Evaluation, 923 and 931 Folsom Street and 218 Shipley Street, San Francisco, California*, November 21, 2012. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

³³ Cornerstone Earth Group, *Preliminary Geotechnical Investigation, 923 Folsom Street, San Francisco, California*, November 12, 2012. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

³⁴ Scott Nakamura, SFDPH, Letter to Mr. Tim Leonoudakis re: Request for Additional Subsurface Investigation Work Plan and Site Mitigation Plan, 923-931 Folsom Street, July 18, 2013. This document is on file and available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case File No. 2012.1333E.

construction activities and (3) submit the Health and Safety Plan and Dust Control Plan with the SMP or at least two weeks prior to beginning earthwork activities.

Because the project sponsor would be required to remediate potential soil contamination described above in accordance with Article 22A of the Health Code (and is undergoing these procedures), the proposed project would not result in any significant impacts related to hazardous materials that were not identified in the Eastern Neighborhoods FEIR.

The project site is not located within an area covered by an airport land use plan, within two miles of a public airport or a public use airport, or in the vicinity of a private airstrip. Therefore, the proposed project would not result in a safety hazard for people residing or working in the project area.

In San Francisco, fire safety is ensured through the provisions of the Building Code and the San Francisco Fire Code. During the review of the building permit application, DBI and the San Francisco Fire Department will review the project plans for compliance with all regulations related to fire safety. Compliance with fire safety regulations would ensure that the proposed project would not impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan or expose people or structures to a significant risk of loss, injury, or death involving fires.

For these reasons, the proposed project would not result in significant impacts related to hazards or hazardous materials that were not identified in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
16. MINERAL AND ENERGY RESOURCES—Would the project:						
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Encourage activities which result in the use of large amounts of fuel, water, or energy, or use these in a wasteful manner?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR determined that the plan would facilitate the construction of both new residential units and commercial buildings. Development of these uses would not result in use of large amounts of fuel, water, or energy in the context of energy use throughout the City and region. The energy demand for individual buildings would be typical for such projects and would meet, or exceed, current state and local codes and standards concerning energy consumption, including Title 24 of the California Code of Regulations enforced by DBI. The Plan Area does not include any natural resources routinely extracted and the rezoning does not result in any natural resource extraction programs. Therefore, the

Eastern Neighborhoods FEIR concluded that implementation of the Area Plan would not result in a significant impact on mineral and energy resources. No mitigation measures were identified in the FEIR.

The proposed project would be required to comply with the standards of Title 24 and the requirements of the San Francisco Green Building Ordinance. The project site is not designated as an area of significant mineral deposits or as a locally important mineral resource recovery site. The proposed project would not result in the loss of mineral resources that are of value to the region or the residents of the state, would not result in the loss of availability of a locally important mineral resource recovery site, and would not encourage activities that result in the use of large amounts of fuel, water, or energy, or use them in a wasteful manner.

For these reasons, the proposed project would not result in significant impacts on mineral and energy resources that were not identified in the Eastern Neighborhoods FEIR, and no mitigation measures are necessary.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
17. AGRICULTURE AND FOREST RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board.— Would the project:						
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code Section 12220(g)) or timberland (as defined by Public Resources Code Section 4526)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR determined that no agricultural resources exist in the Area Plan; therefore the rezoning and community plans would have no effect on agricultural resources. No mitigation measures were identified in the FEIR. The Eastern Neighborhoods FEIR did not analyze the effects on forest resources.

The proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on agriculture and forest resources beyond those analyzed in the Eastern Neighborhoods FEIR.

<i>Topics:</i>	<i>Project-Specific Significant Impact Not Identified in PEIR</i>	<i>Significant Unavoidable Impact Identified in PEIR</i>	<i>Mitigation Identified in PEIR</i>	<i>PEIR Mitigation Applies to Project</i>	<i>PEIR Mitigation Does Not Apply to Project</i>	<i>No Significant Impact (Project or PEIR)</i>
18. MANDATORY FINDINGS OF SIGNIFICANCE—Would the project:						
a) Have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Have impacts that would be individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Have environmental effects that would cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods FEIR identified significant impacts related to land use, transportation, cultural resources, shadow, noise, air quality, and hazardous materials. Mitigation measures reduced all impacts to less than significant, with the exception of those related to land use (cumulative impacts on PDR use), transportation (traffic impacts at nine intersections, and transit impacts), cultural (demolition of historical resources), and shadow (impacts on parks).

The proposed project would involve: 1) demolition of an existing two-story commercial office building and surface parking lot; and 2) construction of a project consisting of two residential buildings (one approximately 85 feet tall and nine stories and the other approximately 44 feet tall and four stories),

containing a total of 115 dwelling units above a ground-floor retail/commercial space and below-grade parking garage containing 87 spaces. As discussed in this document, the proposed project would not result in new, significant environmental effects, or effects of greater severity than were already analyzed and disclosed in the Eastern Neighborhoods FEIR.

MITIGATION MEASURES

Project Requirement M-CP-1 – Archeological Testing (Implements Mitigation Measure J-2 of the Eastern Neighborhoods FEIR)

Based on a reasonable presumption that archeological resources may be present within the project site, the following measures shall be undertaken to avoid any potentially significant adverse effect from the proposed project on buried or submerged historical resources. The project sponsor shall retain the services of an archaeological consultant from the rotational Department Qualified Archaeological Consultants List (QACL) maintained by the Planning Department archaeologist. The project sponsor shall contact the Department archaeologist to obtain the names and contact information for the next three archeological consultants on the QACL. The archeological consultant shall undertake an archeological testing program as specified herein. In addition, the consultant shall be available to conduct an archeological monitoring and/or data recovery program if required pursuant to this requirement. The archeological consultant's work shall be conducted in accordance with this requirement at the direction of the Environmental Review Officer (ERO). All plans and reports prepared by the consultant as specified herein shall be submitted first and directly to the ERO for review and comment, and shall be considered draft reports subject to revision until final approval by the ERO. Archeological monitoring and/or data recovery programs required by this requirement could suspend construction of the project for up to a maximum of four weeks. At the direction of the ERO, the suspension of construction can be extended beyond four weeks only if such a suspension is the only feasible means to reduce to a less than significant level potential effects on a significant archeological resource as defined in CEQA Guidelines Sect. 15064.5 (a)(c).

Consultation with Descendant Communities: On discovery of an archeological site³⁵ associated with descendant Native Americans or the Overseas Chinese an appropriate representative³⁶ of the descendant group and the ERO shall be contacted. The representative of the descendant group shall be given the opportunity to monitor archeological field investigations of the site and to consult with ERO regarding appropriate archeological treatment of the site, of recovered data from the site, and, if applicable, any interpretative treatment of the associated archeological site. A copy of the Final Archaeological Resources Report shall be provided to the representative of the descendant group.

35 By the term "archeological site" is intended here to minimally included any archeological deposit, feature, burial, or evidence of burial.

36 An "appropriate representative" of the descendant group is here defined to mean, in the case of Native Americans, any individual listed in the current Native American Contact List for the City and County of San Francisco maintained by the California Native American Heritage Commission and in the case of the Overseas Chinese, the Chinese Historical Society of America.

Archeological Testing Program. The archeological consultant shall prepare and submit to the ERO for review and approval an archeological testing plan (ATP). The archeological testing program shall be conducted in accordance with the approved ATP. The ATP shall identify the property types of the expected archeological resource(s) that potentially could be adversely affected by the proposed project, the testing method to be used, and the locations recommended for testing. The purpose of the archeological testing program will be to determine to the extent possible the presence or absence of archeological resources and to identify and to evaluate whether any archeological resource encountered on the site constitutes an historical resource under CEQA.

At the completion of the archeological testing program, the archeological consultant shall submit a written report of the findings to the ERO. If based on the archeological testing program the archeological consultant finds that significant archeological resources may be present, the ERO in consultation with the archeological consultant shall determine if additional measures are warranted. Additional measures that may be undertaken include additional archeological testing, archeological monitoring, and/or an archeological data recovery program. If the ERO determines that a significant archeological resource is present and that the resource could be adversely affected by the proposed project, at the discretion of the project sponsor either:

- C) The proposed project shall be re-designed so as to avoid any adverse effect on the significant archeological resource; or
- D) A data recovery program shall be implemented, unless the ERO determines that the archeological resource is of greater interpretive than research significance and that interpretive use of the resource is feasible.

Archeological Monitoring Program. If the ERO in consultation with the archeological consultant determines that an archeological monitoring program shall be implemented the archeological monitoring program shall minimally include the following provisions:

- The archeological consultant, project sponsor, and ERO shall meet and consult on the scope of the AMP reasonably prior to any project-related soils disturbing activities commencing. The ERO in consultation with the archeological consultant shall determine what project activities shall be archeologically monitored. In most cases, any soils- disturbing activities, such as demolition, foundation removal, excavation, grading, utilities installation, foundation work, driving of piles (foundation, shoring, etc.), site remediation, etc., shall require archeological monitoring because of the risk these activities pose to potential archaeological resources and to their depositional context;
- The archeological consultant shall advise all project contractors to be on the alert for evidence of the presence of the expected resource(s), of how to identify the evidence of the expected resource(s), and of the appropriate protocol in the event of apparent discovery of an archeological resource;

- The archeological monitor(s) shall be present on the project site according to a schedule agreed upon by the archeological consultant and the ERO until the ERO has, in consultation with project archeological consultant, determined that project construction activities could have no effects on significant archeological deposits;
- The archeological monitor shall record and be authorized to collect soil samples and artifactual/ecofactual material as warranted for analysis;
- If an intact archeological deposit is encountered, all soils-disturbing activities in the vicinity of the deposit shall cease. The archeological monitor shall be empowered to temporarily redirect demolition/excavation/pile driving/construction activities and equipment until the deposit is evaluated. If in the case of pile driving activity (foundation, shoring, etc.), the archeological monitor has cause to believe that the pile driving activity may affect an archeological resource, the pile driving activity shall be terminated until an appropriate evaluation of the resource has been made in consultation with the ERO. The archeological consultant shall immediately notify the ERO of the encountered archeological deposit. The archeological consultant shall make a reasonable effort to assess the identity, integrity, and significance of the encountered archeological deposit, and present the findings of this assessment to the ERO.

Whether or not significant archeological resources are encountered, the archeological consultant shall submit a written report of the findings of the monitoring program to the ERO.

Archeological Data Recovery Program. The archeological data recovery program shall be conducted in accord with an archeological data recovery plan (ADRP). The archeological consultant, project sponsor, and ERO shall meet and consult on the scope of the ADRP prior to preparation of a draft ADRP. The archeological consultant shall submit a draft ADRP to the ERO. The ADRP shall identify how the proposed data recovery program will preserve the significant information the archeological resource is expected to contain. That is, the ADRP will identify what scientific/historical research questions are applicable to the expected resource, what data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. Data recovery, in general, should be limited to the portions of the historical property that could be adversely affected by the proposed project. Destructive data recovery methods shall not be applied to portions of the archeological resources if nondestructive methods are practical.

The scope of the ADRP shall include the following elements:

- *Field Methods and Procedures.* Descriptions of proposed field strategies, procedures, and operations.
- *Cataloguing and Laboratory Analysis.* Description of selected cataloguing system and artifact analysis procedures.
- *Discard and Deaccession Policy.* Description of and rationale for field and post-field discard and deaccession policies.

- *Interpretive Program.* Consideration of an on-site/off-site public interpretive program during the course of the archeological data recovery program.
- *Security Measures.* Recommended security measures to protect the archeological resource from vandalism, looting, and non-intentionally damaging activities.
- *Final Report.* Description of proposed report format and distribution of results.
- *Curation.* Description of the procedures and recommendations for the curation of any recovered data having potential research value, identification of appropriate curation facilities, and a summary of the accession policies of the curation facilities.

Human Remains and Associated or Unassociated Funerary Objects. The treatment of human remains and of associated or unassociated funerary objects discovered during any soils disturbing activity shall comply with applicable State and Federal laws. This shall include immediate notification of the Coroner of the City and County of San Francisco and in the event of the Coroner's determination that the human remains are Native American remains, notification of the California State Native American Heritage Commission (NAHC) who shall appoint a Most Likely Descendant (MLD) (Pub. Res. Code Sec. 5097.98). The archeological consultant, project sponsor, and MLD shall make all reasonable efforts to develop an agreement for the treatment of, with appropriate dignity, human remains and associated or unassociated funerary objects (CEQA Guidelines. Sec. 15064.5(d)). The agreement should take into consideration the appropriate excavation, removal, recordation, analysis, custodianship, curation, and final disposition of the human remains and associated or unassociated funerary objects.

Final Archeological Resources Report. The archeological consultant shall submit a Draft Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describes the archeological and historical research methods employed in the archeological testing/monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.

Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning division of the Planning Department shall receive one bound, one unbound and one unlocked, searchable PDF copy on CD of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest in or the high interpretive value of the resource, the ERO may require a different final report content, format, and distribution than that presented above.

Project Mitigation Measure M-AQ-1: Construction Emissions Minimization (Implements a portion of Mitigation Measure G-1 of the Eastern Neighborhoods FEIR)

- A. *Construction Emissions Minimization Plan.* Prior to issuance of a construction permit, the project sponsor shall submit a Construction Emissions Minimization Plan (Plan) to the Environmental Review Officer (ERO) for review and approval by an Environmental Planning

Air Quality Specialist. The Plan shall detail project compliance with the following requirements:

1. All off-road equipment greater than 25 hp and operating for more than 20 total hours over the entire duration of construction activities shall meet the following requirements:
 - a) Where access to alternative sources of power are available, portable diesel engines shall be prohibited;
 - b) All off-road equipment shall have:
 - i. Engines that meet or exceed either USEPA or ARB Tier 2 off-road emission standards, *and*
 - ii. Engines that are retrofitted with an ARB Level 3 Verified Diesel Emissions Control Strategy (VDECS).³⁷
 - c) Exceptions:
 - i. Exceptions to A(1)(a) *may* be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that an alternative source of power is limited or infeasible at the project site and that the requirements of this exception provision apply. Under this circumstance, the sponsor shall submit documentation of compliance with A(1)(b) for onsite power generation.
 - ii. Exceptions to A(1)(b)(ii) *may* be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that a particular piece of off-road equipment with an ARB Level 3 VDECS is: (1) technically not feasible, (2) would not produce desired emissions reductions due to expected operating modes, (3) installing the control device would create a safety hazard or impaired visibility for the operator, or (4) there is a compelling emergency need to use off-road equipment that are not retrofitted with an ARB Level 3 VDECS and the sponsor has submitted documentation to the ERO that the requirements of this exception provision apply. If granted an exception to A(1)(b)(ii), the project sponsor must comply with the requirements of A(1)(c)(iii).
 - iii. If an exception is granted pursuant to A(1)(c)(ii), the project sponsor shall provide the next cleanest piece of off-road equipment as provided by the step down schedules in Table A1 below.

³⁷ Equipment with engines meeting Tier 4 Interim or Tier 4 Final emission standards automatically meet this requirement, therefore a VDECS would not be required.

**TABLE A1
OFF-ROAD EQUIPMENT COMPLIANCE STEP DOWN SCHEDULE***

Compliance Alternative	Engine Emission Standard	Emissions Control
1	Tier 2	ARB Level 2 VDECS
2	Tier 2	ARB Level 1 VDECS
3	Tier 2	Alternative Fuel*

*How to use the table. If the requirements of (A)(1)(b) cannot be met, then the project sponsor would need to meet Compliance Alternative 1. Should the project sponsor not be able to supply off-road equipment meeting Compliance Alternative 1, then Compliance Alternative 2 would need to be met. Should the project sponsor not be able to supply off-road equipment meeting Compliance Alternative 2, then Compliance Alternative 3 would need to be met.

**Alternative fuels are not a VDECS

2. The project sponsor shall require the idling time for off-road and on-road equipment be limited to no more than *two* minutes, except as provided in exceptions to the applicable state regulations regarding idling for off-road and on-road equipment. Legible and visible signs shall be posted in multiple languages (English, Spanish, Chinese) in designated queuing areas and at the construction site to remind operators of the two minute idling limit.
 3. The project sponsor shall require that construction operators properly maintain and tune equipment in accordance with manufacturer specifications.
 4. The Plan shall include estimates of the construction timeline by phase with a description of each piece of off-road equipment required for every construction phase. Off-road equipment descriptions and information may include, but is not limited to: equipment type, equipment manufacturer, equipment identification number, engine model year, engine certification (Tier rating), horsepower, engine serial number, and expected fuel usage and hours of operation. For VDECS installed: technology type, serial number, make, model, manufacturer, ARB verification number level, and installation date and hour meter reading on installation date. For off-road equipment using alternative fuels, reporting shall indicate the type of alternative fuel being used.
 5. The Plan shall be kept on-site and available for review by any persons requesting it and a legible sign shall be posted at the perimeter of the construction site indicating to the public the basic requirements of the Plan and a way to request a copy of the Plan. The project sponsor shall provide copies of Plan to members of the public as requested.
- B. *Reporting.* Quarterly reports shall be submitted to the ERO indicating the construction phase and off-road equipment information used during each phase including the information required in A(4). In addition, for off-road equipment using alternative fuels, reporting shall include the actual amount of alternative fuel used.
1. Within six months of the completion of construction activities, the project sponsor shall submit to the ERO a final report summarizing construction activities. The final report shall indicate the start and end dates and duration of each construction phase. For each phase, the

report shall include detailed information required in A(4). In addition, for off-road equipment using alternative fuels, reporting shall include the actual amount of alternative fuel used.

- C. *Certification Statement and On-site Requirements.* Prior to the commencement of construction activities, the project sponsor must certify (1) compliance with the Plan, and (2) all applicable requirements of the Plan have been incorporated into contract specifications.

Project Mitigation Measure M-NO-1: Siting of Noise-Sensitive Uses (Implements Mitigation Measure F-4 of the Eastern Neighborhoods FEIR)

To reduce potential conflicts between existing noise-generating uses and new sensitive receptors, for new development including noise-sensitive uses, the Planning Department shall require the preparation of an analysis that includes, at a minimum, a site survey to identify potential noise-generating uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis shall be prepared by persons qualified in acoustical analysis and/or engineering and shall demonstrate with reasonable certainty that Title 24 standards, where applicable, can be met, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels in the vicinity. Should such concerns be present, the Department may require the completion of a detailed noise assessment by person(s) qualified in acoustical analysis and/or engineering prior to the first project approval action, in order to demonstrate that acceptable interior noise levels consistent with those in the Title 24 standards can be attained.

Project Mitigation Measure M-NO-2: Open Space in Noisy Environments (Implements Mitigation Measure F-6 of the Eastern Neighborhoods FEIR)

To minimize effects on development in noisy areas, for new development including noise sensitive uses, the Planning Department shall, through its building permit review process, in conjunction with noise analysis required pursuant to Mitigation Measure F-4, require that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design.

Project Mitigation Measure M-AQ-2 – Air Filtration (Implements Mitigation Measure G-2 of the Eastern Neighborhoods FEIR)

Air Filtration and Ventilation Requirements for Sensitive Land Uses. Prior to receipt of any building permit, the project sponsor shall submit a ventilation plan for the proposed building(s). The ventilation plan shall show that the building ventilation system removes at least 80 percent of the outdoor PM_{2.5} concentrations from habitable areas and be designed by an engineer certified by ASHRAE, who shall provide a written report documenting that the system meets the 80 percent

performance standard identified in this measure and offers the best available technology to minimize outdoor to indoor transmission of air pollution.

Maintenance Plan. Prior to receipt of any building permit, the project sponsor shall present a plan that ensures ongoing maintenance for the ventilation and filtration systems.

Disclosure to buyers and renters. The project sponsor shall also ensure the disclosure to buyers (and renters) that the building is located in an area with existing sources of air pollution and as such, the building includes an air filtration and ventilation system designed to remove 80 percent of outdoor particulate matter and shall inform occupants of the proper use of the installed air filtration system.

Project Mitigation Measure M-HZ-1 – Hazardous Building Materials (Implements Mitigation Measure L-1 of the Eastern Neighborhoods FEIR)

The City shall condition future development approvals to require that the subsequent project sponsors ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.

IMPROVEMENT MEASURES

Project Improvement Measure I-TR-1: Monitoring and Abatement of Queues

As an improvement measure to reduce the potential for queuing of vehicles accessing the project site, it shall be the responsibility of the property owner to ensure that recurring vehicle queues do not occur within the public right-of-way. A vehicle queue is defined as one or more vehicles (destined to the underground parking garage) blocking any portion of the Shipley Street sidewalk or travel lane for a consecutive period of three minutes or longer on a daily or weekly basis.

If a recurring queue occurs, the owner/operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable). Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as additional bicycle parking, customer shuttles, delivery services; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.

If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Planning Department shall notify the Project Sponsor in writing. Upon request, the

owner/operator shall hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant shall prepare a monitoring report to be submitted to the Planning Department for review. If the Planning Department determines that a recurring queue does exist, the facility owner/operator shall have 90 days from the date or the written determination to abate the queue.

Project Improvement Measure I-TR-2: Installation of Visual/Audible Devices at Underground Garage Driveway

As an improvement measure to reduce potential conflicts between vehicles entering and exiting the underground garage and pedestrians traveling along the north side sidewalk of Shipley Street, the Project Sponsor shall install visual and/or audible notifications (alarms) to alert pedestrians of vehicles traveling in and out of the underground parking garage.

Project Improvement Measure I-TR-3: Coordination of Move-in/Move-Out Operations and Large Deliveries

To reduce the potential for parking of delivery vehicles within the travel lane adjacent to the curb lane on Shipley Streets (in the event that the on-street loading is occupied), residential move-in and move-out activities and larger deliveries shall be scheduled and coordinated through building management. Appropriate move-in/move-out procedures shall be enforced to avoid any blockages of Shipley Street over an extended period of time and reduce any potential conflicts between movers and pedestrians walking along Shipley Street. Curb parking on Shipley Street shall be reserved through SFMTA.

Project Improvement Measure I-TR-4: Construction Truck Deliveries During Off-Peak Periods

Any construction traffic occurring between 7:00 a.m. and 9:00 a.m. or between 3:30 p.m. and 6:00 p.m. would coincide with peak hour traffic and could temporarily impede traffic and transit flow, although it would not be considered a significant impact. Limiting truck movements to the hours between 9:00 a.m. and 3:30 p.m. (or other times, if approved by SFMTA) would further minimize disruption of the general traffic flow on adjacent streets during the a.m. and p.m. peak periods.

As required, the Project Sponsor and construction contractor(s) shall meet with the Sustainable Streets Division of the SFMTA, the Fire Department, Muni, and the Planning Department to determine feasible measures to reduce traffic congestion, including potential transit disruption, and pedestrian circulation impacts during construction of the project. To minimize cumulative traffic impacts due to project construction, the Project Sponsor shall coordinate with construction contractors for any concurrent nearby projects (e.g., along Fifth Street, between Howard and Folsom Streets) that are planned for construction or which later become known.

Project Improvement Measure I-TR-5: Construction Management Plan

In addition to items required in the Construction Management Plan, the project sponsor shall include the following:

- Carpool and Transit Access for Construction Workers – As an improvement measure to minimize parking demand and vehicle trips associated with construction workers, the construction contractor shall include methods to encourage carpooling and transit

use to the project site by construction workers in the Construction Management Plan/contracts.

- Project Construction Updates – As an improvement measure to minimize construction impacts on nearby businesses, the project sponsor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction inquiries or concerns.

Project Improvement Measure I-TR-6: Implement Travel Demand Management (TDM) Measures

Prior to issuance of a temporary permit of building occupancy, the Project Sponsor must execute an agreement with the Planning Department for the provision of TDM services. Recommended components of the TDM program include the following:

- Provide information in the move-in packets and lobby (or electronic) bulletin boards for transit service (Muni and BART lines, schedules and fares), particularly for local trips (such as to the nearest grocery store, hardware store, shopping center, restaurants, and other nearby neighborhood commercial areas), information on where transit passes could be purchased in person and on-line, and information on the Clipper Card and 511 Regional Rideshare Program;
- Include one or more Muni FastPass (loaded onto a Clipper card) as part of the monthly rent, or homeowner association fee;
- Provide TDM training for property managers and coordinators; and have at least one contact person, preferably in the building for tenants with alternative mode travel questions.
- Promote and coordinate ridesharing activities (i.e. establish a “ride board”) for all building residents and employees, particularly to popular local events;
- Facilitate access to carshare spaces provided in the parking garage through on-site signage and information on the carshare company, rates, and how to enroll in the carshare program;
- Ensure that the points of access to bicycle parking through elevators on the ground floor and the garage ramp include signage indicating the location of these facilities.
- Ensure that bicycle safety strategies are developed along the sides of the property, avoiding conflicts with private cars, transit vehicles and loading vehicles, posting signs where necessary to increase awareness of the presence of bicycle traffic;
- Facilitate access to the Folsom Street, Howard Street, and Fifth Street bicycle routes via on-site signage;
- Actively encourage alternative mode choice by actively monitoring above efforts effectiveness, and fostering local deliveries from nearby businesses where appropriate; and
- Participate with other project sponsors in a network of transportation brokerage services.

CONCLUSION:

The *Eastern Neighborhoods FEIR* incorporated and adequately addressed all potential impacts of the proposed project at 923 Folsom Street. As described above, the 923 Folsom Street project would not have any additional or significant adverse effects that were not examined in the *Eastern Neighborhoods FEIR*,

nor has any new or additional information come to light that would alter the conclusions of the *Eastern Neighborhoods FEIR*. Thus, the proposed project at 923 Folsom Street would not have any new significant effects on the environment that were not previously identified in the *Eastern Neighborhoods FEIR*, nor would any environmental impacts be substantially greater than described in the FEIR. No mitigation measures previously found infeasible have been determined to be feasible, nor have any new mitigation measures or alternatives been identified but rejected by the project sponsor. Therefore, in addition to being exempt from environmental review under Section 15183 of the CEQA Guidelines, the proposed project is also exempt under Section 21083.3 of the California Public Resources Code.

DETERMINATION:

On the basis of this review, it can be determined that:

- The proposed project qualifies for consideration of a Community Plan Exemption based on the applicable General Plan and zoning requirements; **AND**
- All potentially significant individual or cumulative impacts of the proposed project were identified in the applicable programmatic EIR (PEIR) for the Plan Area, and all applicable mitigation measures have been or incorporated into the proposed project or will be required in approval of the project.
- The proposed project may have a potentially significant impact not identified in the PEIR for the topic area(s) identified above, but that this impact can be reduced to a less-than-significant level in this case because revisions in the project have been made by or agreed to by the project proponent. A focused Initial Study and MITIGATED NEGATIVE DECLARATION is required, analyzing the effects that remain to be addressed.
- The proposed project may have a potentially significant impact not identified in the PEIR for the topic area(s) identified above. An ENVIRONMENTAL IMPACT REPORT is required, analyzing the effects that remain to be addressed.



Sarah B. Jones

Environmental Review Officer

for

John Rahaim

Director of Planning

DATE June 20, 2014

923 FOLSOM STREET - MITIGATION MONITORING AND REPORTING PROGRAM
(Also includes text for Improvement Measures)

MONITORING AND REPORTING PROGRAM

Adopted Mitigation Measures

Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
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MITIGATION MEASURES AGREED TO BY PROJECT SPONSOR

ARCHEOLOGICAL RESOURCES

<p>Mitigation Measure M-CP-1 – Archeological Testing (Implements Mitigation Measure J-2 of the Eastern Neighborhoods FEIR). Based on a reasonable presumption that archeological resources may be present within the project site, the following measures shall be undertaken to avoid any potentially significant adverse effect from the proposed project on buried or submerged historical resources. The project sponsor shall retain the services of an archaeological consultant from the rotational Department Qualified Archaeological Consultants List (QAACL) maintained by the Planning Department archaeologist. The project sponsor shall contact the Department archeologist to obtain the names and contact information for the next three archeological consultants on the QAACL. The archeological consultant shall undertake an archeological testing program as specified herein. In addition, the consultant shall be available to conduct an archeological monitoring and/or data recovery program if required pursuant to this requirement. The archeological consultant's work shall be conducted in accordance with this requirement at the direction of the Environmental Review Officer (ERO). All plans and reports prepared by the consultant as specified herein shall be submitted first and directly to the ERO for review and comment, and shall be considered draft reports subject to revision until final approval by the ERO. Archeological monitoring and/or data recovery programs required by this requirement could suspend construction of the project for up to a maximum of four weeks. At the direction of the ERO, the suspension of construction can be extended beyond four weeks only if such a suspension is the only feasible means to reduce to a less than significant level potential effects on a significant archeological resource as defined in CEQA Guidelines Sect. 15064.5 (a)(c).</p>	Project sponsor.	Prior to issuance of grading or building permits.	Project sponsor to retain archeological consultant to undertake archaeological monitoring program in consultation with ERO.	Project sponsor, archeologist, and ERO.	Complete when project sponsor retains a qualified archeological consultant.
<p><i>Consultation with Descendant Communities:</i> On discovery of an archeological site¹ associated with descendant Native Americans or the Overseas Chinese an appropriate representative² of the descendant group and the ERO shall be contacted. The representative of the</p>	Project sponsor/archeological consultant	In the event of discovery of an archeological	Contact any individual listed in the current	Archeological consultant and ERO.	Considered complete upon

1 By the term "archeological site" is intended here to minimally included any archeological deposit, feature, burial, or evidence of burial.

2 An "appropriate representative" of the descendant group is here defined to mean, in the case of Native Americans, any individual listed in the current Native American Contact List for the City and County of San Francisco maintained by the California Native American Heritage Commission and in the case of the Overseas Chinese, the Chinese Historical Society of America.

MONITORING AND REPORTING PROGRAM

Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
<p>descendant group shall be given the opportunity to monitor archeological field investigations of the site and to consult with ERO regarding appropriate archeological treatment of the site, of recovered data from the site, and, if applicable, any interpretative treatment of the associated archeological site. A copy of the Final Archaeological Resources Report shall be provided to the representative of the descendant group.</p>	<p>in consultation with any individual listed in the current Native American Contact List and Chinese Historical Society of America.</p>	<p>site associated with descendant Native Americans or Overseas Chinese.</p>	<p>Native American Contact List and Chinese Historical Society of America and implement any further mitigation advised.</p>	<p>notification of appropriate organization and implementation of any further mitigation as advised.</p>	<p>After consultation with and approval by ERO of ATP. Considered complete on submittal to ERO of report on ATP findings.</p>
<p><i>Archeological Testing Program.</i> The archeological consultant shall prepare and submit to the ERO for review and approval an archeological testing plan (ATP). The archeological testing program shall be conducted in accordance with the approved ATP. The ATP shall identify the property types of the expected archeological resource(s) that potentially could be adversely affected by the proposed project, the testing method to be used, and the locations recommended for testing. The purpose of the archeological testing program will be to determine to the extent possible the presence or absence of archeological resources and to identify and to evaluate whether any archeological resource encountered on the site constitutes an historical resource under CEQA.</p>	<p>Project sponsor/ archeological consultant at the direction of the ERO.</p>	<p>Prior to soil-disturbing activities on the project site.</p>	<p>Prepare and submit draft ATP. Implement ATP.</p>	<p>Archeological consultant and ERO.</p>	<p>Considered complete on submittal to ERO of report on ATP findings.</p>
<p>At the completion of the archeological testing program, the archeological consultant shall submit a written report of the findings to the ERO. If based on the archeological testing program the archeological consultant finds that significant archeological resources may be present, the ERO in consultation with the archeological consultant shall determine if additional measures are warranted. Additional measures that may be undertaken include archeological testing, archeological monitoring, and/or an archeological data recovery program. If the ERO determines that a significant archeological resource is present and that the resource could be adversely affected by the proposed project, at the discretion of the project sponsor either:</p>	<p>Project sponsor/ archeological consultant at the direction of the ERO.</p>	<p>After completion of ATP.</p>	<p>Submit report to ERO of the findings of the ATP.</p>	<p>Archeological consultant and ERO.</p>	<p>Considered complete on submittal to ERO of report on ATP findings.</p>
<p>A) The proposed project shall be re-designed so as to avoid any adverse effect on the significant archeological resource; or</p>					

MONITORING AND REPORTING PROGRAM

Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
<p>B) A data recovery program shall be implemented, unless the ERO determines that the archeological resource is of greater interpretive than research significance and that interpretive use of the resource is feasible.</p> <p><i>Archeological Monitoring Program.</i> If the ERO in consultation with the archeological consultant determines that an archeological monitoring program shall be implemented the archeological monitoring program shall minimally include the following provisions:</p> <ul style="list-style-type: none"> • The archeological consultant, project sponsor, and ERO shall meet and consult on the scope of the AMP reasonably prior to any project-related soils disturbing activities commencing. The ERO in consultation with the archeological consultant shall determine what project activities shall be archeologically monitored. In most cases, any soils-disturbing activities, such as demolition, foundation removal, excavation, grading, utilities installation, foundation work, driving of piles (foundation, shoring, etc.), site remediation, etc., shall require archeological monitoring because of the risk these activities pose to potential archaeological resources and to their depositional context; • The archeological consultant shall advise all project contractors to be on the alert for evidence of the presence of the expected resource(s), of how to identify the evidence of the expected resource(s), and of the appropriate protocol in the event of apparent discovery of an archeological resource; • The archeological monitor(s) shall be present on the project site according to a schedule agreed upon by the archeological consultant and the ERO until the ERO has, in consultation with project archeological consultant, determined that project construction activities could have no effects on significant archeological deposits; <p>The archeological monitor shall record and be authorized to collect soil samples and artifactual/ecofactual material as warranted for analysis.</p> <ul style="list-style-type: none"> • If an intact archeological deposit is encountered, all soils-disturbing activities in the vicinity of the deposit shall cease. The archeological monitor shall be empowered to temporarily redirect demolition/excavation/pile driving/construction activities and equipment until the deposit is evaluated. If in the case of pile driving activity (foundation, shoring, etc.), the archeological monitor has cause to believe that the pile driving activity may affect an archeological resource, the pile driving activity shall be terminated until an appropriate evaluation of the resource has been made in consultation with the ERO. The archeological consultant shall immediately notify the 	<p>Project sponsor/ archeological consultant/ archeological monitor / contractor(s) at the direction of the ERO.</p>	<p>ERO and archeological consultant meet prior to commencement of soil- disturbing activity. If ERO determines that an AMP is necessary, monitor throughout all soil-disturbing activities.</p>	<p>Implement AMP.</p>	<p>Archeological consultant and ERO.</p>	<p>Considered complete on findings by ERO that AMP implemented.</p>
<ul style="list-style-type: none"> • If an intact archeological deposit is encountered, all soils-disturbing activities in the vicinity of the deposit shall cease. The archeological monitor shall be empowered to temporarily redirect demolition/excavation/pile driving/construction activities and equipment until the deposit is evaluated. If in the case of pile driving activity (foundation, shoring, etc.), the archeological monitor has cause to believe that the pile driving activity may affect an archeological resource, the pile driving activity shall be terminated until an appropriate evaluation of the resource has been made in consultation with the ERO. The archeological consultant shall immediately notify the 	<p>Archeological consultant.</p>		<p>Notify ERO if intact archeological deposit is encountered.</p>		

MONITORING AND REPORTING PROGRAM

Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
<p>ERO of the encountered archeological deposit. The archeological consultant shall make a reasonable effort to assess the identity, integrity, and significance of the encountered archeological deposit, and present the findings of this assessment to the ERO.</p> <p>Whether or not significant archeological resources are encountered, the archeological consultant shall submit a written report of the findings of the monitoring program to the ERO.</p> <p><i>Archeological Data Recovery Program.</i> The archeological data recovery program shall be conducted in accord with an archeological data recovery plan (ADRP). The archeological consultant, project sponsor, and ERO shall meet and consult on the scope of the ADRP prior to preparation of a draft ADRP. The archeological consultant shall submit a draft ADRP to the ERO. The ADRP shall identify how the proposed data recovery program will preserve the significant information the archeological resource is expected to contain. That is, the ADRP will identify what scientific/historical research questions are applicable to the expected resource, what data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. Data recovery, in general, should be limited to the portions of the historical property that could be adversely affected by the proposed project. Destructive data recovery methods shall not be applied to portions of the archeological resources if nondestructive methods are practical.</p> <p>The scope of the ADRP shall include the following elements:</p> <ul style="list-style-type: none"> • <i>Field Methods and Procedures.</i> Descriptions of proposed field strategies, procedures, and operations. • <i>Cataloguing and Laboratory Analysis.</i> Description of selected cataloguing system and artifact analysis procedures. • <i>Discard and Deaccession Policy.</i> Description of and rationale for field and post-field discard and deaccession policies. • <i>Interpretive Program.</i> Consideration of an on-site/off-site public interpretive program during the course of the archeological data recovery program. • <i>Security Measures.</i> Recommended security measures to protect the archeological resource from vandalism, looting, and non-intentionally damaging activities. • <i>Final Report.</i> Description of proposed report format and distribution of results. • <i>Curation.</i> Description of the procedures and recommendations for the curation of any recovered data having potential research value, identification of appropriate curation 	<p>Archeological consultant at the direction of the ERO.</p>	<p>If there is determination by the ERO that an ADRP is required.</p>	<p>Prepare an ARDP.</p>	<p>Archeological consultant and ERO.</p>	<p>Considered complete on findings by ERO that ADRP is implemented.</p>

MONITORING AND REPORTING PROGRAM

Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
<p>facilities, and a summary of the accession policies of the curation facilities.</p> <p><i>Human Remains and Associated or Unassociated Funerary Objects.</i> The treatment of human remains and of associated or unassociated funerary objects discovered during any soils disturbing activity shall comply with applicable State and Federal laws. This shall include immediate notification of the Coroner of the City and County of San Francisco and in the event of the Coroner’s determination that the human remains are Native American remains, notification of the California State Native American Heritage Commission (NAHC) who shall appoint a Most Likely Descendant (MLD) (Pub. Res. Code Sec. 5097.98). The archeological consultant, project sponsor, and MLD shall make all reasonable efforts to develop an agreement for the treatment of, with appropriate dignity, human remains and associated or unassociated funerary objects (CEQA Guidelines. Sec. 15064.5(d)). The agreement should take into consideration the appropriate excavation, removal, recordation, analysis, custodianship, curation, and final disposition of the human remains and associated or unassociated funerary objects.</p>	Project sponsor/ archeological consultant in consultation with the San Francisco Coroner, NAHC, and MLD.	In the event human remains and/or funerary objects are encountered.	Contact San Francisco County Coroner. Implement regulatory requirements, if applicable, regarding discovery of Native American human remains and associated/ unassociated funerary objects.	Archeological consultant and ERO.	Considered complete on notification of the San Francisco County Coroner and NAHC, if necessary.
<p><i>Final Archeological Resources Report.</i> The archeological consultant shall submit a Draft Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describes the archeological and historical research methods employed in the archeological testing/monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.</p>	Project sponsor/ archeological consultant at the direction of the ERO.	After completion of archeological data recovery, inventorying, analysis, and interpretation.	Submit a draft FARR.	Archeological consultant and ERO.	Considered complete on submittal of FARR.
<p>Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning division of the Planning Department shall receive one bound, one unbound and one unlocked, searchable PDF copy on CD of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest in or the high interpretive value of the resource, the ERO may require a different final report content, format, and distribution than that presented above.</p>	Archeological consultant at the direction of the ERO.	Written certification submitted to ERO that required FARR distribution has been completed.	Distribute FARR.	Archeological consultant and ERO.	Considered complete on distribution of FARR.

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Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
NOISE					
<p>Mitigation Measure M-NO-1: Siting of Noise-Sensitive Uses (Implements Mitigation Measure F-4 of the Eastern Neighborhoods FEIR). To reduce potential conflicts between existing noise-generating uses and new sensitive receptors, for new development including noise-sensitive uses, the Planning Department shall require the preparation of an analysis that includes, at a minimum, a site survey to identify potential noise-generating uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis shall be prepared by persons qualified in acoustical analysis and/or engineering and shall demonstrate with reasonable certainty that Title 24 standards, where applicable, can be met, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels in the vicinity. Should such concerns be present, the Department may require the completion of a detailed noise assessment by person(s) qualified in acoustical analysis and/or engineering prior to the first project approval action, in order to demonstrate that acceptable interior noise levels consistent with those in the Title 24 standards can be attained.</p>	Project sponsor; project contractor(s).	During environmental review process.	Design measures to be incorporated into project design; prior to issuance of a building permit.	Planning Department; Department of Building Inspection.	Considered complete upon approval of final construction drawing set.
<p>Mitigation Measure M-NO-2: Open Space in Noisy Environments (Implements Mitigation Measure F-6 of the Eastern Neighborhoods FEIR). To minimize effects on development in noisy areas, for new development including noise sensitive uses, the Planning Department shall, through its building permit review process, in conjunction with noise analysis required pursuant to Mitigation Measure F-4, require that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design.</p>	Project sponsor; project contractor(s).	During environmental review process.	Design measures to be incorporated into project design; prior to issuance of a building permit.	Planning Department; Department of Building Inspection.	Considered complete upon approval of final construction drawing set.

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Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
AIR QUALITY					
<p>Mitigation Measure M-AQ-1: Construction Emissions Minimization (Implements a portion of Mitigation Measure G-1 of the Eastern Neighborhoods FEIR)</p> <p>A. <i>Construction Emissions Minimization Plan.</i> Prior to issuance of a construction permit, the project sponsor shall submit a Construction Emissions Minimization Plan (Plan) to the Environmental Review Officer (ERO) for review and approval by an Environmental Planning Air Quality Specialist. The Plan shall detail project compliance with the following requirements:</p> <ol style="list-style-type: none"> 1. All off-road equipment greater than 25 hp and operating for more than 20 total hours over the entire duration of construction activities shall meet the following requirements: <ol style="list-style-type: none"> a) Where access to alternative sources of power are available, portable diesel engines shall be prohibited; b) All off-road equipment shall have: <ol style="list-style-type: none"> i. Engines that meet or exceed either USEPA or ARB Tier 2 off-road emission standards, <i>and</i> ii. Engines that are retrofitted with an ARB Level 3 Verified Diesel Emissions Control Strategy (VDECS).³ c) Exceptions: <ol style="list-style-type: none"> i. Exceptions to A(1)(a) may be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that an alternative source of power is limited or infeasible at the project site and that the requirements of this exception provision apply. Under this circumstance, the sponsor shall submit documentation of compliance with A(1)(b) for onsite power generation. ii. Exceptions to A(1)(b)(ii) <i>may</i> be granted if the project sponsor has submitted information providing evidence to the satisfaction of the ERO that a particular piece of off-road equipment with an ARB Level 3 VDECS is: (1) technically not feasible, (2) would not produce desired emissions reductions due to expected operating modes, (3) 	Project sponsor; project contractor(s).	Prior to issuance of a permit specified in Section 106A.3.2.6 of the Francisco Building Code.	Prepare and submit a Plan.	Project sponsor/ contractor(s) and the ERO.	Considered complete on findings by ERO that Plan is complete.

³ Equipment with engines meeting Tier 4 Interim or Tier 4 Final emission standards automatically meet this requirement, therefore a VDECS would not be required.

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Adopted Mitigation Measures

Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
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installing the control device would create a safety hazard or impaired visibility for the operator, or (4) there is a compelling emergency need to use off-road equipment that are not retrofitted with an ARB Level 3 VDECS and the sponsor has submitted documentation to the ERO that the requirements of this exception provision apply. If granted an exception to A(1)(b)(ii), the project sponsor must comply with the requirements of A(1)(c)(iii).

- iii. If an exception is granted pursuant to A(1)(c)(ii), the project sponsor shall provide the next cleanest piece of off-road equipment as provided by the step down schedules in Table A1 below.

TABLE A1

OFF-ROAD EQUIPMENT COMPLIANCE STEP DOWN SCHEDULE*

Compliance Alternative	Engine Standard	Emission	Emissions Control
1	Tier 2		ARB Level 2 VDECS
2	Tier 2		ARB Level 1 VDECS
3	Tier 2		Alternative Fuel*

*How to use the table. If the requirements of (A)(1)(b) cannot be met, then the project sponsor would need to meet Compliance Alternative 1. Should the project sponsor not be able to supply off-road equipment meeting Compliance Alternative 1, then Compliance Alternative 2 would need to be met. Should the project sponsor not be able to supply off-road equipment meeting Compliance Alternative 2, then Compliance Alternative 3 would need to be met.

**Alternative fuels are not a VDECS

- 2. The project sponsor shall require the idling time for off-road and on-road equipment be limited to no more than two minutes, except as provided in

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Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
<p>exceptions to the applicable state regulations regarding idling for off-road and on-road equipment. Legible and visible signs shall be posted in multiple languages (English, Spanish, Chinese) in designated queuing areas and at the construction site to remind operators of the two minute idling limit.</p> <p>3. The project sponsor shall require that construction operators properly maintain and tune equipment in accordance with manufacturer specifications.</p> <p>4. The Plan shall include estimates of the construction timeline by phase with a description of each piece of off-road equipment required for every construction phase. Off-road equipment descriptions and information may include, but is not limited to: equipment type, equipment manufacturer, equipment identification number, engine model year, engine certification (Tier rating), horsepower, engine serial number, and expected fuel usage and hours of operation. For VDECS installed: technology type, serial number, make, model, manufacturer, ARB verification number level, and installation date and hour meter reading on installation date. For off-road equipment using alternative fuels, reporting shall indicate the type of alternative fuel being used.</p> <p>5. The Plan shall be kept on-site and available for review by any persons requesting it and a legible sign shall be posted at the perimeter of the construction site indicating to the public the basic requirements of the Plan and a way to request a copy of the Plan. The project sponsor shall provide copies of Plan to members of the public as requested.</p>					
<p>B. <i>Reporting.</i> Quarterly reports shall be submitted to the ERO indicating the construction phase and off-road equipment information used during each phase including the information required in A(4). In addition, for off-road equipment using alternative fuels, reporting shall include the actual amount of alternative fuel used.</p> <p>Within six months of the completion of construction activities, the project sponsor shall submit to the ERO a final report summarizing construction activities. The final report shall indicate the start and end dates and duration of each construction phase. For each phase, the report shall include detailed information required in A(4). In addition, for off-road equipment using alternative fuels, reporting shall include the actual amount of alternative fuel used.</p>	Project sponsor/ contractor(s).	Monthly.	Submit monthly reports.	Project sponsor/ contractor(s) and the ERO.	Considered complete on findings by ERO that Plan is being/was implemented.
		Within six months of completion of construction activities.	Submit a final report of construction activities.		

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Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
<i>C. Certification Statement and On-site Requirements.</i> Prior to the commencement of construction activities, the project sponsor must certify (1) compliance with the Plan, and (2) all applicable requirements of the Plan have been incorporated into contract specifications.	Project sponsor/contractor(s).	Prior to construction activities requiring the use of off-road equipment.	Submit certification statement.	Project sponsor / contractor(s) and the ERO.	Considered complete on submittal of certification statement.
Mitigation Measure M-AQ-2 – Air Filtration (Implements Mitigation Measure G-2 of the Eastern Neighborhoods FEIR). <i>Air Filtration and Ventilation Requirements for Sensitive Land Uses.</i> Prior to receipt of any building permit, the project sponsor shall submit a ventilation plan for the proposed building(s). The ventilation plan shall show that the building ventilation system removes at least 80 percent of the outdoor PM _{2.5} concentrations from habitable areas and be designed by an engineer certified by ASHRAE, who shall provide a written report documenting that the system meets the 80 percent performance standard identified in this measure and offers the best available technology to minimize outdoor to indoor transmission of air pollution.	Project sponsor; licensed mechanical engineer or authorized professional.	Prior to issuance of building permit.	Submit a ventilation plan.	Department of Public Health (DPH); Planning Department; Department of Building Inspection (DBI).	Considered complete upon DPH approval of enhanced ventilation plan.
<i>Maintenance Plan.</i> Prior to receipt of any building permit, the project sponsor shall present a plan that ensures ongoing maintenance for the ventilation and filtration systems.	Project sponsor.	Prior to issuance of building permit.	Submit a maintenance plan.	DPH; DBI.	Considered complete upon approval of maintenance plan.
<i>Disclosure to buyers and renters.</i> The project sponsor shall also ensure the disclosure to buyers (and renters) that the building is located in an area with existing sources of air pollution and as such, the building includes an air filtration and ventilation system designed to remove 80 percent of outdoor particulate matter and shall inform occupants of the proper use of the installed air filtration system.	Project sponsor.	Prior to sale and/or lease or sub-lease.	Provide disclosure to buyer and renters; include appropriate air filtration and ventilation system in the project.	Project sponsor.	Considered complete upon disclosure to potential and future occupants.

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Adopted Mitigation Measures	Responsibility for Implementation	Mitigation Schedule	Mitigation Action	Mitigation Reporting Responsibility	Monitoring Schedule
HAZARDOUS MATERIALS					
<p>Mitigation Measure M-HZ-1 – Hazardous Building Materials (Implements Mitigation Measure L-1 of the Eastern Neighborhoods FEIR). The City shall condition future development approvals to require that the subsequent project sponsors ensure that any equipment containing PCBs or DEHP, such as fluorescent light ballasts, are removed and property disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.</p>	Project sponsor, contractor(s).	Prior to demolition of structures.	Ensure equipment containing PCBs or DEHP and other hazardous materials is properly disposed.	Project sponsor, contractor(s), DPH, various federal and state agencies.	Considered complete when equipment containing PCBs or DEHP or other hazardous materials is properly disposed.

IMPROVEMENT MEASURES AGREED TO BY PROJECT SPONSOR

	Responsibility for Implementation	Implementation Schedule	Implementation Action	Implementation Reporting Responsibility	Monitoring Schedule
TRANSPORTATION AND CIRCULATION					
<p>Improvement Measure I-TR-1: Monitoring and Abatement of Queues. As an improvement measure to reduce the potential for queuing of vehicles accessing the project site, it shall be the responsibility of the property owner to ensure that recurring vehicle queues do not occur within the public right-of-way. A vehicle queue is defined as one or more vehicles (destined to the underground parking garage) blocking any portion of the Shipley Street sidewalk or travel lane for a consecutive period of three minutes or longer on a daily or weekly basis.</p> <p>If a recurring queue occurs, the owner/operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable). Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as additional bicycle parking, customer shuttles, delivery services; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.</p> <p>If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Planning Department shall notify the Project Sponsor in writing. Upon request, the owner/operator shall hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant shall prepare a monitoring report to be submitted to the Planning Department for review.</p> <p>If the Planning Department determines that a recurring queue does exist, the facility owner/operator shall have 90 days from the date of the written determination to abate the queue.</p>	Owner/operator of off-street parking facility.	Upon operation of the off-street parking facility.	Ensure a vehicle queue does not block any portion of public street, alley, or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis.	Owner/operator; Planning Department.	Ongoing during operation.
<p>Improvement Measure I-TR-2: Installation of Visual/Audible Devices at Underground Garage Driveway. As an improvement measure to reduce potential conflicts between vehicles entering and exiting the underground garage and pedestrians traveling along the north side sidewalk of Shipley Street, the Project Sponsor shall install visual and/or</p>	Project sponsor, contractor(s).	Upon operation of the off-street parking facility.	Install visual and/or audible notification.	Owner/operator; Planning Department.	Considered complete upon

audible notifications (alarms) to alert pedestrians of vehicles traveling in and out of the underground parking garage.

installation.

Improvement Measure I-TR-3: Coordination of Move-in/Move-Out Operations and Large Deliveries. To reduce the potential for parking of delivery vehicles within the travel lane adjacent to the curb lane on Shipley Streets (in the event that the on-street loading is occupied), residential move-in and move-out activities and larger deliveries shall be scheduled and coordinated through building management. Appropriate move-in/move-out procedures shall be enforced to avoid any blockages of Shipley Street over an extended period of time and reduce any potential conflicts between movers and pedestrians walking along Shipley Street. Curb parking on Shipley Street shall be reserved through SFMTA.

Project sponsor, building management.

Upon building occupancy.

Develop procedures for coordinating move-in/move-out activities and large deliveries.

Project sponsor.

Considered complete upon submittal of procedures to Planning Department.

Improvement Measure I-TR-4: Construction Truck Deliveries During Off-Peak Periods. Any construction traffic occurring between 7:00 a.m. and 9:00 a.m. or between 3:30 p.m. and 6:00 p.m. would coincide with peak hour traffic and could temporarily impede traffic and transit flow, although it would not be considered a significant impact. Limiting truck movements to the hours between 9:00 a.m. and 3:30 p.m. (or other times, if approved by SFMTA) would further minimize disruption of the general traffic flow on adjacent streets during the a.m. and p.m. peak periods.

Project sponsor, construction contractor(s), SFMTA, Fire Department, Muni and Planning Department.

Prior to construction commencement.

Coordinate regarding limiting truck movements to the hours between 9:00 a.m. and 3:30 p.m.. Schedule a meeting between project sponsor, construction contractor(s), SFMTA, Fire Department, Muni and Planning Department.

Project sponsor.

Upon completion of project construction.

As required, the Project Sponsor and construction contractor(s) shall meet with the Sustainable Streets Division of the SFMTA, the Fire Department, Muni, and the Planning Department to determine feasible measures to reduce traffic congestion, including potential transit disruption, and pedestrian circulation impacts during construction of the project. To minimize cumulative traffic impacts due to project construction, the Project Sponsor shall coordinate with construction contractors for any concurrent nearby projects (e.g., along Fifth Street, between Howard and Folsom Streets) that are planned for construction or which later become known.

Improvement Measure I-TR-5: Construction Management Plan. In addition to items required in the Construction Management Plan, the project sponsor shall include the following:

Project sponsor, contractor(s).

Prior to and during construction.

Implement Construction Management Plan.

Project sponsor.

Upon completion of project construction.

Carpool and Transit Access for Construction Workers – As an improvement measure to minimize parking demand and vehicle trips associated with construction workers, the construction contractor shall include methods to encourage carpooling and transit use to the project site by construction workers in the Construction Management Plan/contracts.

Project Construction Updates – As an improvement measure to minimize construction impacts on nearby businesses, the project sponsor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction

inquiries or concerns.

Improvement Measure I-TR-6: Implement Travel Demand Management (TDM)

Measures. Prior to issuance of a temporary permit of building occupancy, the Project Sponsor must execute an agreement with the Planning Department for the provision of TDM services. Recommended components of the TDM program include the following:

- Provide information in the move-in packets and lobby (or electronic) bulletin boards for transit service (Muni and BART lines, schedules and fares), particularly for local trips (such as to the nearest grocery store, hardware store, shopping center, restaurants, and other nearby neighborhood commercial areas), information on where transit passes could be purchased in person and on-line, and information on the Clipper Card and 511 Regional Rideshare Program;
- Include one or more Muni FastPass (loaded onto a Clipper card) as part of the monthly rent, or homeowner association fee;
- Provide TDM training for property managers and coordinators; and have at least one contact person, preferably in the building for tenants with alternative mode travel questions.
- Promote and coordinate ridesharing activities (i.e. establish a “ride board”) for all building residents and employees, particularly to popular local events;
- Facilitate access to carshare spaces provided in the parking garage through on-site signage and information on the carshare company, rates, and how to enroll in the carshare program;
- Ensure that the points of access to bicycle parking through elevators on the ground floor and the garage ramp include signage indicating the location of these facilities.
- Ensure that bicycle safety strategies are developed along the sides of the property, avoiding conflicts with private cars, transit vehicles and loading vehicles, posting signs where necessary to increase awareness of the presence of bicycle traffic;
- Facilitate access to the Folsom Street, Howard Street, and Fifth Street bicycle routes via on-site signage;
- Actively encourage alternative mode choice by actively monitoring above efforts effectiveness, and fostering local deliveries from nearby businesses where appropriate; and
- Participate with other project sponsors in a network of transportation brokerage services.

Project sponsor, building management, Planning Department staff.

Prior to and during occupancy.

Implement TDM measures.

Project sponsor.

Ongoing during occupancy.



July 15, 2014

Ms. Cindy Wu, President
San Francisco Planning Commission
1650 Mission Street, STE 400
San Francisco, CA 94103

Re: 923-931 Folsom Street (Case No. 2012.1333) – July 24, 2014, Hearing on Large Project Authorization

Dear President Wu and Commissioners,

I am writing on behalf of my client, Trumark Urban, the project sponsor for 923-931 Folsom Street (Case No. 2012.1333). On July 24, 2014, the Planning Commission will consider a Large Project Authorization for the development of a 138,980 square foot mixed-use residential building with approximately 1,600 square feet of ground floor retail or commercial on a site currently occupied by a bus storage lot and corresponding commercial uses (“Project”). **The Project will add one hundred and fourteen (114) new dwelling units to the City’s housing supply, including fourteen (14) new on-site below market rate (BMR) units.**

The Project is located in the heart of Eastern SoMa, an area that is in the process of significant transformation. It is located on a thru lot with two frontages providing a great opportunity to design and develop a Project that reflects the varied nature of the neighborhood. The Project is comprised of two structures, an, 85 foot building along Folsom Street and a 43 foot 9 inch building along Shipley Street separated by a large internal courtyard and connected via an interior walkway along the western property line. Along Folsom Street the façade is striking and dramatic, appropriately designed for the width and prominence of the street. Along Shipley Street, the design is broken up into smaller pieces with all ground floor dwelling units having private stoops and direct street access, and a majority of the upper units having balconies overlooking Shipley Street. Combined the Project reflects the best of Eastern SoMa and provides a significant number of new dwelling units that are consistent with the intent and vision of the Eastern Neighborhoods Area Plan.

Over the past two years, Trumark Urban has worked with the neighborhood, surrounding community and the Planning Department on the Project. The Project complies with the density, intensity and height and bulk requirements of the Planning Code and General Plan and seeks minor and appropriate modifications given the Project benefits and site conditions. For all these reasons and as discussed in more detail below, **Trumark Urban respectfully requests that the Planning Commission grant the approvals requested.**

A. Property Background

923 Folsom Street is an irregularly shaped through lot comprised of three contiguous parcels totaling 24,438 square feet and bound by Folsom Street to the north, 5th Street to the east, Shipley Street to the south and Falmouth Street to the west (“Project Site”). It has 125 feet of frontage

along Folsom Street, 175 feet of frontage along Shipley Street and is 165 feet in depth. It is located in the East SoMa Eastern Neighborhoods Plan Area in a mixed-use neighborhood comprised of commercial, retail and residential uses, including community serving and public uses.

Across Folsom Street is 900 Folsom Street, a recently approved 463 unit apartment complex (i.e., Mosso) that is under construction and slated for opening later this summer or early fall. Along Folsom Street to the east is a four-story hotel with ground-floor commercial uses and to the west is a City-owned fire station (Fire Station 1). Along Shipley Street to the east is a four-story multi-family residential building and to the west is an empty lot. Across Shipley Street are one to two story residential uses and a surface parking and storage lots.

The Project Site is occupied by an approximately 11,000 square foot two-story commercial office building located in the southeast corner of the site along Shipley Street on approximately 5,500 square feet of lot area. The remainder of the site is occupied by a surface parking lot located on approximately 18,938 square feet of the lot. The Project Site currently operates as a parking lot for commuters, but was previously used as a bus parking lot for a passenger bus operation and corresponding company management and bus maintenance facilities.

The Project Site is within the Mixed Use Residential (MUR) zoning district and a 45-X/85-X height and bulk district. The portion of the Project Site fronting along Shipley Street is also in the South of Market Youth and Family Special Use District (SUD), which imposes a higher inclusionary housing obligation.

B. Project Description

The Project is the demolition of the existing structure and surface parking lot and the construction of two independent structures surrounding an internal central courtyard. The structure fronting along Folsom Street is a 85 foot high, 88,780 square foot mixed-use residential development with approximately 1,600 square feet of ground floor retail. The structure fronting along Shipley Street is a 43 foot 9 inch high 50,220 square foot residential structure. The two structures are connected via an interior walkway along the western property line.

The Project includes 114 new dwelling units comprised of eight (8) junior 1-bedrooms or studios, fifty nine (59) 1-bedroom units and forty-seven (47) 2-bedroom units. Fourteen (14) on-site BMR dwelling units comprised of one (1) junior 1-bedroom or studio, eight (8) 1-bedroom unit and five (5) 2-bedroom units are also included. The Project includes 114 Class 1 bicycle parking spaces, a bicycle repair facility and seventy-eight (78) parking spaces including two (2) car share spaces. Off-street loading is provided in a designated loading area along Shipley Street.

The design of the structure along Folsom Street presents a unified façade of an appropriate size and scale for the street and includes an architectural ribbon motif of composite resin panels that wraps up the building, breaking up the massing of the structure. This motif creates a distinctive façade design, adding interest to the high-performance, lightly-tinted, non-reflective vision glass that comprises the balance of the residential floors. Juliet balconies are sprinkled throughout this façade, providing residents access to the outdoors, but recognizing limited usefulness of balconies along Folsom Street, a major arterial roadway. At the top of the Folsom Street building is a distinctive architectural element with subtly placed accent colors that balances the building, drawing the eye up to the top to the fritted glass and interior lighting that create a signature visual element to the Project.

Along Shipley Street, the building is a smaller scale incorporating a variety of materials including two colors of cladding, decorative perforated metal gates and balcony guardrails, horizontal plank tile and patina steel planters. The design includes articulation and massing breaks consistent with the scale of the surrounding structures with a majority of the units having private open space looking out, or opening onto, Shipley Street.

The Project includes approximately 9,124 square feet of code complying open space through a combination of private balconies and stoops and common open space. Common open space is provided in an 8,773 square foot 5th floor roof deck overlooking Shipley Street and 351 square feet of private open space in nine (9) units fronting Shipley Street. The Project also will add fifteen (15) new street trees – six (6) along Folsom Street and nine (9) along Shipley Street. Street furniture and other landscape improvements and amenities are also provided as required by the City’s Better Streets and Neighborhood Program. The existing large curb cut along Folsom Street will be removed as all ingress/egress to the below grade parking will occur along Shipley Street.

The ground floor along Folsom Street includes a seventeen (17) foot high ceiling with clear glass facade to allow natural light into ground floor spaces and provide visual interest at the pedestrian level. To clearly mark the residential lobby entry, it is set back 3’ 6” from the front property line and delineated by distinctive color accent panels and a 3’ deep canopy. It also is flanked on either side by a public art wall or a bike repair area to create interest and depth at a pedestrian scale.

C. Project Approvals

The Project is requesting a Large Project Authorization under Planning Code section 329. As part of that authorization, the following exceptions to provisions of the Planning Code are requested: (1) the Rear Yard requirement (Planning Code section 134), (2) off-street parking (Planning Code section 151), (3) off-street loading (Planning Code section 152) and (4) height limits for vertical non-habitable architectural elements (Planning Code section 263.21). As discussed in more detail below, **the exceptions requested are minor and warranted given the site conditions and exceptional design proposed.**

1. Rear Yard

The Project is seeking an exception to Planning Code section 134(a)(1), which requires a rear yard equal to 25% of the total lot depth but in no case less than 15 feet. The Project Site is approximately 24,438 square feet. A code compliant rear yard (i.e., 25% of lot area) would be 6,109 square feet. The Project includes an internal courtyard of approximately 5,660 square feet with a depth of approximately 38 feet. This is approximately ninety-two percent (92%) of the code requirement and is twenty-three percent (23%) of the lot area. While below the code required rear yard, the Project includes approximately 9,124 square feet of code complying open space in addition to the 5,660 square foot interior courtyard. This is more than twice, or 240% of, the required rear yard area.

In addition, the Project is in an area without a pattern of rear yards or interior block open space. The lots surrounding the Project Site have structures abutting their rear property lines and/or are built to the property lines. The Project’s interior courtyard will establish a new pattern of interior block open space for future development and is of a size and scale that will provide ample

access to light and air to the interior facing units while not impeding light and air adjacent properties.

For all these reasons, an exception to the rear yard requirement is warranted.

2. Off-Street Parking

The Project is seeking an exception to Planning Code section 151.1 to exceed the principally permitting parking ratio of one parking space per four dwelling units (.25). The Project includes 78 parking spaces plus two car share spaces¹. This is a parking ratio of .68, which is below the maximum ratio of .75 parking spaces per unit.

Parking will be provided in the basement via a drive aisle along Shipley Street. Locating the parking entrance along Shipley Street, a small east-west alley, ensures vehicular ingress and egress will not impact vehicle or non-vehicle movement around the Project Site as the primary circulation for both pedestrians and vehicles is along Folsom Street. Because parking is in the basement, below grade, it will not be visible outside the building envelope and will not degrade the overall urban design quality of the Project.

For all these reasons, an exception to allow parking above the principally permitted ratio is warranted.

3. Off-Street Loading

The Project is seeking an exception to Planning Code section 152 to allow on-street loading due to site constraints. Below grade loading is not feasible due to the slope of the drive aisle ramp, the clearance below grade and the turning radius in the garage. On-grade off-street loading was explored, but required not only freight vehicles to back into any off-street space, but also resulted in the loss of two (2) units at the second floor to accommodate the height of such vehicles. An at-grade off-street space also required a larger curb cut along Shipley and potentially conflicted with the residential atmosphere being developed along this frontage.

In addition, because the Project is for sale, not for rent, move-in/move-outs are expected to be limited.² To ensure, however, that on-street loading was feasible, Trumark Urban worked with CitiScape, a property management firm, to create a plan to effectively and efficiently accommodate move-in/move-outs. Under the plan, an existing utility room will be fitted with a scissor lift to address grade changes at that location and allow at-grade access from the on-street loading space to the interior hallway.

For all these reasons, an exception to allow on-street loading is warranted.

4. Height Limits For Vertical Non-Habitable Architectural Elements

The Project is seeking an exemption under Planning Code section 263.21 to allow a non-habitable architectural element at the easternmost corner in the Folsom Street structure. One architectural element is proposed that is below 1,000 gross square feet and is no greater than 42 feet

¹ Car share spaces are not counted against the parking ratio under Planning Code section 166.

² The discussion of on-street loading focuses on move-in/move-out because loading for the retail or commercial uses along Folsom Street has and has always been contemplated in a loading zone along Folsom Street.

6 inches, as is required by code. The architectural element is fully integrated with the building and is consistent with the design, materials and character of the building, meeting the intent of the Planning Code by providing a unique and signature element at the top of the structure, breaking up the monotony of individual buildings, and creating a visual focal point.

For all these reasons, an exemption to the height limit for an architectural element is warranted.

D. Project Benefits³

Trumark Urban believes its residential communities should reflect and add to the neighborhoods where they are located and nowhere else is this more true than the Project Site. The Project design is of exceptional quality and reflects Trumark Urban's commitment to work with the neighborhood, the community, and City staff to bring forth a development that meets or exceeds citywide standards.

In addition to the exceptional design the Project includes significant neighborhood and citywide benefits. Those benefits include:

- Revitalization and Re-Use: The Project will replace an under-utilized parking lot and former bus storage lot and corresponding commercial uses with 114 high quality residential dwelling units and 14 on-site BMR units. The Project will activate Shipley Street with residents and walk-up dwelling units.
- Green Development: The Project will be a “green” development committed to reducing energy and water demand associated with new construction. The building will be GreenPoint Rated.
- Infill Residential Development: In developing the Project Site with residential uses, the Project provides much needed residential units in an ideal location for infill development.
- Job Creation: The Project **will create 340 union construction jobs over an 22 month period** as well as provide an apprentice, from the SoMa Pathways Program,⁴ an opportunity to work on the construction site.
- Inclusionary Housing Commitment: The Project will include fourteen (14) on-site below market rate units including one (1) junior 1-bedroom or studio, eight (8) 1-bedroom unit and five (5) 2-bedroom units.
- Impact Fees: The Project is estimated to pay over \$2 million in impact fees including the Eastern Neighborhood Impact Fee which will directly benefit the surrounding neighborhood and Eastern Neighborhoods.

³ A separate letter from Trumark Urban, dated July, 14, 2014, has been submitted outlining their community outreach efforts to date.

⁴SoMa Pathways is a partnership between Trumark Urban and United Playaz that aims to educate and connect youth to potential local employment and education opportunities within real estate, development and construction.

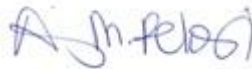
Trumark Urban has spent the past two years working with United Playaz, a violence prevention and youth leadership organization located at 1038 Howard Street between 6th and 7th, assisting with fundraisers, donations and creating a new program, SoMa Pathways, to connect at risk youth with information, education and a pathway to building a career in the construction trades. Trumark Urban believes in the importance of neighborhood organizations like United Playaz and is committed to ensuring that as the neighborhood changes United Playaz remains a permanent fixture in this vibrant and eclectic community. For this reason, Trumark Urban is honored to list the following additional Project benefit:

- Neighborhood Commitment: Future residents of the Project will, through their Home Owners Association donate annually to United Playaz and the important programs they provide. Trumark Urban also will continue the SoMa Pathways Program and commit to assisting youth from that program into various apprenticeship programs for Trumark Urban projects.

* * * * *

In sum, the Project before you is an excellent example of green, infill development. It adds one hundred and fourteen (114) new dwelling units to the City's housing stock including fourteen (14) below market rate units. It is an exceptional Project and one that we respectfully request you support and approve.

Very truly yours,



Alexis M. Pelosi

July 14, 2014

Mr. Doug Vu
City of San Francisco, Planning Department
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Trumark Urban's Community Outreach Update

Dear Mr. Vu,

I wanted to send you a follow-up to letter outlining our continued community outreach efforts related to Trumark Urban's project located at 923 Folsom in San Francisco ("Project"). A copy of our April 25, 2014, letter is attached for your reference (Attachment A).

We have met with or spoken to more than 150 neighbors, local businesses, community groups and interested parties about the Project and have 110+ signatures of support and 25 letters of support. We have prepared a map showing where the individuals that have provided letters of support and where the individuals that signed the petition live or work (Attachment B). As the map clearly shows, the support we are hearing from the community comes from the neighborhood where the Project is located.

923 Folsom Community Outreach Summary:

Supervisor Jane Kim

- We have kept Supervisor Kim's office updated on the proposed Project and our community outreach efforts to date. Supervisor Kim's office continues to be supportive of all of our outreach efforts, specifically related to the support provided to United Playaz, City Crossroads, and Asian Neighborhood Design.

United Playaz & SOMA Pathways

- Trumark Urban, in partnership with United Playaz, launched SOMA Pathways in 2013. SOMA Pathways is a program aimed to educate and connect youth to potential local employment and education opportunities, within real estate, development and construction. The 2013 & 2014 SOMA Pathways program were great successes with over 70 youth participating in the sessions. Presenters included: Trumark Urban, Sheet Metal

Workers Local Union No. 104, Carpenters Local Union No. 22, Operating Engineers Local 3, Forum Design Architects, Cahill Contractors, CIWI, Laborers' Local 261, IBEW Local 6, and Polaris Pacific.

- The 2014 SOMA Pathways program series recently wrapped-up with a certificate ceremony held at Asian Neighborhood Design. We are currently in the process of working to have participants from the SOMA Pathways program placed as apprentices on our proposed developments.
- Trumark Urban will be providing an ongoing commitment to United Playaz through an innovative Homeowners Association (HOA) structure at the proposed Project. Once residents are in place, ongoing monthly contributions will be made to United Playaz through the HOA dues meet United Playaz ongoing organizational needs. In establishing this HOA program, Trumark's goal has been to maintain a long-term connection between the new residents of the Project and the SOMA neighborhood.
- To date, through various fundraisers, Trumark Urban has raised over \$50,000 to contribute to United Playaz capital campaign to help them purchase their existing building and is committed to continuing to help United Playaz with these efforts.

Asian Neighborhood Design

- Asian Neighborhood Design (AND) aims to reduce poverty and revitalize neighborhoods in the Bay Area by building healthy communities and providing opportunities for low-income residents to become economically self-sufficient. AND has a pre-apprentice program focused on Employment Training with a comprehensive approach to preparing disadvantaged young people and adults for long-term employment by providing not only job training, but also job placement and career development.
- United Playaz connected AND's pre-apprentice program with our SOMA Pathways program, as discussed above. We are in the process of working with AND to have participants from the SOMA Pathways program placed as apprentices on our proposed developments.

Signatures of Support

- To ensure that all interested parties are aware of our Project and to make sure that we are hearing from everyone, Trumark Urban has walked the neighborhood talking to residents, merchants, and others who frequent the area. To date, after talking with community members, we have obtained 110+ signatures on our petition of support. Copies of all the signatures are attached for your reference. (Attachment B)

Additional Letters of Support

- To date, Trumark Urban has obtained letters of support from United Playaz, City Crossroads, WestBay Pilipino Multi-Services Center, Asian Neighborhood Design, SOMA Pathways Participant, City Park, Webpass, Museum Plaza LLC, and Walid Mando.
- In addition, 10 additional letters of support from residents in the area have also been received.
- The Project has also received letters of support from these additional organizations: Carpenters Local Union No. 22, Sheet Metal Workers Local 104, Laborers' Local 261, and Plumbers Local Union 38, SPUR, the Housing Action Coalition (HAC), and Larkin Street Youth Services. (All letters of support provided in Attachment B)

As we move toward the July 24, 2014, Planning Commission hearing date Trumark Urban intends to continue its community outreach efforts and will provide an update, as needed, before the hearing. Also, Trumark Urban is committed to ensure that throughout the duration of the Project to work with neighborhood groups and existing stakeholders, regarding the status of the Project. If you have any questions about the information provided or need any additional information regarding the benefits of the Project, please let us know.

Sincerely,



Kim Diamond
Development Director
Project Sponsor
Kdiamond@trumarkco.com
(925) 570-9342

Attachments:

- Attachment A - Community Outreach Letter dated April 25, 2014
- Attachment B - Signatures & Letters of Support Summary

Attachment A

Community Outreach Letter April 25, 2014

April 25, 2014

Mr. Doug Vu
City of San Francisco, Planning Department
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street– Trumark Urban’s Community Outreach-to-date

Dear Mr. Vu,

I thought it would be helpful to provide you with a summary of Trumark Urban’s community outreach efforts to date related to our proposed project at 923 Folsom Street in San Francisco (“Project”). Trumark Urban believes in learning from the communities where it seeks to locate its projects, and as a result, we have been on a ‘listening tour’, hearing from neighbors, neighborhood groups and other stakeholders about the concerns and needs of the neighborhood. The information we have gained from our listening tour has resulted in the Project that is before you today.

Some specific changes to the Project that have occurred as a direct result of meeting with the community and local stakeholders include:

- (1) onsite inclusionary housing
- (2) additional bike parking with a residents ground floor bike-shop to create a greater connection between the Project and cycling; and,
- (3) improving safety measures for the residents and the neighborhood, with the addition of security cameras and lighting.

These are just a few of the changes that have been incorporated into the Project in direct response to what we have heard from the neighbors and community.

While we will prepare a complete summary of all community outreach efforts as part of our materials for Planning Commission consideration of the Project, we thought it would be helpful to list the individuals and groups we have met with and their responses, if applicable. The outreach efforts for 923 Folsom have created unique lasting partnerships that further builds upon Trumark’s mission to take great places and make them even better.

923 Folsom Community Outreach Summary:

Supervisor Jane Kim

- We have met with the Supervisor to review Trumark’s community outreach efforts for the proposed project. Supervisor Jane Kim has been involved with supporting our SOMA Pathways program and has spoken at our United Playaz fundraising events to benefit the neighborhood organization (discussed below).

Community Meeting

- In May of 2013, Trumark Urban kicked-off our outreach efforts with a community meeting prior to submitting our Large Project Authorization. The majority of the comments made at the meeting related to construction logistics and noise, which we have taken into account.

United Playaz

- United Playaz (UP) is a violence prevention and youth leadership organization, based in SOMA, that works with San Francisco's hardest to reach youth through street outreach, case management, in-school services, recreational activities, and support to incarcerated youth. United Playaz has committed more than 15 years to improving the lives of young people who are surviving in vulnerable environments and show high incidence of truancy and low academic performance, or have been involved in the juvenile justice system.
- Our partnership with United Playaz began at our May community meeting for 923 Folsom. At that meeting, Rudy Corpuz, the Executive Director of UP, asked what our contribution would be to the community - not just by building as a developer, but how we could help make a difference in the long-term with the people who actually live in the neighborhood. Through many conversations with Rudy and his staff, we partnered with UP, and together created a new program called SOMA Pathways, a dynamic program aimed to educate and connect youth to potential local employment opportunities within real estate, development, and construction. *(Please see attached the SOMA Pathways Executive Summary, and SOMA Pathways 2014 Schedule)*
- SOMA Pathways is a program that Trumark Urban takes great pride in. We had one participant in 2013 placed with the Electricians Union (details in SOMA Pathways Executive Summary), and our goal in 2014 and in future years, is to continue to help facilitate additional job placements within the industry. Through the program, we have made connections to other local community organizations such as Mission Hiring Hall, CityBuild Academy, Asian Neighborhood Design - Employment Training Center, that have helped strengthen the program and have also served to bring the community closer together.
- Trumark kicked off the SOMA Pathways sessions in 2013 with a fundraiser to benefit United Playaz. The event raised \$20,000+ towards UP's youth summer programs. We recently held another fundraiser in connection with our 2014 SOMA Pathways. All parties came together - the youth provided tours of the clubhouse, a variety of Unions attended, and Supervisor Jane Kim spoke on behalf of United Playaz. Trumark and United Playaz delivered powerful speeches that demonstrated our respective organizations' unified goal to build stronger communities. There were UP testimonials - including a young student who had her life changed by the support of UP and a staff member who built her career by helping the community. *(Please see attached United Playaz Fundraising Flyer)*
 - At the fundraising event, Trumark Urban presented its ongoing commitment to United Playaz through an innovative Homeowners Association (HOA) structure at our 923 Folsom development. Once residents are in place, ongoing monthly contributions will be made to UP through the HOA dues for UP to utilize for its ongoing organizational needs. In establishing this HOA program, Trumark's goal has been to maintain a long-term connection between the new residents of the Project and the SOMA neighborhood.

City Crossroads Ministries

- City Crossroads works directly with residents in SOMA, where the City Crossroads center is located. They provide a range of after-school programming, enrichment services, and a safe haven for multi-ethnic, low-income families. City Crossroads is a pillar in the community and for 30 years the organization has helped children and teens reach their full potential. City Crossroads and United Playaz work closely together to serve SOMA youth.

- Trumark Urban has met with Heather Phillips, the Executive Director, on multiple occasions to learn more about the work City Crossroads provides to the community. During one of our meetings at City Crossroads Heather mentioned they were in the process of planning a Rebuilding Together San Francisco day. Rebuilding Together SF mobilizes teams of volunteers to revitalize neighborhoods by repairing homes and renovating nonprofit facilities and schools. By using and re-using available resources in our communities, repairs are provided free of charge to homeowners and communities. Trumark Urban was interested in becoming involved and we were delighted to step in and sponsor the day when a previous sponsor fell through. The City Crossroads Rebuilding Together SF day was held on Saturday April 26, 2014. The day was an amazing tie to our SOMPA Pathways program, where 6 of the participants from the program volunteered alongside Trumark Urban staff, IBEW Local 6 Electricians, Polaris Pacific staff (the Sales and Marketing Firm on 923 Folsom), United Playaz staff, and various other volunteers associated with City Crossroads. The participants from SOMA Pathways were paired up with trades they were interested in pursuing for the day, which gave them hands on experience while giving back to the community.

Bessie Carmichael School – Community Based Organizations (CBO)

- On a regular basis, we have been attending the CBO monthly meetings at Bessie Carmichael. The CBO meetings are inspiring meetings where local community organizations provide updates and collaborate to strengthen the community in SOMA.

WestBay Pilipino Multi-Service Center

- We have met with Rudy Asercion, the Executive Director of WestBay that provides a variety of services to low income youth, families, and seniors in SOMA. WestBay is supportive of new development in the area and welcomes the addition of market rate and affordable units.

South of Market Community Action Network - SOMCAN

- We have met with Angelica Cabande, the Organizational Director of SOMCAN, a neighborhood organization that delivers a variety of programs to individuals and organizations in the SOMA. Through our meeting with SOMCAN we heard the importance of ensuring that the affordable housing to be provided onsite at our proposed Project.

Yerba Buena Community Benefit District

- Trumark Urban has met with and presented the Project to Cathy Maupin, the Executive Director and Andrew Robinson, the Director of Neighborhood Partnerships with the Yerba Buena Community Benefit District (even though the Project is technically outside of the CBD's boundaries). There was general support for the Project, and some helpful design suggestions were provided to Trumark.

SF Fire Station

- Trumark Urban has met with staff from the Fire Station (directly adjacent to the Project) to understand how we can work together on logistics to ensure that we take their needs into account both during construction and on an ongoing basis.

SFMade

- We had a productive initial meeting with SFMade. Executive Director Kate Sofis suggested we meet with Noah Snyder, also with SFMade. Mr. Snyder provided valuable information about SFMade's mission and efforts, and we look forward to working with the organization to find ways to locate local SF artisans/makers/retailers in our proposed PDR/retail space at our Project.

City CarShare

- Trumark Urban has met with Rick Hutchinson, and others at City CarShare, on the proposed City CarShare spot at our proposed Project. Discussion topics included access for City CarShare members into the project, routes of travel, and other operational issues.

San Francisco Bike Coalition

- Trumark Urban has met with the Bike Coalition on several occasions to ensure we are following best practices to provide a bike friendly development that encourages increased bike ridership.

Project Presentations

- Trumark Urban presented the project to the SPUR Project Review Committee on August 13, 2013; they have provided us with a letter of support.
- Trumark Urban also presented the project to the San Francisco Housing Action Coalition Design Review Committee on June 26, 2013. A letter of support was subsequently provided for the proposed development.

Trumark Urban intends to continue its community outreach efforts and is in the process of scheduling meetings with other local neighborhood groups as well as continuing to update existing contacts regarding the status of the Project.

While this letter may be a bit early in the process, we thought it might be useful to provide a bit of background on some of the many ways that Trumark Urban is working with the community and neighborhood. If you have any questions about the information provided or need any additional information regarding the benefits of the Project, please let us know.

Sincerely,

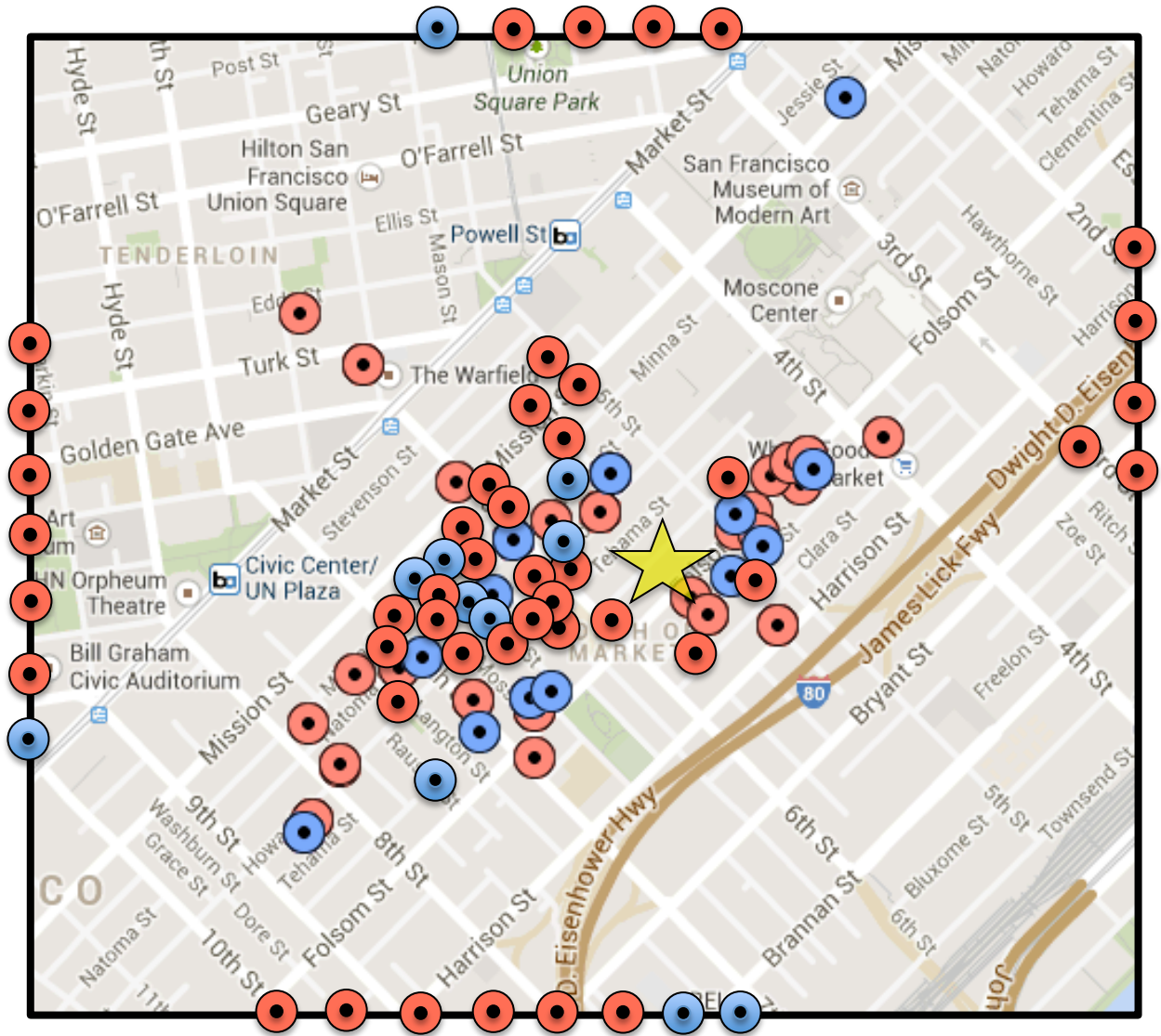


Kim Diamond
Development Director
Project Sponsor
Kdiamond@trumarkco.com
(925) 570-9342

Attachment B

Signatures & Letters of Support Summary

923 Folsom Street Community Support Summary



● Signatures of Support (112)
 ● Letters of Support (25)

Letters of Support:





June 25, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom -- (Case # 2012.1333U)

Dear Planning Commissioners,

United Playaz strongly supports Trumark Urban and their outstanding and genuine commitment to the community. We have been so fortunate to have created a lasting relationship with Trumark Urban, and are extremely thankful for all that they have provided to our team here at UP.

Trumark Urban, in partnership with United Playaz, launched SOMA Pathways in 2013. SOMA Pathways is a program aimed to educate and connect youth to potential local employment and education opportunities, within real estate, development and construction. In 2013, SOMA Pathways was launched with a kick-off fundraiser to benefit United Playaz, and raised over \$20,000 to contribute to our organization's mission.

The 2013 SOMA Pathways program was truly a great success consisting of 8 sessions, occurring twice a month, with a total of 50 youth participating. Presenters included: Trumark Urban, Sheet Metal Workers Local Union No. 104, Carpenters Local Union No. 22, Operating Engineers Local 3, Forum Design Architects, Cahill Contractors, Laborers' Local 261, IBEW Local 6, and Polaris Pacific.

We greatly appreciate Trumark Urban's time and commitment to educating and exposing youth to the variety of disciplines and career opportunities available in the industry. We recently concluded our 2nd phase of the SOMA Pathways program with Trumark Urban and look forward to more successful programs in 2014 and in the years ahead.

Trumark held another a fundraiser in 2014 that raised over \$50,000 to contribute to our capital campaign to help purchase our existing building. In addition, Trumark Urban has provided an ongoing commitment to United Playaz through an innovative Homeowners Association (HOA) structure at the proposed 923 Folsom development. Once residents are in place, ongoing monthly contributions will be made to UP through the HOA dues for UP to utilize for its ongoing organizational needs. In establishing this HOA program, Trumark's goal has been to maintain a long-term connection between the new residents of the Project and the SOMA neighborhood.

United Playaz strongly supports Trumark Urban's commitment to the community and their proposed project. I encourage you to support Trumark Urban, as they are an excellent developer and a valued partner in the community.

Sincerely,

A handwritten signature in blue ink that reads "Rudy Corpuz Jr." with a stylized flourish at the end.

Rudy Corpuz Jr.
Director
United Playaz
1038 Howard Street
San Francisco, CA 94105



City Crossroads Ministries
Serving San Francisco since 1984

June 24, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom - (Case # 2012.1333U)

Dear Planning Commissioners,

City Crossroads works directly with residents in San Francisco's South of Market neighborhood, where our community center is located. For 30 years, our organization has helped children and teens reach their full potential in many different aspects of their lives. We provide after-school programming, enrichment services, and a safe haven for multi-ethnic, low-income families.

Trumark Urban has been such an incredible asset to the neighborhood and has made considerable efforts to be involved with the community. On April 26th, 2014, Trumark Urban proudly sponsored Rebuilding Together SF and volunteered alongside SoMA Pathways participants and other neighbors to make over and improve our community center. We are so very grateful for their contribution; not just in sponsorship but in taking a hands-on approach to the project. We look forward to continuing to work with them in the future.

I am writing to support Trumark Urban's development located at 923 Folsom Street, a mixed-use residential building consisting of 114 residential units with ground floor retail. We commend Trumark Urban for including the affordable housing on site within the proposed development, creating a mixed-income neighborhood on site.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

I encourage you to support Trumark Urban, as they are an excellent developer, neighbor, and a valued partner in the community.

Many thanks,

Sincerely,

Heather Phillips
Executive Director
City Crossroads Ministries

CITY CROSSROADS MINISTRIES

An Urban Ministry in Association with the Church of God – Anderson, Indiana
160 6th Street P.O. Box 77026 San Francisco, CA 94107-0026 (415) 626-2954
www.citycrossroads.org

AsianNeighborhoodDesign

1245 Howard Street
San Francisco, CA 94103
415.575.0423, fax 415.575.0424

www.andnet.org

July 8, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom (Case # 2012.1333U)

Dear Planning Commissioners,

Asian Neighborhood Design (AND) aims to reduce poverty and revitalize neighborhoods in the Bay Area by building healthy communities and providing opportunities for low-income residents to become economically self-sufficient. AND has a pre-apprentice program focused on Employment Training with a comprehensive approach to preparing disadvantaged young people and adults for employment long term, not only providing job training, but also job placement and career development.

United Playaz connected our pre-apprentice program with Trumark Urban's SOMA Pathways program. The participants have greatly benefited from their involvement with the SOMA Pathways program and we admire Trumark Urban's commitment to educating and connecting participants to employment opportunities. We are in the process of working with Trumark Urban to have participants from the SOMA Pathways program placed as apprentices at their proposed developments.

We greatly appreciate Trumark Urban's commitment to educating and exposing youth to the variety of disciplines and career opportunities available in the industry and look forward to working with them further as they move further with their development proposals.

Sincerely,



Erica Roetman Sklar
Executive Director



July 8th, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Planning Commissioners,

West Bay's mission is to provide social, health, education, and economic services to children, youth, and their families in the South of Market District of San Francisco and the greater Bay Area. We have been fortunate to work with Trumark Urban and really appreciate their commitment to working with and getting so involved with the local community.

WestBay supports Trumark Urban's development located at 923 Folsom Street, a mixed-use residential building consisting of 114 residential units, including 14 on-site affordable homes, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban is a choice developer and a respected community partner. I encourage you to support the mixed-use development at 923 Folsom as proposed.

Sincerely,

A handwritten signature in black ink, appearing to read "Rudy Asercion".

Rudy Asercion, ED
West Bay Pilipino Multi-Service Center
175 7th Street
San Francisco, CA 94103

June 18, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom
Case # 2012.1333U

Dear Planning Commissioners,

I am writing to support Trumark Urban's proposed development as 923 Folsom Street, and to share with you my personal story of how I was acquainted with SOMA Pathways and Trumark Urban. As a participant of SOMA Pathways, I can personally attest that the program created meaningful employment and career opportunities.

I attended several of the SOMA Pathways sessions and learned about different trades and organizations. During the Electrician's Local 6 presentation, Mike McKenna, the Business Development Director, provided his contact information and encouraged the attendees to get in touch with him if they were interested in the career path of an Electrician.

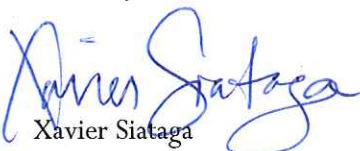
I had an interest in becoming an Electrician for quite some time. At one point, prior to attending the SOMA Pathways program, I even paid for and attended an Electrical Course through Wyotech College. Unfortunately, because they weren't able to help with job placement, I had to put that path on hold and continued his work at a local hardware store.

After the SOMA Pathways session, I contacted Mike to get more information on the apprenticeship program and entry-level positions that could be obtained. Mike encouraged me to apply for the Residential Helper list to get me active in the field with on-the-job training.

I was employed on a multi-family project in San Francisco as part of the Residential Helper Program and I am planning to apply for the Inside Wireman Apprenticeship Program this July. With the contacts I gained through SOMA Pathways, I will have an excellent chance at becoming a Local 6 Inside Wireman and having a great career.

Trumark Urban's genuine community involvement is admired and I request that you approve the development at 923 Folsom as proposed.

Sincerely,


Xavier Siataga

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely,



1226 Eddy Street
San Francisco, CA

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely,



1038 Howard Street
San Francisco, CA

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Carolyn Caldwell', with a stylized flourish extending to the right.

Carolyn Caldwell

1038 Howard Street
San Francisco, CA

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely,



Misha Olivas
1038 Howard Street
San Francisco, CA

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely,



Rudy Corpuz II
535 Minna Street
San Francisco, CA

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely,

A handwritten signature in blue ink that reads "Dante Lopez". The signature is written in a cursive style with a large, stylized "Z" at the end.

1285 Howard Street
San Francisco, CA

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely, 

Norrell Anderson
785 Brannon Street
San Francisco, CA

July 10th, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Commissioners,

Trumark Urban's development located at 923 Folsom Street is a mixed-use residential building consisting of 114 residential units, including 14 below market rate units, with ground floor retail.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban's proposed project is critical to activate the site and it is a very welcome improvement to the neighborhood. I encourage you to support this exciting and desirable new development as proposed.

Sincerely,



Ronnel Gamit
1038 Howard Street
San Francisco, CA

July 10, 2014

City of San Francisco
Planning Commission
1650 Mission Street
Suite 400
San Francisco, CA 94103

*RE: 923 Folsom Street
Case # 2012.1333U*

Dear Commissioners,

I am writing to support Trumark Urban's development located at 923 Folsom Street, a mixed-use residential building consisting of 114 residential units with ground floor retail. As long time resident of 75 Moss Street, close to the proposed development, I strongly endorse the development as proposed.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. The mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

I encourage you to support this exciting and desirable new mixed-use development as proposed by Trumark Urban for the reasons mentioned above.

Regards,


Paul Nguyen
75 Moss Street
San Francisco, CA 94103

07/07/2014

City of San Francisco
Planning Commission
1650 Mission Street
Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street - Case # 2012.1333U

Dear Commissioners,

As the commercial retail owner of 829 Folsom, I am writing to support the mixed-use residential development at 923 Folsom Street, by Trumark Urban. The 114 residential units and ground floor retail will provide needed housing, improve the safety of the neighborhood, enhance the pedestrian experience along Folsom and Shipley Streets, and bring life to the streets.

Trumark Urban has worked with the local community to address the needs of the neighborhood and I encourage you to support this desirable new development.

Best Regards,

A handwritten signature in black ink, appearing to read "W. Mando".

Museum Plaza LLC
Walid Mando
Managing Member
829 Folsom Commercial Unit
San Francisco, California



June 26, 2014

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street - Case # 2012.1333U

Dear Commissioners,

I am writing to support Trumark Urban's development located at 923 Folsom Street, a mixed-use residential building consisting of 114 residential units with ground floor retail. As a business owner located on the adjacent block, I look forward to the improvements the proposed development will bring to the area.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban has done an exceptional job engaging with the local community and I encourage you to support this exciting and desirable new development.

Regards,

Tim Leonoudakis - CEO
City Park
325 5th Street
San Francisco, CA 94107

06/26/2014

City of San Francisco
Planning Commission
1650 Mission Street
Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street - Case # 2012.1333U

Dear Commissioners,

I am writing to support Trumark Urban's development located at 923 Folsom Street, a mixed-use residential building consisting of 114 residential units with ground floor retail. As a property owner of 896 Folsom Street, close to the proposed development, I strongly endorse the development as proposed.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

Trumark Urban has done an exceptional job engaging with the local community and I encourage you to support this exciting and desirable new development.

Regards,



Walid Mando
896 Folsom Street
San Francisco, CA

City of San Francisco
Planning Commission
1650 Mission Street
Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street - Case # 2012.1333U

Dear Commissioners,

I am writing to support Trumark Urban's development located at 923 Folsom Street, a mixed-use residential building consisting of 114 residential units, including 14 onsite inclusionary homes, and ground floor retail. Trumark Urban has done an exceptional job engaging with the local community and as business located in the area we are glad to see their involvement in neighborhood.

The proposed project will activate and greatly improve the site and the pedestrian experience along Folsom and Shipley Streets with the addition of street trees, on-street guest bike parking, and neighborhood serving retail. In addition, the mixed-use residential development will activate the site, improve safety and security in the neighborhood, and provide needed housing to the area.

I endorse the proposed project at 923 Folsom Street for the reasons mentioned above, and strongly encourage you to endorse the project to improve the neighborhood and support local job creation.

Regards,



Joshua Grosser
Webpass, Inc.
262 7th Street
San Francisco, CA 94103



YOUTH SERVICES

Larkin Street Youth Services
701 Sutter Street, Suite 2
San Francisco, CA 94109
Tel (415) 673.0911
Fax (415) 749.3838
www.larkinstreetyouth.org

Board of Directors

Charles Dicke, Chair
Laura Powell, Vice Chair
Art Fatum, Treasurer
Victoria J. Johnson, Secretary
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Phillip S. Estes, At-Large
Nina Hatvany, At-Large
Blake Jorgensen, At-Large
Terry Kramer, At-Large
Sherilyn Adams, Executive Director

February 18, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

Suzi Alexander
Jeremy Avenier
Chris Brahm
Daniel A. Cody
Mark A. Edmunds
Conor Famulener
Julie B. Harkins
Jim Henry
Phillip Schlein
Aaron C. Schwartz
Sandra Stangl
Christine A. Tsingos
Carla Washington
John Whiting
Charles J. Wibbelsman
Allison N. Wysocki

Dear Planning Commissioners,

Larkin Street Youth Services has been delighted to have Trumark Urban's participation in a variety of different aspects of our organization. We truly value their staff's involvement and generosity.

Trumark Urban has become a welcome addition to Larkin Street's volunteer network. Their staff has attended youth graduation ceremonies, prepared and served lunch at the center, repainted our facilities, and volunteered on a variety of different occasions.

In addition, a few members of Trumark's staff are active in 'Friends of Larkin Street', which raises awareness of Larkin Street Youth Services to San Francisco's professional community, and hosts a variety of events to raise funds for the organization. Trumark also recently provided a generous holiday donation that we genuinely appreciate.

Trumark Urban is a quality developer, committed to supporting the local communities in which they are building as well as non-profit organizations throughout San Francisco. We look forward to continue working with them in the future.

Sincerely,

Sherilyn Adams
Executive Director

Honorary Board

Terry Allen-Rouman
Ray Brown
William F. Campbell
James E. Canales, Jr.
Laurence A. Colton
Jay Cuetara
Penelope Douglas
Tim Emanuels
Anita Feiger
Judy Getto
Vince Hoeningman
Steve Janowsky
Debbie G. Jorgensen
John E. Kalin
Stephen C. Kirmse
Stephen Koch
Mary V. Lester
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John E. Martinez
Paul R. Mohun
Maria Muzio
David J. Nygren
Carla B. Oakley
Mary M. Prchal
Jean Richardson
Henry Safrin
J. Milton Seropan
Anne B. Stanton
Gregory W. Wendt
Victoria Willock
David B. Zenoff



United Brotherhood of Carpenters and Joiners of America

LOCAL UNION NO. 22

July 8, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: Case # 2012.1333U
923 Folsom Street

Dear Planning Commissioners,

Carpenters Local Union 22 of the United Brotherhood of Carpenters and Joiners of America endorse Trumark Urban's proposed mixed-use residential development project located at 923 Folsom Street.

The proposed development will create new local construction jobs as well as associated employment opportunities.

The proposed project will be an asset to the neighborhood by improving and activating the existing site. The project will add 114 homes, neighborhood serving retail, and is a welcome change to the neighborhood.

Carpenters Local 22 has participated in Trumark Urban's SOMA Pathways program that was created in collaboration with United Playaz. The dedication Trumark Urban has shown towards United Playaz and the youth they serve is commendable.

We endorse the proposed project at 923 Folsom Street for the reasons mentioned above, and strongly encourage your endorsement to improve the neighborhood and support local job creation.

Sincerely,

Peter Garza Sr.
Field Representative

sko/opeiu-3-afl-cio (38)

2085 3RD STREET • SAN FRANCISCO, CA 94107
TELEPHONE: (415) 355-1322 • FAX: (415) 355-1422



LABORERS' INTERNATIONAL UNION OF NORTH AMERICA

LOCAL UNION NO. 261

June 23, 2014

RAMON HERNANDEZ
Business Manager

DAVID DE LA TORRE
Secretary-Treasurer

JESUS VILLALOBOS
President

JAVIER FLORES
Vice President

VINCE COURTNEY
Recording Secretary

OSCAR DE LA TORRE
Executive Board

JOSE DE LA MORA
Executive Board

SAN FRANCISCO
3271 18th Street
San Francisco, CA 94110
Phone: (415) 826-4550
Fax: (415) 826-1948

SAN MATEO COUNTY
300 7th Avenue
San Mateo, CA 94401
Phone: (650) 344-7168
Fax: (650) 344-5357

MARIN COUNTY
4174 Redwood Highway
San Rafael, CA 94903
Mail P.O. Box 4250
San Rafael, CA 94913
Phone: (415) 492-0936
Fax: (415) 492-8233

City of San Francisco
Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: 923 Folsom Street
Case # 2012.1333U

Dear Planning Commissioners,

On behalf of the brothers and sisters of Laborer's Local 261, I am writing to voice our strong support for Trumark Urban's proposed mixed-use, residential development at 923 Folsom Street.

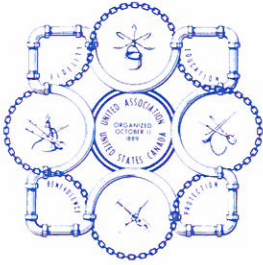
The development will greatly improve the pedestrian experience, and provide much needed housing in the area, including onsite affordable units. Additionally, both direct and indirect local construction-related jobs will be created from the proposed development.

We have enjoyed participating in an exciting program established by Trumark Urban and United Playaz – the SoMA Pathways program -- and commend Trumark for their ongoing commitment to educate and support local at-risk youth.

The Laborer's Local 261, strongly supports the development at 923 Folsom Street as proposed, and urges you to support the development in order to create local jobs and provide needed housing to the City.

Sincerely,

RAMON HERNANDEZ
Business Manager



**UNITED ASSOCIATION OF JOURNEYMEN AND APPRENTICES
OF THE PLUMBING AND PIPE FITTING INDUSTRY**

LOCAL UNION NO. 38

1621 MARKET STREET • SAN FRANCISCO, CA 94103

July 3, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

Re: 923 Folsom Street – Case # 2012.1333U

Dear Planning Commissioners,

The United Association of Journeymen and Apprentices of the Plumbing and Pipe Fitting Industry – Local Union No. 38 San Francisco - strongly supports the project proposed, located at 923 Folsom Street.

The mixed-use residential development will provide 114 homes to the SoMA neighborhood, greatly improve the pedestrian experience on Folsom and Shipley Streets, and create employment opportunities for members of Local Union No. 38.

Local 38 has become involved with Trumark Urban's SoMA Pathways program to support the youth of United Playaz. We are excited to work closely with Trumark Urban to inform local youth about the apprentice and career pathways Local 38 provides.

We fully back the residential development proposed at 923 Folsom Street, and urge you to join us in supporting the development as proposed.

Sincerely,


LARRY MAZZOLA, JR.
Bus. Mgr. & Fin. Secty-Treas.

LMJR/la
opeiu-afl-cio (19)

INTERNATIONAL ASSOCIATION OF SHEET METAL, AIR, RAIL AND TRANSPORTATION WORKERS
SHEET METAL WORKERS' LOCAL UNION No. 104
WEST BAY DISPATCH OFFICE

June 18, 2014

City of San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: *923 Folsom - Case # 2012.1333U*

Dear Planning Commissioners,

The brothers and sisters of the Sheet Metal Workers' Local Union No. 104 strongly support Trumark Urban's proposed development at 923 Folsom Street. We urge you to join us in supporting the mixed-use residential development that will create 114 new homes in SoMA.

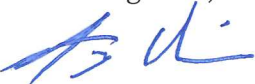
By supporting the project, you will guarantee the addition of local employment, both indirect and direct through construction, and other related jobs associated with the development.

The proposed development will enhance the pedestrian experience by activating a currently underutilized site, thus improving safety and security in the area. We also support the project as Trumark Urban has opted to include the affordable housing units onsite.

Trumark Urban partnered with United Playaz for an exciting new program called SOMA Pathways. We participated in SOMA Pathways by presenting to local youth, and really enjoyed the experience. We appreciate and respect Trumark Urban for their dedication and involvement in the community.

The Sheet Metal Workers' Local Union No. 104 strongly supports the development proposed at 923 Folsom Street for the above-mentioned reasons, and ask that you join us to support the development to create local jobs.

Kind Regards,



Anthony Urbina
Business Representative



September 23, 2013

Ms. Kim Diamond
Trumark Urban
90 New Montgomery Street, Suite 750
San Francisco, CA 94105

Ref: 923 Folsom Street – Mixed-Used Development

Dear Ms. Diamond,

On behalf of the San Francisco Housing Action Coalition (SFHAC), I am pleased to inform you of our endorsement of your 923 Folsom Street project. Following review and discussion, our Endorsement Committee believes the project has many merits and will make a substantial contribution to SFHAC's mission of increasing the supply of well-designed, well-located housing in San Francisco. We have proposed several recommendations in our letter focused on parking, sustainability and urban design that we suggest you consider as you move forward with this project. We believe that these suggestions improve its conformance with SFHAC's principals of good urban design.

A copy of the endorsement guidelines we applied in reviewing your project is attached. The proposed project meets our guidelines in the following ways:

Project Description:

Your project we reviewed consists of a 114-unit mixed-use development, composed of two independent structures connected by an internal courtyard, with 1,800 square feet of ground-floor retail/commercial space and below-ground, off street parking.

Land Use:

The SFHAC believes the project is an appropriate use at this location. It is a positive addition to the surrounding community and provides badly needed housing while enhancing the neighborhood's livability. The site was recently rezoned to Mixed Use – Residential (MUR) as part of the Eastern Neighborhoods Plan, which calls for medium-to-high density residential development due to its proximity to downtown. A bus storage lot, with corresponding commercial uses, currently occupies the site.

The SFHAC is enthusiastic to see residential projects close to various transit options. This project exemplifies the transit-oriented-development we strongly support. The proposed project is close to routes along Folsom Street, 5th Street and 6th Street. Additionally, the site is located within a block of many public transit lines that provide easy access to BART stations and Caltrain. Increasing housing supply close to downtown SOMA and Mission Bay employment centers is exactly right.

The project site is located in the proposed Central Corridor Plan Area. Under that draft land use plan, the project site would be re-designated Mixed-Use General (MUG). There are slight differences between the MUR and MUG districts, as they relate to non-residential uses. However, the project's height and bulk conform to the Central Corridor Plan, which are 85 feet, high facing Folsom Street and 45 feet high along Shipley Street.

Density:

The proposed project meets the SFHAC's criteria for density. The Planning Code requires forty percent two-bedroom units or thirty percent three-bedroom units. In response, the project proposes 46 two-bedroom units, 52 one-bedroom units and 16 Jr. one-bedroom units.

Affordability:

The SFHAC is delighted the Project Sponsor will be providing the affordable units on-site. Trumark Urban has ensured they will comply with the required number of units.

Parking and Alternative Transportation:

The site is served by robust transit options. As a result, SFHAC believes the need for parking is reduced. We would therefore prefer fewer parking spots. SFHAC understands that your team faces challenges balancing the level of parking demanded by the neighborhood with our views. Nevertheless, we believe reducing the parking ratios is helpful.

The project seeks an exception for parking because it proposes an amount greater than one parking space per four housing units (0.25 spaces per unit) but less than the maximum of 0.75 spaces per unit. The maximum parking allowed is 90 spaces. The project currently proposing 87 total parking spaces, including four handicap accessible spaces and one car share space.

The SFHAC would prefer that a greater effort be made towards reducing the total on-site parking by increasing the car share spots. Separately, we are happy to see an ample amount of bicycle parking, totaling 100 spots.

Preservation:

There are no structures of significant historic or cultural merit on the site or nearby that would be negatively affected by the project.

Urban Design:

The proposed project faces unique site circumstances that have been addressed creatively. The proposal consists of two buildings, with one structure fronting Folsom Street and the other fronting Shipley Street that is designed to reinforce the existing urban edges of both street frontages.

The SFHAC enthusiastically supports the abundant amount of open space proposed. This will connect the two structures and create a strong sense of place for the residents. The project provides 9,124 square feet of open space and 8,800 square feet of usable roof-deck on the building facing Shipley Street. In addition, it has 324 square feet of open space on the balconies of units located along Shipley Street. The project also

Ms. Kim Diamond
Page 3

proposes 5,660 square feet of outdoor space in an internal courtyard between the structures.

The project is being marketed as a family-oriented development. However, the SFHAC believes more could be done to make the project suitable to families with children. We would prefer the addition of more family amenities to the site. We suggest that the project team create flex space that could be altered later according to the needs of the residents.

In summary, however, the Endorsement Committee believes this development will be an excellent addition to the neighborhood.

Environmental Features:

The SFHAC compliments your adding features that will reduce the project's carbon footprint. However, it seems the precise measures have not been chosen yet, so it is not possible to fully evaluate them. The construction and finish materials for the project will also be selected at a future date to optimize their sustainability.

The SFHAC would like to encourage your team to continue to look at ways to improve the project's efficiency and reduce its carbon footprint.

Community Input:

Trumark Urban has been actively engaged in reaching out to the neighborhood. As part of the City's pre-application requirements, a neighborhood meeting was held at which members of the community were able to voice their views and concerns. In addition, the project team met independently with various neighborhood organizations.

We compliment your efforts to encourage community input.

Thank you for submitting this project to the SFHAC Endorsements Committee for our review. Please keep us abreast of any changes or updates with this project. We are pleased to support your excellent project as it moves forward. Let us know how we may be of assistance.

Sincerely,

A handwritten signature in black ink, appearing to read "Tim Colen", with a long horizontal flourish extending to the right.

Tim Colen, Executive Director

ENDORSEMENT GUIDELINES

Adopted January 2010

The SFHAC will consider endorsing housing developments and mixed-use projects with a housing component. The following guidelines will be used to evaluate the project:

Land Use: Housing should be an appropriate use of the site given the context of the adjacent properties and the surrounding neighborhood and should enhance neighborhood livability.

Density: The project should take full advantage of the maximum unit density and/or building envelope, allowable under the zoning rules.

Affordability: The need for affordable housing, including middle income (120-150 of median) housing, is a critical problem and SFHAC gives special support to projects that propose creative ways to expand or improve unit affordability beyond the legally mandated requirements.

Parking and Alternative Transportation: SFHAC expects the projects it endorses to include creative strategies to reduce the need for parking, such as ample bicycle storage, provision of space for car-share vehicles on-site or nearby, un-bundling parking cost from residential unit cost, and measures to incentivize transit use. Proximity to transit should result in less need for parking.

In districts with an as-of-right maximum and discretionary approval up to an absolute maximum, SFHAC will support parking exceeding the as-of-right maximum only to the extent the Code criteria for doing so are clearly met. In districts where the minimum parking requirement is one parking space per residential unit (1:1), the SFHAC will not, except in extraordinary circumstances, support a project with parking in excess of that amount.

Preservation: If there are structures of significant historic or cultural merit on the site, their retention and/ or incorporation into the project is encouraged. If such structures are to be demolished, there should be compelling reasons for doing so.

Urban Design: The project should promote principles of good urban design: Where appropriate, contextual design that is compatible with the adjacent streetscape and existing neighborhood character while at the same time utilizing allowable unit density; pleasant and functional private and/or common open space; pedestrian, bicycle and transit friendly site planning; and design

treatments that protect and enhance the pedestrian realm, with curb cuts minimized and active ground floor uses provided projects with a substantial number of multiple bedroom units should consider including features that will make the project friendly to families with children.




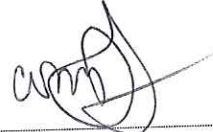
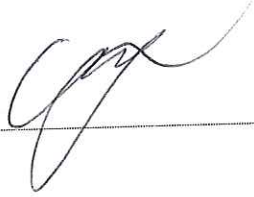
Environmental Features: SFHAC is particularly supportive of projects that employ substantial and/or innovative measures that will enhance their sustainability and reduce their carbon footprint.

Community Input: Projects for which the developer has made a good faith effort to communicate to the community and to address legitimate neighborhood concerns, without sacrificing SFHAC's objectives, will receive more SFHAC support.

TRUMARK URBAN

The existing underutilized site will be enhanced with a 114-unit mixed-use residential community, including on-site affordable housing. New homeowners, ground floor retail, and greening of the street will help to activate and improve the neighborhood. Trumark Urban greatly appreciates your support of our development at 923 Folsom.

By signing below, I hereby support the proposed project at 923 Folsom.

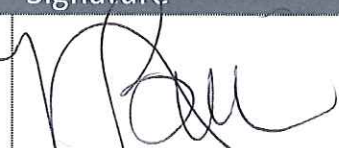
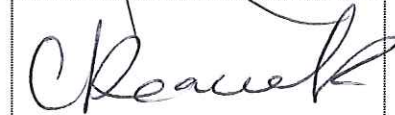
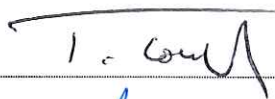


#	Name / Company	Address	Contact Number	Email	Signature
1	Kevin Bixler	75 Moss St	415.509.9322	Kevinbixler@gmail.com	
2	Terry Hager	828 Folsom S.F.	415-424-5547	tahager1@yahoo.com	
3	Mindy Yang	1099 Folsom	559-706-8663	mngyang@gmail.com	
4	Thuan Dong	1072 Folsom	415 652-4747	tzca@gmail.com	
5	Mandy Hua	1072 Folsom	415 370-5010		

June 2014

TRUMARK URBAN

The existing underutilized site will be enhanced with a 114-unit mixed-use residential community, including on-site affordable housing. New homeowners, ground floor retail, and greening of the street will help to activate and improve the neighborhood. Trumark Urban greatly appreciates your support of our development at 923 Folsom.

By signing below, I hereby support the proposed project at 923 Folsom.






#	Name / Company	Address	Contact Number	Email	Signature
1	NICOLE BICHO Webpass	262 Folsom	1800 1800 Webpass	nicole@webpass.net	
2	Casey Peacock Webpass	262 Folsom	1800 webpass	casey@webpass.net	
3	TODD COLEMAN	345 4TH STREET		TODD@FACTBIKER.COM	
4	Shannon Glasheen	3222 Mission	603-706-0599	shannonglasheen design@gmail.com	
5	ABHIK MAJUMDAR	180 Brauner	415 322 0705	abhikees@gmail	

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Clara

TRUMARK URBAN

The existing underutilized site will be enhanced with a 114-unit mixed-use residential community, including on-site affordable housing. New homeowners, ground floor retail, and greening of the street will help to activate and improve the neighborhood. Trumark Urban greatly appreciates your support of our development at 923 Folsom.

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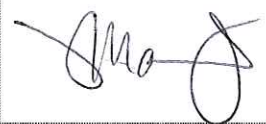
#	Name / Company	Address	Contact Number	Email	Signature
1	Philip West	2420 Steiner St # 7	650-283-0804	pcwest1@yahoo.com	
2	Steven Harris	829 Folsom #412	415-269-4229	sgarms@stevengharris.com	
3	Robert Taylor	893A Folsom	415-795-1149	rtaylor@ymail.com	
4	Francis Au	821 Folsom St	415-861-6888	sales@artecomm.net	
5	M. Vanika	840 Folsom	415-777-5144	thegourmetdepot.com	

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Clara
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#	Name / Company	Address	Contact Number	Email	Signature
1	STEPHEN LAFERRIERE JAMBER WINE PUB	858 FOLSOM ST	415-273-9192	MANAGER@JAMBERSF.COM	
2	Larry Mitchell My Mechanic	888 Folsom St	415-552-0555	Larry@mymechanicSF.com	
3	Auto Trust Services	255 SHIPLEY ST.	415-552-8998		
4	Jack	255 Shipley St	415-816-6889	Jack.Li@Hotmail.com	
5	JULIENE MZULOS	150 CORONADO AVE DUBLIN CITY 94015	620-2431521		

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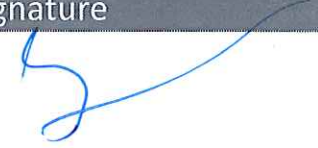

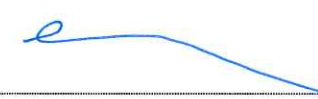



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#	Name / Company	Address	Contact Number	Email	Signature
1	BDE Architecture	950 Howard	206 399 1567	brandy@bdearch.com	Barnali Brandy
2	Mark Thompson	299 5th St.	(310) 709-3326	mark.1000@yahoo.com	Mark
3	Dorly Acosta	3431 -25th St	(415) 4471870		Dorly
4	Maria Comoda	1825 Mission St #			me
5	CELESTINA LUGO	50 CAMBON DR. SF CA. 94132	415) 469 8074		

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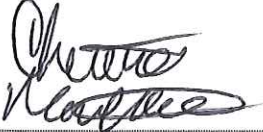




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#	Name / Company	Address	Contact Number	Email	Signature
1	LUMEN SIVITZ MIGHTY SPRING	920 HARVARD ST	515-547-8539	LUMEN.SIVITZ@ gmail.com	
2		3425 th ST 94110	554-552-2772	austin@mighty spring.com	
3	Ben Kreisel Folsom Gulch	947 Folsom	415 405 6402	folsomgulch@ sbcglobal.net	
4	CHRISTOPHER D A-N-D		415-424-3513	CHRISTOPHERD3513@ gmail.com	
5	CARL Chatman		415 623 6278	Carl Chatman JR@ Gmail.com	

TRUMARK **URBAN**

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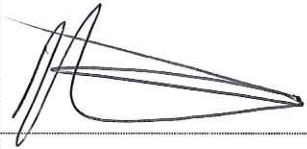

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#	Name / Company	Address	Contact Number	Email	Signature
1	Cherena Martinic A.N.D.	6324 Geney Blvd.	415 240 9741	cherenamartinic@gmail.com	
2	Paris Mays / A.N.D.	424 Guerrero St	925-752 5010	parismays415@gmail.com	
3	Justin Alderman A.N.D.	869 Ellis	415-684-0870	Justin.Alderman@gmail.com	
4	Donald Goodwin A.N.D.	197 Edinburgh	415-756-8705	Goodwinantonys1@yahoo.com	
5	Oleg Nevidomy A.N.D.	482 32nd Ave. SR, Apt. 94421	415-641-1750	nevidos89@gmail.com	

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#	Name / Company	Address	Contact Number	Email	Signature
1	Chi Young AND	1180 Howard St. S.F. CA 94103	(415) 240-010	chi.young@gmail.com	
2	Ashley Roth	1140 Pine St #5 San Francisco	415- 727 637 5233	ashley.s.roth @gmail.com	
3	Ashley Dennis	3512 Sweetwater Dr. San Leandro, CA 94578	510-552- 9725	N/A	
4	Manique Williams Essex	2601 Hilltop #622 Richmond		williamsmanique @essex.com	
5	Staci Larrabee SELF	350 Rhode Island SF CA 94127	650-202-9836	slarrabee@gmail.com	

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#	Name / Company	Address	Contact Number	Email	Signature
1	Jason Klemoff Parking Guy	1348 48th Ave. 94122	(415) 513-8222		
2	Thomas Jay Parking	2406 Bryant St SF CA	(510) 941-6972	-	
3	Matthew Arthur GIS	3212 Ingalls SF			
4	Laura Roth Goldilock Creative	829 Folsom			
5	Martin J. Gilvary	920 Harrison SF CA	415-516-7651		

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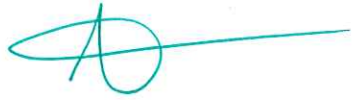



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#	Name / Company	Address	Contact Number	Email	Signature
1	Tefanon Enguano	855 Folsom 142	415 350 0563	ststevenc@yahoo.com	
2	Anthony Manansala	479 Napa St		gauthman@yahoo.com	
3	CLAUDE NORIEGA	926 Howard St	917 746 5652	Claude@techshop.ws	
4	Micah Oberst	926 Howard		micahoberst	
5	DENNIS McCLURE	1245 Howard ST S.F.	415 575-0423 Ext 202	dmcclure@audnet.org	

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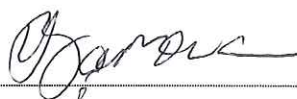

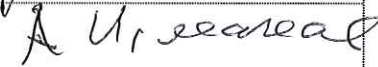
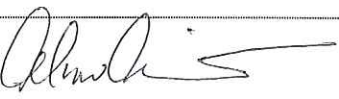

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#	Name / Company	Address	Contact Number	Email	Signature
1	JULIANA BRODSKY	950 HOWARD	415-967-6800	jbrodsky@bdearch.com	
2	Brenna Williams	950 HOWARD	858.354.3887	brennaawilliams@gmail.com	
3	NATHAN SIMPSON	950 HOWARD	415.967.6885	NSIMPSON@BDEARCH.COM	
4	SETH EUSWORTH	950 HOWARD	415.967.6806	seth@studioellsworth.com	
5	CATIE BALDWIN	950 HOWARD	415.967.6802	cbaldwin@bdearch.com	

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#	Name / Company	Address	Contact Number	Email	Signature
1	Veny P Zamora	401 Silver Ave	415-990-9456	n/A	
2	Felix Damij	683 Mission St SF CA 94103	415-487-0513	—	
3	Antoneta Villanar	2909 San Bruno Ave	650-455-8990		
4	Alma Animo	311 Baden St. S. F CA 94131	(415) 571-5765		
5	MARY BEA VILLANUEVA	401 SILVER AVE SF, CA 94112	415 889-3561		

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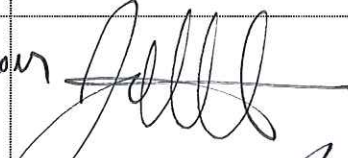


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#	Name / Company	Address	Contact Number	Email	Signature
1	Angie Baganes	603 Natoma St Apt. 209 SF CA 94103	415-621-6040	angiebaganes@yahoo.com	
2	Georgio Ferrer	900 STEPHEN AVE. VALLEJO, CA 94591	(408) 849-5007	shura	
3	Aida Otolop	1830 Haight SF CA 94117	415 624 6121	aidatolop@yahoo.com	
4	Robert Ursua	1210 HAIGHT SF CA 94117	415 802 9153	barneyursua@gmail.com	
5	LETICIA C. MURRY	1250 HAIGHT ST. CA SFO 94117	415 802 9153	lettymurry@yahoo.com	

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#	Name / Company	Address	Contact Number	Email	Signature
1	BRUCE FINDLING BDE ARCHITECTURE	950 HOWARD ST.	(480) 329-3027	BFINDLING@BDEARCH.COM	
2	MEGAN AASEN BDE ARCHITECTURE	950 HOWARD ST.	415 844 0763.	MHMEGS@GMAIL.COM MHOWARD@BDEARCH.COM	
3	MILLS MARTIN BDE ARCH	950 HOWARD ST.		mmartin@bdearch.com	
4	JON ENNIS	950 HOWARD ST	415 595 5758	JENNIS@BDEARCH.COM	
5	IAN MURPHY	950 HOWARD ST.		IMURPHY@BDEARCH.COM	

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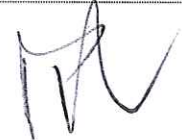
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#	Name / Company	Address	Contact Number	Email	Signature
1	Elenteria Dimadanta	11-90 Howard St	415 860 4485		
2	ALMELODE MEIMBAN	SF, CA. 94109 1880 Pine St. #507	(415) 988-3377	Ameimban@gmail.com	
3	Helen M. CARTER	849 Paris St. SF CA 94112	415 375 1801	hcarter4ever@yahoo.com	
4	Virginia M. Edlagan	486 madred st San F. CA. 94112	(415) 684-3796		
5					

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#	Name / Company	Address	Contact Number	Email	Signature
1	Heather Phillips	227 7th St ASOT SF CA 94103	415-254 6869	c.heatherphillips@ gmail.com	
2	Lucinda Haas	11 ↑	415-525 1595		
3	MARC FARISS	2934 PINE ST SF, CA 94115	(415) 573- 4520		
4	John DARESA	88 6th St			
5	Raymond VIVAS	3728 Cesar chavez. SF, CA 94110	415 5187678	ampsk12@gmail.com	

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#	Name / Company	Address	Contact Number	Email	Signature
1	PAVLO <i>[Signature]</i>	240 JONES #322			<i>[Signature]</i>
2	<i>Jennifer Moore</i>	160 6th Street (Church)	415 602 1770	jleis1@hotmail.com	<i>Jennifer Moore</i>
3	<u>Ben</u>	123 North way			<i>[Signature]</i>
4	<i>[Signature]</i> Mookie Corpuz	535 MINNA ST.			M.C.
5	Ann Moberg COG Outpost	3728 Cesar Chavez St	415- 647-1330		<i>Ann Moberg</i>

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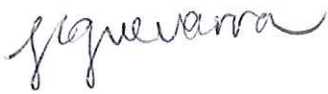
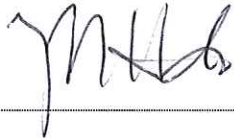


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#	Name / Company	Address	Contact Number	Email	Signature
1	Kimberly Wilson A.N.O	1245 Howard	915-575-0243 EXT 203	Kwilson@andnet.org	
2	Sam Salzeder	1233 Howard	241-2453	sam.salzeder@ mikesbikes.com	
3	Jimmy Wittance	1233 Howard St	415-241-2453	yourentsit@gmail	
4	Grant Weaver	2235 Third St. / 950 Howard St.	231-999-5443	gweaver@bdearch.com	
5	Amunda Gault / BDE Arch.	950 Howard St.	850.508.5114	agault@bdearch.com	

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#	Name / Company	Address	Contact Number	Email	Signature
1	Jeremy Steffensen			jeremy.steffensen@gmail.com	
2	Jessica Gornear	57 Taylor St.			
3	Mandi Hand	160 6th St (WORK)		mandi@sfma.org	
4	TRAVIS STEVENSON	160 6th St SF (CHURCH)		ENOUGH-BULL@YAHOO.COM	
5	Robert Panholzer	160 6th St			

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#	Name / Company	Address	Contact Number	Email	Signature
1	Joshua Rivera	57 Taylor St.			
2	DIANE LAVIN	1601-16TH AVE			
3					
4					
5					

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#	Name / Company	Address	Contact Number	Email	Signature
1	Arturo Yambao	1839 48th Ave S.F. CA 94122	(415) 810-8067	arturoyambao @comcast.com	
2	German Yambao	1812 40th Ave. S.F. CA 94122	415 260-9027	german.yambao@ gmail.com	
3	Salvador Villalobos	535 Mission St #101 SF CA 94103	(415) 559-0375	Sal@united playaz.org	
4	Carolyn Caldwell	301 Clippin St #1 SF, CA 94103	415-299-0156	carolyn@unitedplayaz.org	
5	Luis Li	555 Broadway APT #1	415-565-9872	luisli321@ yahoo.com	

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#	Name / Company	Address	Contact Number	Email	Signature
1	Norrell Anderson	785 Brannan	1-415-309-9258	nanamtd0813@gmail.com nanamtd0813@gmail.com	Norrell Anderson
2	Greg Gonzales Computer Press	1048 Howard ST.	415-255-9519	gon2@computerpress.com	
3	Dalia Sanchez	1226 Eddy St	(415) 688-6377	N/A	Dalia Sanchez
4	Rudy Cornejo III	535 Mission St. #302	N/A	cornejo32041@gmail.com	Rudy Cornejo III
5	David Mansapit	1287 Howard St	(415) 623-0054	N/A	David Mansapit

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#	Name / Company	Address	Contact Number	Email	Signature
1	 Misha OIVAS	1038 Howard	975-2929	mishaoivas@yahoo.com	
2	 Ronnel Gamit United Playaz	535 Hinner St	415-573-6219	UP94Life@yahoo.com	
3	Ronnel Gamit J.P	1038 Howard		Ronnelgamit.eg@gmail.com	
4	ACE PAYNE U.P	1038 Howard	510-984-8604	Prostiesnow@yahoo.com	
5	Donke Corpuz U.P	1285 HOWARD	650 921400	donke@unitedplayaz.org	

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#	Name / Company	Address	Contact Number	Email	Signature
1	Elijah Corpuz/United Playaz	535 Minna St	(415) 806-2219	corpuzchoppa@yahoo.com	Elijah
2	Deion Nazareta/United Playaz	1285 Howard St	(415) 756-2835	dnazareta@gmail.com	Deion Nazareta
3	Ro Idan Galaraga/UP	1190 Howard St	(415) 214-2335	roldaaang@gmail.com	Ro Idan
4	Christian Anicete/UP	670 Natoma	(415) 748-4885	christiananicete@gmail.com	Anicete
5	Dale Harris	324 Allison St	(707) 654-5322	dr.harris@gmail	Dale Harris

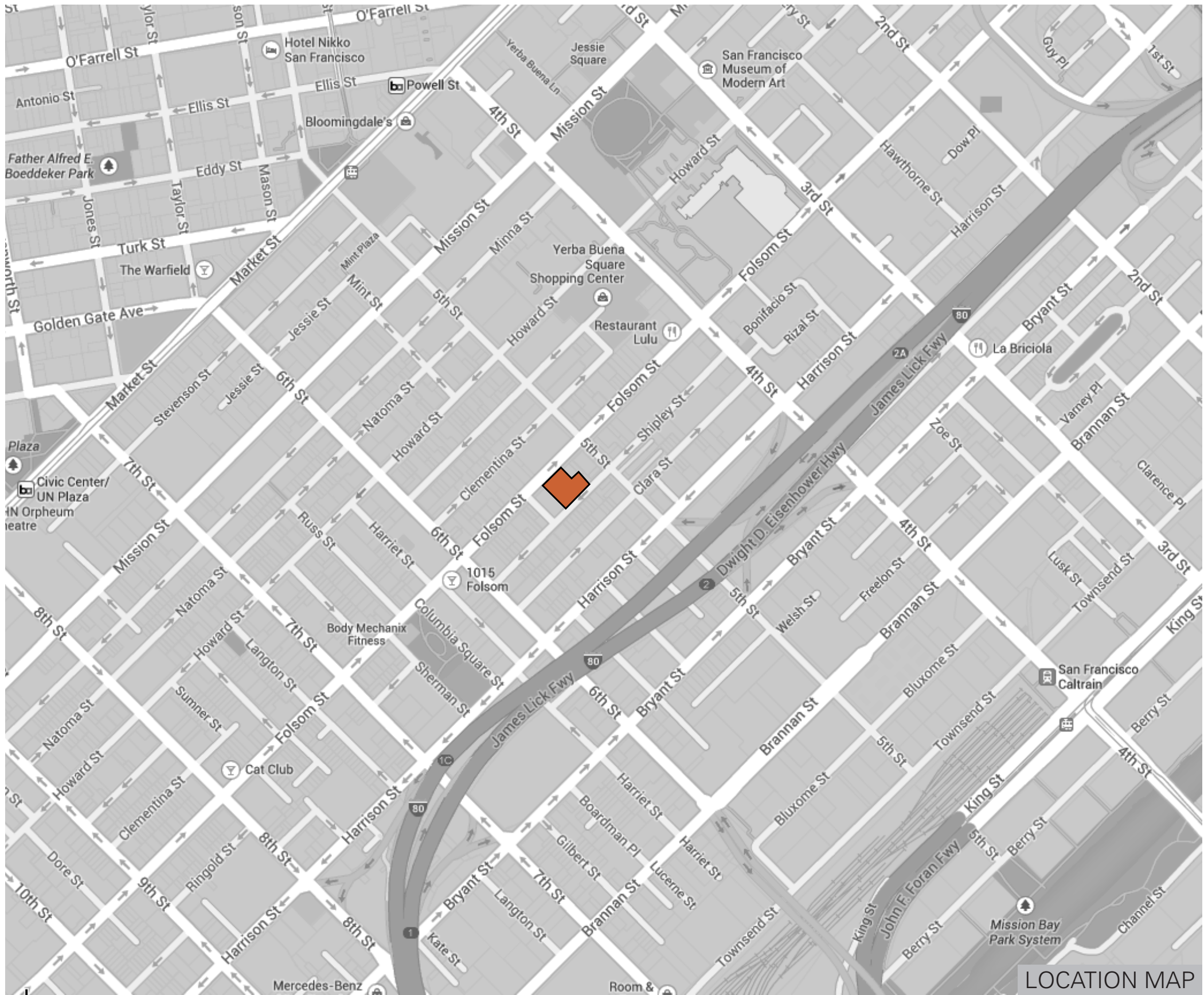
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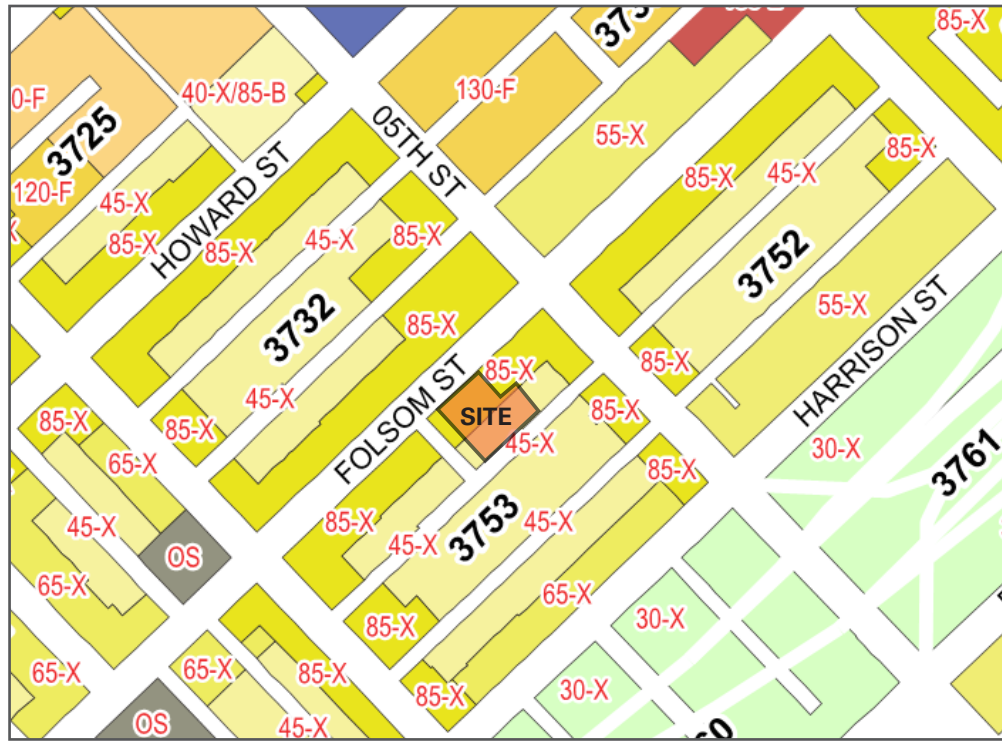
#	Name / Company	Address	Contact Number	Email	Signature
1	Shaiyenne Avimataji	759 N 410th St.	(415) 573-4495	N/A	SA.
2	Bianca Catalan	6222 3 rd Street San Francisco, 94124	(415) 632-0520	bianca.catalan@yahoo.com	Bianca Catalan
3	Angelica De los Reyes	567 Bay St.	(415) 766-1840	a.delosreyes31@yahoo.com	Angelica T.
4	Deion Nazarete	1051 Howard St.	(415) 756-2835		Deion Nazarete
5	Ah'Jame Helton	240 Bridgeway Dr.	415-574-6106	Beautiful23.ah@gmail.com	Ah'jame Helton



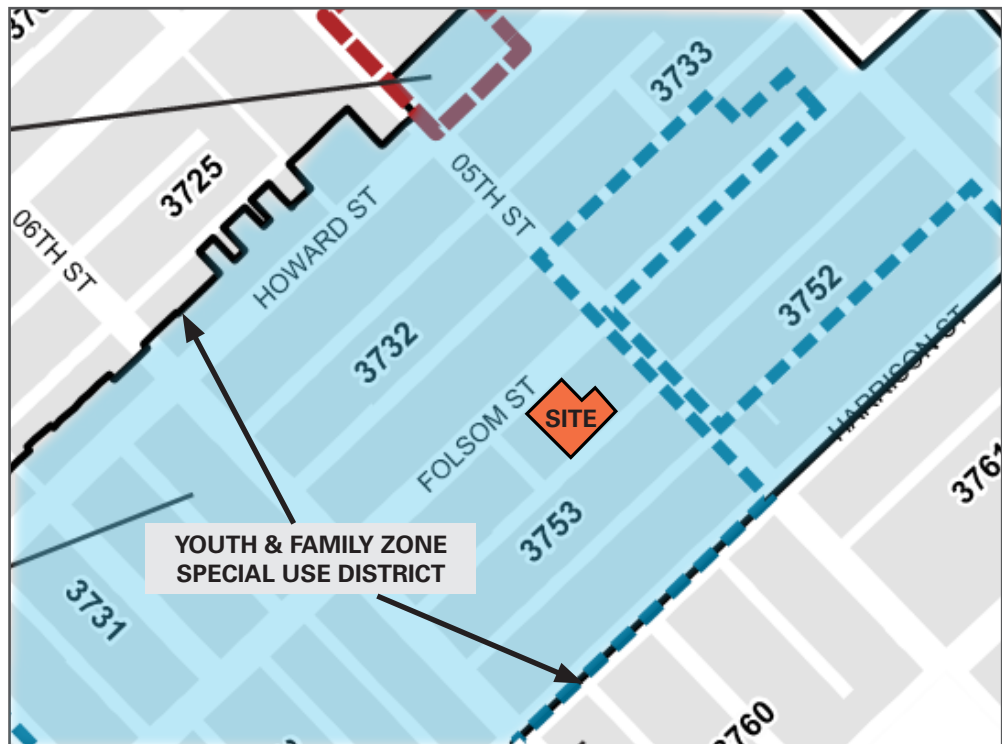


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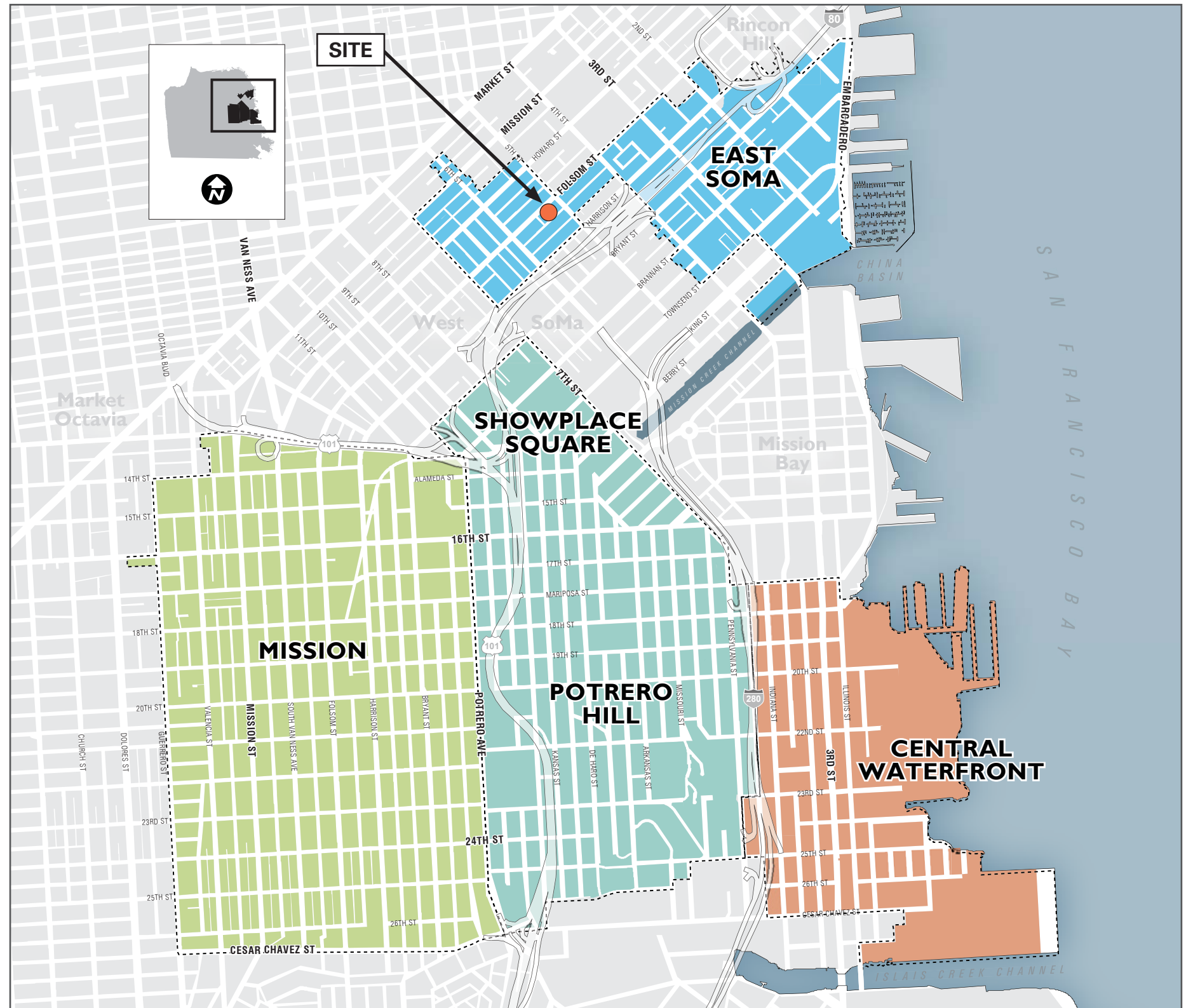
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HEIGHT & BULK DISTRICTS



YOUTH & FAMILY ZONE
SPECIAL USE DISTRICT



EASTERN NEIGHBORHOODS
PLAN AREA



SITE AERIAL, LOOKING SOUTH



SITE AERIAL, LOOKING NORTH





1. NEIGHBORING PROPERTY; LOOKING SOUTH-WEST DOWN FOLSOM



2. SUBJECT PROPERTY



3. SUBJECT PROPERTY



4. SUBJECT PROPERTY



5. NEIGHBORING PROPERTY; LOOKING NORTH-EAST UP FOLSOM



6. NEIGHBORING PROPERTY, LOOKING NORTH-EAST UP SHIPLEY



7. SUBJECT PROPERTY



8. NEIGHBORING PROPERTY, LOOKING SOUTH-WEST DOWN SHIPLEY

923 FOLSOM

SITE: 24,438 sf

Area / Unit Analysis

unit:	JR. 1B				1 BEDROOMS								2 BEDROOMS														
	01	19	20		02	03	04	06	08	11	12	13	14	16	17	18	01	05	06	07	09	10	11	14	15	19	
mech																											
9					754	691	577		596								820	948	987	865	932						
8					754	691	577		596								820	948	897	865	932						
7					754	691	577		596								820	948	897	865	932						
6					754	691	577		596								820	948	897	865	932						
5		492			754	691	577		596									948	897	865	932						
4	492				754	691	577		596	665	665	660		592	591	593		948	897	865	932	920		946	924	969	
3	492				754	691	577		596	665	665	660		592	591	593		948	897	865	932	920		946	924	969	
2	492	485	462		754	691	577	682		665	665	660		592	591	593		948				920		946	924		
G	492	431			754	691	577			558	562	660	592	591	593			948					816		904		
LL																											
			8												59											47	
			7.0%												51.8%											41.2%	

units / flr	avg.	nsf	eff.	resid. gsف	parking gsف	retail gsف	total gsف
-	-	-	-	1,637			1,224
9	797	7,170	81.8%	8,761			8,761
9	787	7,080	80.8%	8,761			8,761
9	787	7,080	80.8%	8,761			8,761
9	787	7,080	80.8%	8,761			8,761
9	750	6,752	77.1%	8,761			8,761
19	751	14,277	78.0%	18,303			18,303
19	751	14,277	78.0%	18,303			18,303
17	685	11,647	63.7%	18,287			15,838
14	655	9,169	61.0%	15,042	1,057	1,576	17,675
0	0	0	0.0%	3,043	20,789	0	23,832
114	742	84,532	71.4%	118,420	21,846	1,576	138,980
units	avg.unit	nsf	eff.	resid. gsف	parking gsف	retail gsف	total gsف

78 resid spaces
2 car share

14 BMR UNITS FOLSOM UNITS SHIPLEY UNITS

BIKES

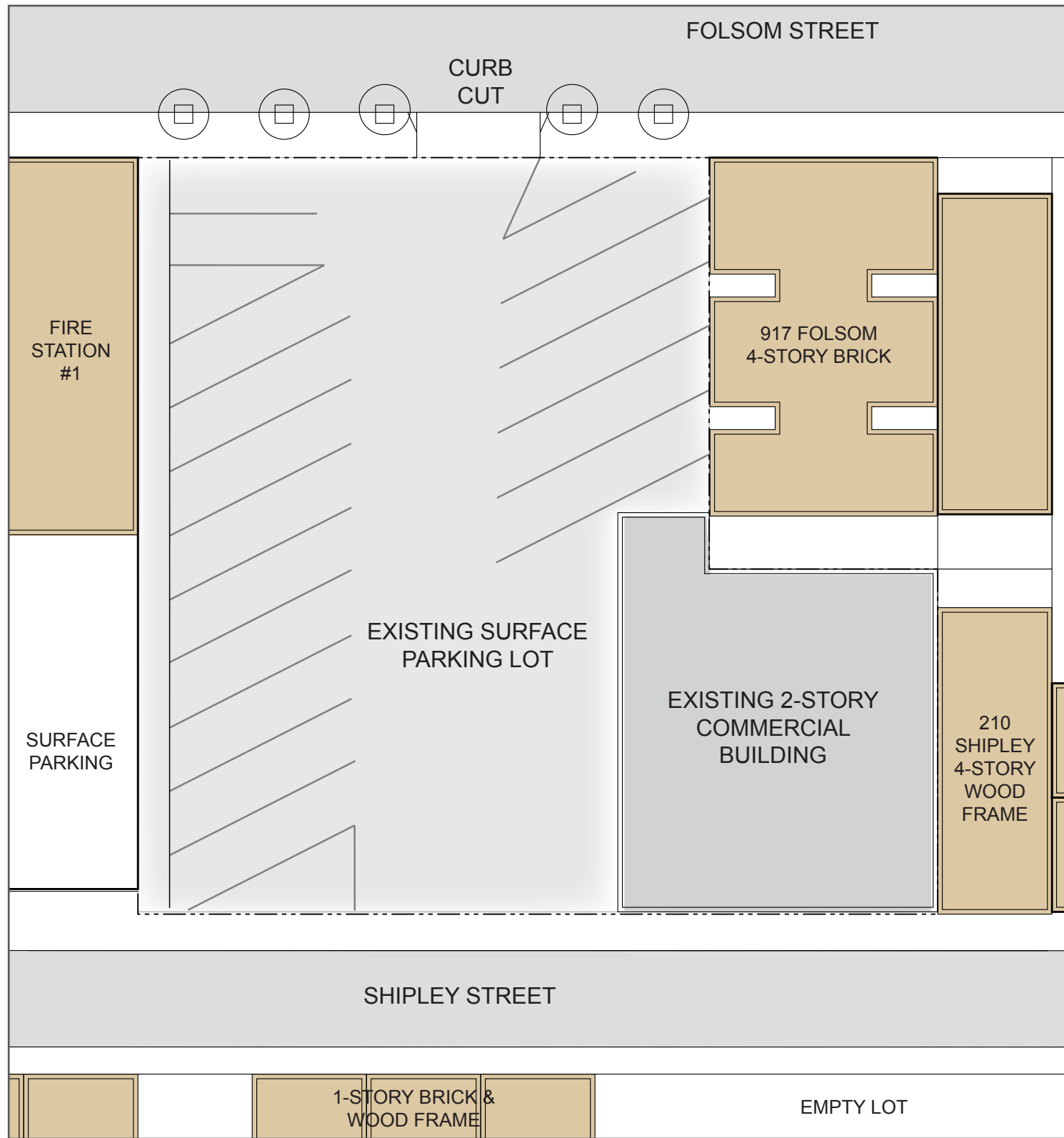
- CLASS 1, RESIDENTIAL (1) CLASS 1 SPACE FOR FIRST 100 UNITS
(1) CLASS 1 SPACE FOR EVERY 4 UNITS OVER 100
104 SPACES REQUIRED
- CLASS 1, COMMERCIAL (1) CLASS 1 SPACE / 7,500 SF, MIN. OF 2
2 SPACE REQUIRED
106 TOTAL SPACES REQUIRED
114 SPACES PROVIDED
- CLASS 2, RESIDENTIAL (1) SPACE PER 20 UNITS
5.70 SPACES REQUIRED
- CLASS 2, COMMERCIAL (1) SPACE PER 750 GSF
2.1 SPACES REQUIRED
8 TOTAL SPACES REQUIRED
8 SPACES PROVIDED

PARKING

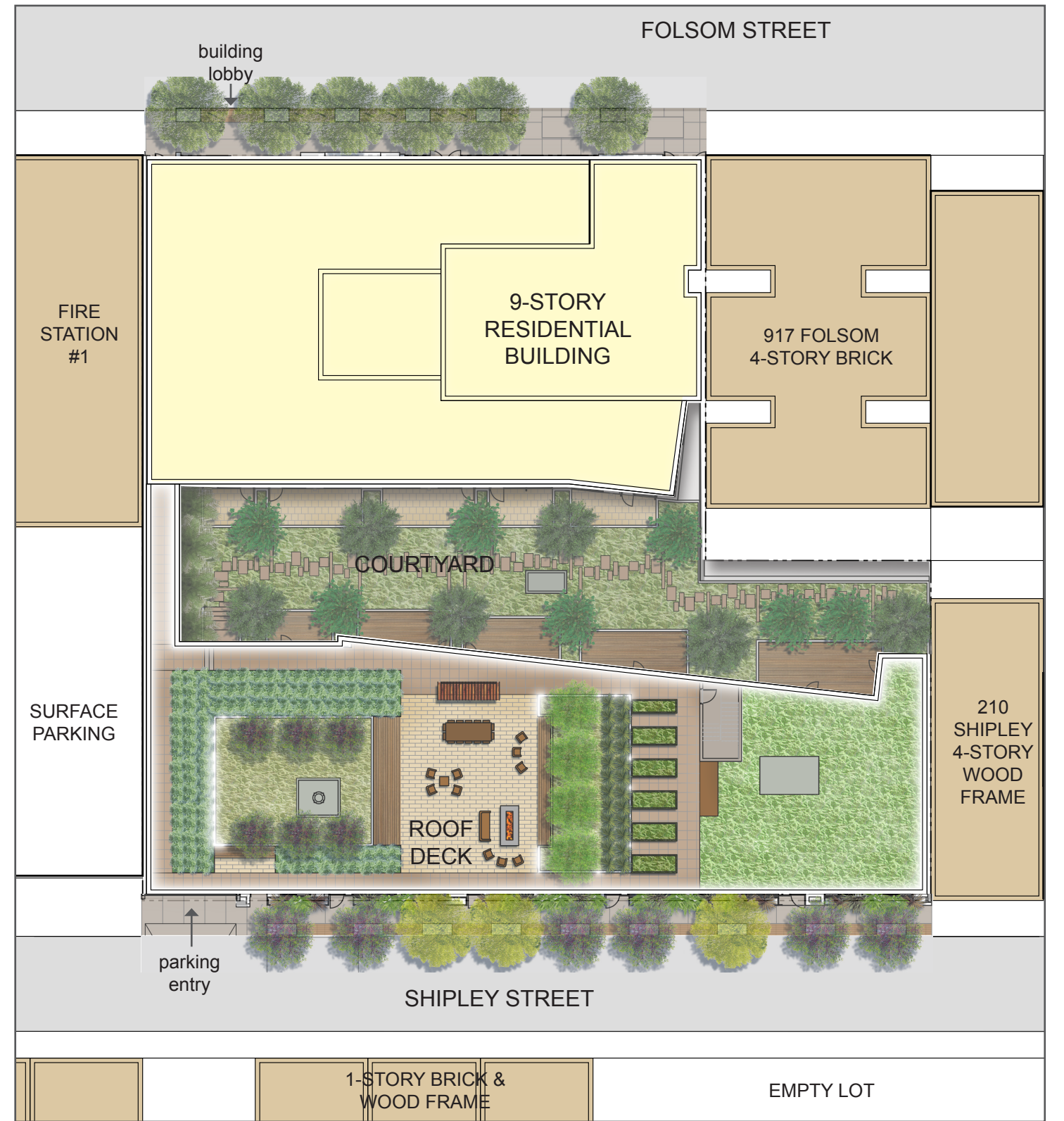
- MAXIMUM ALLOWED: .25 / UNIT; .75 SPACES / UNIT ALLOWED WITH CONDITIONAL USE AUTHORIZATION
1 SPACE / UNIT IF 2 BEDROOMS AND 1,000 SF
- | | # of units | spaces | = | total |
|-------------------------|------------|--------|---|-----------|
| UNITS < 1,000 SF: | 114 | 0.75 | = | 86 |
| 2B units > 1,000 SF: | 0 | 1.00 | = | 0 |
| MAXIMUM ALLOWED: | | | | 86 |
- PROVIDED: 78 SPACES FOR RESIDENTIAL
CAR SHARE: 2 SPACES PROVIDED (1 REQ'D)
80 TOTAL SPACES

RESIDENTIAL OPEN SPACE

- REQUIRED: 80 SF / UNIT
- | | | | | | |
|--|----|---|-----------|---|-------|
| | 80 | x | 114 UNITS | = | 9,120 |
|--|----|---|-----------|---|-------|
- PROVIDED:
5th FLOOR ROOF DECK: = 8,773
- | | | | | | |
|------------|----|---|---------|---|-----|
| BALCONIES: | 39 | x | 9 UNITS | = | 351 |
|------------|----|---|---------|---|-----|
- 9,124**



EXISTING SITE PLAN



PROPOSED SITE PLAN





Permeable paver strip

Bike racks parallel to street

Private patios with concrete pavers and screen gates

Raised flow-through planter with shade tolerant trees and shrubs

Private patios with wood decks and screen gates

Steel flow-through planters

Permeable paver strip

FOLSOM STREET

Evergreen street trees

Step stones for maintenance access

Shrub and perennial planting

Columnar street trees with seasonal interest

SHIPLEY STREET



Street Trees



Street Perennials



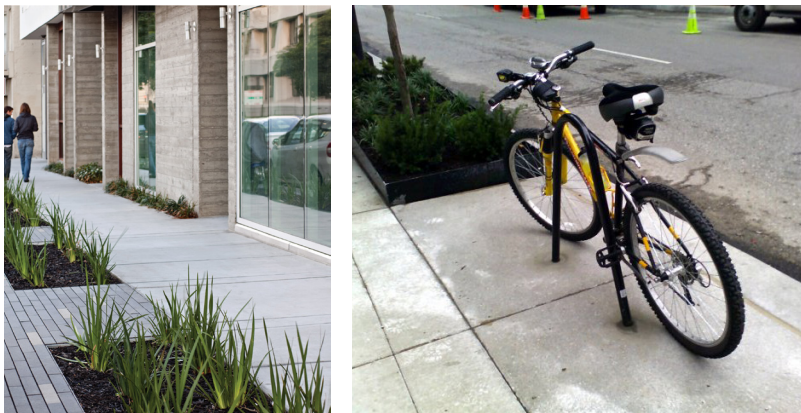
Second Floor Patio - Trees



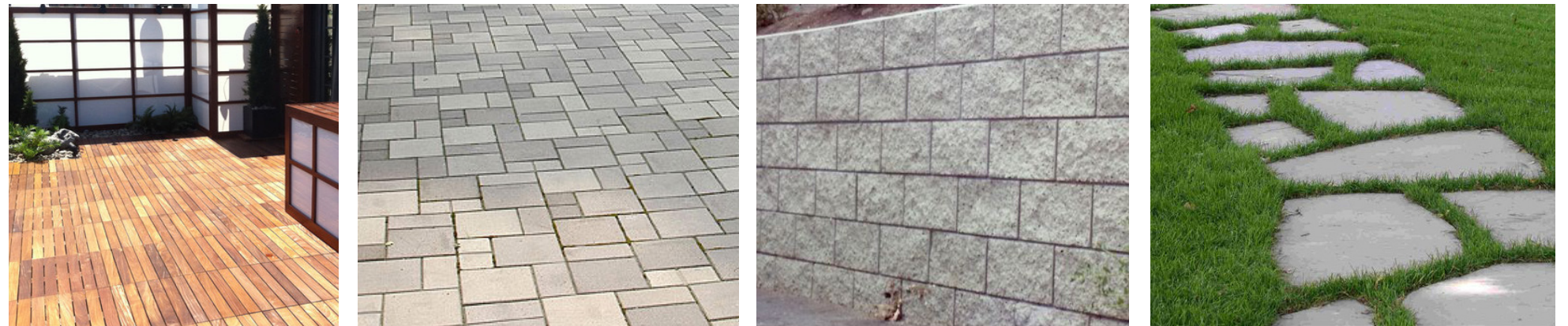
Second Floor Patio - Perennials

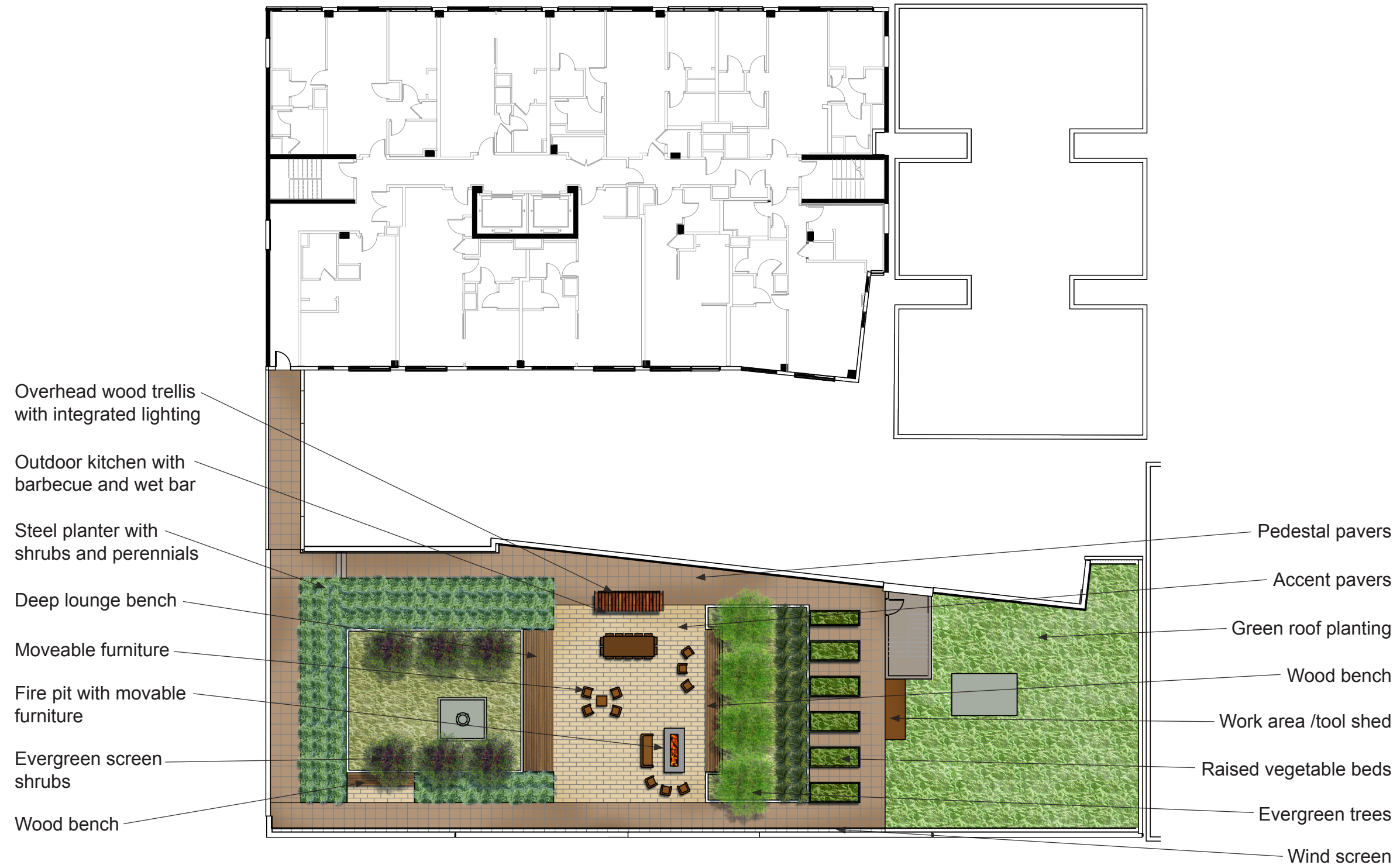


Street - Materials and Furnishing



Second Floor Patio - Materials





Roof Garden - Trees



Green Roof Planting

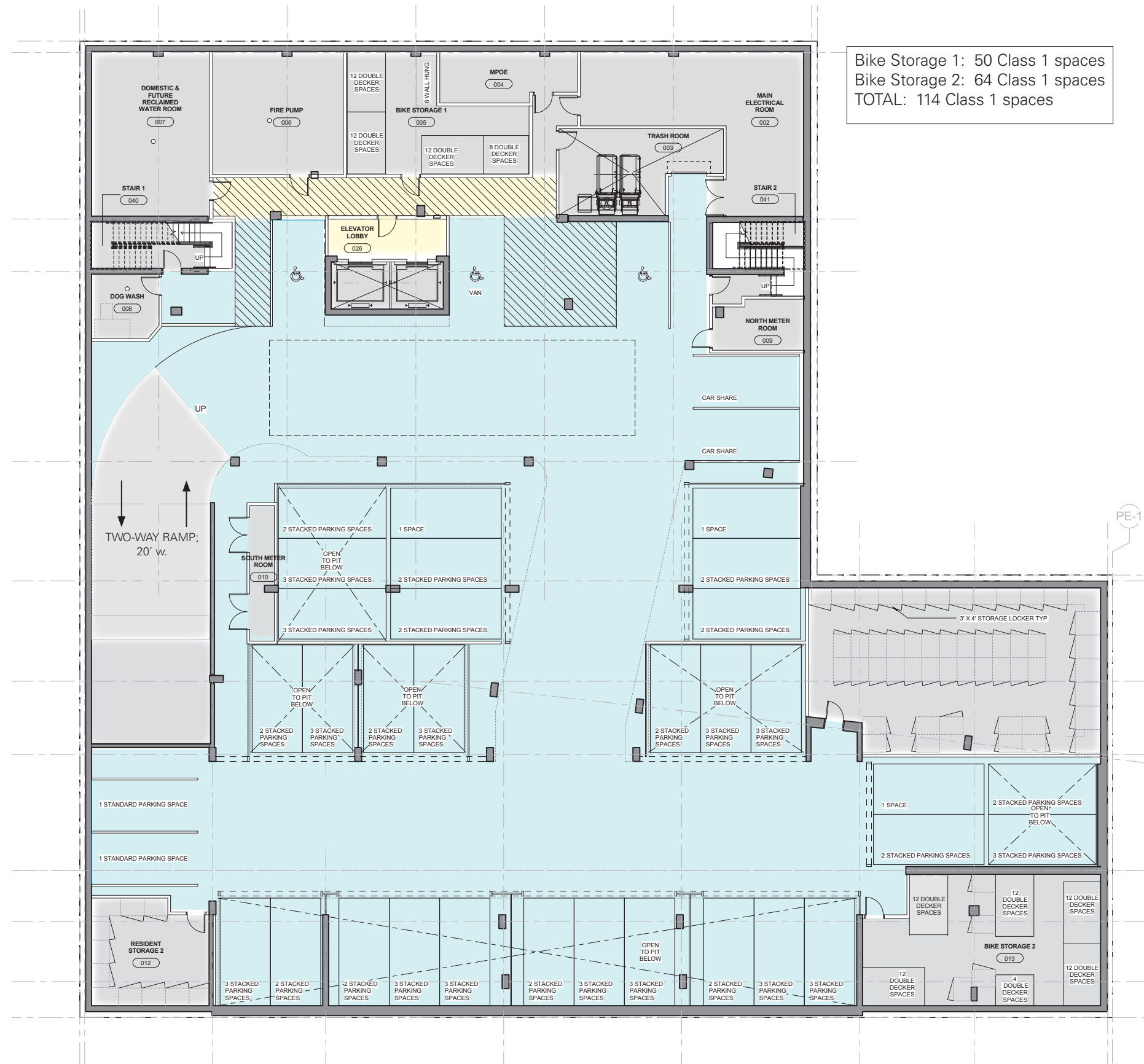


Roof Garden - Perennials



Roof Garden - Outdoor Furniture





Bike Storage 1: 50 Class 1 spaces
 Bike Storage 2: 64 Class 1 spaces
 TOTAL: 114 Class 1 spaces





*Bike Parking: (8) Class 2 spaces provided; (8) required

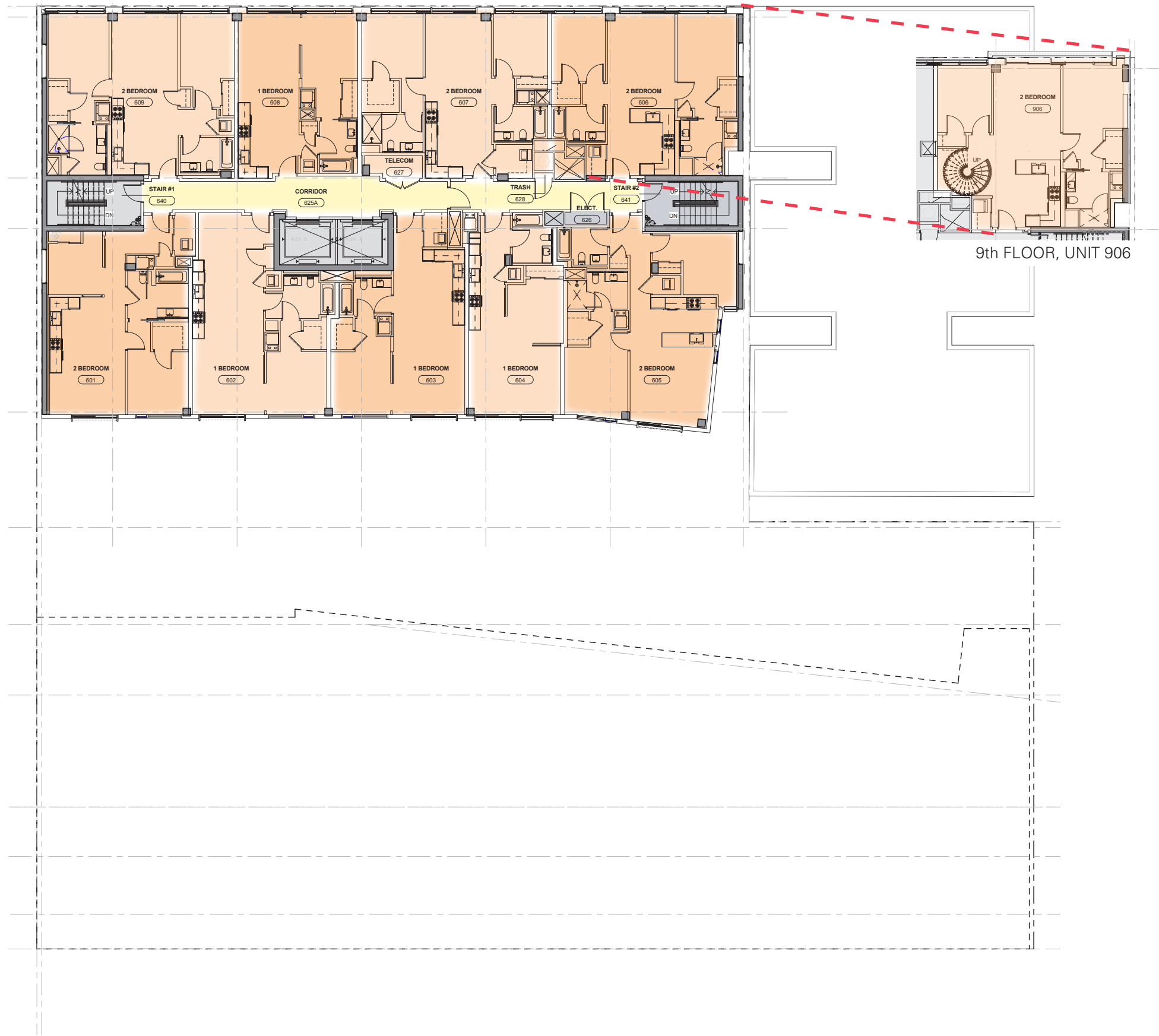
923 FOLSOM

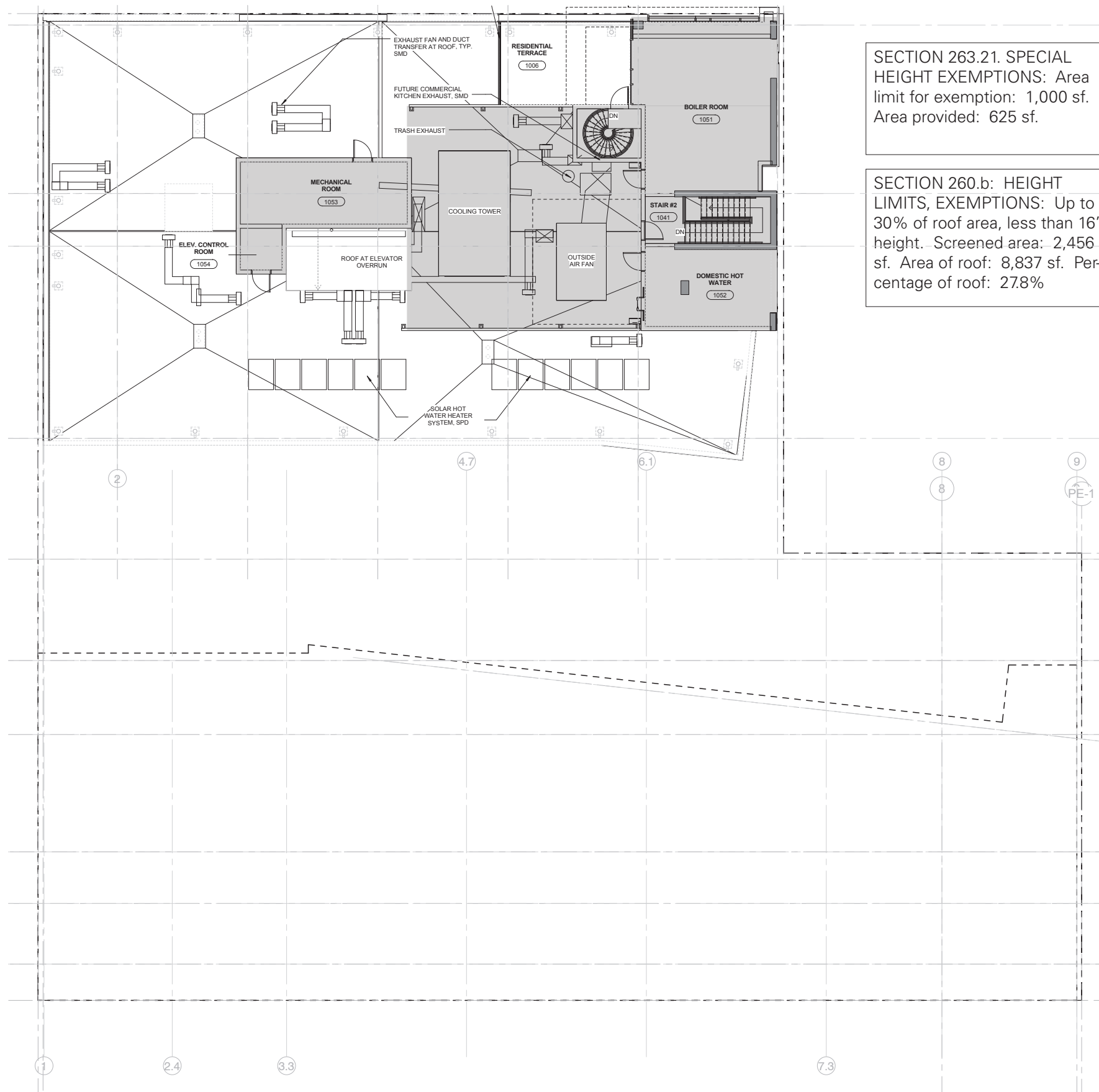
GROUND FLOOR PLAN







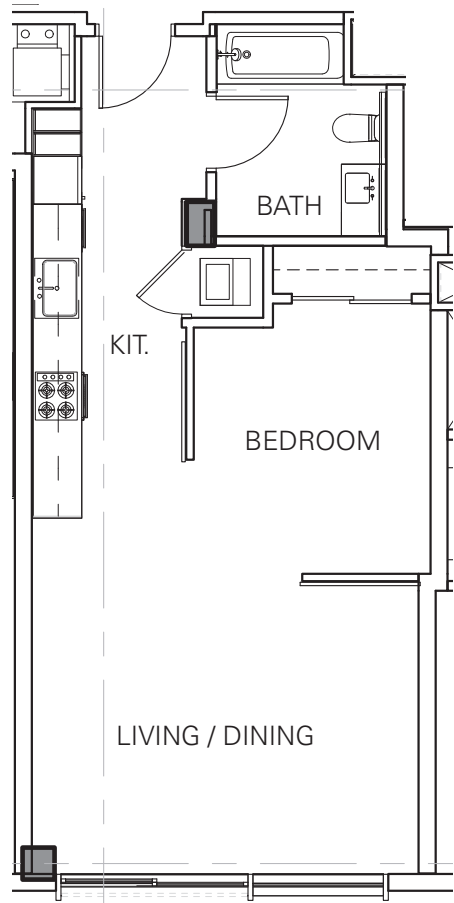




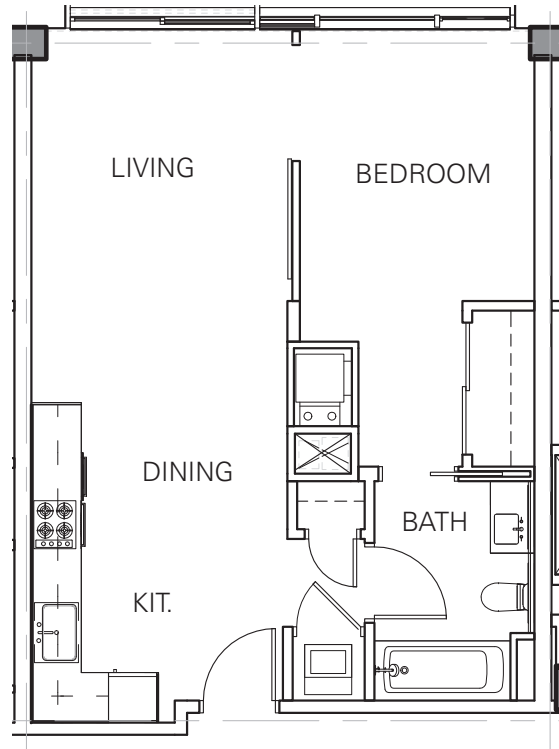
SECTION 263.21. SPECIAL HEIGHT EXEMPTIONS: Area limit for exemption: 1,000 sf. Area provided: 625 sf.

SECTION 260.b: HEIGHT LIMITS, EXEMPTIONS: Up to 30% of roof area, less than 16' height. Screened area: 2,456 sf. Area of roof: 8,837 sf. Percentage of roof: 27.8%

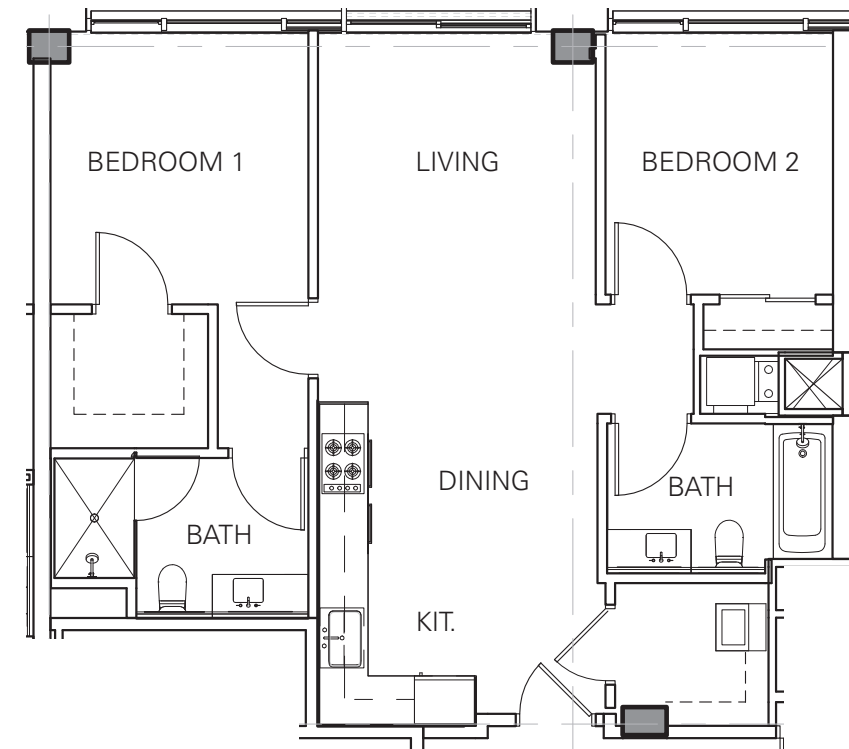




1 BEDROOM



1 BEDROOM



2 BEDROOM

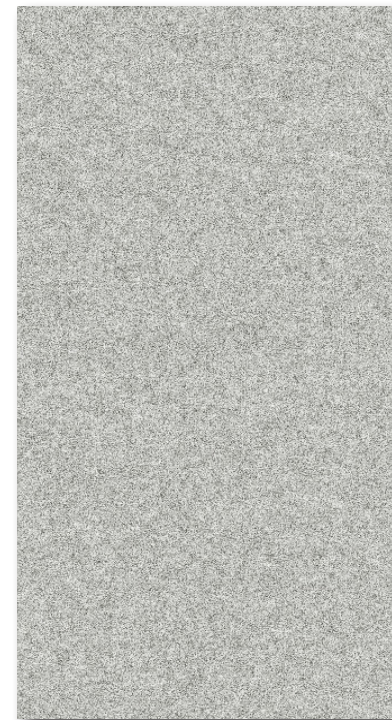




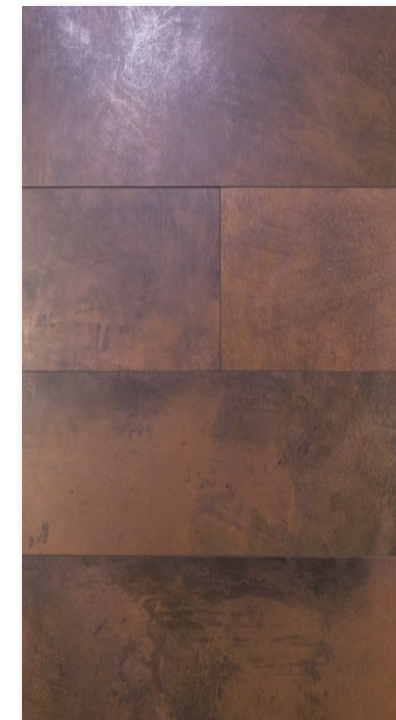
1. Vision glass



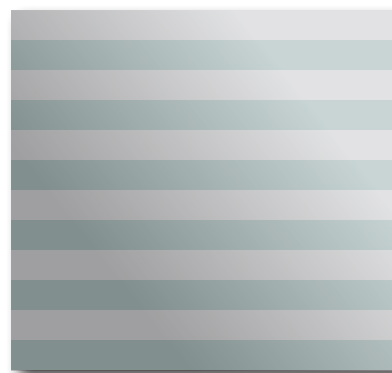
3. "Black Opal" cladding



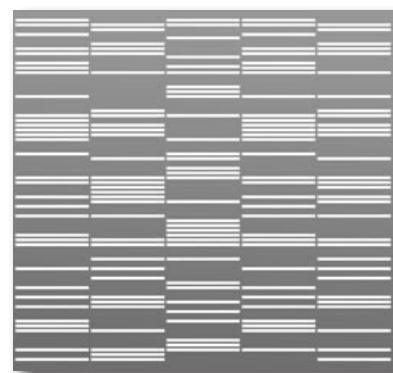
5. "Platinum" cladding



8. Tile at walk-up residential



2. Spandrel glass



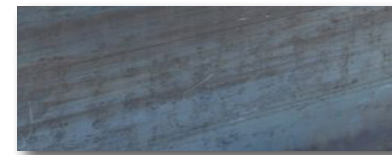
4. Metal railing & glass frit pattern



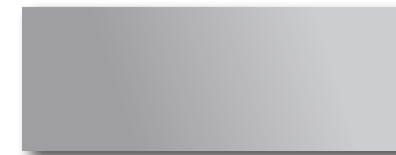
6. Accent color



9. Metal mullions



7. Steel planters



10. Metal slab edge covers



- 1. Vision glass
- 2. Spandrel glass
- 3. "Black Opal" cladding
- 4. Metal railing & glass frit pattern
- 5. "Platinum" cladding
- 6. Accent color
- 7. Steel planters
- 8. Accent tile
- 9. Metal mullions
- 10. Metal slab edge covers

FOLSOM STREET



- 1. Vision glass
- 2. Spandrel glass
- 3. "Black Opal" cladding
- 4. Metal railing & glass frit pattern
- 5. "Platinum" cladding
- 6. Accent color
- 7. Steel planters
- 8. Accent tile
- 9. Metal mullions
- 10. Metal slab edge covers
- 11. Building identification art
- 12. Clear retail & lobby glazing
- 13. Entry canopy

FOLSOM STREET RESIDENTIAL ENTRY



- 1. Vision glass
- 2. Spandrel glass
- 3. "Black Opal" cladding
- 4. Metal railing & glass frit pattern
- 5. "Platinum" cladding
- 6. Accent color
- 7. Steel planters
- 8. Accent tile
- 9. Metal mullions
- 10. Metal slab edge covers
- 11. Building identification art
- 12. Clear retail & lobby glazing

FOLSOM STREET RETAIL



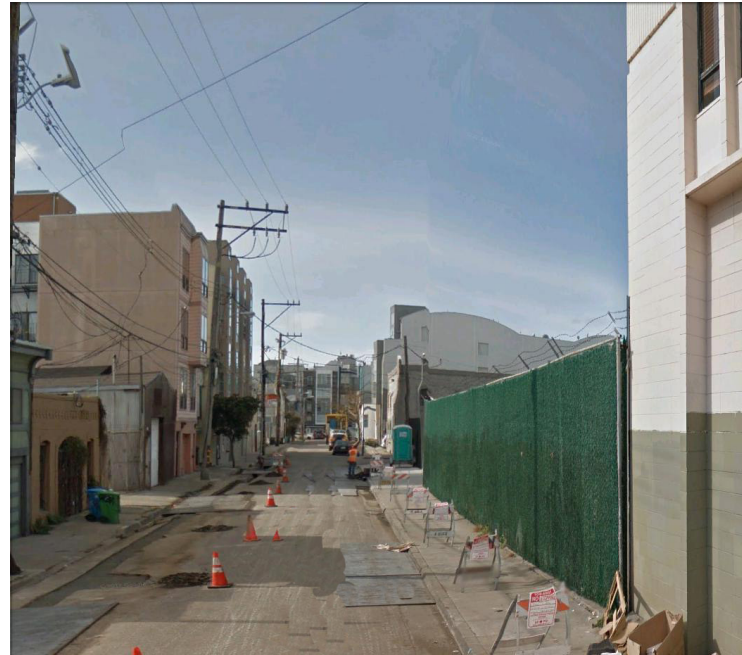
- 1. Vision glass
- 2. Spandrel glass
- 3. "Black Opal" cladding
- 4. Metal railing & glass frit pattern
- 5. "Platinum" cladding
- 6. Accent color
- 7. Steel planters
- 8. Accent tile
- 9. Metal mullions
- 10. Metal slab edge covers

SHIPLEY STREET



- 1. Vision glass
- 2. Spandrel glass
- 3. "Black Opal" cladding
- 4. Metal railing & glass frit pattern
- 5. "Platinum" cladding
- 6. Accent color
- 7. Steel planters
- 8. Accent tile
- 9. Metal mullions
- 10. Metal slab edge covers

DETAIL AT WALK-UP RESIDENTIAL ON SHIPLEY STREET



EXISTING: SHIPLEY STREET



EXISTING: FOLSOM STREET



PROPOSED: SHIPLEY STREET



PROPOSED: FOLSOM STREET

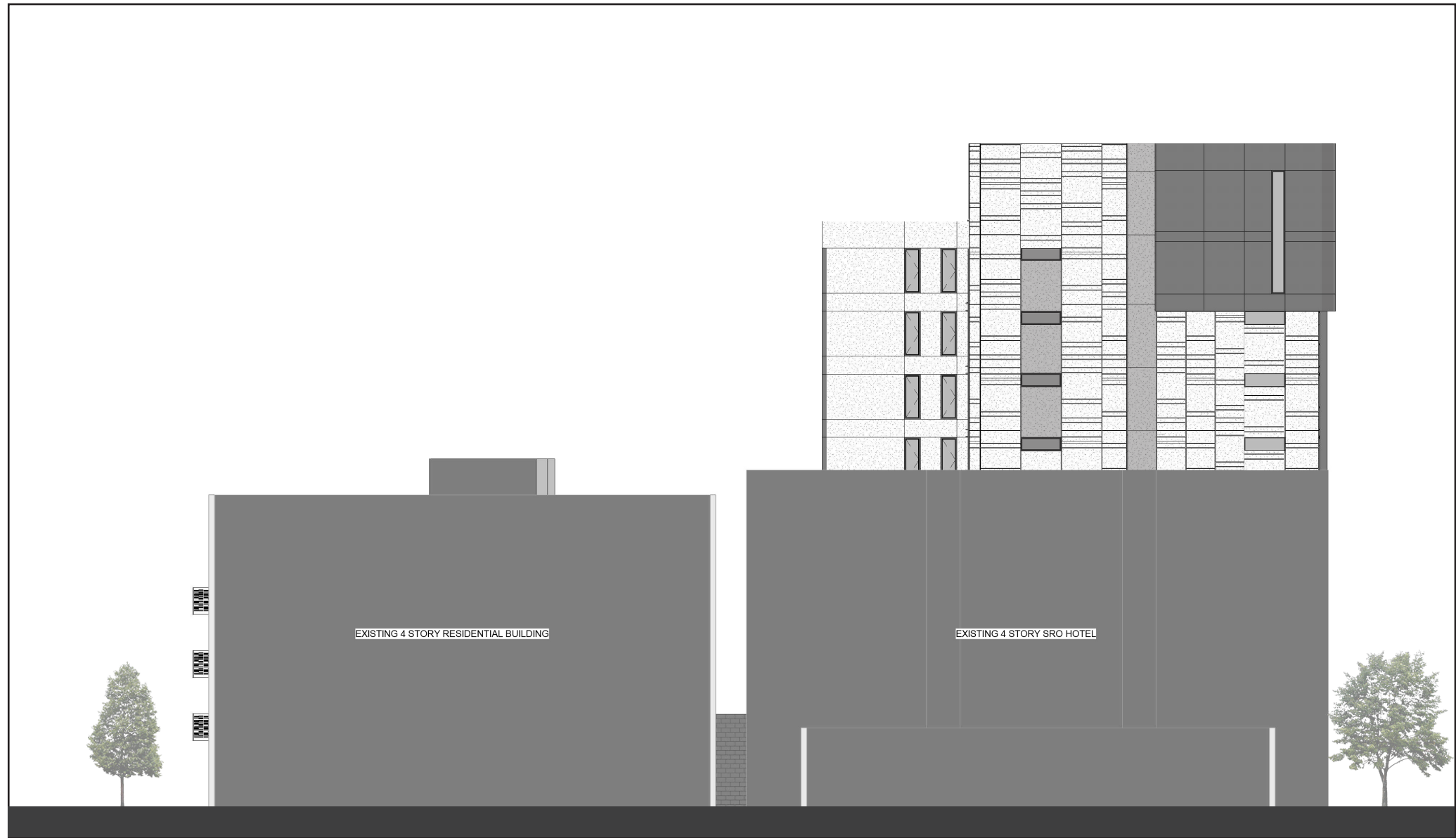


SHIPLEY (SOUTH-EAST) ELEVATION



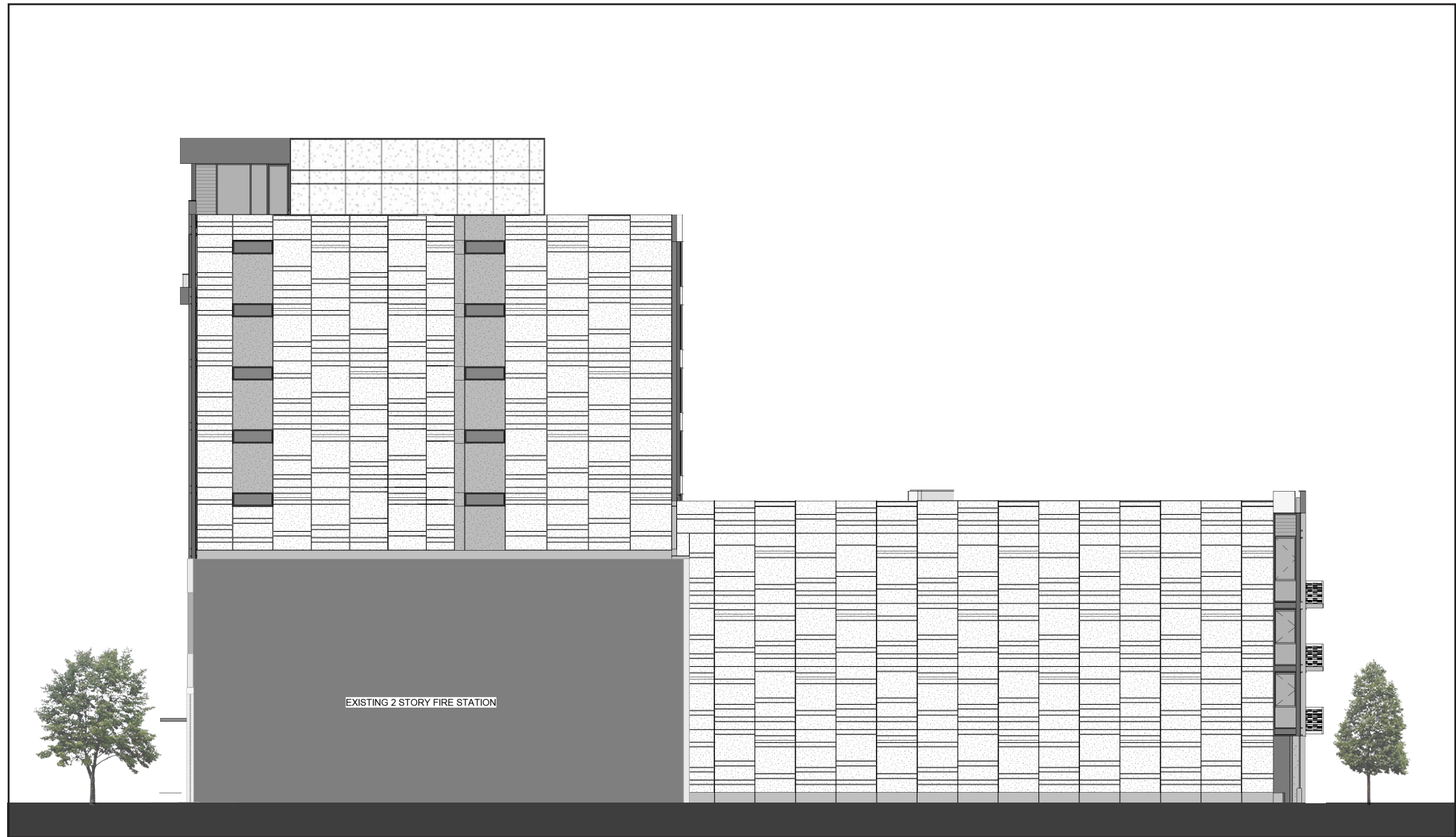
NORTH-WEST ELEVATION (COURTYARD SIDE)





NORTH-EAST ELEVATION





SOUTH-WEST ELEVATION





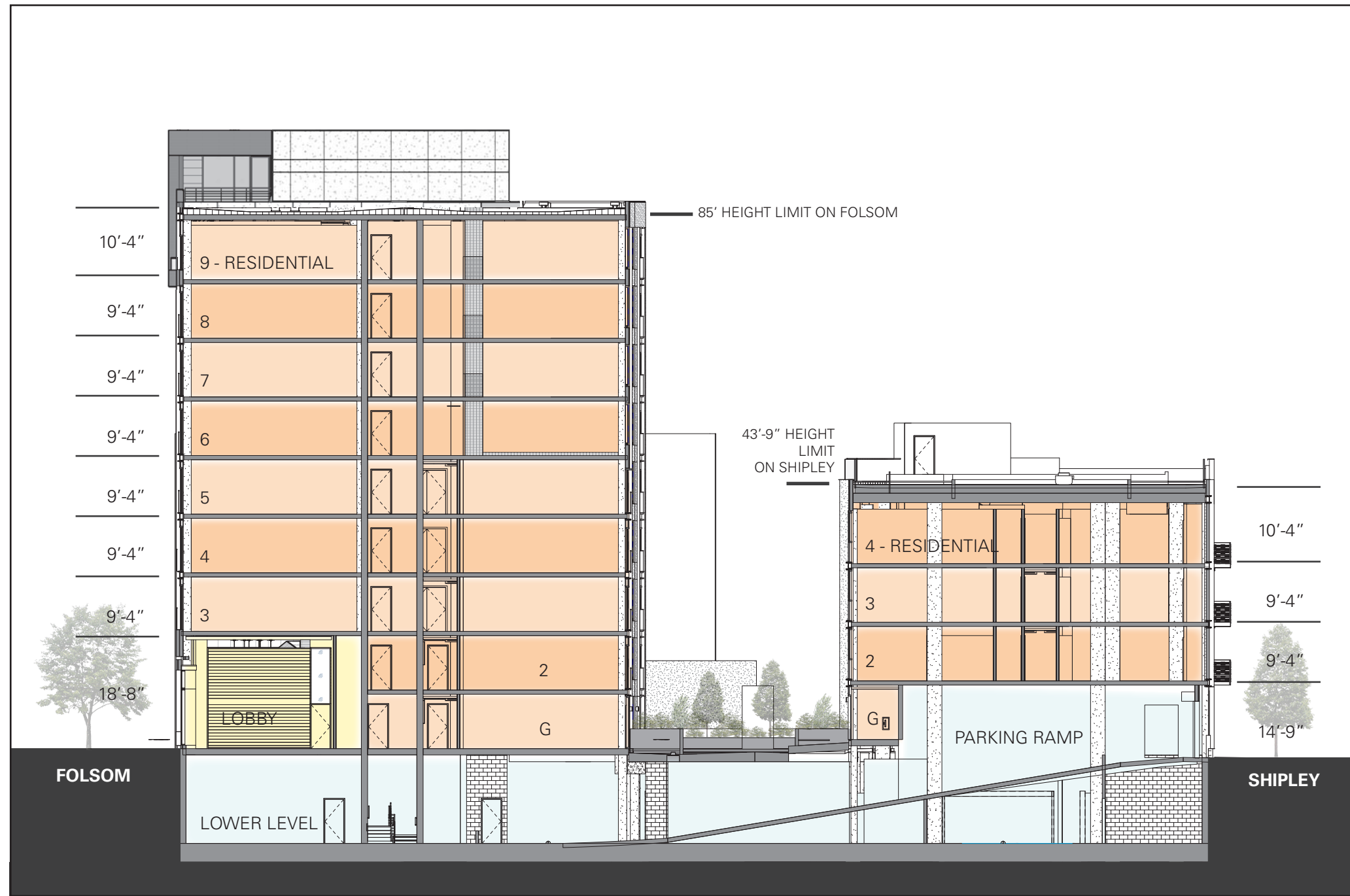
SOUTH-EAST ELEVATION (COURTYARD SIDE)





NORTH-WEST ELEVATION (FOLSOM)





BUILDING SECTION



A.L.T.A. / A.C.S.M. LAND TITLE SURVEY

OF THE LANDS OF THE:
ELL & KAY PROPERTIES, A CALIFORNIA LIMITED PARTNERSHIP
AS DESCRIBED IN THE PRELIMINARY REPORT FOR TITLE INSURANCE PREPARED BY:
FIRST AMERICAN TITLE INSURANCE COMPANY
ORDER NO. 0131-619137ala
DATED: OCTOBER 3, 2012 AT 7:30 A.M.

REFERENCED COMMITMENT FOR TITLE INSURANCE

TITLE COMPANY: FIRST AMERICAN TITLE INSURANCE COMPANY
6883 OWENS DRIVE
PLEASANTON, CA 94588

ORDER NUMBER: 0131-619137ala
DATED: OCTOBER 3, 2012

TITLE OFFICER: KIM SPEER
ESCROW OFFICER: DIANE BURTON

NATURE OF TITLE: FEE

TITLE VESTED IN: ELL & KAY PROPERTIES, A CALIFORNIA LIMITED PARTNERSHIP

PROPERTY ADDRESS(ES): 923 & 931 FOLSOM STREET & 218 SHIPLEY STREET

APN(S): SEE SURVEY

FLOOD ZONE: ZONE D (UNDETERMINED RISK AREA)
COMMUNITY PANEL NUMBER 060298
UNMAPPED AREA PER FEMA

ZONING: SUBJECT REAL PROPERTY OF THIS SURVEY IS ZONED MUR (MIXED USE RESIDENTIAL)

PARKING: 0 REGULAR
16 CUSTOM BUS STALLS
METERED PARKING ALONG FOLSOM ST.

LEGAL DESCRIPTION(S)

REAL PROPERTY IN THE CITY OF SAN FRANCISCO, COUNTY OF SAN FRANCISCO, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

PARCEL ONE:
BEGINNING AT A POINT ON THE NORTHWESTERLY LINE OF SHIPLEY STREET, DISTANT THEREON 150 FEET SOUTHWESTERLY FROM THE SOUTHWESTERLY LINE OF FIFTH STREET, RUNNING THENCE SOUTHWESTERLY AND ALONG SAID LINE OF SHIPLEY STREET 50 FEET, THENCE AT A RIGHT ANGLE NORTHEASTERLY 75 FEET, THENCE AT A RIGHT ANGLE NORTHEASTERLY 50 FEET, THENCE AT A RIGHT ANGLE SOUTHEASTERLY 75 FEET TO THE POINT OF BEGINNING, BEING PART OF 100 VARA BLOCK NO. 383.

PARCEL TWO:
BEGINNING AT A POINT ON THE SOUTHEASTERLY LINE OF FOLSOM STREET, DISTANT THEREON 150 FEET SOUTHWESTERLY FROM THE SOUTHWESTERLY LINE OF FIFTH STREET, RUNNING THENCE SOUTHWESTERLY ALONG SAID LINE OF FOLSOM STREET 75 FEET, THENCE AT A RIGHT ANGLE SOUTHEASTERLY 150 FEET TO THE NORTHEASTERLY LINE OF SHIPLEY STREET, THENCE AT A RIGHT ANGLE NORTHEASTERLY ALONG SAID LINE OF SHIPLEY STREET 50 FEET, THENCE AT A RIGHT ANGLE NORTHEASTERLY 50 FEET TO THE POINT OF BEGINNING, BEING PART OF 100 VARA BLOCK NO. 383.

PARCEL THREE:
BEGINNING AT A POINT ON THE SOUTHEASTERLY LINE OF FOLSOM STREET, DISTANT THEREON 225 FEET SOUTHWESTERLY FROM THE SOUTHWESTERLY LINE OF FIFTH STREET, RUNNING THENCE SOUTHWESTERLY AND ALONG SAID LINE OF FOLSOM STREET 50 FEET, THENCE AT A RIGHT ANGLE SOUTHEASTERLY 150 FEET TO THE NORTHEASTERLY LINE OF SHIPLEY STREET, THENCE AT A RIGHT ANGLE NORTHEASTERLY ALONG SAID LINE OF SHIPLEY STREET 50 FEET, THENCE AT A RIGHT ANGLE NORTHEASTERLY 165 FEET TO THE POINT OF BEGINNING, BEING PART OF 100 VARA BLOCK NO. 383.

APN: LOT 106; BLOCK 3753 (PARCEL ONE), LOT 142; BLOCK 3753 (PARCEL TWO) AND LOT 141; BLOCK 3753 (PARCEL THREE)

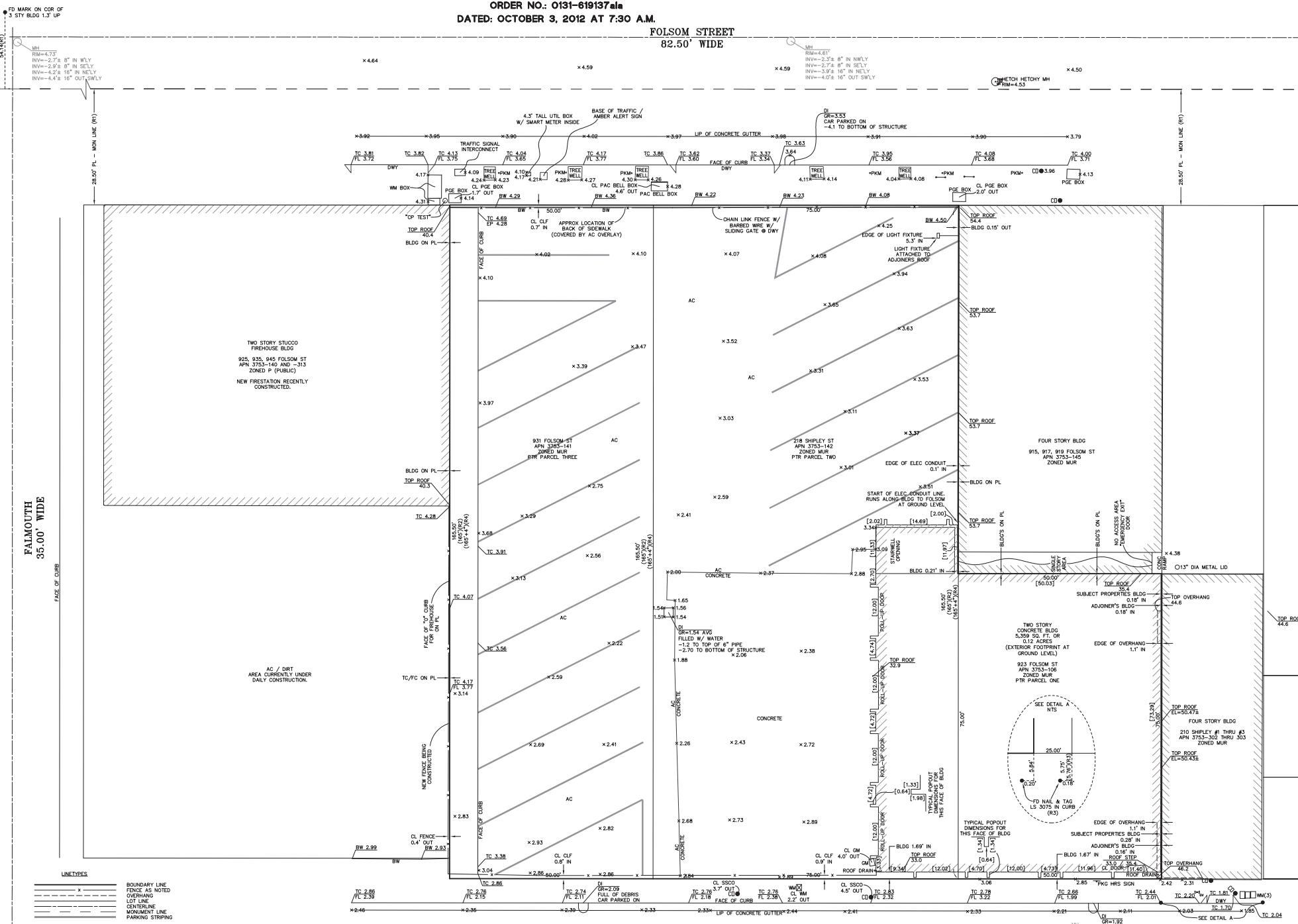
EXEMPTIONS TO COVERAGE

- GENERAL AND SPECIAL TAXES AND ASSESSMENTS FOR THE FISCAL YEAR 2012-2013.
FIRST INSTALLMENT: \$2,078.17, PAYABLE \$2.00
SECOND INSTALLMENT: \$2,078.17, PAYABLE \$2.00
PENALTY: \$0.00
TAX RATE AREA: 01-008
A. P. NO.: LOT 106; BLOCK 3753
AFFECTS: PARCEL ONE
NOT PLOTTABLE
- GENERAL AND SPECIAL TAXES AND ASSESSMENTS FOR THE FISCAL YEAR 2012-2013.
FIRST INSTALLMENT: \$828.71, PAYABLE \$0.00
SECOND INSTALLMENT: \$828.71, PAYABLE \$0.00
PENALTY: \$0.00
TAX RATE AREA: 01-008
A. P. NO.: LOT 142; BLOCK 3753
AFFECTS: PARCEL TWO
NOT PLOTTABLE
- GENERAL AND SPECIAL TAXES AND ASSESSMENTS FOR THE FISCAL YEAR 2012-2013.
FIRST INSTALLMENT: \$581.40, PAYABLE \$0.00
SECOND INSTALLMENT: \$581.40, PAYABLE \$0.00
PENALTY: \$0.00
TAX RATE AREA: 01-008
A. P. NO.: LOT 141; BLOCK 3753
AFFECTS: PARCEL THREE
NOT PLOTTABLE
- ASSESSMENT LIENS, IF APPLICABLE, COLLECTED WITH THE GENERAL AND SPECIAL TAXES, INCLUDING BUT NOT LIMITED TO THOSE DISCLOSED BY THE REFLECTION OF THE FOLLOWING ON THE TAX MAPS:
COMMUNITY FACILITIES DISTRICT SF UNIFIED SCHOOL DISTRICT CTD.
NOT PLOTTABLE
- THE LIEN OF SUPPLEMENTAL TAXES, IF ANY, ASSESSED PURSUANT TO CHAPTER 83 COMMENCING WITH SECTION 70 OF THE CALIFORNIA REVENUE AND TAXATION CODE.
NOT PLOTTABLE
- THE FACT THAT THE LAND LIES WITHIN THE BOUNDARIES OF THE SOUTH OF MARKET EARTHQUAKE RECOVERY REDEVELOPMENT PROJECT AREA, AS DISCLOSED BY THE DOCUMENT RECORDED DECEMBER 11, 2008 AS INSTRUMENT NO. 2008-096009-00 IN REEL 0284, IMAGE 0772 OF OFFICIAL RECORDS.
NOT PLOTTED
- A DOCUMENT ENTITLED "DECLARATION OF RESTRICTIONS AND STATEMENT REGARDING EMINENT DOMAIN LIMITATIONS IN THE SOUTH OF MARKET REDEVELOPMENT PROJECT AREA" RECORDED DECEMBER 11, 2008 AS INSTRUMENT NO. 2008-096009-00 IN REEL 0284, IMAGE 0773 OF OFFICIAL RECORDS.
DOCUMENT(S) DECLARING MODIFICATIONS THEREOF RECORDED DECEMBER 31, 2007 AS INSTRUMENT NO. 2007-051284-00 IN REEL 0447, IMAGE 0278 OF OFFICIAL RECORDS.
NOT PLOTTED
- A DEED OF TRUST TO SECURE AN ORIGINAL INDEBTEDNESS OF \$2,000,000.00 RECORDED FEBRUARY 23, 2011 AS INSTRUMENT NO. 2011-046269-00 IN REEL K338, IMAGE 0469 OF OFFICIAL RECORDS.
DATED: DECEMBER 1, 2010
TRUSTOR: ELL & KAY PROPERTIES, A CALIFORNIA LIMITED PARTNERSHIP
TRUSTEE: U.S. BANK TRUST COMPANY, N.A.
BENEFICIARY: U.S. BANK, N.A.
NOT PLOTTABLE
- RIGHTS OF PARTIES IN POSSESSION.
NOT PLOTTABLE
- ANY FACTS, RIGHTS, INTERESTS OR CLAIMS WHICH WOULD BE DISCLOSED BY A CORRECT ALTA/ACSM SURVEY, PRIOR TO THE ISSUANCE OF ANY POLICY OF TITLE INSURANCE, THE COMPANY WILL REQUIRE.
NOT PLOTTABLE
- WITH RESPECT TO ELL & KAY PROPERTIES, A CALIFORNIA LIMITED PARTNERSHIP:
A. THAT A CERTIFIED COPY OF THE CERTIFICATE OF LIMITED PARTNERSHIP (FORM LP-1) AND ANY AMENDMENTS THERETO (FORM LP-2) BE RECORDED IN THE PUBLIC RECORDS;
B. A FULL COPY OF THE PARTNERSHIP AGREEMENT AND ANY AMENDMENTS;
C. SATISFACTORY EVIDENCE OF THE CONSENT OF A MAJORITY IN INTEREST OF THE LIMITED PARTNERS TO THE CONTINUED TRANSACTION;
D. OTHER REQUIREMENTS WHICH THE COMPANY MAY IMPOSE FOLLOWING ITS REVIEW OF THE MATERIAL REQUIRED HEREIN AND OTHER INFORMATION WHICH THE COMPANY MAY REQUIRE.
NOT PLOTTABLE
- AN ALTA/ACSM SURVEY OF RECENT DATE WHICH COMPLES WITH THE CURRENT MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS.
NOT PLOTTABLE

BASIS OF SURVEY
RELATIONSHIPS BETWEEN MONUMENT LINES AND THE BLOCK LINES SHOWN HEREON WERE ESTABLISHED IN CONFORMANCE WITH LINES OF OCCUPATION AND FIELD SURVEY DATA.

BENCHMARK STATEMENT
ALL ELEVATIONS SHOWN HEREON ARE BASED UPON THE CITY AND COUNTY OF SAN FRANCISCO'S VERTICAL DATUM. MONUMENT BENCHMARK 4 FOUND SHOWN OUT ON THE OUTER NW CORNER LOCATED AT THE NORTHWESTERLY CORNER OF FOLSOM AND 5TH STREET HAVING AN ELEVATION OF 3.296 FEET.

REFERENCES
(R1) CACIF MONUMENT MAP 314
(R2) PFR LEGAL DESCRIPTIONS
(R3) BOOK 89 OF CONDOMINIUM MAPS AT PAGES 4-6
(R4) OLD CACIF BLOCK MAP ENTITLED "100 VARA BKG 383", NO VISIBLE DATE



ALTA SURVEY NOTES

- ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
- DIMENSIONAL TIES TO IMPROVEMENTS ARE 90° TO THE PROPERTY LINES UNLESS OTHERWISE NOTED.
- IN OR OUT REFERENCES INSIDE OR OUTSIDE OF THE SUBJECT PROPERTY.
- THE FIELD SURVEY WAS COMPLETED ON FEBRUARY 20, 2013.
- BUILDING MEASUREMENTS WERE TAKEN AT WAIST HEIGHT AND ARE SHOWN IN BRACKETS [].
- STRIPPED PARKING EXISTS ON THE SUBJECT PROPERTY AS NOTED.
- PUBLIC METERED PARKING EXISTS ON FOLSOM ST.
- THE GROSS LAND AREA OF PFR PARCEL 1 THRU 3 IS 24,438 SQUARE FEET OR 0.56 ACRES.
- AT THE TIME OF THE SURVEY, THERE WAS NO OBSERVED EVIDENCE OF CURRENT LEARN WORK, CONSTRUCTION, OR BUILDING ADDITIONS (EXCEPT AS NOTED FOR PROPERTY TO THE WEST, NEW FIREHOUSE).
- AT THE TIME OF THE SURVEY, THERE WAS NO OBSERVED EVIDENCE OF THE SITE BEING USED AS A SOLID WASTE DUMP, SAND, OR SANITARY LANDFILL.
- ALL ADDRESSES, APN(S), AND ZONING ARE PER THE CITY AND COUNTY OF SAN FRANCISCO'S GIS WEB SYSTEM.
- AT THE TIME OF SURVEY, A TEMPORARY CONSTRUCTION FENCE EXISTED ALONG THE EASTERLY BOUNDARY OF THE NEW FIREHOUSE.

UTILITY NOTE
THE UTILITIES SHOWN ON THIS PLAN ARE DERIVED FROM SURFACE OBSERVATIONS AND ARE APPROXIMATE ONLY. NO WARRANTY, EITHER EXPRESSED OR IMPLIED, IS GIVEN AS TO THE ACTUAL LOCATION, SIZE, OR PRESENCE OF ANY ADDITIONAL UTILITIES OR UTILITY LINES NOT SHOWN ON THIS PLAN.

LEGEND AND SYMBOLS

- BW BUREL BACK OF SIDEWALK
- CL CENTERLINE
- CONC CONCRETE
- CR CORNER
- DR DRIVE
- FL FLOORLINE
- FM FIRE METER
- GP GAS VALVE
- JP JOINT POLE
- MC MANHOLE
- PKM PARKING METER
- PL PLUMBING
- PS PULL BOX
- SL STREET LIGHT
- SS CLEANOUT
- SW SW
- TC TOP OF CURB
- WM WATER METER
- WV WATER VALVE

REVISIONS

No.	Description
1	AS SHOWN

SURVEYOR'S STATEMENT
I, ALEX W. CALDER, A CALIFORNIA LICENSED LAND SURVEYOR, AND FIRST AMERICAN TITLE INSURANCE COMPANY:
THIS IS TO CERTIFY THAT THIS MAP OR PLAN AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA / ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NPSA, AND INCLUDES ITEMS 2, 3, 4, 6A, 7A, 7B, 8, 9, 13, 14, 16, AND 18 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON FEBRUARY 20, 2013.

ALEX W. CALDER, PLS 8863
FEBRUARY 20, 2013
DATED

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BKF
Engineers / Surveyors / Planners

STATE OF CALIFORNIA
COUNTY OF SAN FRANCISCO
CITY OF SAN FRANCISCO

ALTA / ACSM LAND TITLE SURVEY
923 & 931 FOLSOM ST. AND 218 SHIPLEY ST

Order: 2/20/2013
Drawing: 1 of 10
Drawing Date: 2/20/2013
Drawing Title: ALTA / ACSM LAND TITLE SURVEY
Drawing No.: 20130010-04

Drawing Number:
ALTA
1 OF 1